

# City of White Rock Official Community Plan

**WHITE ROCK**  
*City by the Sea*





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# Part A Purpose & Context



# 1. Purpose & Scope of this Official Community Plan

## 1.1 Purpose & Relationship to Other Plans

~~This Official Community Plan (OCP) is intended to protect cherished characteristics of White Rock while managing growth and shaping change in a manner that will help bring the community's vision and goals to life. This OCP influences how people live, work, shop, play, and move around in the community. It shapes how White Rock looks and feels, and it influences the day-to-day experiences and quality of life of residents and visitors.~~

~~An OCP is a document with objectives and policies that guide decisions on planning and land use management. As the overarching city-wide policy for White Rock, this OCP is the guiding framework for all departments and decision-making within the City. OCPs are living documents that are flexible and fluid by nature, and are intended to evolve over time to address changing conditions in the community. As White Rock's first OCP from 1958 explains, "city planning cannot be a 'once and for all' matter. Any plan needs constant modification to meet the community's changing needs."~~

~~Municipalities in British Columbia are given the authority to adopt an OCP through the Local Government Act, which stipulates what must and may be included in an OCP. Provincial legislation also establishes adoption procedures. This OCP has been prepared in compliance with all relevant legislation.~~

This Official Community Plan (OCP) is intended to protect cherished characteristics of White Rock while managing growth and shaping change in a manner that will help bring the community's vision and goals to life. This OCP influences how people live, work, shop, play, and move around in the community. It shapes how White Rock looks and feels, and it influences the day-to-day experiences and quality of life of residents and visitors.

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An OCP is a document with objectives and policies that guide decisions on planning and land use management. As the overarching city-wide policy for White Rock, this OCP is the guiding framework for all departments and decision-making within the City. It informs higher-level strategies like Council's Strategic Priorities and is shaped in part by different legislative requirements. This includes the incorporation of a Regional Context Statement, which indicates how the City's OCP aligns with the goals of Metro 2050.

Additionally, the Provincial Government's Housing Supply Act and Housing Statutes Amendment Acts, introduced in 2022 and 2023, have altered municipal planning frameworks, and amongst other changes, require the OCP to take into consideration the community's latest assessment of its housing needs, as determined by the Housing Needs Report. The Province now requires that municipalities review and update their Official Community Plans and Zoning Bylaws to ensure that statements, maps, and land use designations permit the number of housing units identified as needed in each community's unique Housing Needs Report (HNR).

Municipalities in British Columbia are given the authority to adopt an OCP through the Local Government Act, which stipulates what must and may be included in an OCP. Provincial legislation also establishes adoption procedures. This OCP has been prepared in compliance with all relevant legislation.

## 1.2 Plan Organization & Scope

~~This OCP includes policy areas identified in the Local Government Act in Section 473 (“content and process requirements”), Section 474 (“policy statements that may be included”) and Section 488 (“designation of Development Permit Areas”). While an OCP does not obligate a Council to undertake specific actions or projects, all bylaws enacted and public works undertaken must be consistent with the goals, objectives, and policies outlined in an adopted OCP. The OCP provides the base from which future land use decisions are made regarding new development proposals and other municipal decisions and initiatives.~~

~~This Plan is organized into four parts:~~

- ~~• Part A sets the stage by providing an overview of the OCP purpose and scope, outlining the public process undertaken in developing the Plan; presenting the community’s Vision, Guiding Principles, and Goals as the foundation for all policies; and articulating a Regional Context Statement.~~
- ~~• Part B outlines objectives and policies relating to community character, growth management, land use, the Town Centre, the Waterfront, housing, environmental management, transportation and mobility, economic development, parks and recreation, infrastructure, quality of life, and arts, culture, and heritage.~~
- ~~• Part C presents implementation directions relating to plan administration, evaluation, and monitoring.~~
- ~~• Part D outlines guidelines for the City’s Development Permit Areas.~~

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This Plan is organized into four parts and two appendices:

- **Part A** sets the stage by providing an overview of the OCP purpose and scope, outlining the public process undertaken in developing the Plan, and presenting the community’s Vision, Guiding Principles, and Goals as the foundation for all policies.
- **Part B** outlines objectives and policies relating to community character, growth management, land use, the Town Centre, the Waterfront, housing, environmental management, transportation and mobility, economic development, parks and recreation, infrastructure, quality of life, and arts, culture, and heritage.
- **Part C** presents implementation directions relating to plan administration, evaluation, and monitoring.
- **Part D** outlines guidelines for the City’s Development Permit Areas.
- **Appendix A** articulates a Regional Context Statement.
- **Appendix B** presents the various maps relating to the different parts within the OCP.



Figure 1. Imagine White Rock 2045 Process

## 2. The Imagine White Rock 2045 Process

### 2.1 An Inclusive Public Process

The OCP planning process took place over four phases, as illustrated in Figure 1, and was launched in early 2015. This Plan brings together nearly two years of background research, analysis, planning and design, broad community engagement with stakeholders and residents, and iterative review with staff across departments and City Council.

The first two Phases culminated in a vision and set of goal statements adopted by City Council in 2016. Building on previous work, Phase 3 involved further analysis of existing conditions and forecasts for long range population growth and residential, retail and service, and office development. Public engagement during Phase 3 built on the vision and goal statements developed in Phases 1 and 2 and focused on growth management and policy directions. During Phase 4, the Plan was drafted and presented for public input and was refined based on the feedback received.

Engagement opportunities included public open houses, surveys, community workshops, stakeholder meetings, a citizen OCP Working Group, and interactive pop-up booths at community events and in high people-traffic areas to capture the input of passersby.

Over the life of the process, over 1,500 distinct interactions with residents, business owners, employees, developers, and other participants resulted in extensive public input in all phases of the Imagine White Rock 2045 planning process.



Imagine White Rock 2045 Phase 3 Public Engagement Sessions on Policy Directions and Growth Management



### 3. Vision, Guiding Principles, & Goals

This Official Community Plan (OCP) is the culmination of two years of broad and meaningful engagement with thousands of residents. The Vision paints a picture of what White Rock will be like in 2045 and, along with the Guiding Principles and Goals, provides direction for the development and implementation of policies.

#### 3.1 Vision

##### **The year is 2045...**

Our City by the Sea is a **beautiful, distinctive, and engaged** community of people from all walks of life who choose to live, work, and play in White Rock.

Our residents and visitors experience an **extraordinary quality of life** due to White Rock's temperate climate, safe and healthy neighbourhoods, thriving urban Town Centre, accessible ocean waterfront and historic pier, rich cultural and natural heritage, and diverse open space and recreational amenities.

Our progress and **commitment to sustainability** builds community identity and pride, while ensuring White Rock meets the needs of current and future generations.



## 3.2 Guiding Principles

The OCP is strategically guided by six principles that articulate strategic elements and opportunities inherent in the vision. Together with the Goals, they provide the foundation for more detailed policies outlined in Parts B and development permit area guidelines outlined in Part D.



### *Principle 1*

## Connect to the Water

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White Rock is first and foremost a seaside community. The waterfront and Marine Drive are cherished assets, however steep topography makes them difficult to access from other key destinations such as the Town Centre. This OCP will support making it easier and more inviting for residents and visitors to access the waterfront.





## Principle 2

# Enjoy the Town Centre

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If the waterfront is the soul of White Rock, then the Town Centre is the heart. This area is the economic and cultural centre of the community, with the greatest concentration of homes, jobs, shops, and amenities. This OCP will support reinforcement of the Town Centre as a mixed-use anchor, and will encourage the creation of delightful public places for socializing, dining, resting, people-watching, shopping, and taking in the view.





## Principle 3

# Grow Up & Grown Old in the Same Neighbourhood

---

White Rock has diverse housing types across the City, however neighbourhoods themselves are fairly homogeneous. This OCP will support the existing character of established neighbourhoods, while also providing more housing choices for diverse households, incomes, and needs. This includes young adults, families with children, and seniors who wish to downsize without leaving their neighbourhood behind. The OCP will also promote greater transportation choices for everyone, making walking, cycling, and transit use convenient and accessible.





## Principle 4

### See the Sea

Few things distinguish White Rock from other places more than its stunning views of the ocean. Through the celebration of views, this OCP will help to shape an urban form that continually reminds residents and visitors that they are in a seaside community.





## Principle 5

# Share the Streets

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White Rock is blessed with an abundance of local small-scale shops designed around people rather than cars. Not only do these shops and restaurants provide services and jobs, they help animate public spaces and create interesting, intimate, and walkable streets. This OCP will help to strengthen the relationship between businesses and residents, making access on foot both convenient and enjoyable.





## Principle 6

# Live & Play in Green Places

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White Rock is rich with natural beauty, including ecologically sensitive bluffs, ravines, and marine environments. At the same time, White Rock has limited park space, the tree canopy is inconsistent along streets and in neighbourhoods, and trees are largely situated on private property. This OCP will support increasing the quality and amount of green spaces within White Rock, enhancing tree canopy, and protecting natural resources and ecological areas.



### 3.3 Goals

Along with the Vision and Guiding principles, 13 Goals provide the foundation for this Plan. Policy sections in Part B are organized under these 13 headings.

#### 1. Community Character

The City of White Rock has great places that foster positive social interaction, community pride, and appreciation of natural features.



#### 2. Growth Management

The City of White Rock encourages citizen involvement while managing growth and development, characterized by high-quality design and the provision of community amenities and infrastructure.



#### 3. Land Use

The City of White Rock maximizes its limited land resource by creating a complete community where residents have convenient access to jobs, services, open space, and amenities.



#### 4. Town Centre

The City of White Rock promotes and develops the Town Centre as a distinctive, lively, and pedestrian-focused growth area.



#### 5. Waterfront

The City of White Rock enhances and promotes its beautiful Waterfront and historic pier as a memory-making destination where people can shop, dine, gather, play, and connect with nature.



#### 6. Housing

The City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives.



#### GUIDING PRINCIPLES KEY



1: Connect to the Water



3: Grow Up & Grow Old in the Same Neighbourhood



5: Share the Streets



2: Enjoy the Town Centre



4: See the Sea



6: Live & Play in Green Places

## 7. Environmental Management

The City of White Rock preserves and protects natural areas, features, and habitat, and encourages the responsible use of resources through sustainable development.



## 8. Transportation & Mobility

The City of White Rock facilitates the movement of people by providing an inclusive, interconnected transportation network.



## 9. Economic Development

The City of White Rock attracts a diversified economy by supporting local businesses, creating a desirable place to visit, work, shop, and invest.



## 10. Parks & Recreation

The City of White Rock provides a diverse range of recreational facilities and open space, offering a wide variety of programs for residents of all ages and abilities.



## 11. Infrastructure

The City of White Rock is a safe and resilient community with responsive, high-quality, and well-maintained infrastructure that meets the needs of the community.



## 12. Arts, Culture & Heritage

The City of White Rock promotes and celebrates its thriving local arts, culture, and heritage to its fullest potential.




## 13. Quality of Life

The City of White Rock provides an environment where all residents can realize their potential in living healthy, happy, and well-balanced lives.





### GUIDING PRINCIPLES KEY

 1: Connect to the Water

 2: Enjoy the Town Centre

 3: Grow Up & Grow Old in the Same Neighbourhood

 4: See the Sea

 5: Share the Streets

 6: Live & Play in Green Places

## 4. Context & Projections

### 4.1 Context

Since time immemorial, the Semiahmoo and their ancestors have inhabited these lands. “Semiahmoo” translates to half-moon, referring to the shape of the southern bay of White Rock. Today, the Semiahmoo First Nation occupies 133 hectares of land within Surrey’s municipal boundaries along Semiahmoo Bay.

White Rock is situated near the Canada - United States border, which provides economic benefits such as cross-border shopping and tourism, as well as shared jurisdictions and responsibilities, such as the management of Semiahmoo Bay. White Rock is part of the region consisting of Canada’s Georgia Basin and the US Puget Sound, and is a member of Metro Vancouver, as shown in Figure 2.

North Bluff Road/16th Avenue is an important east-west corridor and boundary between White Rock and Surrey. Past and projected future rapid growth in Surrey, particularly southern or ‘South’ Surrey, influences the City of White Rock due to their close proximity. Together, White Rock and South Surrey have shaped and will continue to shape the character of the peninsula in terms of the economy, land use, and transportation.

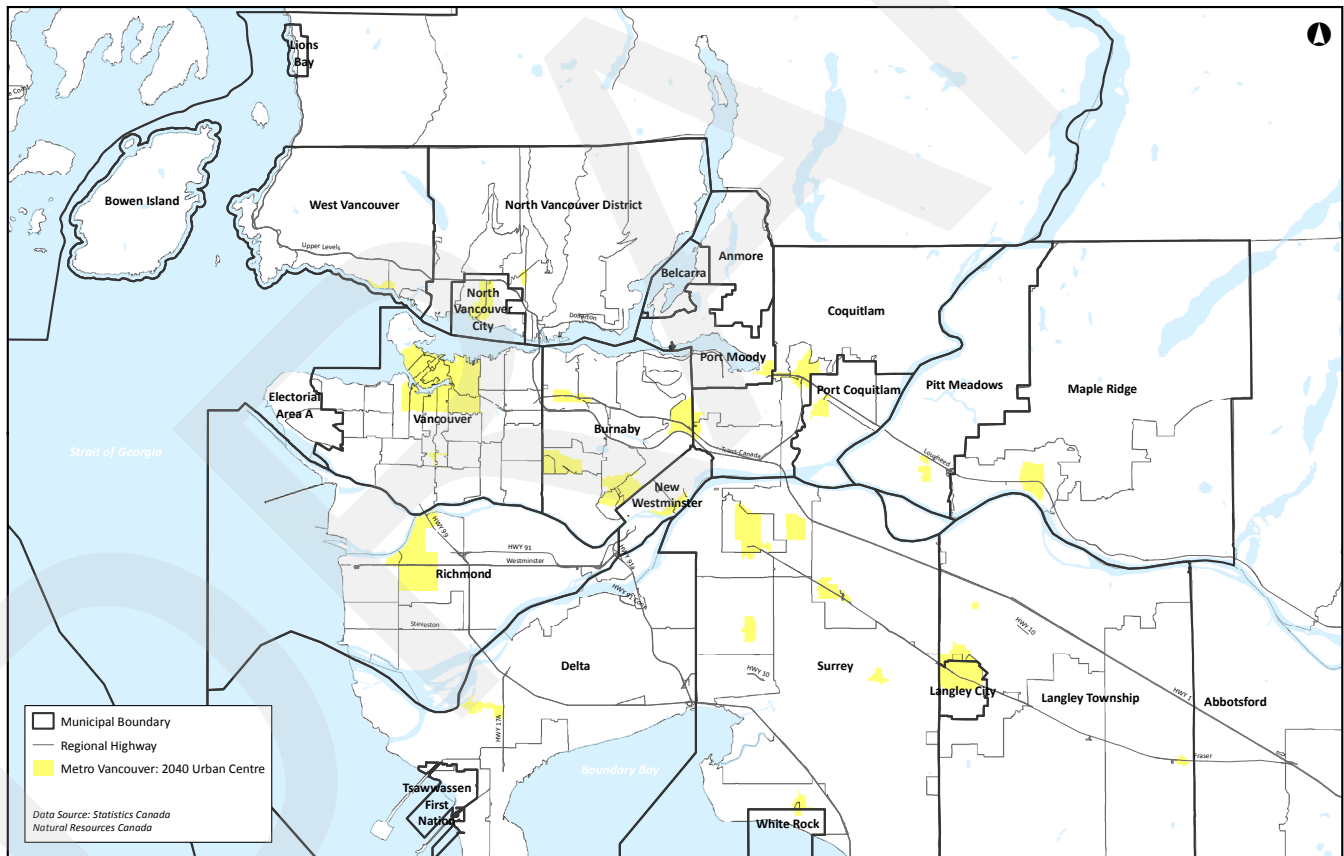


Figure 2. Metro Vancouver Regional Context

## 4.2 Population Growth

The South Surrey/White Rock Federal Electoral District (Figure 3) grew from 94,678 people in 2011 to 104,051 people in 2016, an average growth rate of about 2.0% per year. The pace of population growth in White Rock has been slower, with an average growth rate of about 0.6% per year over the same period as the population increased from 19,339 to 19,952. This reflects a long term trend in which White Rock's population has grown at a moderate rate, as presented in Figure 4. Based on recent and long term trends, as well as BC Stats projections, the population of the City of White Rock is expected to reach between 23,900 and 27,300 people by 2045.

The South Surrey–White Rock Federal Electoral District (Figure 3) grew from 104,051 people in 2016 to 119,672 people in 2021, reflecting an average annual growth rate of approximately 2.9%. These figures are based on the boundaries defined by the 2013 Federal Electoral Boundary, which remains in use for census reporting until the next federal election. A new Federal Electoral Boundary was proclaimed in 2023, but it will only take effect for the next Census, as per Statistics Canada.

In contrast, the City of White Rock experienced more modest growth, increasing from 19,952 people in 2016 to 21,939 people in 2021, corresponding to an average annual growth rate of about 1.9%. This continues a long-term trend of moderate population growth in White Rock, as shown in Figure 4.

Looking ahead, BC Stats projects White Rock's population to reach between 23,900 and 27,300 people by 2045, depending on the growth scenario. Statistics Canada's national projections support similar growth trajectories.

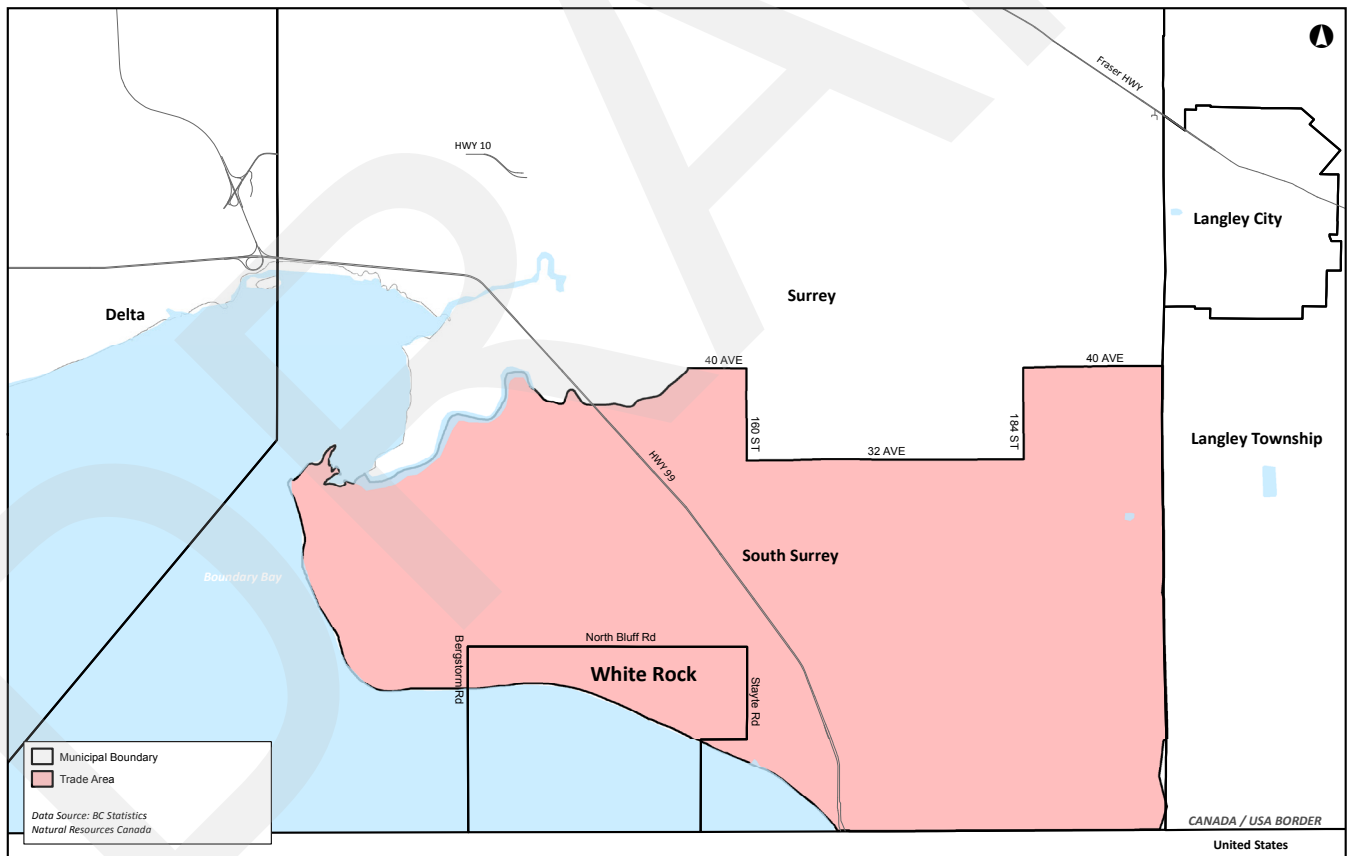


Figure 3. White Rock/South Surrey Trade Area

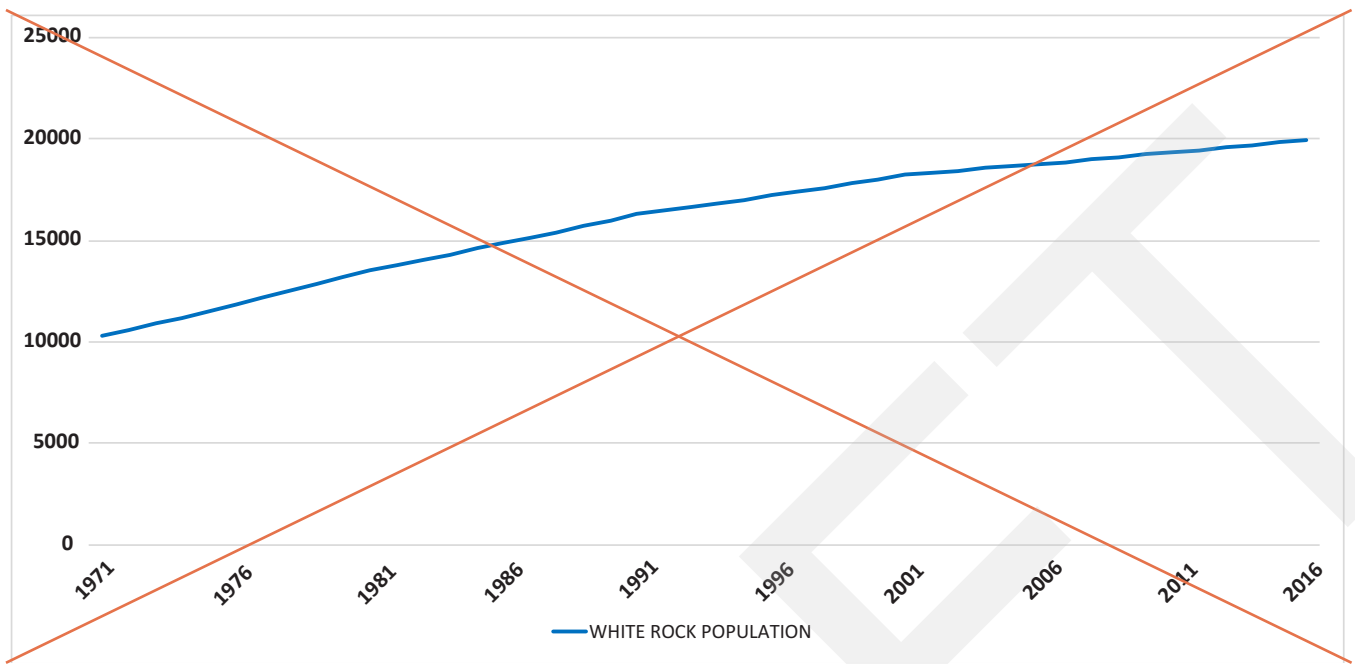


Figure — Census Population for White Rock, 1971 to 2016

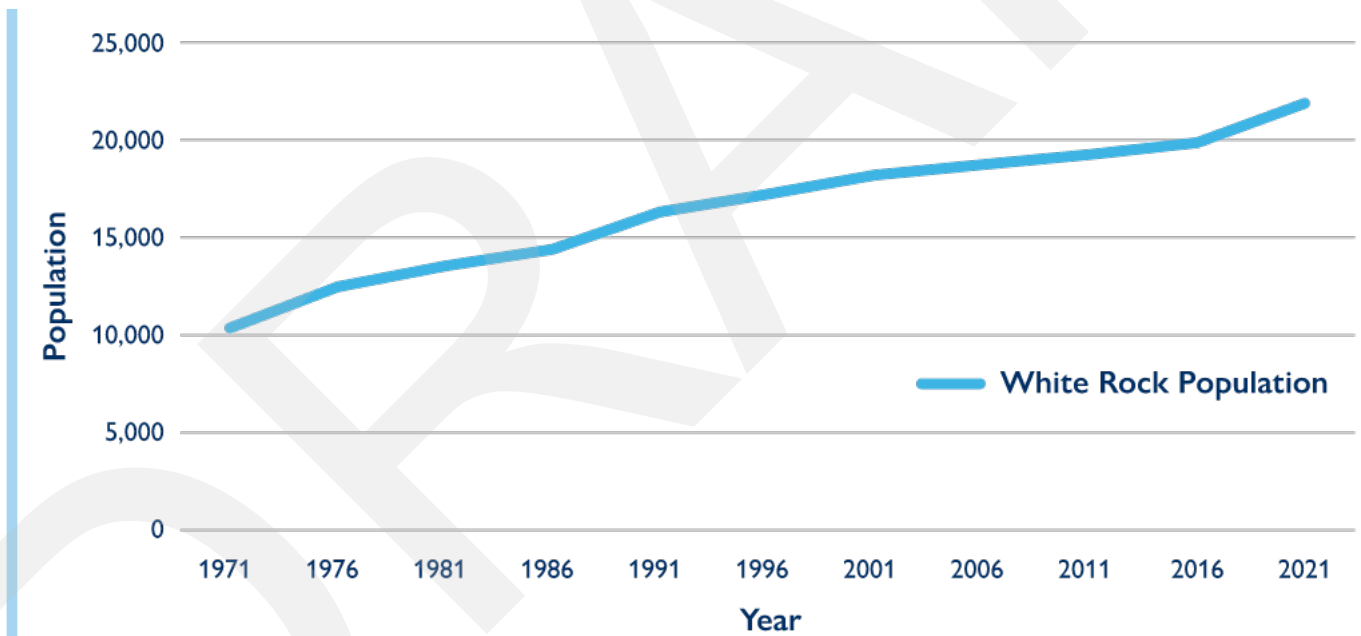


Figure 4. Census Population for White Rock, 1971 to 2021

### 4.3 Housing Growth

~~Between 2006 and 2016, there was an average of 164 new housing starts per year in the City of White Rock. Approximately 76% of these housing starts were apartment units, about 4% were duplexes/rowhomes, and remaining 20% were single-detached houses. The total number of private dwellings grew from 9,870 in 2011 to 10,856 in 2016, an increase of 10.0%.~~

~~Projections for residential development in White Rock from 2016 to 2045 are between 4,320 and 5,110 new units. Estimated annual totals by type include 5-10 new duplex/townhouse units and 145-170 new apartment units (including secondary suites in new single detached houses). This works out to a total of 110-240 new duplex/townhouse units and 4200-4900 new apartment units between 2016 and 2045. No significant net increase in single-detached homes is expected, as most new starts will likely involve demolition and replacement of existing houses.~~

Between 2020 and 2024, the City of White Rock recorded a total of 1,043 new housing starts, averaging about 209 units per year. Of these, apartment units accounted for 84% (879 units), while single-detached houses made up 15% (157 units). Duplexes and row homes represented a very small share, with only 4 semi-detached and 3 row units built over the five-year period. These figures reflect a continuation of White Rock's shift toward higher-density residential development.

From 2016 to 2045, projections estimate between 4,320 and 5,110 new residential units will be added in White Rock. To help address housing diversity and attainability, Small-Scale Multi-Unit Housing (SSMUH) is now permitted in areas that previously allowed only single-detached homes. These housing types contribute to the City's goals for Missing Middle Housing while providing more attainable options that fit within existing neighbourhood character.

While SSMUH will play an important role in expanding housing choice and supply, it is not a complete solution. Additional strategies and housing types will be necessary to meet the full range of housing needs as identified in the 2024 Interim Housing Needs Report.

### 4.4 Employment Projections

~~White Rock's employed labour force grew from 8,695 in 2001 to 10,050 in 2011, an increase of 15.6%. As the City's population projections are consistent with the population projections in the Regional Growth Strategy, it is anticipated that the employment projections will also be consistent. Total employment in the City is expected to increase to approximately 9,300 by 2045.~~

White Rock's employed labour force grew from 8,695 in 2001 to 10,015 in 2021, an increase of approximately 15.2% over two decades. This steady growth reflects broader regional trends and aligns with population increases during the same period. As the City's population projections are consistent with those in the Regional Growth Strategy, it is anticipated that employment growth will continue in parallel. According to Metro Vancouver's 2024 Growth Projections Update, total employment in White Rock is expected to increase to approximately 10,590 to 11,830 jobs by 2045, depending on the growth scenario.

### 4.5 Commercial Growth

~~White Rock has a total inventory of approximately 595,000 square feet of retail and service floorspace, of which 74% is situated in the Town Centre and Lower Town Centre, 16% percent is located in the West Beach area, 8% is found in the East Beach area, and the balance (2%) is in scattered locations. The main commercial growth prospect for White Rock is locally-serving retail and service floorspace. Analysis based on the projected population growth for the City suggests that an additional 80,000 to 209,000 square feet of locally-oriented retail and service space could be supported by City residents by 2045.~~

~~Grocery stores tend to be anchor tenants in most neighbourhood commercial centres, acting as a strong draw for day-to-day shopping trips that are often combined with other errands. As of 2016, White Rock has two grocery stores totaling 22,000 square feet, and with current demand and a growing population could support up to a total of 48,000 to 54,000 square feet of grocery store space by 2045.~~

White Rock has a total inventory of approximately 739,000 square feet of retail and service floorspace. The vast majority of this space is located in the Town Centre and Lower Town Centre, with smaller concentrations found in the West Beach and East Beach areas, and a limited amount scattered throughout other parts of the city.

The main commercial growth prospect for White Rock is locally-serving retail and service floorspace. Analysis based on the projected population growth for the City suggests that an additional 52,700 to 145,800 square feet of locally-oriented retail and service space could be supported by City residents by 2045, assuming growth projections.

~~Grocery stores tend to be anchor tenants in most neighbourhood commercial centres, acting as a strong draw for day-to-day shopping trips that are often combined with other errands. With current demand and a growing population could support up to a total of 48,000 to 54,000 square feet of grocery store space by 2045.~~

## 4.6 Office Growth

~~White Rock has a total inventory of approximately 163,400 square feet of office floorspace, of which 75% is located in the Town Centre (56%) and Lower Town Centre (19%), 21% percent is in the West Beach area, and 4% is found in the East Beach area. Analysis based on the projected population growth for the City suggests that an additional 30,000 to 79,000 square feet of office space could be supported by 2045.~~

# 5. Regional Context Statement

## 5.1 Introduction

The Metro Vancouver Regional Growth Strategy (RGS), Metro Vancouver 2040: Shaping Our Future (Metro 2040), provides the policy framework for accommodating regional population and employment growth up to the year 2041 while maintaining and enhancing the region's livability. Metro 2040 was adopted on July 29, 2011 by the Metro Vancouver Regional District, 21 municipalities, Tsawwassen First Nation, and TransLink.

Following section 429 of the Local Government Act, an RGS must cover a period of at least 20 years from the time of its initiation and while it may deal with any regional matter, it is required to include:

- a) a comprehensive statement on the future of the region, including the social, economic and environmental objectives of the board in relation to the regional district;
- b) population and employment projections for the period covered by the regional growth strategy;
- c) to the extent that these are regional matters, actions proposed for the regional district to provide for the needs of the projected population in relation to:
  - i) housing;
  - ii) transportation;
  - iii) regional district services;
  - iv) parks and natural areas, and
  - v) economic development;
- d) to the extent that these are regional matters, targets for the reduction of greenhouse gas emissions in the regional district, and policies and actions proposed for the regional district with respect to achieving those targets.

The Metro Vancouver RGS (Metro 2040) has five goals:

- Create a compact urban area
- Support a sustainable economy
- Protect the environment and respond to climate change impacts
- Develop complete communities
- Support sustainable transportation choices

Section 446 of the Local Government Act requires that official community plans include a regional context statement (RCS) that identifies the relationship between the official community plan and the RGS, and if applicable, how the official community plan is to be made consistent with the RGS over time.

White Rock's RCS indicates how the policies of this Official Community Plan are related to and will help meet the goals and strategies of the RGS. Table 2 at the end of this RCS provides information on the sections of the Official Community Plan that relate to the goals and strategies of the RGS.

Based on recent and long-term trends, the population of the City of White Rock is expected to reach between 23,900 and 27,300 people over the time period covered by this Official Community Plan, a relatively modest growth rate of between 0.6% and 1.0% annually, which is less than the region's overall growth rate as new growth can only be accommodated in White Rock through infill and redevelopment. The majority of this growth over the life of the OCP will be focused in the Town Centre. It is recognized that growth projections represent a low and high development scenario, and are intended to serve as a reference for planning rather than as mandatory targets. Table 1: Population, Dwelling, and Employment Projections to 2045 provides the midpoint between the low and high growth scenarios.

Table ~~White Rock Population, Dwelling, and Employment Projections to 2045~~

<b>White Rock Population, Dwelling, and Employment Projections to 2045</b>					
	<b>2016*</b>	<b>2021</b>	<b>2031</b>	<b>2041</b>	<b>2045</b>
<b>Population</b>	19,950	20,925	22,870	24,820	25,600
<b>Dwellings</b>	10,860	11,670	13,290	14,910	15,560
<b>Employment</b>	7,400	7,800	8,400	9,300	9,600

*\*The 2016 population and dwelling estimates are taken from 2016 Census published counts. Population and dwelling estimates prepared by Metro Vancouver may include additional estimates of population and dwelling Census undercount.*

Figure 5 below depicts the Urban Containment Boundary, along with the regional land use designations and boundaries of White Rock's Urban Centre (the 'Town Centre' which is a portion of the Semiahmoo Municipal Town Centre identified in the Metro 2040).

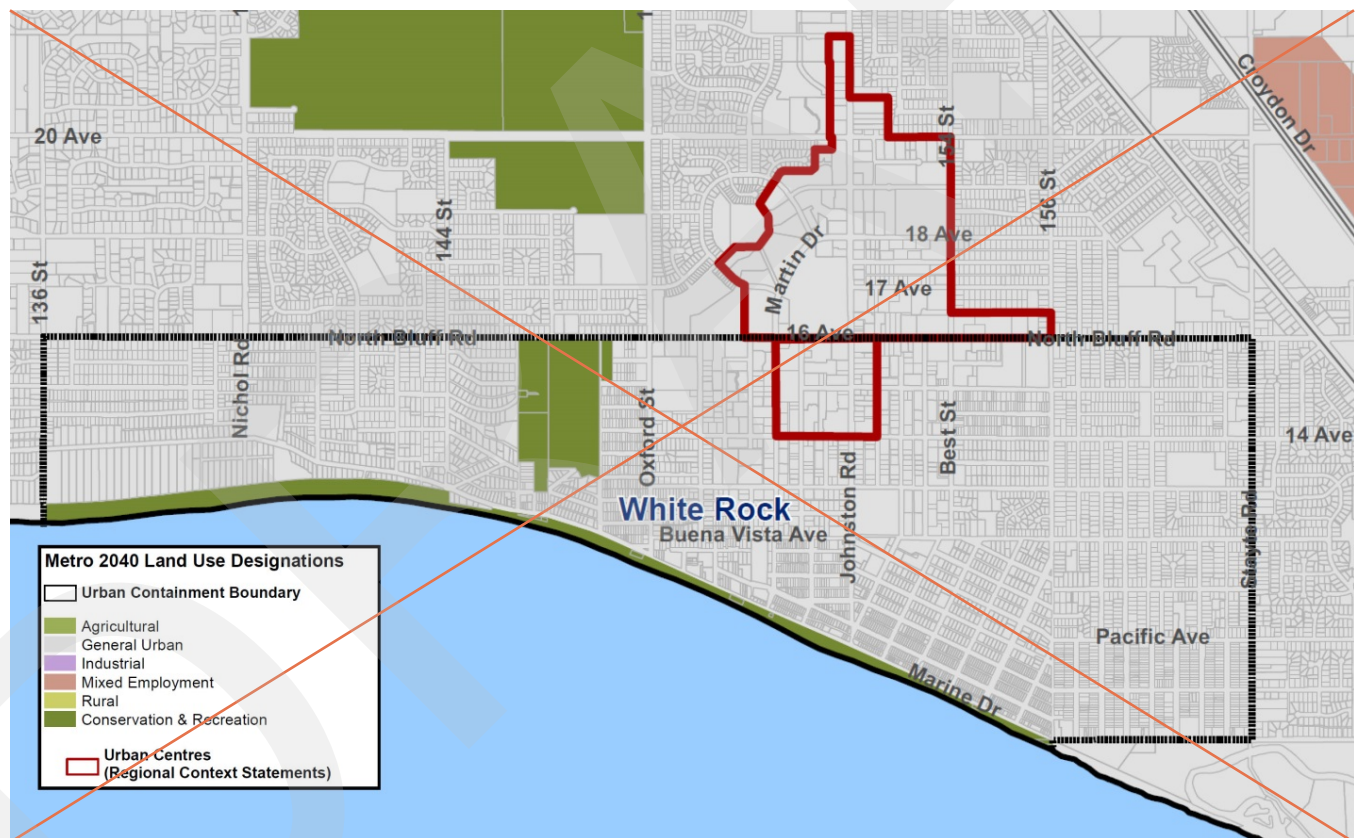


Figure ~~Urban Containment Boundary~~

The OCP will be made and kept consistent with Metro 2040 over time by way of annual reviews by Council and meetings between City staff and staff from the Regional District to discuss intersecting local and regional matters, as required. The Regional Context Statement will also be considered by Council when reviewing amendments to the RGS that relate to regional matters.

RGS Goals & Strategies	White Rock OCP Policy Alignment
<b>Goal 1: Create a Compact Urban Area</b>	
<p><del>Metro Vancouver's growth is concentrated in compact communities with access to a range of housing choices, and close to employment, amenities and services. Compact transit-oriented development patterns help reduce greenhouse gas emissions and pollution, and support both the efficient use of land and an efficient transportation network.</del></p>	<p><del>With 3,893 people per square kilometre, White Rock is the fourth most compact community in the Metro Vancouver region, behind the cities of Vancouver, North Vancouver and New Westminster. The majority of new population and housing growth will be directed to the Town Centre and adjacent areas, which are where most employment, amenities, and services are located. Objective 7.1 states the City's intent is to manage growth in a way that is consistent with the Regional Growth Strategy.</del></p>
<p><del>Strategy 1.1 Contain urban development within the Urban Containment Boundary</del></p>	<p><del>As the entire City of White Rock is located inside the Urban Containment Boundary, all projected population, dwelling unit and employment growth will occur within the boundary.</del></p>
<p><del>Strategy 1.2 Focus growth in Urban Centres and Frequent Transit Development Areas</del></p>	<p><del>White Rock's Town Centre is part of the Semiahmoo Municipal Town Centre, an 'Urban Centre' in the Regional Growth Strategy which extends into the City of Surrey. The overall development of the Semiahmoo Municipal Town Centre will require consultation with the City of Surrey. The Town Centre area is identified as the focus for the majority of future growth over the life of this Official Community Plan and is noted as the City's centre for cultural, civic, economic, and public life in the City. Policies 8.1.1 to 8.1.4 for the 'Town Centre' land use designation permit mixed use development at the highest density considered in the plan.</del></p> <p><del>Further policies under 9.1.1 to 9.5.1 elaborate strategies for achieving the City's goal of developing the Town Centre as a distinctive, lively, and pedestrian-focused growth area, and policy 13.5.5 encourages reducing parking requirements in denser areas. While the majority of growth is anticipated to be focused within the Town Centre area, limited growth is permitted at a generally lower density in the following land use designations which have existing transit services and/or which are in close proximity to employment, amenities, and services: Town Centre Transition, Lower Town Centre, Urban Neighbourhood, Waterfront Village, North Bluff East and West, and Neighbourhood Commercial.</del></p>
<p><del>Strategy 1.3 Protect Rural areas from urban development</del></p>	<p><del>Not applicable in the City of White Rock; there are no Rural areas.</del></p>

**Goal 2: Support a Sustainable Economy**

The land base and transportation systems required to nurture a healthy business sector are protected and supported. This includes supporting regional employment and economic growth. Industrial and agricultural land is protected and commerce flourishes in Urban Centres throughout the region.

The majority of businesses in the City of White Rock are located in the Town Centre, which the Official Community Plan continues to promote as the primary employment hub and business centre of the community. New employment generating uses are also permitted and encouraged in the Lower Town Centre area and in the Waterfront Village area.

Strategy 2.1—Promote land development patterns that support a diverse regional economy and employment close to where people live

Given White Rock’s land constraints, there are no opportunities for major commercial development outside of the Town Centre, but any increase in employment will be close to where people live due to White Rock’s compact nature. Policy 7.3.1 calls for a focus on new commercial growth in the Town Centre, Lower Town Centre, and Waterfront Village land use designations, while the limited commercial growth supported in policy 7.4.3 for Neighbourhood Commercial areas is intended to serve the local population. Areas within the Peace Arch Hospital and adjacent to it are encouraged to redevelop with compatible medical-related commercial uses in Policy 8.7.1 and 8.7.2. Objective 9.1 promotes offices development within the Town Centre as the primary employment hub, and Policy 14.4.1 calls for the City to protect the commercial land base from conversion to non-employment uses, and to allow home occupations.

Strategy 2.2—Protect the supply of industrial land

Not applicable in the City of White Rock; there are no industrial areas.

Strategy 2.3—Protect the supply of agricultural land and promote agricultural viability with an emphasis on food production

There are no Agricultural areas within the City of White Rock, however the Official Community Plan does support urban agriculture as well as related activities such as food processing, food sales, and nutrient recovery in policies 18.4.1 to 18.4.5.

**Goal 3: Protect the Environment and Respond to Climate Change Impacts**

Metro Vancouver's vital ecosystems continue to provide the essentials of life—clean air, water and food. A connected network of habitats is maintained for a wide variety of wildlife and plant species. Protected natural areas provide residents and visitors with diverse recreational opportunities. Strategies also help Metro Vancouver and member municipalities meet their GHG targets, and prepare for, and mitigate risks from, climate change and natural hazards:

The City of White Rock's goal for environmental management is to preserve and protect natural areas, features, and habitat, and encourage the responsible use of resources through sustainable development. Further objectives are to mitigate risk of natural disasters and climate change (12.4) and to conserve energy and reduce greenhouse gas emissions (12.5):

Strategy 3.1—Protect Conservation and Recreation lands

The Official Community Plan includes specific policies for protecting Semiahmoo Bay and watercourses in policies 12.1.1 to 12.1.87 and recreation lands in policies 15.1.1:

Strategy 3.2—Protect and enhance natural features and their connectivity

Objective 12.2 and policies 12.2.1 to 12.2.4 provide direction for protection and enhancing habitat and natural areas in the city, and promotes ecological greenways to increase connection between isolated habitats. Development permit area guidelines (Section D) promote the retention and protection of mature trees in the process of new development:

Strategy 3.3—Encourage land use and transportation infrastructure that reduce energy consumption and greenhouse gas emissions, and improve air quality

Policies 12.5.1 to 12.5.7 provide strategies for conserving energy and reducing greenhouse gas emissions. Policy 12.3.1 supports Metro Vancouver's Air Quality Management Plan and supports the shift to more active travel modes and transit use:

Strategy 3.4—Encourage land use and transportation infrastructure that improve the ability to withstand climate change impacts and natural hazard risks

Policies 12.4.1, 12.4.2, and 16.2.3 promote the use of infrastructure that is able to withstand climate change impacts and natural hazard risks. Policy 12.1.4 supports shoreline restoration measures and the protection of the foreshore from erosion. The City has development permit guidelines for floodplain areas (Section 23.3), watercourses (Section 23.5) and ravine lands and significant trees (Section 23.4) to minimize natural hazard risk for new developments:

**Goal 4: Develop Complete Communities**

Metro Vancouver is a region of communities with a diverse range of housing choices suitable for residents at any stage of their lives. The distribution of employment and access to services and amenities builds complete communities throughout the region. Complete communities are designed to support walking, cycling and transit, and to foster healthy lifestyles.

The City of White Rock's goal for land use is maximize its limited land resource by creating a complete community where residents have convenient access to jobs, services, open space, and amenities.

Strategy 4.1—Provide diverse and affordable housing choices

The City of White Rock's goal for housing is to provide a mix of housing choices that are appropriate and affordable for residents at various stages of their lives. Policies 11.1.1 through 11.2.3 speak to increasing housing diversity by requiring minimum percentages of two and three-bedroom units in new residential developments, incorporating age-friendly measures in housing, supporting secondary suites, and encouraging the development of new affordable and market rental housing in transit-accessible locations. These policies provide a framework on which a Housing Action Plan could be prepared in the future.

Strategy 4.2—Develop healthy and complete communities with access to a range of services and amenities

Policy 18.3.1 supports the enhancement of recreation opportunities at civic facilities to meet the diverse needs of the community. Policy 18.3.2 supports improving access to open space so that all residents live within a 5-minute walk of a park, green space, or other civic space. Policy 18.4.2 and 18.4.4 supports access to the local food system through community gardening and farmer's market.

Goal 5: Support Sustainable Transportation Choices

Metro Vancouver's compact, transit-oriented urban form supports a range of sustainable transportation choices. This pattern of development expands the opportunities for transit, multiple-occupancy vehicles, cycling and walking, encourages active lifestyles, and reduces energy use, greenhouse gas emissions, household expenditure on transportation, and improves air quality. The region's road, transit, rail and waterway networks play a vital role in serving and shaping regional development, providing linkages among the region's communities and providing vital goods movement networks.

The City of White Rock's goal for transportation and mobility is to facilitate the movement of people by providing an inclusive, interconnected transportation network. In line with the City's Strategic Transportation Plan, the priority for transportation modes is walking, cycling, transit, goods movement, and personal vehicles.

Strategy 5.1—Coordinate land use and transportation to encourage transit, multiple-occupancy vehicles, cycling and walking

A main objective for the Town Centre area, where the majority of growth is intended to occur, is to improve connections for walking and cycling by breaking up blocks with pedestrian pathways (Policy 9.3.1). Objective 13.1 is to encourage walking as the first choice for short trips, and policies 13.1.1 to 13.1.10 detail strategies on how to support this objective. Objective 13.2 is to improve the environment for cycling within White Rock, and policies 13.2.1 to 13.2.5 relate to this objective. Objective 13.3 and related policies 13.3.1 to 13.3.4 outline the ways the City intends to work with transit service providers to improve the frequency, comfort and convenience of transit use.

Strategy 5.2—Coordinate land use and transportation to support the safe and efficient movement of vehicles for passengers, goods and services

Objective 13.5 and policies 13.5.1 to 13.5.5 provide support for enabling the safe movement of vehicles and effectively managing parking supply. Objective 13.4 and policies 13.4.1 to 13.4.3 relate to the efficient delivery of goods to businesses in White Rock. Policy 13.4.2 notes that the City's goods are delivered on dedicated Truck Routes as identified in the City's Street and Traffic Bylaw, which functions as the City's Goods Movement Network map.

# Part B Policies



# 5. Community Character

**Goal:** The City of White Rock has great places that foster positive social interaction, community pride, and appreciation of natural features.



## Overview

~~White Rock is made up of distinct character areas, each with its own unique urban design qualities. These areas include the Town Centre, the Lower Town Centre, Waterfront Village, North Bluff Road, Urban Neighbourhoods, and Mature Neighbourhoods.~~

~~The mixed-use Town Centre and Lower Town Centre have strong character in areas where there are continuous small-scale storefronts, public realm amenities, and mature street trees. Views to the ocean are first experienced along Johnston Road in the Lower Town Centre, which helps reinforce a strong sense of place and identity for White Rock as a seaside community.~~

~~The Waterfront Village along Marine Drive, and the waterfront itself, are unique assets and part of the defining qualities of White Rock as a seaside community. The mixed-use areas on Marine Drive create destinations and activate the public realm, while a promenade along the waterfront reinforces this area as a regional draw. The steep topography provides clear waterfront views while also creating a physical barrier to and from the rest of the city.~~

~~North Bluff Road is the boundary between White Rock and Surrey. The character is diverse along this transit corridor, ranging from low-to-high density residential environments to larger format retail environments.~~

~~The Urban Neighbourhoods, areas around the Town Centre and Lower Town Centre, include mainly low-rise residential buildings with generous setbacks and varying amounts of tree canopy. These areas are diverse in terms of its views and public realm treatments.~~

~~Mature Neighbourhoods reflect the historic settling of White Rock with small single detached lots. Occupying both higher and lower grounds, these neighbourhoods have variable physical and visual access to the waterfront.~~

White Rock is made up of distinct character areas, each with its own unique urban design qualities. These areas include the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use, Lower Town Centre, Health District, Waterfront Village, North Bluff East, Neighbourhood Commercial, Urban Neighbourhoods, and Mature Neighbourhoods.

The Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use and Lower Town Centre have strong character in areas where there are continuous small-scale storefronts, public realm amenities, and mature street trees. Views of the ocean are first experienced along Johnston Road in the Lower Town Centre, which helps reinforce a strong sense of place and identity for White Rock as a seaside community.

The Waterfront Village, along Marine Drive, and the waterfront itself, are unique assets and part of the defining qualities of White Rock as a seaside community. The mixed-use areas on Marine Drive create destinations and activate the public realm, while a promenade along the waterfront reinforces this area as a regional draw. The steep topography provides clear waterfront views while also creating a physical barrier to and from the rest of the city.

North Bluff Road is the boundary between White Rock and Surrey. The character is diverse along this transit corridor, ranging from low-to-high density residential environments to larger format retail environments.

The Urban Neighbourhoods, areas around the Town Centre, Town Centre Transition areas, and Lower Town Centre, include mainly low-rise residential buildings with generous setbacks and varying amounts of tree canopy. These areas are diverse in terms of their views and public realm treatments.

Mature Neighbourhoods reflect the historic settling of White Rock with small single detached lots. Occupying both higher and lower grounds, these neighbourhoods have variable physical and visual access to the waterfront.

## Objectives & Policies

**Objective 5.1** To retain and enhance the existing character of White Rock, including each of its character sub-areas, through development.

~~**Policy 5.1.1** **Character-Sensitive Growth**—Focus most growth in high activity areas that are already characterized by medium to high intensities, primarily in the Town Centre and secondarily in adjacent areas and the Lower Town Centre. Require transition areas identified in Sections 7 and 8 to sensitively integrate the edges of the Town Centre with adjacent neighbourhoods, protecting their own established scale and character.~~

**Policy 5.1.1** **Character-Sensitive Growth**—Focus most growth in high activity areas that are already characterized by medium to high intensities, primarily in the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use and secondarily in adjacent areas and the Lower Town Centre. Require transition areas identified in Sections 6 and 7 to sensitively integrate the edges of the Town Centre with adjacent neighbourhoods, protecting their own established scale and character.

~~**Policy 5.1.2** **Commercial Areas**—Focus commercial growth in areas characterized by mixed-use and commercial environments, including in the Town Centre, Lower Town Centre, and Waterfront Village areas.~~

**Policy 5.1.2** **Commercial Areas**—Focus commercial growth in areas characterized by mixed-use and commercial environments, including in the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use, Lower Town Centre, Neighbourhood Commercial and Waterfront Village areas.

**Policy 5.1.3** **Retail Environments**—Require that retail uses be small or medium scale, protecting the established and character-defining qualities of White Rock’s established retail environment.

**Policy 5.1.4** **Streetscape Design**—Consistent with the policies in Part B and the guidelines in Part D, require that future investment in the built environment and public realm reinforce and enhance the character of existing streetscapes.

**Policy 5.1.5** **Building Design**—Consistent with the guidelines in Part D, require that the use of materials, textures, and other architectural treatments that reinforce White Rock’s seaside village character.

**Policy 5.1.6** **Gateways and Signage**—Develop and erect gateway features and signage at key entrances to the City and at entrances to commercial areas, including at the intersection of Johnston and North Bluff Roads.

**Objective 5.2** To protect and reinforce views to the Waterfront from upland areas of the city, to assist in the protection of White Rock’s identity as a seaside community.

**Policy 5.2.1** **Views and Development**—Balance redevelopment with the protection of views to the water by limiting the area of tower floorplates and establishing appropriate tower setbacks from the street, as per the Development Permit Guidelines dealing with form and character in Part D.

**Policy 5.2.2** **Views and Trees**—Select area-appropriate tree species for street plantings to limit conflicts between the protection of views and the retention of trees.

**Policy 5.2.3** **View to the Water from Public Spaces**—Prioritize views to the water from public places, such as active walking streets, parks, and plazas. Specifically celebrate and reinforce vistas from the Lower Town Centre by creating inviting public spaces for pausing and enjoying the view.

## 6. Growth Management

**Goal:** The City of White Rock encourages citizen involvement while managing growth and development, characterized by high-quality design and the provision of community amenities and infrastructure.



## Overview

White Rock's population has been growing at a moderate rate for decades, with most of the recent growth taking place in the Town Centre and adjacent areas. In the last decade, approximately 73% of new housing starts have been in the form of apartments. Over the last 10 years, over half of new retail and service development has taken place in the Town Centre, and approximately one third has taken place in the eastern commercial portion of Marine Drive. Detailed growth trends for population, housing, and commercial development are outlined in Part A.

The general approach to growth management in the City is to reinforce the existing pattern of growth and continue to focus density in the Town Centre, as presented in Figure 6. Additional growth is to be located in the Town Centre Transition and Lower Town Centre areas. The highest heights and densities are to be concentrated at the intersection of Johnston and North Bluff Roads, with heights and densities generally transitioning down to the south, east, and west. The Conceptual Urban Form for the City is highlighted in Figures 7 and 8.

White Rock's population has been growing at a moderate rate for decades, with most of the recent growth taking place in the Town Centre, Town Centre Transition- Residential, Town Centre Transition- Mixed Use and adjacent areas. In the last decade, approximately 84% of new housing starts have been in the form of apartments. Over the last 10 years, over half of new retail and service development has taken place in the Town Centre, Town Centre Transition- Residential, Town Centre Transition- Mixed Use, and approximately one third has taken place in the eastern commercial portion of Marine Drive. Detailed growth trends for population, housing, and commercial development are outlined in Part A.

The general approach to growth management in the City is to reinforce the existing pattern of growth and continue to focus density in the Town Centre, Town Centre Transition-Residential, Town Centre Transition-Mixed Use, as presented in Figure 5. Additional growth is to be located in the Town Centre, Town Centre Transition- Residential, Town Centre Transition- Mixed Use and Lower Town Centre areas. The highest heights and densities are to be concentrated at the intersection of Johnston and North Bluff Roads, with heights and densities generally transitioning down to the south, east, and west. The Conceptual Urban Form for the City is highlighted in Figures 6 and 7.

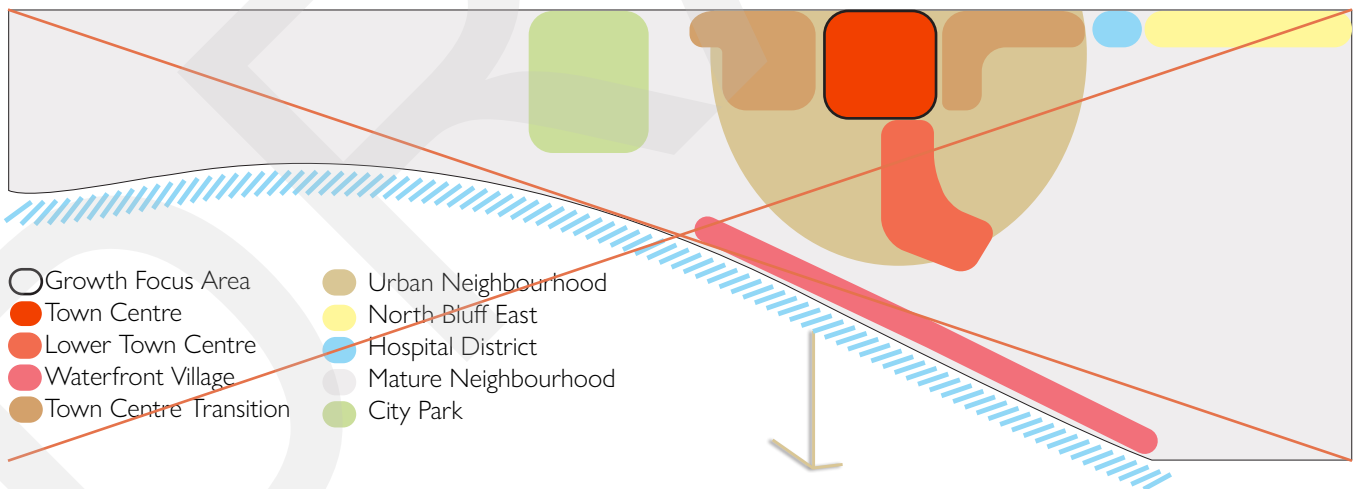


Figure 6 White Rock Growth Plan

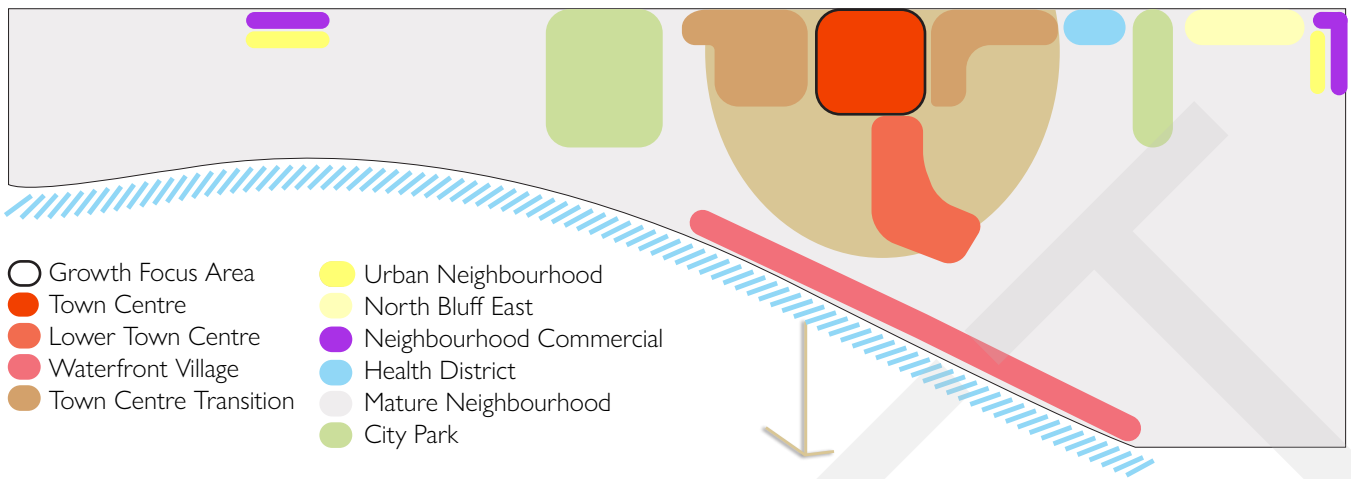


Figure 5. White Rock Growth Plan

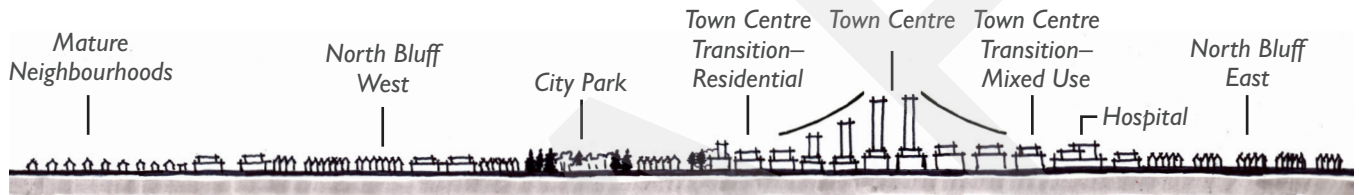


Figure 6. Conceptual Urban Form—East-West Section

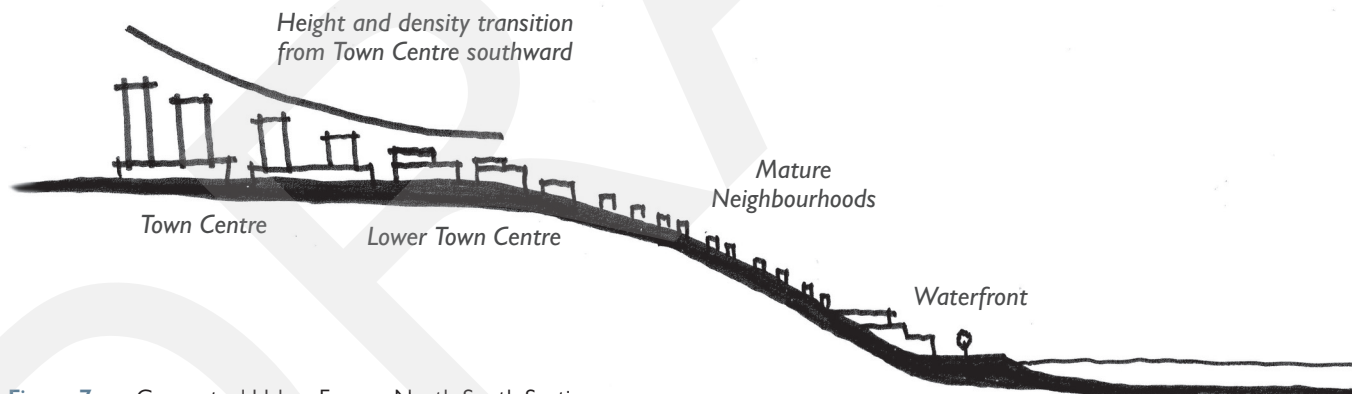


Figure 7. Conceptual Urban Form—North-South Section

## Objectives & Policies

**Objective 6.1** To manage growth in a way that is consistent with the Regional Growth Strategy.

**Policy 6.1.1** **City within a Region**—Reinforce compactness and completeness throughout White Rock, focusing future residential intensification and new commercial development in the regionally designated Town Centre, which includes areas served by frequent transit.

**Objective 6.2** To encourage growth that is consistent with the Conceptual Urban Form for the City.

**Policy 6.2.1** **Transitions**—Focus the tallest and densest development at the intersection of Johnston and North Bluff Roads, with heights and densities decreasing gradually to the south, east, and west.

**Policy 6.2.2** **Height Variations**—Encourage some variation in building heights within the general transition to lower heights moving away from the intersection at Johnston and North Bluff Roads.

~~**Objective 6.3** To maximize the proportion of residents who can access transit and meet daily needs within walking distance of home, and to reinforce a “City of Centres” with the Town Centre, Lower Town Centre, and Waterfront Village as the focal points of public, economic, and cultural life.~~

**Objective 6.3** To maximize the proportion of residents who can access transit and meet daily needs within walking distance of home, and to reinforce a “City of Centres” with the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use, and Waterfront Village as the focal points of public, economic, and cultural life.

~~**Policy 6.3.1** **Focused Commercial Growth**—Focus new commercial growth in the Town Centre, Lower Town Centre, and Waterfront Village land use designations.~~

**Policy 6.3.1** **Focused Commercial Growth**—Focus new commercial growth in the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use and Waterfront Village land use designations.

~~**Policy 6.3.2** **Focused Residential Growth**—Focus the majority of residential growth and the greatest residential intensities in the Town Centre over the life of this Plan. Additional growth can be accommodated in the Town Centre Transition and Lower Town Centre areas.~~

**Policy 6.3.2** **Focused Residential Growth**—Focus the majority of residential growth and the greatest residential intensities in the Town Centre, Town Centre Transition–Residential, Town Centre Transition–Mixed Use over the life of this Plan. Additional growth can be accommodated in the Town Centre Transition and Lower Town Centre areas.

**Objective 6.4** To support new housing choices and the ability of residents to meet more daily needs within walking distance of home within Mature Neighbourhoods.

~~**Policy 6.4.1** **Gentle Infill**—Enable moderate residential growth in Mature Neighbourhoods, primarily in the form of secondary suites, and houseplexes.~~

**Policy 6.4.1** **Gentle Infill**—Enable moderate residential growth in Mature Neighbourhoods, primarily in the form of secondary suites, accessory dwelling units, semi-detached residential buildings, rowhouse residential buildings and houseplexes.

**Policy 6.4.2** **Neighbourhood-Serving Retail**—Enable limited commercial growth in the form of local-serving retail in Neighbourhood Commercial areas.

## 7. Land Use

**Goal:** The City of White Rock maximizes its limited land resource by creating a complete community where residents have convenient access to jobs, services, open space, and amenities.



## Overview

The majority of the land area in the City of White Rock is dedicated to single family dwellings in Mature Neighbourhoods. Low-rise multi-unit buildings are the dominant form surrounding the Town Centre and Lower Town Centre, while a combination of low-rise, mid-rise, and high-rise residential and mixed-use buildings are located in the Town Centre. Commercial development is focused along Johnston Road and Marine Drive, and institutional and open space uses are scattered throughout the City.

Land use designations allow for the management of future development in the City. Future growth is to be focused in the Town Centre as well as the adjacent Town Centre Transition areas and the Lower Town Centre. Allowable densities (Gross Floor Area Ratio or FAR) for future development for these three land use designations are highlighted in Figure 9. Maximum heights (in storeys) for the same areas are outlined in Figure 10. Schedule A indicates the land use designation that applies to each property in the City.

The majority of the land area in the City of White Rock is dedicated to single-family dwellings in Mature Neighbourhoods. Low and mid-rise multi-unit buildings are the dominant form surrounding the Town Centre and Lower Town Centre, while a combination of low-rise, mid-rise, and high-rise residential and mixed-use buildings is located in the Town Centre. Commercial development is focused along Johnston Road and Marine Drive, with small-scale neighbourhood commercial developments situated along Stayte Road and North Bluff Road. Institutional and open space uses are scattered throughout the City.

Land use designations allow for the management of future development in the City. Future growth is to be focused on the Town Centre as well as the adjacent Town Centre Transition–Mixed Use and Town Centre–Residential and the Lower Town Centre. Allowable densities (Gross Floor Area or Gross FAR) for future development for these four land use designations are highlighted in Figure 8. Maximum heights (in storeys) for the same areas are outlined in Figure 9. Map A indicates the land use designation that applies to each property in the City.

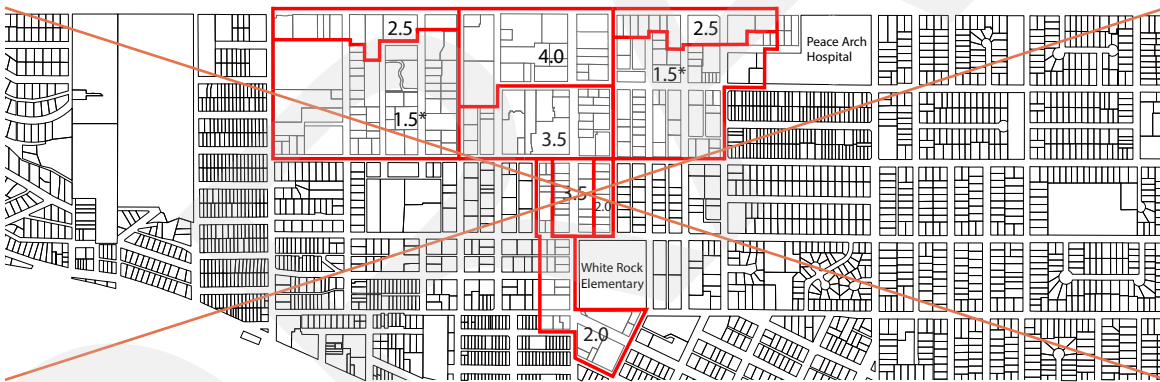


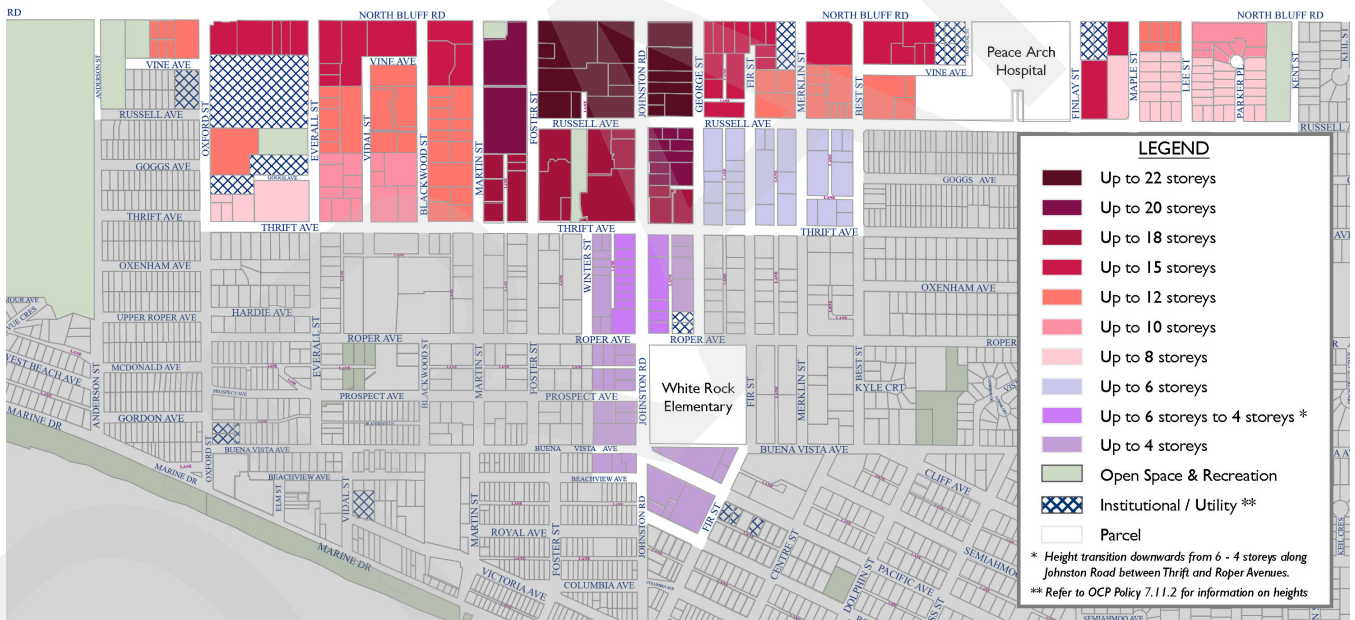
Figure 9 Maximum FAR in the Town Centre, Town Centre Transition, and Lower Town Centre Areas  
\* indicates density may be increased with inclusion of an affordable housing component



Figure 10 Maximum Building Heights in the Town Centre, Town Centre Transition, and Lower Town Centre Areas  
\* indicates density may be increased with inclusion of an affordable housing component



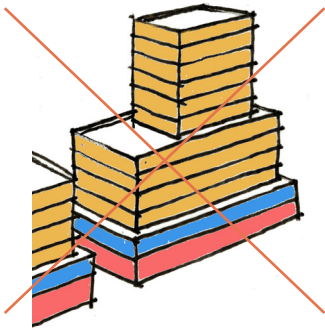
**Figure 8.** Maximum Gross FAR in the Town Centre, Town Centre Transition–Mixed Use, Town Centre Transition–Residential and Lower Town Centre Areas



**Figure 9.** Maximum Building Heights in the Town Centre, Town Centre Transition–Mixed Use, Town Centre Transition–Residential and Lower Town Centre Areas

# Objectives & Policies

The objectives and policies in this section are organized by the following Land Use Designations.

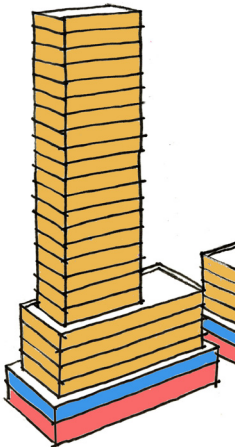


## TOWN CENTRE

The Town Centre is the centre for cultural, civic, economic, and public life in the City. It is characterized by a diverse mix of uses, and it is focus for the majority of future growth over the life of this Plan.

### Objective 7.1

To enable a concentrated mix of multi-unit residential and commercial uses to strengthen the heart of the city, reinforcing it as a neighbourhood, city-wide, and regional destination, and to provide sensitive transitions between the Town Centre and adjacent areas.



Town Centre mixed-use  
(North Bluff Road)

#### Policy 7.1.1

~~Uses and Building Types—Allow mixed-use (commercial/residential/civic), multi-unit residential, and civic uses in a range of low-rise to high-rise buildings, with any new high-rises not to exceed 12 storeys.~~

#### Policy 7.1.1

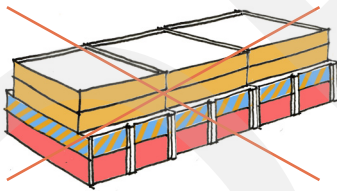
**Uses and Building Types**—Allow mixed-use (commercial/ residential/ civic), multi-unit residential, and civic uses in a range of mid-rise to high-rise buildings.

#### Policy 7.1.2

~~Density and Height—Concentrate the highest densities and heights in the area bounded by North Bluff Road, Johnston Road, Russell Avenue, and George Street. Maximum allowable densities (FAR) are outlined in Figure 9 and maximum heights (in storeys) are illustrated in Figure 10. Any portion of a building within 15 metres of Johnston Road is not to exceed four storeys in height.~~

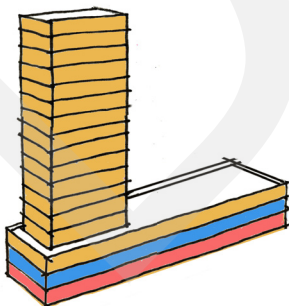
#### Policy 7.1.2

**Density and Height**—Maximum allowable heights are generally up to 16 storeys and maximum densities up to 4.0 gross FAR, with limited opportunities for a height and density bonus of up to 22 storeys and up to 5.0 gross FAR where a significant amenity contribution is provided. Figures 8 and 9 will specify the maximum gross FAR and building height for individual parcels. Guidelines for transitional maximum allowable densities (gross FAR) are provided in Figure 8, while transitions for maximum building height (in storeys) are illustrated in Figure 9.

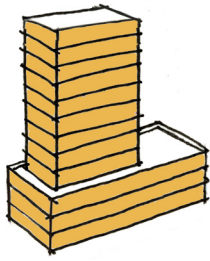


#### Policy 7.1.3

**Affordable Housing Bonus**—Rental and non-market housing contributions will be encouraged in all projects over 6 storeys and will be expected for projects that include buildings that are 12 storeys or higher.



Town Centre mixed-use  
(Johnston Road)



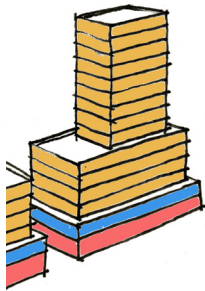
Town Centre residential

**Policy 7.1.4**

**Retail Streets**—Strengthen existing retail streets by requiring continuous street-fronting commercial uses on Johnston Road. Street-fronting commercial uses are encouraged on North Bluff Road and on Thrift and Russell Avenues adjacent to Johnston Road, and will be considered on George Street. Small scale commercial uses may be appropriate in other areas of the Town Centre.

**Policy 7.1.5**

**Urban Design**—Enhance the built and public realms through policies identified in Section 8 and guidelines in the Town Centre Development Permit Area in Part D.



Town Centre Transition–  
Mixed Use

**TOWN CENTRE TRANSITION–MIXED USE**

The Town Centre Transition–Mixed Use designation is characterized by a mix of residential, civic and commercial uses in a walkable, community-oriented setting and is intended to provide a gradual transition in built form and character between the higher-density Town Centre and adjacent residential areas, promoting a cohesive and livable urban environment.

**Objective 7.2**

This designation is intended to facilitate ground-level retail and active commercial uses with additional storeys located above the ground-level up to a maximum of 4 storeys above street-level may contain singularly or in combination the following uses: a mixture of office, passive commercial, civic uses and residential uses.

**Policy 7.2.1**

**Uses and Building Types**—Allow multi-unit residential uses, with mixed-use (commercial/residential) on George Street and adjacent to the hospital. Allow residential with or without ground-level retail and active commercial uses on the ground with additional storeys located above the ground-level up to a maximum of 4 storeys above street-level may contain singularly or in combination the following uses: a mixture of office, passive commercial, civic uses and residential uses. All storeys above the 4th storey can only contain residential use, as well as amenity uses associated with the residential use.

**Policy 7.2.2**

**Density and Height**—Allow a maximum height (in storeys) of 6 storeys and a maximum density of 2.5 gross FAR, with a height and density bonus up to 15 storeys and up to 4.0 gross FAR where a significant amenity contribution is provided. Any portion of a building within 15 metres of a road is not to exceed four storeys in height. Figures 8 and 9 will specify the maximum gross FAR and building height for individual parcels. Guidelines for transitional maximum allowable densities (gross FAR) are provided in Figure 8,

while transitions for maximum building height (in storeys) are illustrated in Figure 9.

**Policy 7.2.3**

**Affordable Housing Bonus**—Rental and non-market housing contributions will be encouraged in all projects over 6 storeys and will be expected for projects that include buildings that are 10 storeys or higher.

**Policy 7.2.4**

**Urban Design**—Enhance the built and public realms through policies identified in Section 8 and guidelines in the Town Centre Transition Development Permit Area in Part D.

**TOWN CENTRE TRANSITION**

The Town Centre Transition area is residential in character, with densities and heights that support the Town Centre and provide transitions to neighbouring low to mid-rise residential areas:

**Objective 7.3**

To enable a concentration of multi-unit residential uses to provide easy access to and to strengthen the commercial uses in the Town Centre and the transit corridor on North Bluff Road, and to provide a transition to surrounding low- to mid-rise residential areas.

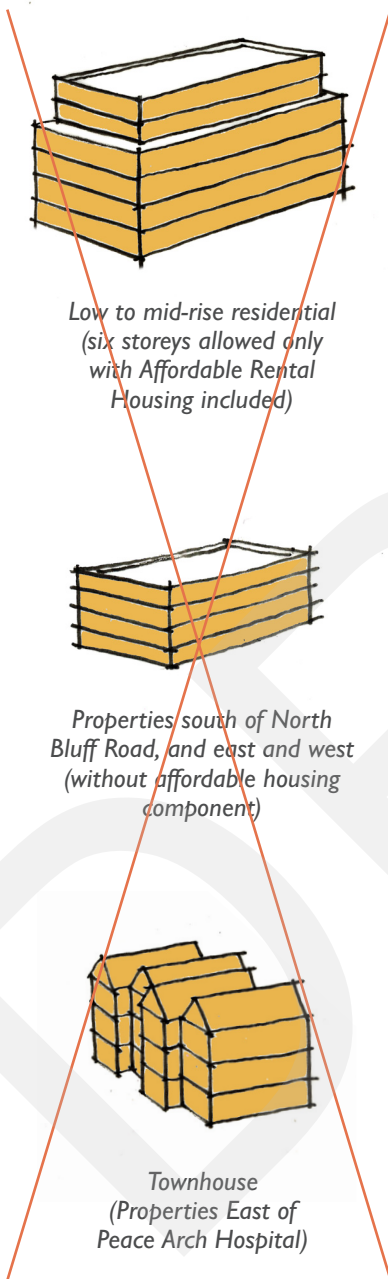
**Policy 7.3.1**

**Uses and Building Types**—Allow multi-unit residential uses, with mixed-use (commercial/residential) on George Street and adjacent to the hospital. Existing institutional and utility uses are also supported and may be mixed in new buildings with multi-unit residential uses. Building types range from low-rise to mid-rise:

**Policy 7.3.2**

**Density and Height**—Concentrate the highest heights and densities adjacent to the Town Centre along North Bluff Road. Maximum allowable densities (FAR) are outlined in Figure 9 and policy 8.2.3, and maximum heights (in storeys) are illustrated in Figure 10:

**Density and Height For East of Peace Arch Hospital**—Allow mixed-use buildings on Finlay Street with a maximum density of 2.5 FAR in buildings of up to six storeys in height. Allow ground-oriented townhouse on Maple Street with a maximum density of 1.5 FAR in buildings of up to three storeys. On North Bluff Road, allow townhouses and lowrise buildings up to 1.5 FAR, in buildings up to four storeys east of Lee Street, and up to three storeys west of Lee Street, with six storeys and 2.5 FAR west of Lee Street if Affordable Rental Housing is included as outlined in Policy 11.2.1.c. Density and height maximum for single family homes shall be as required in the City's Zoning Bylaw.

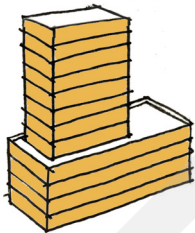
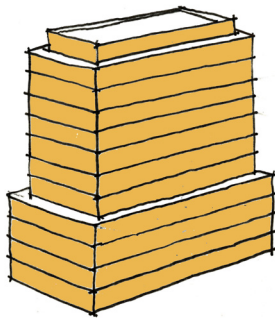


**Policy 7.3.3**

~~Affordable Housing Density/Height Bonus—Allow properties in the areas identified with the \* on Figure 9 and 10 to rezone up to six storeys and 2.5 FAR (2.8 FAR if providing replacement rental units) where the development includes the one of the affordable housing components identified in policy 11.2.4.~~

**Policy 7.3.4**

~~Urban Design—Enhance the built and public realms through guidelines included in the Multi-Family Development Permit Area in Part D. Focus on the establishment of a greenway between the Town Centre and Centennial Park, as per Sections 13 and 15.~~



Town Centre Transition–Residential



Townhouse  
(Properties East of Peace Arch Hospital)

**TOWN CENTRE TRANSITION–RESIDENTIAL**

Town Centre Transition–Residential designation is to enhance the existing character of the Town Centre to the east and west, featuring building heights and densities that both support the city centre and create a gradual transition to adjacent low- to mid-rise residential neighbourhoods.

**Objective 7.3**

To enable a concentration of multi-unit residential uses to provide easy access to and to strengthen the commercial uses in the Town Centre and Town Centre Transition–mixed use, and the transit corridor on North Bluff Road, and to provide a transition to surrounding low- to mid-rise residential areas.

**Policy 7.3.1**

**Uses and Building Types**—Allow multi-unit residential uses. Existing institutional and utility uses are also supported and may be mixed in new buildings with multi-unit residential uses. Building types range from low-rise to high-rise.

**Policy 7.3.2**

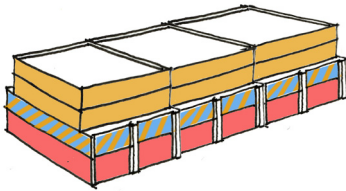
**Density and Height**—Allow a maximum height (in storeys) of 6 storeys and a density 2.5 gross FAR, with a height and density bonus up to 15 storeys and up to 4.0 gross FAR where a significant amenity contribution is provided. Any portion of a building within 15 metres of a road is not to exceed three storeys in height. For properties bounded by Russell Avenue, Thrift Avenue, George Street and Best Street, allow a maximum height (in storeys) of 4 storeys and a maximum density of 1.5 gross FAR, with a height and density bonus up to 6 storeys and up to 2.5 gross FAR where an affordable housing component is included. Figures 8 and 9 will specify the maximum gross FAR and building height for individual parcels. Guidelines for transitional maximum allowable densities (gross FAR) are provided in Figure 8, while transitions for maximum building height (in storeys) are illustrated in Figure 9.

**Policy 7.3.3**

**Affordable Housing Bonus**—Rental and non-market housing contributions will be encouraged in all projects over 6 storeys and will be expected for projects that include buildings that are 10 storeys or higher. For properties bounded by Russell Avenue, Thrift Avenue, George Street and Best Street, rental and non-market housing contributions are expected for projects that include buildings that are over 4 storeys.

**Policy 7.3.4**

**Urban Design**—Enhance the built and public realms through policies identified in Section 8 and guidelines in the Town Centre Transition Development Permit Area in Part D.

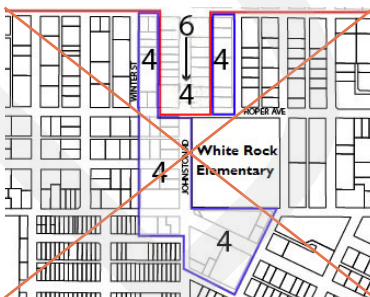


Primary form: mixed-use low rise buildings

~~From the Town Centre, transition from 6-4 Storeys along Johnston Road between Thrift and Roper~~



Gross FAR in Lower Town Centre



Maximum Heights in Lower Town Centre

**LOWER TOWN CENTRE**

The Lower Town Centre has a village-like character, and provides a physical and visual connection to the waterfront. While it is comprised of a diverse mix of uses, it is smaller in scale than the Town Centre.

**Objective 7.4**

To enable a mix of multi-unit residential and commercial uses, to strengthen the heart of the city and relationship to the waterfront while maintaining a village character. To reinforce the low-rise, pedestrian-scale of the area while providing for modest residential intensification to support local businesses and public transit.

**Policy 7.4.1**

**Uses and Building Types**—Allow multi-unit residential uses and mixed-uses (commercial/residential) in primarily mid-rise and low-rise buildings.

**Policy 7.4.2**

**Density and Height**—Concentrate the highest heights and densities adjacent along Johnston Road between Thrift and Roper Avenues. Maximum allowable densities (gross FAR) are outlined in Figure 8 and maximum heights (in storeys) are illustrated in Figure 9. Buildings adjacent to Roper Avenue should step down to four storeys on the Roper Avenue frontage.

**Policy 7.4.3**

**Retail Streets**—Strengthen existing retail streets by requiring street-fronting commercial uses on Johnston Road, and on Pacific Avenue from Johnston Road to Fir Street.

**Policy 7.4.4**

**Urban Design**—Enhance the built and public realms through guidelines included in the Lower Town Centre Development Permit Area in Part D.

**Policy 7.4.5**

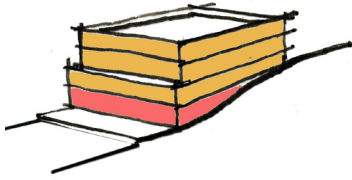
**Edges**—Explore opportunities to create a welcoming edge along Johnston Road adjacent to White Rock Elementary School.

**Policy 7.4.6**

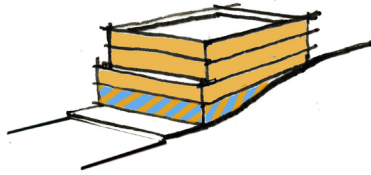
**Views**—Strengthen the village and seaside character of the Lower Town Centre by optimizing views to the water from the public realm, and as per the Lower Town Centre Development Permit Area guidelines in Part D.

## WATERFRONT VILLAGE

The Waterfront Village has a seaside village character, with small scale mixed use buildings, as well as small scale multi-unit residential buildings that can readily accommodate future commercial uses as demand grows.



Mixed-use



Flex-residential

**Objective 7.5** To enable a mix of multi-unit residential and commercial uses that establishes a seaside village character and supports local businesses and public transit.

**Policy 7.5.1** **Uses and Building Types**—Allow multi-unit residential uses and mixed-uses (commercial/residential) in low-rise buildings.

**Policy 7.5.2** **Density and Height**—Allow a density of up to 2.0 gross FAR, in buildings up to three storeys in height, in the West Beach area west of Foster Street; properties without frontage on Marine Drive are limited to a density of up to 1.5 gross FAR. East of Foster Street, allow a density up of up to 2.0 gross FAR, in buildings up to three storeys in height, or four storeys where the building's height does not exceed 3.5 metres above the highest ground elevation along the property line.

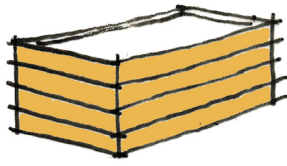
**Policy 7.5.3** **Retail Areas**—Strengthen existing retail areas by requiring street-fronting commercial uses on Marine Drive between Oxford Street and Foster Streets, and between Balsam Street and Maple Street. Allow street-fronting commercial uses elsewhere on Marine Drive.

**Policy 7.5.4** **Urban Design**—Enhance the built and public realms through policies identified in Section 9 and guidelines in the Waterfront Development Permit Area in Part D. For development proposals on Elm Street, consideration shall be given to the heritage context of existing buildings.

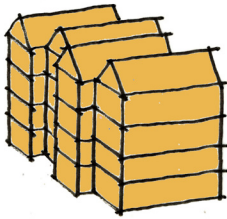
**Policy 7.5.5** **Flexible Housing**—Require the ground floor of new residential buildings fronting on Marine Drive outside of existing retail areas to be designed as flex spaces for potential future use as retail or office space.

**Policy 7.5.6** **Access**—Strengthen access to the waterfront through strategies identified in Sections 9 and 12.

## URBAN NEIGHBOURHOOD



Low-rise residential



Townhouse

~~The Urban Neighbourhood areas build on the existing character of apartment areas, protecting existing stock while providing opportunities for more low-scale multi-unit residences within walking distance of the Town Centre.~~

The Urban Neighbourhood areas build on the existing character of apartment areas, protecting existing stock while providing opportunities for more low-scale multi-unit residences within walking distance of the Town Centre area.

**Objective 7.6** To enable a mix of multi-unit residential uses that support existing affordable housing stock, strengthen nearby commercial uses, and provide further transition from the Town Centre, Lower Town Centre, and Town Centre Transition areas.

**Policy 7.6.1** ~~Uses and Building Types—Allow multi-unit residential uses in townhouses and low-rise buildings.~~

**Policy 7.6.1** **Uses and Building Types**—Allow multi-unit residential uses in townhouses and low-rise buildings.

**Policy 7.6.2** ~~Density and Height—Allow a density of up to 1.5 FAR in buildings up to four storeys in height. For properties located at 1081 Martin Street and 1153-1169 Vidal Street, the maximum building heights is three storeys.~~

**Policy 7.6.2** **Density and Height**—Allow a density of up to 1.5 gross FAR in buildings up to four storeys in height. For properties located at 1081 Martin Street and 1153-1169 Vidal Street, the maximum building heights is three storeys.

**Policy 7.6.3** ~~Urban Design—Enhance the public and built realms as outlined in the Multi-Family Development Permit Area guidelines in Part D.~~

**Policy 7.6.3** **Urban Design**—Enhance the public and built realms as outlined in the Multi-Family Development Permit Area guidelines in Part D.

**Policy 7.6.4** ~~Connectivity—Maintain a fine-grained block structure in the Urban Neighbourhood area, and establish direct and continuous walking connections, particularly leading to and from the Town Centre and Lower Town Centre areas.~~

**Policy 7.6.4** **Connectivity**—Maintain a fine-grained block structure in the Urban Neighbourhood area, and establish direct and continuous walking connections, particularly leading to and from the Town Centre and Lower Town Centre areas.

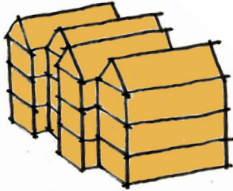
**Policy 7.6.5** ~~Design and Context—Encourage designs that respond to the form of adjacent development, particularly when abutting Mature Neighbourhood areas.~~

**Policy 7.6.5**

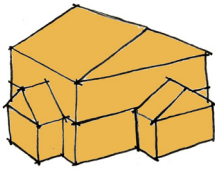
**Design and Context**—Encourage designs that respond to the form of adjacent development, particularly when abutting Mature Neighbourhood areas.

**Policy 7.6.6**

**Lot Consolidation**—Encourage a minimum consolidation of two parcels for new development within areas designated as Urban Neighbourhood. Development proposals must demonstrate that no adjacent parcel will be orphaned or rendered undevelopable as a result of the consolidation.



Ground-oriented townhouse



Single family home



Houseplex

**NORTH BLUFF EAST**

The North Bluff East area strengthens the transit corridor along North Bluff and provides gentle transitions to low-rise residential neighbourhoods to the south. This area is characterized by low-scale multi-unit and single-detached homes.

**Objective 7.7**

To enable multi-unit residential units that support the transit corridor on North Bluff Road.

**Policy 7.7.1**

**Uses and Building Types**—Allow multi-unit residential uses in townhouses, as well as houseplexes, and single family homes.

**Policy 7.7.2**

**Density and Height**—Allow a density of up to 1.0 gross FAR for townhouses, in buildings up to three storeys in height. Density and height maximums for houseplexes, and single family homes shall be as required in the City’s Zoning Bylaw.

**Policy 7.7.3**

**Urban Design**—Enhance the public and built realms as outlined in the Multi-Family Development Permit Area guidelines in Part D.

**MATURE NEIGHBOURHOOD**

~~Mature Neighbourhoods are comprised largely of White Rock’s existing single family housing stock. These areas will continue to be characterized by low-scale residential uses while accommodating gentle infill.~~

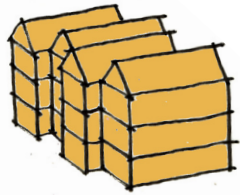
Mature Neighbourhoods are comprised largely of White Rock’s existing single-family housing stock. These areas will continue to be characterized by a mix of low-scale residential uses and small-scale multi-unit housing.

**Objective 7.8**

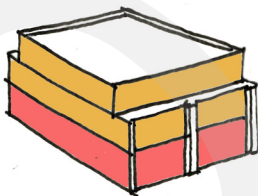
~~To enable single-detached and gentle infill opportunities, to protect the character of existing mature single family neighbourhoods while supporting housing choice and affordability.~~



Ground Oriented Townhouse



Rowhouse



Mixed-use

### Objective 7.8

To enable single-detached, semi-detached buildings, rowhouses and houseplexes through gentle infill opportunities, to protect the character of existing mature single-family neighbourhoods while supporting housing choice and affordability.

#### Policy 7.8.1

~~Uses and Building Types—Allow single family homes, and houseplexes. Allow secondary suites in single-detached homes.~~

#### Policy 7.8.1

**Uses and Building Types**—Allow single-family homes, semi-detached, rowhouses and houseplexes.

#### Policy 7.8.2

~~Density and Height—Allow density and height maximums for houseplexes, and single family homes as outlined in the City's Zoning Bylaw.~~

#### Policy 7.8.2

**Density and Height**—Allow density and height maximums for semi-detached buildings, rowhouses and houseplexes and single-family homes as outlined in the City's Zoning Bylaw.

#### Policy 7.8.3

~~Urban Design—Enhance the public and built realms, and maintain the existing residential character of established neighbourhoods with gentle infill (houseplexes), as per the Mature Neighborhood Houseplex Development Permit Area guidelines in Part D.~~

#### Policy 7.8.3

**Urban Design**—Enhance the public and built realms, and maintain the existing residential character of established neighbourhoods with gentle infill (houseplexes), as per the Mature Neighbourhood SSMUH Development Permit Area guidelines in Part D.

## NEIGHBOURHOOD COMMERCIAL

~~Neighbourhood Commercial areas are small-scale retail nodes within largely residential areas, providing amenities and locally-oriented commercial services in established neighbourhoods.~~

Neighbourhood Commercial areas are small-scale retail nodes within largely residential areas, providing amenities and locally-oriented commercial services in residential areas.

### Objective 7.9

~~To enable limited, small-scale, neighbourhood-serving commercial uses that provides services and amenities in largely residential commercial uses that provides services and amenities in largely residential areas.~~

## Objective 7.9

To enable limited, small-scale, neighbourhood-serving commercial uses that provides services and amenities in largely residential commercial uses that provide services and amenities in largely residential areas.

### Policy 7.9.1

~~Uses and Building Types—Allow commercial uses and mixed-uses (commercial/residential) in low-rise buildings.~~

### Policy 7.9.1

**Uses and Building Types**—Allow commercial uses and mixed-uses (commercial/residential) in low-rise buildings.

### Policy 7.9.2

~~Density and Height—Allow a density of up to 1.0 FAR in buildings up to three storeys in height.~~

### Policy 7.9.2

**Density and Height**—Allow a density of up to 1.0 gross FAR in buildings, and buildings between 2 to 4 storeys.

### Policy 7.9.3

~~Urban Design—Enhance the built and public realms as outlined in the Neighbourhood Commercial Development Permit Area guidelines in Part D.~~

### Policy 7.9.3

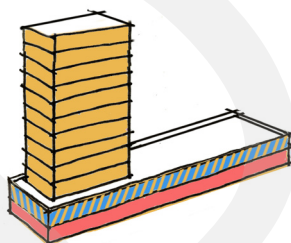
**Urban Design**—Enhance the built and public realms as outlined in the Neighbourhood Commercial Development Permit Area in the Neighbourhood Commercial Development Permit Area guidelines in Part D.

### Policy 7.9.4

**Lot Consolidation**—Encourage a minimum consolidation of two parcels for new development within Neighbourhood Commercial areas. Development proposals must demonstrate that no adjacent parcel will be orphaned or rendered undevelopable as a result of the consolidation.

### Policy 7.9.5

**Design and Context**—Encourage designs that respond to the form of adjacent development, particularly when abutting Mature and Urban Neighbourhood areas.



Health District

## HEALTH DISTRICT

The Health District is a specialized land use designation intended to support the delivery of health services and the development of a complete, integrated health precinct. It encourages a mix of medical, wellness, and supportive residential uses in proximity to major health institutions.

## Objective 7.10

To support the development of a health-focused district that integrates Hospital, medical services, wellness facilities, and housing for health workers and patients.

### Policy 7.10.1

**Uses and Building Types**—Permit a range of health-related uses including clinics, medical offices, outpatient facilities,

wellness centres, and mixed-use buildings that incorporate residential units above ground-floor health services.

**Policy 7.10.2**

**Incentives for Health Uses**—Support the inclusion of health-related uses through density bonusing and Development Cost Charges (DCC) waivers for projects that deliver medical services, non-profit health facilities, or affordable housing for health workers.

**Policy 7.10.3**

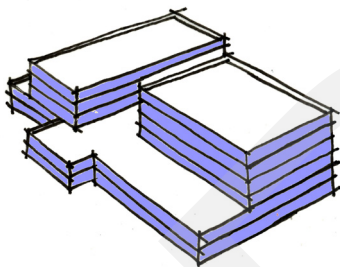
**Density and Height**—Allow a maximum height of 6 storeys and a maximum density of 2.5 gross FAR, with a height and density bonus as illustrated in Figures 8 and 9, for projects that deliver medical services, non-profit health facilities, or affordable housing for health workers. Any portion of a building within 15 metres of a road is not to exceed 4 storeys in height.

**Policy 7.10.4**

**Streamlined Approvals**—Prioritize development applications that include health related uses or housing for health sector employees through an expedited review process.

**Policy 7.10.5**

**Urban Design**—Encourage vertical integration of uses and high-quality public realm design that supports accessibility, healing environments, and active transportation, as outlined in the Health District Development Permit Area guidelines in Part D.



*Institutional*

**INSTITUTIONAL & UTILITY**

These uses are public or semi-public in character, providing needed services to the community.

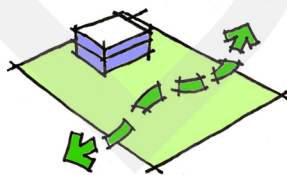
**Objective 7.11** To enable institutional and civic facilities such as schools, hospitals, major complex care facilities, civic buildings and facilities, and major utility buildings and facilities.

**Policy 7.11.1**

**Uses and Building Types**—Allow institutional and utility uses, and mixed-use (institutional, residential, and accessory commercial) in primarily mid-rise and low-rise buildings.

**Policy 7.11.2**

**Density and Height**—Determine the scale and height of buildings based on compatibility with surrounding development.



*Park with accessory building*

**OPEN SPACE & RECREATION AREA**

Open spaces and recreation areas are diverse in character, scale, and programmatic qualities, providing recreational amenities and habitat protection throughout the city.

**Objective 7.12** To enable both passive and active recreation, habitat protection, and natural area preservation.

**Policy 7.12.1**

**Uses**—Provide for recreation areas and protected areas.

## GENERAL

**Objective 7.13** To utilize land use efficiently and in a manner that provides social, economic, and ecological benefits across the city.

**Policy 7.13.1** **Community Amenity Contribution**—Require community amenity contributions in accordance with the City’s Density Bonus/Amenity Contribution Policy.

**Policy 7.13.2** **Home-Based Businesses**—Enable home-based businesses in neighbourhoods throughout the City.

**Policy 7.13.3** **Child Care**—Encourage childcare uses in areas with higher concentrations of housing, including in multi-family and mixed-use areas.

**Policy 7.13.4** **Vacant and Under-Utilized Lands**—Encourage the redevelopment of vacant lands and parking lots, particularly those on retail streets.

**Objective 7.14** To ensure development applications are reviewed and processed in a consistent manner.

**Policy 7.14.1** **Maximum Densities**—Apply maximum allowable gross FARs to all new development applications, but not to previously approved developments where the existing zoning allows density beyond the maximum gross FAR contemplated in this Plan, buildings under construction, or existing buildings. An amendment to this Plan is required to allow a gross FAR above the maximum contemplated in this Plan.

**Policy 7.14.2** **Blended Density**—Calculate allowable gross FAR for sites that straddle FAR boundaries in Figure 8 by taking a weighted average.

~~**Policy 7.14.3** **Land Dedications and Density**—Calculate allowable gross FAR prior to any land dedications that may be required with new developments.~~

**Policy 7.14.3** **Land Dedications and Density**—Calculate the allowable gross FAR following any land dedications that may be required with new developments.

**Policy 7.14.4** **Maximum Heights**—Apply maximum allowable heights to all new development applications, but not to previously approved developments where the existing zoning allows height beyond the maximum height contemplated in this Plan, buildings under construction, or existing buildings. An amendment to this Plan is required to allow height above the maximum contemplated in this Plan.

**Policy 7.14.5** **Lot Consolidation**—Encourage lot consolidations where smaller sites do not allow for functional development at the allowable gross FARs.

**Policy 7.14.6** **Permitted Uses**—Require the uses in new developments to be consistent with the uses specifically permitted in the applicable land use designation. An amendment to this Plan is required to change the land use designation that is applicable to a given site as outlined in Map A.

**Policy 7.14.7** **Existing and Previously Approved Developments**—To the extent that existing site-specific (Comprehensive Development) zoning on a property inhibits the ability of buildings to fully achieve Development Permit Area guidelines in Section 21, the guidelines shall not prevent the issuance or amendment of a Development Permit in that zone.

# 8. Town Centre

**Goal:** The City of White Rock promotes and develops the Town Centre as a distinctive, lively, and pedestrian-focused growth area.



## Overview

~~The Town Centre is currently the hub of commercial, economic, and civic life, and includes the greatest intensities of residential uses in White Rock. Buildings are diverse, ranging from single-story commercial uses to 20-storey mixed use buildings.~~

The Town Centre is currently the hub of commercial, economic, and civic life, and includes the greatest intensities of residential uses in White Rock. Buildings are diverse, ranging from single-story commercial uses to mixed-use buildings exceeding 20 storeys.

The Town Centre straddles Johnston Road, which is the “high street” of White Rock and provides the stage for much of the city’s public life. The continuous small-scale storefronts frame the public realm and create a vibrant village-like setting. Johnston Road extends northward into Surrey, providing a strong link between the two municipalities. To the south below Russell Avenue, Johnston street begins to slope toward the waterfront, bringing it into view for pedestrians. East-west streets are also diverse in character, with a range of buildings heights, setbacks, and uses.

Building on the growth management and land use policies for the Town Centre, the policies in this section further articulate a long-term vision for the Town Centre as the heart of the community. In addition to supporting the greatest concentration of homes, jobs, shops, and amenities, Town Centre policies and Development Permit Guidelines encourage the creation of delightful public places for socializing, dining, resting, people-watching, shopping, taking in the view, and more.

## Objectives & Policies

**Objective 8.1** To attract office and employment generating uses that reinforce the Town Centre as the primary employment hub.

**Policy 8.1.1** **Office and Employment Hub**—Enhance the Town Centre as the primary employment hub and business centre by encouraging the development of office, event, and hotel space, along with other employment generating uses.

**Objective 8.2** To enable the establishment of civic and community uses, as well as other important destinations, reinforcing the Town Centre as a centre for cultural, civic, and public life in the city.

**Policy 8.2.1** **Civic and Cultural Heart**—Establish the Town Centre as the cultural and civic heart of White Rock by creating public space at the corner of Johnston Road and Russell Avenue and in the block bounded by North Bluff Road, Russell Avenue, Johnston Road, and Foster Street. Cluster civic, social, cultural, and retail uses around these spaces.

~~**Policy 8.2.2** **City Hall**—Explore the possibility of relocating City Hall to the Town Centre, establishing a new civic centre with other potential civic facilities.~~

**Policy 8.2.3** **Grocery Store**—Support the ongoing presence of a full service grocery store in the Town Centre.

**Policy 8.2.4** **High Street Character**—Strengthen the high street character of the Town Centre and deliver predictable, high quality development in the Town Centre as per the Town Centre Development Permit Area guidelines in Part D. Undertake public realm improvements with new sidewalks, street trees, landscaping, street furniture, and improved pedestrian crossings, and coordinate with Surrey when appropriate.

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**Objective 8.3** To strengthen the permeability of the Town Centre, and the integration of open spaces and connections for walking and cycling.

**Policy 8.3.1** **Connectivity**—Improve pedestrian connectivity throughout the Town Centre by breaking up blocks with pedestrian pathways. Establish a greenway connection that extends Russell Avenue westwards to Martin Street and then on to Centennial Park.

**Policy 8.3.2** **Open Spaces**—Enhance the network of parks and public open spaces by:

- Providing a northern extension of Bryant Park across Russell Avenue, and creating a new neighbourhood park and playground at the centre of the residential precinct in the block bounded by North Bluff Road, Russell Avenue, Johnston Road, and Foster Street;
- Establishing a civic plaza at Johnston Road and Russell Avenue;
- Establishing a green buffer on North Bluff Road, which could include a setback between Foster and George Streets with a double row of street trees with enhanced pedestrian facilities and physically separated cycling facilities; and
- Create a gateway plaza or other open space at North Bluff Road and Johnston Road.

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**Objective 8.4** To efficiently and strategically utilize land and development to enhance the character and quality of the Town Centre.

**Policy 8.4.1** **Parking**—Consolidate surface parking areas into new developments and restrict future surface parking.

**Policy 8.4.2** **Town Centre Community Amenity Contribution**—Through redevelopment, contribute to the creation of appropriate public amenities that are consistent with the City’s Community Amenity Contribution Policy, this Section, and the Town Centre Development Permit Area guidelines in Part D.

**Policy 8.4.3** **Density Transfer**—Allow the transfer of density from small lots to adjacent lots or lots located across City streets and laneways.

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**Objective 8.5** To advance sustainable urbanism in the Town Centre.

**Policy 8.5.1** **Sustainability**—Conserve water and energy, and reduce greenhouse gas emissions as per the Town Centre Development Permit Area guidelines in Part D.



# 9. Waterfront

**Goal:** The City of White Rock enhances and promotes its beautiful Waterfront and historic pier as a memory-making destination where people can shop, dine, gather, play, and connect with nature.



## Overview

The Waterfront is a cherished community amenity and defining feature of White Rock, establishing it as a seaside community and regional destination and connecting residents and visitors to the natural setting. It was the waterfront and its views that attracted the first cottage developments in the city, and continues to shape the community's identity.

The City's steep topography provides clear waterfront views from many areas of White Rock, but also creates a physical barrier for many pedestrians and those with mobility challenges. The promenade and sidewalks along Marine Drive—along with restaurants and other shops—provide key waterfront connections and exceptional pedestrian experiences.

Building on the growth management and land use policies for the Waterfront Village area, the policies in this section further articulate directions for the Waterfront, seeking to strengthen existing character and connection to and from the Waterfront.

## Objectives & Policies

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### **Objective 9.1** To improve comfortable and convenient access to the Waterfront.

**Policy 9.1.1** **Getting to the Waterfront**—Promote accessible movement of people to the Waterfront by:

- a) Connecting Johnston Road from the Five Corners area to the waterfront, using community amenity contributions to fund a people movement system as outlined in Section 12;
- b) Improving access to the beach for persons with mobility impairments;
- c) Enhancing north-south pathways and staircases, or 'Hill Walks,' and providing amenities such as rest areas to improve the accessibility and desirability of these connecting spaces;
- d) Creating visual cues and other wayfinding mechanisms to direct pedestrians toward connecting spaces; and
- e) Prioritizing public realm enhancements to the 'high and medium priority' sidewalks that are identified in the White Rock Integrated Transportation and Infrastructure Master Plan (ITIMP).

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### **Objective 9.2** To improve the comfort, safety, and delight of pedestrian spaces at the Waterfront.

**Policy 9.2.1** **Pedestrian Spaces**—Strengthen and expand pedestrian spaces on Marine Drive and the waterfront by:

- a) Protecting the pedestrian-scale and unique character of Marine Drive, and enhancing the pedestrian realm as per the Waterfront Development Permit Area guidelines in Part D; and
- b) Providing pavement markings and painted crosswalks that direct pedestrians through parking lots.

**Policy 9.2.2** **Parking**—Identify strategies to reduce surface parking, including through the development of parking structures, in order to provide seasonal programming and more continuous pedestrian access at the waterfront without compromising business vitality.

**Policy 9.2.3** **Rail Line**—Continue to explore opportunities to relocate the Burlington Northern Santa Fe rail line away from the waterfront, in order to address safety issues and concerns about the increase in rail traffic.

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**Objective 9.3** To ensure efficient access, circulation, and parking at the Waterfront.

**Policy 9.3.1** **Access and Circulation**—Improve vehicle access and circulation along the Waterfront, without compromising pedestrian movement.

**Policy 9.3.2** **Parking Management**—Manage the parking supply and demand in the Waterfront area, with signage improvements and the provision of a new parking facility at Victoria Avenue and Vidal Street. Consider converting surface parking spaces into public spaces where parking can be provided elsewhere.

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**Objective 9.4** To preserve and maintain the natural scenic beauty and oceanside village character of the Waterfront, strengthening it as a year-round destination and amenity.

**Policy 9.4.1** **Form and Character**—Continue to preserve the unique and historic form and character of the built environment on Marine Drive through the Waterfront Development Permit Area guidelines in Part D.

**Policy 9.4.2** **Pier and Marine Activities**—Promote the pier and promenade, and complimentary marine activities, which form an integral part of White Rock's Waterfront.

**Policy 9.4.3** **Public Realm**—Develop public realm design guidelines to enhance the image of Marine Drive, including directions for street lighting and furniture, landscaping, sidewalks, right-of-ways, signage, open spaces, and transit amenities.

**Policy 9.4.4** **Tourism**—Allow for temporary commercial activity south of Marine Drive as part of City-sanctioned special events and on-going leisure services programming.



# 10. Housing

**Goal:** The City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives.



## Overview

White Rock currently has a mix of residential uses that characterize different areas of the city. While single family homes in the Mature Neighbourhood area account for the large majority of land area in White Rock, the greatest proportion of housing starts in the last decade have been apartments.

White Rock experiences housing affordability challenges along with the rest of the Lower Mainland, in which housing costs are well beyond a price-to-income ratio that justifies the price of housing. Regionally, 33.5% of households spent greater than 30% of their income on housing in 2011. In White Rock, this value was slightly lower at 31.4%.

Supporting new Secure Market Rental and Affordable Rental Housing will help improve housing affordability in the City. Secure Market Rental units are designated for rental purposes only and protected with a covenant, lease agreement, or housing agreement registered against title for the life of the building.

New Affordable Rental Housing is defined as being intended to be affordable for very low and low income households by making the maximum rent at least 20% below the average rent in the White Rock area, as determined by the most recent rental market report from Canada Mortgage and Housing Corporation. For reference purposes, the following table provides the income thresholds for different income groups, using 2016 Census gross household income data for White Rock. By capping the rent for Affordable Rental Housing at no higher than 20% below the average rent in White Rock, these rents would generally not exceed 30% of the gross household income at the top end of the low income range, or near the low end of the moderate income range. Further reductions below average rent that would make the Affordable Rental Housing units even more affordable would likely require grants or operating subsidies from senior levels of government.

Table 1 Household Income Group, Percent of Median Income, and Income Range

Household Income Group	Percent of Median Income	Income Range (2016 Census)
Very Low Income	Less than 50%	Less than \$31,173
Low Income	50% to 80%	\$31,173 to \$49,875
Moderate Income	80% to 120%	\$49,876 to \$74,813
Above Moderate Income	120% to 150%	\$74,814 to \$93,516
High Income	More than 120%	\$93,517 or greater

The policies in this section support elements of existing housing patterns, while expanding housing choice for a broader range of individuals, households, and income levels. Policies in this section also treat housing as a strategic asset that can be used to leverage other outcomes. For example, concentrating housing near transit and in mixed use areas supports transportation choice, reduces congestion, supports local businesses, promotes healthy lifestyles, and addresses many of the other goals of this OCP.

## Overview

White Rock currently has a mix of residential uses that characterize different areas of the city. While single family homes in the Mature Neighbourhood area account for the large majority of land area in White Rock, the greatest proportion of housing starts in the last decade have been apartments.

White Rock experiences housing affordability challenges along with the rest of the Lower Mainland, which housing costs are well beyond a price-to-income ratio that justifies the price of housing. Regionally, 29.8% of households spent greater than 30% of their income on housing in 2021. In White Rock, this value was slightly higher at 32.4%.

Supporting new Secure Market Rental and various types of affordable housing, including Affordable Rental Housing, will help improve housing affordability in the City. Secure Market Rental units are designated for rental purposes only and protected with a covenant, lease agreement, or housing agreement registered against title for the life of the building.

New Affordable Rental Housing is defined as being intended to be affordable for very low and low-income households by making the maximum rent at least 20% below the average rent in the White Rock area, as determined by the most recent Rental Market Report from Canada Mortgage and Housing Corporation.

For reference purposes, the following table provides the income thresholds for different income groups, using 2021 Census gross household income data for White Rock. By capping the rent for Affordable Rental Housing at no higher than 20% below the average rent in White Rock, these rents would generally not exceed 30% of the gross household income at the top end of the low-income range, or near the low end of the moderate-income range.

Further reductions below average rent that would make the Affordable Rental Housing units even more affordable would likely require grants or operating subsidies from senior levels of government.

**Table 1.** Household Income Group, Percent of Median Income, and Income Range

Household Income Group	Percent of Median Income	Income Range (2021 Census)
Very Low Income	Less than 50%	Less than \$36,500
Low Income	50% to 80%	\$36,500 to \$58,400
Moderate Income	80% to 120%	\$58,400 to \$87,600
Above Moderate Income	120% to 150%	\$87,600 to \$109,500
High Income	More than 120%	More than \$109,500

The policies in this section support elements of existing housing patterns while expanding housing choice for a broader range of individuals, households, and income levels. Policies in this section also treat housing as a strategic asset that can be used to leverage other outcomes. For example, concentrating housing near transit and in mixed-use areas supports transportation choice, reduces congestion, supports local businesses, promotes healthy lifestyles, and addresses many of the other goals of this OCP.

## Housing Needs Report

In April 2019, the Ministry of Municipal Affairs and Housing introduced amendments to the Local Government Act requiring that all municipalities complete Housing Needs Reports (HNR) for their communities by April 2022, and that such reports be updated every five (5) years thereafter. The purpose of the HNR was to:

1. Enable the province to gain an understanding of changes in demographics and housing to plan for future housing needs;
2. Enable municipalities to better understand their current and future housing needs; and,
3. Assist local governments in implementing policies and bylaws that respond to current and projected housing needs.

The Housing Needs Report (HNR) recognizes six (6) key areas of local housing need and includes a summary of demographic and housing-related data, as well as the results of a series of public engagement activities held between January and July 2021. The six areas of local housing need recognized in the HNR include Affordable Ownership Housing, Rental Housing, Special Needs Housing, Seniors Housing, Family Housing and Shelters and Housing for People at Risk of Homelessness.

Beginning in 2023, municipalities are required to use the HNR Method to complete an HNR every five years, which will identify the housing needs of the community over the 5- and 20-year horizon.

Under Bill 44 *Housing Statutes (Residential Development) Amendment Act, 2023*, all local governments were required to complete an Interim Housing Needs Report (HNR) by January 1, 2025. The Interim Housing Needs Report only included three new, additional items:

- The number of housing units required to meet current and anticipated needs for the next 5 and 20 years, as calculated using the HNR Method provided in the Regulation;
- A statement about the need for housing in close proximity to transportation infrastructure that supports walking, bicycling, public transit or other alternative forms of transportation; and,
- A description of the actions taken by the local government, since receiving the most recent Housing Needs Report, to reduce housing needs.

**White Rock’s Interim Housing Needs Report identified the City’s 5-year need as 2,780 new dwelling units and its 20-year need as 8,816 new dwelling units.** The table below shows the results of the HNR calculations and the area of need with the required number of new units to address each.

Table 2. Interim HNR 5-year and 20-year Needs by Component

Component	5-year Need	20-year Need
Extreme Core Housing Need	206.19	824.78
Persons Experiencing Homelessness	45.28	90.56
Suppressed Household Formation	195.52	782.07
Anticipated Household Growth	1,979.81	5,706.89
Rental Vacancy Rate Adjustment	22.57	90.26
Additional Demand	330.31	1,321.24
<b>Total New Units—5 years</b>		<b>2,780</b>
<b>Total New Units—20 years</b>		<b>8,816</b>

The Interim Housing Needs Assessment was calculated using the HNR Calculator which is an online tool developed by UBC’s Housing Assessment Resource Tools (HART) that is called the HNR Calculator.

Section 7 Land Use provides the land use policies that will facilitate the required new units. Section 10 Housing details the objectives and policies that the City will use to achieve the number of new units for each area of need as identified in the 2024 Interim House Needs Report.

## Objectives & Policies

**Objective 10.1** To expand housing choices for existing and future residents, and increase diversity of housing types for a variety of household sizes, incomes, tenures, needs, and preferences.

**Policy 10.1.1**

~~Family-Friendly Housing—Increase the attractiveness and affordability of housing in White Rock for families by:~~

- ~~a) Encouraging applicants to provide ground floor units with front door access to the street in all rezoning applications for residential developments greater than three storeys in height;~~
- ~~b) Providing a minimum of 10% of units with three bedrooms and a minimum of 35% with either two or three bedrooms in all rezoning applications for residential developments with more than 20 dwelling units; and~~
- ~~c) Establishing outdoor amenity space requirements for multi-unit residential developments.~~

**Policy 10.1.1**

**Family-Friendly Housing**—Increase the attractiveness and affordability of housing in White Rock for families by:

- a) Encouraging applicants to provide ground floor units with front door access to the street in all rezoning applications for residential developments greater than three storeys in height;
- b) Encourage townhouse podiums in residential buildings to create more family-friendly options at street level;
- c) Providing a mix of two- and three-bedroom units in all rezoning applications for residential and mixed-use developments with more than 20 dwelling units, as follows:
  - i) Residential strata developments: a minimum of 15% of units as three bedrooms and a minimum of 35% of units as two bedrooms; and/or
  - ii) Rental developments: a minimum of 10% of units with three bedrooms, and a minimum of 30% with two bedrooms;
- d) Establishing outdoor amenity space requirements for multi-unit residential developments.

**Policy 10.1.2**

~~Age-Friendly Housing for People with Disabilities—Incorporate age-friendly measures that respond to the needs of older individuals and people with disabilities by:~~

- ~~a) Developing design criteria for accessible units and establishing a minimum number of units required to be accessible in new developments; and~~
- ~~b) Reducing parking requirements for dwelling units that are secured by a housing agreement for occupancy by persons with disabilities.~~

**Policy 10.1.2**

~~Housing Choices Everywhere—Focus residential densities in the Town Centre, but ensure that housing choices are distributed throughout the city in all neighbourhoods. Allow houseplexes throughout the Mature Neighbourhoods.~~

**Policy 10.1.2**

**Housing Choices Everywhere**—Diversifying housing choices helps meet the evolving needs of residents, supports affordability goals, meets anticipated household growth, and ensures people can live, work, and stay in their communities by:

- a) Focusing residential densities in the Town Centre and Town Centre Transition areas but ensuring that housing choices are distributed throughout the city in all neighbourhoods;
- b) Encourage a range of housing types within each of White Rock’s neighbourhoods, including small-scale multi-unit, missing middle, seniors-oriented, rental, affordable, family-sized, and housing with supports;
- c) Exploring density bonusing, where permitted, to encourage market rental, affordable rental housing and non-market housing for development particularly, but not limited to, over 6 storeys.
- d) Monitoring the uptake of small-scale multi-unit housing (SSMUH) typologies to ensure they continue to align with the City’s need for Missing Middle Housing;

- e) Encourage new multi-unit and mixed-use developments to include usable open space and play areas on-site, especially in locations without nearby parks or recreation facilities;
- f) Support innovative or non-traditional forms and models of housing, such as modular housing, adaptive re-use, co-ops or cohousing.

**Policy 10.1.3**

~~Secondary Suites—Explore opportunities to allow additional secondary suites to increase rental housing stock and improve housing affordability for homeowners, where relevant requirements such as parking and BC Building Code issues are addressed, including:~~

- ~~a) Considering secondary suites in houseplexes;~~
- ~~b) Considering two secondary suites in single-detached houses; and~~
- ~~c) Reviewing and updating parking requirements for secondary suites.~~

**Objective 10.2 To support rental housing and a range of non-market housing options and needs along the housing spectrum.**

**Policy 10.2.1**

~~New Non-Market and Rental Housing—Support new affordable and rental housing, especially in transit-accessible locations, by:~~

- ~~a) Using the Affordable Housing Reserve Fund to support the creation of new Affordable Rental Housing units in partnership with other agencies and organizations;~~
- ~~b) Waiving community amenity contributions for affordable rental housing developments and considering reductions for developments that include secure market rental units;~~
- ~~c) Allowing rezonings for projects with at least 20% of the units secured as affordable rental housing with a density of up to 2.5 FAR and a maximum of six storeys in the areas identified in Figure 11. Despite the present definition of affordable rental housing in the overview, zoning applications which have received third reading under a prior determination of affordable rental housing may be approved;~~
- ~~d) Considering making City-owned land available to non-profit organizations for use in affordable housing developments;~~
- ~~e) Supporting the addition of ancillary rental housing on institutional sites, such as places of worship, where additional development can be accommodated;~~
- ~~f) Requiring a minimum one-to-one replacement of the existing rental dwelling units when an existing rental building is proposed for redevelopment, with the average unit size of the replacement units at least 80% that of the units being replaced; and/or~~
- ~~g) Reviewing parking requirements to determine the extent to which they can be relaxed for non-market and rental housing within walking distance (i.e. 400-800 metres of real travel distance) of frequent transit service and/or commercial areas.~~

**Policy 10.2.1**

**New Non-Market and Affordable Rental Housing**—Support new non-market and affordable rental housing, by:

- a) Using the Affordable Housing Reserve Fund to support the creation of new Affordable Rental Housing units in partnership with other agencies and organizations;
- b) Developing a target non-market and/or affordable rental housing contribution policy for new developments in the Town Centre and Town Centre Transition areas;
- c) Requiring developments that result in the loss of existing rental units to provide tenant compensation in accordance with the City’s Tenant Relocation Policy, or Tenant Protection

Bylaw once adopted, in addition to ensuring a minimum of 5% of the new units are delivered as Affordable Rental Housing;

- d) Maintaining and periodically reviewing the City's Tenant Relocation policy, or Tenant Protection Bylaw once adopted;
- e) Waiving community amenity contributions for affordable rental residential floor space in new developments;
- f) Encouraging the creation of seniors-focused housing opportunities in transit-accessible areas that are adaptable and affordable;
- g) Considering making City-owned land available to non-profit organizations for use in affordable housing developments;
- h) Facilitating and supporting development of new non-market and affordable rental housing, housing with supports, and housing for groups with specific needs on lands owned by community groups, non-profit associations, and faith-based organizations;
- i) Identifying opportunities to work with senior levels of government and other partners to assist in increasing the supply of permanent, affordable, and supportive housing units for persons at risk of and/or experiencing homelessness;
- j) Requiring a minimum one-to-one replacement of the existing rental dwelling units when an existing rental building is proposed for redevelopment, with the average unit size of the replacement units at least 80% that of the units being replaced; and/or
- k) Reviewing parking requirements to determine the extent to which they can be relaxed for non-market housing within walking distance (i.e. 400-800 metres of real travel distance) of frequent transit service and/or commercial areas.

### **Policy 10.2.2**

**New Market Rental Housing**—Support the creation of new market rental housing and protect existing rental stock by:

- a) Encouraging the retention, renewal, expansion and creation of purpose-built rental housing;
- b) Requiring new purpose-built rental housing to be secured for 60 years or the life of the building, whichever is greater;
- c) Developing a Rental Protection mechanism to preserve existing rental housing, reduce redevelopment impacts on rental supply, and support the creation of new rental and affordable housing to meet community needs;
- d) Maintaining and periodically reviewing the City's Tenant Relocation policy, or Tenant Protection Bylaw once adopted;
- e) Considering community amenity contribution reductions for developments that include secure market rental units;
- f) Supporting the addition of ancillary rental housing on institutional sites, such as places of worship, where additional development can be accommodated;
- g) Requiring a minimum one-to-one replacement of the existing rental dwelling units when an existing rental building is proposed for redevelopment, with the average unit size of the replacement units at least 80% that of the units being replaced; and/or
- h) Reviewing parking requirements to determine the extent to which they can be relaxed for new rental housing within walking distance (i.e. 400-800 metres of real travel distance) of frequent transit service and/or commercial areas.

**Policy 10.2.3**

**Existing and Previously Approved Developments**—Despite the present definition of affordable rental housing in Section 10 Housing, zoning applications which have received third reading under a prior determination of affordable rental housing may be considered for approval. Additionally, Projects with applications submitted prior to 2021 may be approved with at least 5% of the affordable rental units secured at average rents for a period of 10 years, instead of 20% below average for the life of the building.

**Policy 10.2.4**

**Existing Rental Housing**—Retaining low-rise rental building stock in the Urban Neighbourhood areas south of Thrift Avenue to preserve affordable and rental housing. Allow expansions to rental buildings with an additional 0.3 FAR and an additional 20 units per acre beyond the maximums allowed under current zoning.

**Policy 10.2.5**

**Tenant Relocation**—Follow the tenant relocation policy to support those affected by redevelopment of existing rental housing. Plans for relocating existing residents will be the responsibility of the developer.

**Policy 10.2.6**

~~Affordable Housing Bonus—Allow rezonings in the Town Centre Transition area to permit buildings up to six storeys and 2.5 FAR where the development provides:~~

- ~~a) For strata housing development, at least 20% of the units as Affordable Rental Housing;~~
- ~~b) For secured market rental development without a requirement to replace existing rental units, at least 10% of the units as Affordable Rental Housing; and~~
- ~~c) For a development replacing existing rental units and providing compensation to tenants in accordance with the City’s Tenant Relocation Policy, at least 5% of the units as Affordable Rental Housing. Projects with applications submitted prior to 2021 may be approved with at least 5% of the units secured at average rents for a period of 10 years, instead of 20% below average for the life of the building. Developments in this category may have a density up to 2.8 FAR.~~

~~For projects owned or operated by regional or provincial housing agencies, or non-profit housing co-operatives, Council may approve rezonings up to six storeys and 2.5 FAR according to the merits and affordability in the proposal.~~

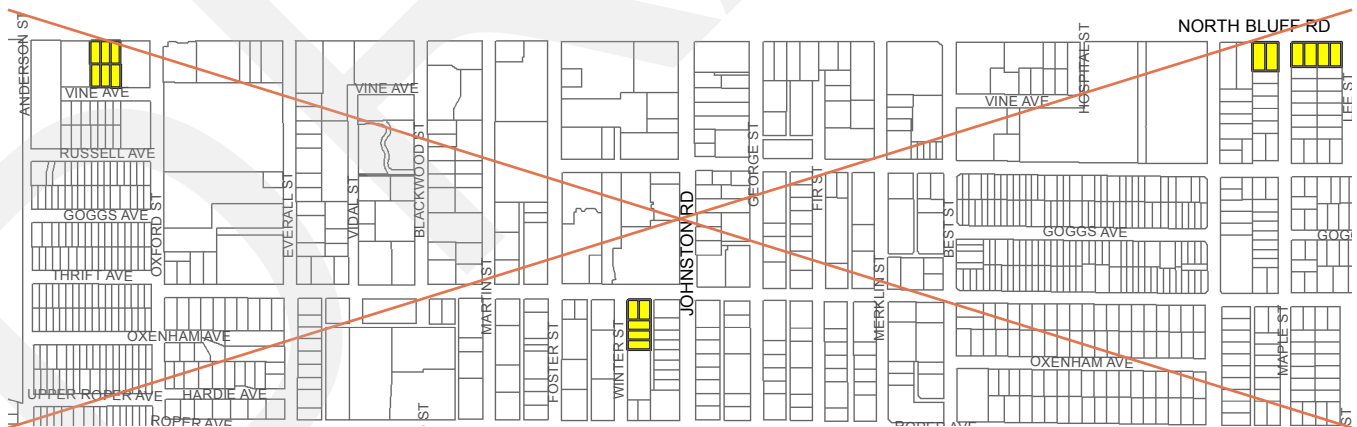


Figure — Potential Affordable Rental Housing Sites

# 11. Environmental Management

**Goal:** The City of White Rock preserves and protects natural areas, features, and habitat, and encourages the responsible use of resources through sustainable development.



## Overview

White Rock is part of the Georgia Basin and Puget Sound ecosystem, with residents of this region sharing an airshed, a flyway for migratory birds, and habitat for anadromous fish. Further, Semiahmoo Bay has been designated as the Boundary Bay Wildlife Management Area (WMA) by the Ministry of Forests, Lands, and Natural Resource Operations, in order to conserve and manage wildlife habitat. There are also important considerations for tsunami preparedness and climate change adaptation.

White Rock has sensitive environmental features, including the foreshore, bluffs, and ravines. These ecological areas provide services and intrinsic value to humans, as well as habitat to other species. A mature tree canopy forms part of White Rock's ecological network and natural beauty.

The policies in this section seek to protect the ecosystems of which all people are a part and depend upon, and which shape much of White Rock's identity and natural beauty. The policies in this section recognize that human and environmental health are inextricably linked, providing direction for water, air, and soil quality. This section also provides direction on conservation of water and energy, and the reduction of greenhouse gas emissions, in tandem with Development Permit Area guidelines in Part D.

## Objectives & Policies

### **Objective 11.1** To protect the ecological integrity of watercourses and Semiahmoo Bay.

- Policy 11.1.1** **Aquatic Environment Protection**—Protect and enhance the aquatic environment of Semiahmoo Bay.
- Policy 11.1.2** **Riparian Buffers**—Protect and enhance riparian areas and buffers adjacent to watercourses, as per the Development Permit Area guidelines in Part D.
- Policy 11.1.3** **Watershed Management**—Approve only uses, programming, and infrastructure adjacent to the waterfront that are compatible with the Boundary Bay WMA's management objective of providing priority to fish, wildlife, and foreshore habitats.
- Policy 11.1.4** **Shoreline**—Continue supporting shoreline restoration measures, and the protection of the foreshore from erosion.
- Policy 11.1.5** **Rainwater Run-Off**—Manage the quality and quantity of rainwater runoff by:
- Requiring that stormwater be addressed on-site, including through integrated stormwater management practices and other green infrastructure measures;
  - Protecting and enhancing the quality of water entering Semiahmoo Bay by reducing the amount of rainwater flowing over impervious surfaces and collecting pollutants, in accordance with the City's Integrated Stormwater Management Plan and the Development Permit Guidelines contained in Part D; and
  - Applying Fisheries and Oceans Canada and the Ministry of Environment's Land Development Guidelines for the Protection of Aquatic Habitat or other relevant guidelines to new developments where sedimentation, runoff, and erosion are potential concerns, and review streamside development applications in accordance with the Riparian Areas Development Permit Area in Part D.
- Policy 11.1.6** **Marine Food Sources**—Explore opportunities to protect marine food sources, including fish and shellfish stocks.
- Policy 11.1.7** **Partnerships**—Partner with other governments and agencies to ensure environmentally responsible stewardship of Semiahmoo Bay, and to raise awareness among residents, business owners, and visitors regarding the environmental sensitivity of the Bay. Support cross-border initiatives to promote the environmental well-being of the Georgia Basin area.

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**Objective 11.2** To protect and expand habitat and natural areas in the city.

**Policy 11.2.1** **Natural Landforms**—Protect ecologically sensitive areas and retain natural landforms in White Rock, including bluffs and ravines.

**Policy 11.2.2** **Tree Preservation**—Preserve and protect mature, healthy trees throughout the City, by adopting and adhering to an Urban Forest Management Plan and requiring development projects to be designed with the intent of preserving and protecting mature, healthy trees.

**Policy 11.2.3** **Native Plant Species**—Use native plant materials and nativescape principles for landscaping initiatives on City-owned lands with a view toward supporting biodiversity.

**Policy 11.2.4** **Ecological Greenways**—Enhance and restore ecological links between existing natural areas, increasing connections between isolated habitats.

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**Objective 11.3** To protect environmental and human health.

**Policy 11.3.1** **Air Quality**—Participate in initiatives to support Metro Vancouver’s Regional Air Quality Management Plan, and follow Regional Growth Strategy policies related to reducing transportation-related pollution by helping shift to more active travel modes and transit use.

**Policy 11.3.2** **Public Stewardship**—Support and encourage community stewardship programs that preserve and enhance natural areas. The City will also support and participate in environmental education initiatives.

**Policy 11.3.3** **Coordination with Other Governments**—Cooperate with senior government agencies, including the Department of Fisheries and Oceans and the Ministry of the Environment, as well as Metro Vancouver, nearby municipalities, and Semiahmoo First Nation, to protect the environment. Consider the Regional Growth Strategy Conservation and Recreation areas in decision-making.

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**Objective 11.4** To mitigate risk of natural disasters and climate change.

**Policy 11.4.1** **Risk Assessment**—Conduct an assessment of municipal infrastructure to determine the level of risk and impact from rising sea levels, more frequent and larger storms, or other natural disasters, and ensure future development implements adaptation measures.

**Policy 11.4.2** **Flood Damage Protection**—Require that new buildings within the floodplain areas shown in Map C are adequately flood-proofed to a standard established by the City and Provincial Government.

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**Objective 11.5** To conserve energy and reduce greenhouse gas emissions.

**Policy 11.5.1** **Greenhouse Gas Emissions Reduction Targets**—Undertake a study to establish updated greenhouse gas emissions targets, including city-wide targets as well as targets for buildings, transportation, and solid waste. Until such targets are updated, reduce greenhouse gas emissions in accordance with the recommendations of the City of White Rock Community Climate Action Plan:

- a) 10% below 2007 levels by 2020; and
- b) 50% below 2007 levels by 2050.

- Policy 11.5.2**      **Development Permit Areas**—Implement DPA guidelines in Part D relating to energy conservation and greenhouse gas emissions reductions through landscaping, siting of buildings and other structures, form and exterior design of buildings and other structures, specific features in development, and machinery, equipment, and systems external to buildings and other structures.
- Policy 11.5.3**      **Green Building Strategy**—Develop a strategy to enhance the environmental and human health performance of buildings.
- ~~**Policy 11.5.4**      **Energy Step Code**—Adopt the Province’s new Energy Step Code as means to move toward net-zero energy ready buildings.~~
- Policy 11.5.5**      **On-Site Energy Generation**—Support the use of on-site renewable energy generation systems to supply electricity, heating, and cooling to buildings and other structures.
- Policy 11.5.6**      **District Energy**—Explore opportunities to implement a district energy system to distribute thermal energy to consumers in the Town Centre and other areas with supportive residential densities, potentially in collaboration with Surrey where appropriate. Consider supplying the district energy system with renewable energy resources.
- ~~**Policy 11.5.7**      **Electric Vehicles**—Require one electric vehicle charging station for every 10 parking spaces in new multi-unit residential and mixed use buildings. Provide rough-ins for an additional one electric vehicle charging station for every 10 parking spaces, for future use as electric vehicle use increases.~~



## 12. Transportation & Mobility

**Goal:** The City of White Rock facilitates the movement of people by providing an inclusive, interconnected transportation network.



## Overview

Population densities and land use mixes in White Rock are generally supportive of transit and active modes of transportation. Fine-grained street grids and mixes of uses create short travel distances between destinations, making White Rock highly connected and providing a strong foundation for walkability. Barriers to walkability in White Rock include steep topography (which is also an issue for cycling), discontinuous sidewalks, and lack of pedestrian amenities in some areas.

Transportation policies in this OCP are consistent with the City's Strategic Transportation Plan, with a mode hierarchy in which transportation investments, new space allocation, and public realm improvements are made based on the following order of priorities: walking (including accessibility devices), cycling, transit, goods movement and commercial vehicles, and personal vehicles. This Section addresses transportation systems and highlights that streets are for much more than movement. Streets are for resting, socializing, shopping, playing, and generally participating in public life.

White Rock's compact urban form, fine-grained street grid, and diverse land use patterns create a strong foundation for sustainable transportation. These characteristics support short travel distances and high connectivity, making walking, cycling, and transit viable and attractive options for many residents. However, steep topography, fragmented sidewalk networks, and limited pedestrian amenities in some areas hinder walkability and accessibility. Addressing these barriers is essential to achieving a transportation system that is inclusive and universally accessible.

Transportation policies outlined in this OCP are guided by the City's Integrated Transportation & Infrastructure Master Plan (ITIMP), which envisions a safe, efficient, and environmentally responsible multi-modal network prioritizing transportation investments and public realm improvements necessary to enhance walking, cycling and traffic safety while complementing our unique seaside and hillside community. These facilities aim to connect the Waterfront, the Town Centre, and residential neighbourhoods in a way that is comfortable for people of all ages and abilities, promoting and encouraging an active life to inspire a dynamic, sustainable community for current and future generations.

This section reflects White Rock's commitment to creating a transportation system that is not only functional but also enriching to daily life. By prioritizing active and sustainable modes, enhancing safety and accessibility, and designing streets as vibrant public spaces, the City aims to foster a connected, inclusive, and resilient community. This approach supports a healthier population, reduces environmental impact, and strengthens the social and economic fabric of White Rock—ensuring that mobility contributes meaningfully to the quality of life for all residents, now and into the future.

## Objectives & Policies

**Objective 12.1** To improve the safety, comfort, convenience, and enjoyment of walking in White Rock, encouraging it as the first choice for short trips.

**Policy 12.1.1** **Connectivity**—Reduce travel distances by planning uses close together and creating more direct connections to destinations. Strengthen compact and mixed-use areas and support a fine-grained pedestrian network in which multiple routes exist between destinations. Establish greenways throughout the City, including an east-west greenway connecting the Town Centre to Centennial Park.

**Policy 12.1.2** **Sidewalk Design**—Increase comfort and enjoyment of sidewalks and pedestrian pathways by improving the design of streets as they are replaced or upgraded. Potential improvements include:

- Expanding sidewalk coverage to achieve continuous sidewalks, prioritizing high pedestrian activity areas, as per the White Rock Integrated Transportation and Infrastructure Master Plan (ITIMP);
- Encouraging a sufficient sidewalk width, including a minimum clear walking width of 1.5 metres in predominantly residential areas, 1.8 metres on major roads, and 2.5 metres in commercial areas;

- c) Using different coloured or textured materials for pathways and raised sidewalks in parking areas;
- d) Minimizing the number and width of driveways that cross sidewalks;
- e) Planting street trees and ensuring significant mature tree canopies along streets are protected and enhanced over time;
- f) Using landscaping, bike lanes, and/or street parking to separate sidewalks from vehicle lanes; and
- g) Testing and adapting innovative street approaches, such as shared streets and pedestrian streets, to the White Rock context.

- Policy 12.1.3 Pedestrian Street Crossings**—Improve the safety and accessibility of pedestrian crossings, giving priority to measures outlined in the White Rock Integrated Transportation and Infrastructure Master Plan (ITIMP), by:
- a) Providing accessible curb letdowns that align between the sidewalk and street at intersections;
  - b) Narrowing crossing distances by adjusting or extending curbs at intersections, and providing bus bulges;
  - c) Maximizing crosswalk visibility through lighting, pavement markings, curb extensions, and clear sight lines; and
  - d) Providing enhanced and accessible pedestrian signals.
- Policy 12.1.4 Rail Crossings**—Explore opportunities to upgrade and improve safety of rail crossings.
- Policy 12.1.5 Engaging and Comfortable Streets**—Design the public realm and buildings to create streets that are interesting, engaging, dynamic, comfortable, and inviting, as per the Form and Character Development Permit Area Guidelines in Part D.
- Policy 12.1.6 Treatments for Hills**—Implement measures to mitigate some of the impact that hills and steep slopes have on pedestrians by providing places to rest, additional stair treatments, and railings along the sides of buildings.
- Policy 12.1.7 Walkways and Stairs**—Improve connectivity to the Waterfront by enhancing walkways and stairways, particularly those on or extending from Johnston Road, Centre Street, Cypress Street, and Foster Street, with public art, benches, lighting, community gardens, play areas, landscaping, and flower beds.
- Policy 12.1.8 Public Open Space**—Provide parklets, plazas, and other gathering spaces in areas of high pedestrian activity to provide inviting spaces for people to linger and gather, providing further incentives to choose walking as a mode of travel and enhancing the public life of streets.
- Policy 12.1.9 Public Seating**—Provide opportunities for rest, with seating at regular intervals on sidewalks and other pedestrian paths. Strategically locate seating in areas that are pleasant for people-watching or are adjacent to ravines, parks, and other green and natural areas.
- Policy 12.1.10 Wayfinding**—Develop an expanded wayfinding system that will help guide pedestrians to key activity areas and other destinations.
- Policy 12.1.11 Health District**—Prioritize patient safety and accessibility by implementing streetscape improvements such as sidewalks, marked crosswalks, shorter crossing distances, enhanced lighting, and clear “Health District” signage and pavement markings.

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**Objective 12.2** To improve the safety, comfort, convenience, and enjoyment of cycling within and through White Rock.

- Policy 12.2.1** **Bicycle Network**—Enhance the bicycle network to improve connectivity and provide more cycling options throughout the city by:
- Providing more north-south bicycle routes, in particular to connect the Town Centre and Waterfront;
  - Providing ramps for bicycles at key stairways in areas of steep topography and where there are no through streets, including Johnston Road, Centre Street, Dolphin Street, Cypress Street, and Bay Street;
  - Providing high quality bicycle facilities along North Bluff Road;
  - Connecting existing gaps in the network, including extending the off-street facilities on Stayte Road south to Marine Drive and extending the shared use land on Bergstrom Road; and
  - Implementing bicycle route improvements as part of major street capital projects and as other opportunities arise.
- Policy 12.2.2** **Bicycle Facilities**—Enhance the comfort of bicycle facilities for all ages and abilities by implementing different approaches and degrees of separation from motor traffic according to context and as outlined in the White Rock Integrated Transportation and Infrastructure Master Plan (ITIMP). These include off-street pathways, cycle tracks, bicycle lanes, and neighbourhood bikeways.
- Policy 12.2.3** **Conflict Zones**—Highlight potential conflict zones, such as intersections and driveways, through coloured markings and dashed bicycle lane markings, and consider inclusion of bicycle signal crossings.
- Policy 12.2.4** **Bicycle Parking**—Provide abundant, weather-protected, secure, and conveniently located bicycle parking in all new multi-family developments, and at key employment and visitor destinations, schools, Peace Arch Hospital, and transit stops with regional connections where space is available. Provide short-term bicycle parking in commercial areas, the community and civic centre, at the waterfront, and in parks.
- Policy 12.2.5** **End-of-Trip Facilities**—Encourage end-of-trip facilities including showers and clothing lockers in major employment developments and new civic facilities.
- Policy 12.2.6** **Strengthen Active Transportation Links to Transit**—Collaborate with TransLink and the City of Surrey to establish safe, convenient pedestrian and cyclist connections between the proposed Semiahmoo Town Centre Bus Rapid Transit (BRT) exchange and the North Bluff Road Corridor Area.

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**Objective 12.3** To utilize land use and design tools, and to work with transit service providers, to improve the frequency, comfort, and convenience of transit use.

- Policy 12.3.1** **Enhance Transit System**—Work with TransLink to make transit more attractive and convenient by:
- Enhancing local service frequencies;
  - Enhancing local circulator service;
  - Improving local service periods of operation;
  - Ensuring a universally accessible transit system;
  - Improving the White Rock centre exchange;
  - Enhancing the transit customer experience; and
  - Supporting regional transit improvements.

- Policy 12.3.2**      **Enhance Transit Passenger Amenities**—Work with TransLink to improve passenger amenities at all bus stops within the Study Area by providing bus shelters, benches, Garbage and Recycling bins, and Safe Transitions to Floating Bus Stops.
- Policy 12.3.3**      **Transit Stops**—Improve infrastructure around bus stops by ensuring there are sidewalks leading to the bus stop, crosswalks near bus stops, and accessible curb letdowns.
- Policy 12.3.4**      **Transit Shelters**—Provide seating, lighting, and customer information at all bus stops along high frequency corridors, such as Johnston Road, Pacific Avenue, Columbia Avenue, eastern Marine Drive, Stayte Road, and North Bluff Road.
- Policy 12.3.5**      **Bus Exchange**—Support the development of a new bus exchange in the Semiahmoo Town Centre.
- Policy 12.3.6**      **Hillside Connector**—Continue exploring the feasibility of a fixed transit link or “people mover” between the Waterfront and Town Centre that does not involve a conventional bus, but may include a covered/enhanced escalator or funicular.
- Policy 12.3.7**      **Transit Focus in the Town Centre**—Identify the Town Centre as a key regional and local transit service ‘anchor point’, to ensure additional frequent and local transit route and infrastructure investments are directed to this area and recognize the Town Centre’s role as a high-density, mixed-use growth focus area in White Rock and the Semiahmoo Peninsula.
- Policy 12.3.8**      **Transit Advocacy**—Advocate for expanded transit service from TransLink to better serve the Study Area and its growing population.
- Policy 12.3.9**      **Transportation Demand Management**—Require Transportation Demand Management (TDM) strategies as part of all new developments to encourage transit use and reduce reliance on private vehicles. Consider implementing a TDM bylaw to require measures such as:
  - Developer-funded transit passes
  - Car share memberships
  - Additional secure bike parking
  - Micromobility charging stations and storage

**Objective 12.4**      **To enable the efficient delivery of goods to local businesses in White Rock.**

- Policy 12.4.1**      **Deliveries**—Establish delivery times during off-peak hours in areas where there is congestion with goods delivery.
- ~~**Policy 12.4.2**      **Truck Routes**—Maintain existing dedicated Truck Routes in White Rock’s Street and Traffic Bylaw.~~
- Policy 12.4.2**      **Truck Routes**—Maintain dedicated Truck Routes in White Rock’s Street and Traffic Bylaw.
- Policy 12.4.3**      **Rail Line**—Continue to explore opportunities to relocate the Burlington Northern Santa Fe rail line away from the waterfront, in order to address safety issues and concerns about the increase in rail traffic.

**Objective 12.5**      **To enable the safe movement of vehicles, effectively manage parking, and encourage greener solutions for personal vehicle use.**

- Policy 12.5.1**      **Network and Improvements**—Implement improvements to streets and intersections, as per the White Rock Integrated Transportation and Infrastructure Master Plan (ITIMP).

- Policy 12.5.2**      **Traffic Calming**—Implement traffic calming to reduce vehicle speeds and discourage short-cutting through residential streets and lanes.
- Policy 12.5.3**      **Wayfinding**—Direct visitors to destinations with wayfinding signage to reduce impacts of visitor traffic. Enhance highway signage exposure along Highway 99 to increase awareness of White Rock services, attractions, accommodation, and other amenities.
- Policy 12.5.4**      **Electric Vehicles**—Explore opportunities to install public charging stations in the Town Centre and along the Waterfront, and provide designated stalls for electric vehicles.
- Policy 12.5.5**      **Parking Supply**—Manage parking supply by reviewing parking standards for new developments to ensure oversupply does not occur, and consider reducing requirements in denser areas. Review parking standards for Mature Neighbourhoods to determine an appropriate approach for secondary suites and other gentle forms of infill.

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## **Objective 12.6**    North Bluff Road Corridor Improvements

- Policy 12.6.1**      **Complete Street Coordination**—Support the implementation of the complete street cross-section for North Bluff Road/16th Avenue in coordination with the City of Surrey by removing on-street parking and acquiring additional right-of-way to accommodate all users.
- Policy 12.6.2**      **Corridor Safety Upgrades**—Implement upgrades to North Bluff Road to improve safety and align with the ITIMP.
- Policy 12.6.3**      **Emergency Route Designation**—Designate and maintain emergency routes within the North Bluff Road Corridor Area to ensure effective and safe travel for emergency vehicles at all times.
- Policy 12.6.4**      **Greenway Network**—Develop a connected, safe, and inclusive greenway transportation network on North Bluff Road to help support more sustainable travel choices while improving safety, comfort, and convenience for residents, workers, and visitors in alignment with the ITIMP.



# 13. Economic Development

**Goal:** The City of White Rock attracts a diversified economy by supporting local businesses, creating a desirable place to visit, work, shop, and invest.



## Overview

Economic development is a vital to quality of life and many other community goals. As retail is so fundamental to the economic prosperity of White Rock, attracting tourists and other visitors to sustain local businesses has long been a community priority.

The policies in this section seek to support White Rock as a place to not only live and play, but also to work, shop, and conduct business. They provide direction on strategies and partnerships to enhance overall economic prosperity, and to differentiate White Rock from other communities.

## Objectives & Policies

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### **Objective 13.1** To promote the city as a place to move to and reside in over the long term.

**Policy 13.1.1** **Attractive Community**—Promote White Rock as a community that provides:

- a) A mix of high quality housing choices, including diverse forms and tenures that accommodate workers of differing incomes, abilities, stages of life, and lifestyles;
  - b) Distinct mixed-use precincts and local and boutique retail opportunities;
  - c) Leisure-supportive spaces and tourism areas;
  - d) Arts, heritage, and cultural events and activities; and
  - e) A wide range of community services.
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### **Objective 13.2** To coordinate efforts in support of tourism development in White Rock.

**Policy 13.2.1** **Tourism Economy**—Work with Tourism White Rock and other stakeholders to enhance the value of the tourism economy.

**Policy 13.2.2** **Destinations**—Work with the Business Improvement Association to improve and promote retail and amenity areas, including along Marine Drive and Johnston Road, by focusing on the city as a “full day destination” and on the waterfront as a “year round destination”. Explore the possibility of expanding programming on the waterfront., and actively promote the development of new hotel space and related uses in this area.

**Policy 13.2.3** **Year-Round Activities**—Encourage year-round tourist-oriented and community activities such as outdoor markets and street festivals, particularly at the waterfront.

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### **Objective 13.3** To leverage partnerships that will assist in business development.

**Policy 13.3.1** **Business-Friendliness**—Encourage the involvement of the business community in the improvement and promotion of White Rock as a place to do business.

**Policy 13.3.2** **Mutual Benefits**—Work with the City of Surrey, the City of Blaine, and the Semiahmoo First Nation to promote economic development opportunities.

**Policy 13.3.3** **P3 Opportunities**—Be receptive to public-private partnership opportunities

**Policy 13.3.4** **Expanded Business Sector**—Work with local businesses to explore ways to encourage an expanded business sector, including both retail and office uses, through initiatives such as branding, partnerships, incentives, and “buy local” campaigns.

**Objective 13.4** To utilize land use and marketing measures to promote local jobs, support local businesses, and increase the tax base.

**Policy 13.4.1** **Commercial Land**—Foster a mix of employment opportunities by protecting the commercial land base from conversion to non-employment uses, and by allowing home occupations.

**Policy 13.4.2** **Retail Niche**—Protect and further develop a specialized small-scale market niche that capitalizes on White Rock’s seaside location and village character.

**Policy 13.4.3** **Film Industry**—Capitalize on Metro Vancouver’s identity as “North Hollywood” by marketing White Rock as a prime destination for the film industry.



## 14. Parks & Recreation

**Goal:** The City of White Rock provides a diverse range of recreational facilities and open space, offering a wide variety of programs for residents of all ages and abilities.



## Overview

If buildings are the backdrop to public life, then open spaces are the stage. Open spaces foster public life, provide passive and active recreation opportunities, and establish connections to healthy ecosystems and a holistic food system. White Rock is home to approximately 31 hectares of park land, as well as several indoor and outdoor recreation facilities and a community garden. Some Hill Walks, while not considered to be parks, provide amenities like playgrounds. Policies in this section are consistent with the Parks and Recreation Master Plan, with an approach to open space that celebrates unique local community character, values the natural environment, promotes active transportation, and contributes to economic vitality by improving the desirability and livability of White Rock.

## Objectives & Policies

**Objective 14.1** To meet community open space needs, and improve parks, plazas, and lookout areas.

**Policy 14.1.1** **Park and Open Space Distribution**—Provide new park space in areas identified as being deficient and in areas projected for future population growth, such as the Town Centre, in order to ensure that existing and planned parks can meet the needs of residents.

**Policy 14.1.2** **Park Acquisition**—Acquire park space through dedication or statutory rights-of-way with development projects where it fits within the broader open space network as outlined in this Plan and the Parks and Recreation Master Plan.

**Policy 14.1.3** **Town Centre**—Utilize the open space network, including park space, greenways, plazas, and sidewalks, to increase the pedestrian permeability of the Town Centre and to provide open space access for residents and visitors. Establish a plaza or park in the block bounded by North Bluff Road, Russell Avenue, Johnston Road, and Foster Street.

**Policy 14.1.4** **Civic Plaza**—Establish a Town Square at Russell Avenue and Johnston Road, to strengthen a community focal point in the heart of the Town Centre.

**Policy 14.1.5** **Open Space Diversity**—Better utilize a greater diversity of public open spaces, including through small improvements such as seating areas along sidewalks, on trails, in ravines, and in other open space areas.

**Policy 14.1.6** **Co-Location**—Cluster parks and open space, where possible, near areas of active and passive recreation use, including indoor and outdoor recreation facilities and retail and restaurant areas.

**Policy 14.1.7** **Street Allowances**—Retain unopened or unimproved street allowances that can be upgraded for walkways, viewing areas, landscaped parks, and natural areas.

**Policy 14.1.8** **Creative Partnerships**—Build partnerships and explore joint use opportunities with the School District at public schools, including White Rock Elementary and Peace Arch Elementary.

**Objective 14.2** To establish multi-use greenways and expand the City's tree canopy along streets.

**Policy 14.2.1** **Multi-Use Pathways**—Connect neighbourhoods to one another and to the Town Centre, Lower Town Centre, and Waterfront through multi-use pathways and other trails.

**Policy 14.2.2** **East-West Green Spine**—Prioritize an east-west greenway that will connect the Town Centre to Centennial Park.

- Policy 14.2.3** **Green Edge**—Encourage the establishment of an engaging green edge in the Lower Town Centre, along Johnston Road adjacent to White Rock Elementary, by incorporating seating, planting, and other programmatic elements.
- Policy 14.2.4** **Tree Canopy**—Increase the amount of space available for trees by minimizing front driveways or eliminating them on properties where laneways are present in the Urban and Mature Neighbourhoods, to allow for the retention and expansion of the tree canopy.
- Policy 14.2.5** **Space for Trees**—Strategically manage new building setbacks in multi-family and mixed-use areas to create sufficient space for trees while maintaining an intimate, urban relationship with the street.

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**Objective 14.3** To protect the riparian, foreshore, and aquatic habitat and biodiversity in the operation of City parks and open spaces.

- Policy 14.3.1** **Ecosystem Impacts**—Plan, design, develop, and maintain parks and open space in a manner that minimizes impacts on local riparian, foreshore, and aquatic ecosystems.
- Policy 14.3.2** **Stewardship Education**—Encourage initiatives and consider an interpretive centre in the waterfront area that relate to the stewardship of local wildlife and ecology.

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**Objective 14.4** To improve access to fresh, local food, and to provide community gardening in public spaces.

- Policy 14.4.1** **Community Gardens**—Support the establishment of community gardens and other forms of urban agriculture in public parks.



## 15. Infrastructure

**Goal:** The City of White Rock is a safe and resilient community with responsive, high-quality, and well-maintained infrastructure that meets the needs of the community.



## Overview

Infrastructure addresses fundamental community needs that support human health, but also influences resource consumption and environmental integrity. The City maintains roads and sidewalks, and manages the storm and sanitary sewer systems. The City also provides water services to residents and businesses. Plans are in place for two new water treatment facilities as part of overall water infrastructure upgrades to treat for arsenic and manganese.

The policies in this section seek to protect the ecosystems of which all people are a part and depend upon—and which shape much of White Rock’s identity and natural beauty—while also responsibly managing City assets and delivering essential services that are vital to the quality of life for residents.

## Objectives & Policies

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**Objective 15.1** To achieve appropriate levels of services and infrastructure improvements to accommodate growth and support human health.

**Policy 15.1.1** **Phased Improvements**—Undertake a logical, sequential, and phased program of water, sanitary, and storm sewer infrastructure improvement and replacement. Balance expenditures and system requirements to maintain current levels of service.

**Policy 15.1.2** **Consistency with Plans**—Develop new Water, Stormwater, and Sanitary Sewer Management Plans, and manage infrastructure and related services in an efficient manner consistent with these Management Plans.

**Policy 15.1.3** **Drinking Water**—Ensure drinking water supply and distribution is managed to safeguard public health, protect the environment, and provide an adequate supply for a growing population. Monitor demand and implement conservation strategies, and protect groundwater and the aquifer from contamination. Improve water quality through reduction of point and non-point source pollution, and through watershed planning in partnership with neighbouring and regional jurisdictions.

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**Objective 15.2** To protect or enhance ecological health through the delivery of infrastructure services.

**Policy 15.2.1** **Solid Waste**—Support ongoing initiatives that will provide for the effective and responsible solid waste management of recyclables, compostables, and garbage through programs, services, policies, and guidelines. Require all new multi-unit residential, commercial, and institutional development to include space for waste-stream separation.

**Policy 15.2.2** **Fish Habitat and Riparian Areas**—Plan new sanitary and storm sewer services in a manner that will prevent or minimize design and operation impacts to fish habitat by avoiding alignments with fish habitat areas including watercourse crossings and riparian areas.

**Policy 15.2.3** **Watershed Planning**—Take an integrated watershed planning approach for the comprehensive management of surface water, stormwater, and ground water resources that promotes healthy aquatic ecosystems, resilience to climate change, and the maintenance of hydraulic systems. Support cross-border initiatives to promote the ecological well-being of the Georgia Basin areas.

**Policy 15.2.4**

**Stormwater Management**—Take an integrated stormwater management approach that:

- a) Reduces base flows and the frequency and magnitude of peak flows;
- b) Minimizes impervious surfaces in new developments;
- c) Includes strategies to mitigate stormwater runoff impacts for all developments;
- d) Includes low impact biofiltration systems on select streets and public parking lots, and uses enhanced stormwater treatments;
- e) Incorporates stormwater features that form part of the broader open space and habitat network; and
- f) Improves the quality of water flowing into Semiahmoo Bay.

**Policy 15.2.5**

**Green Infrastructure**—Encourage the use of green infrastructure, including bioswales, permeable pavement, rain gardens, soil cells, green roofs, and green walls, and the planting of street trees to assist in the management of stormwater. Consider adopting additional policies from Metro Vancouver’s ‘Connecting the Dots.’

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**Objective 15.3**

**To enhance the public realm and address the location of utilities and services within streets.**

**Policy 15.3.1**

**Road Standards**—Establish road standards for a hierarchy of streets that identify widths for sidewalks and overall right-of-ways, and location of infrastructure services.

**Policy 15.3.2**

**Undergrounding of Utilities**—Move overhead utility cables underground, making them less susceptible to outages during storm events and creating more space in the public realm for pedestrians and trees.

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**Objective 15.4**

**To ensure there is appropriate infrastructure capacity and stormwater management systems are in place to support both new and existing development within the North Bluff Road Corridor Area.**

**Policy 15.4.1**

**Utilities Servicing Plan**—Develop a utilities servicing plan to enable implementation of the North Bluff Road Corridor Plan.

**Policy 15.4.2**

**Coordination with Metro Vancouver**—Engage with Metro Vancouver to assess their asset capacity and explore opportunities for how their funding policy can support potential upgrades.

**Policy 15.4.3**

**On-Site Drainage Standards**—Develop standards for on-site drainage management for new development, and incorporate these standards into a Subdivision & Development Servicing Bylaw.

**Policy 15.4.4**

**Development Cost Charge Review**—Regularly review and update the City’s Development Cost Charge Bylaw to ensure growth-related projects are sufficiently funded.

## 16. Arts, Culture & Heritage

**Goal:** The City of White Rock promotes and celebrates its thriving local arts, culture, and heritage to its fullest potential.



## Overview

Culture is what residents of a community share in common and how they celebrate their differences. The culture of a community is an important part of a 'place', setting it apart from other communities. Semiahmoo First Nation, a Coast Salish sub-group, are the traditional occupants of the area and are still actively involved in preserving the history and cultural activities of the Salish people. Semiahmoo First Nation run the Spirit Stage Bandshell, where a variety of cultural events and performances occur.

White Rock is home to an array of artists and creative sector workers. In addition to city and community-run facilities that offer spaces for cultural programming and events, several non-profit and privately run businesses serve the cultural and creative needs of the community, ranging from visual and performance arts, to music and film.

The policies in this section seek to reinforce a unique sense of place through tangible and intangible cultural qualities—including art and artistic expression, and the celebration of heritage—to create a sense of identity and meaning and foster community pride. Cultural policies also strive to contribute to economic vitality, since culture is what makes places like White Rock special and interesting and an inviting place to live, do business, visit, and invest.

## Objectives & Policies

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### **Objective 16.1** To protect and reinforce White Rock's heritage.

- Policy 16.1.1** **Historic Streetscapes**—Protect the unique historic character of the commercial streetscape on Marine Drive by ensuring strong compatibility of all new development, as per the Development Permit Guidelines in Part D.
- Policy 16.1.2** **Artifacts and Archival Collections**—Work with the White Rock Museum and Archives to ensure the long term preservation of its artifact and archival collections.

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### **Objective 16.2** To support and reinforce cultural inclusivity, cultural involvement, and the arts and artistic expression in White Rock.

- Policy 16.2.1** **Cultural Inclusiveness**—Ensure cultural resources and activities are inclusive and respond to the cultural needs and aspirations of diverse populations through culturally relevant programs, services, and facilities. Encourage cultural expression—through events, public art, and other means—that reflect diverse populations and community needs and interests.
- Policy 16.2.2** **Partnerships**—Explore opportunities to work with Semiahmoo First Nation and the City of Surrey.
- Policy 16.2.3** **Public Art and Artistic Expression**—Provide appropriate spaces for and encourage the installation of formal and informal public art and artistic expression in buildings, streets, parks, and other areas of the public realm, giving preference to local artists.
- Policy 16.2.4** **Art Spaces**—Explore the feasibility of a multi-use civic facility that includes a Public Art Gallery, and consider including space for arts and cultural activities in future Municipal building projects where possible.
- Policy 16.2.5** **Artistic Connections**—Strengthen connections between the Town Centre and the Waterfront through artistic interventions, such as sculpture gardens and an arts walk, and incorporate artistic expression in new functional street furnishings.

- Policy 16.2.6**     **Arts and Culture Infrastructure**—Consider directing community amenity contributions for space for arts, culture, and heritage programs and activities through major private developments.
- Policy 16.2.7**     **Creative Economy**—Promote culture-led economic development by considering offering tax incentives to buildings owners who rent to arts and culture businesses or organizations, and working with Tourism White Rock to better establish White Rock as a cultural destination.
- Policy 16.2.8**     **Johnston Road**—Promote Johnston Road as an arts and culture corridor or district that could possibly expand into 152nd Street in Surrey.
- Policy 16.2.9**     **Promenade**—Support the development of a conceptual design for a permanent and designated space for an Artists Walk Program on the promenade, which could include colourful shelters for artists to display their work.
- Policy 16.2.10**    **Programming and Events**—Support diverse arts programs, artistic opportunities, cultural festivals, and other events that celebrate and contribute to White Rock’s cultural identity.



## 17. Quality of Life

**Goal:** The City of White Rock provides an environment where all residents can realize their potential in living healthy, happy, and well-balanced lives.



## Overview

Quality of life is complex, and it is shaped by all of the policy realms in this OCP, ranging from housing and transportation, to parks and the natural environment, to food security and local food production. As noted in other policy sections, White Rock is home to many amenities and natural and cultural assets that contribute to a high quality of life for residents. Implementation of this OCP will maintain and enhance the quality of life for residents and visitors into the future.

A direct link between the City of White Rock and the quality of life and health of residents is through the provision of essential community services and access to these services. Community services foster community health, safety, and overall well-being. The policies in this section embrace these aspirations, and seek to influence community identity and pride, which in turn help make White Rock a desirable place to live.

## Objectives & Policies

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### **Objective 17.1** To support the effective delivery of essential community and emergency services.

- Policy 17.1.1** **Community Services**—Continue support and coordination with and between local community service providers, including Sources Community Resource Centres, to optimize programs available to the residents of White Rock.
- Policy 17.1.2** **Police and Fire Services**—Continue to support and coordinate with the White Rock Fire Department and White Rock RCMP, and continue to provide a high level of police and fire services to citizens.
- Policy 17.1.3** **Schools**—Work cooperatively with School District 36 to ensure school facilities meet the needs of White Rock residents, to coordinate parks and recreation programs, to provide safe access to schools, and to partner for resource sharing where feasible.
- Policy 17.1.4** **Emergency Response**—Continue monitoring and updating the White Rock Emergency Response Plan as needed, and coordinate with Surrey where appropriate.
- Policy 17.1.5** **Health Care Facilities and Services**—Continue to support the expansion of the Peace Arch Hospital and associated health care facilities and services.
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### **Objective 17.2** To strengthen identity and pride of place in White Rock.

- Policy 17.2.1** **Community Events**—Continue hosting and supporting initiatives and special events that foster civic identity and pride, including the Tour de White Rock bicycle road race and the Sea Festival.
- Policy 17.2.2** **Business Partnership**—Encourage and partner with the business community in the improvement and promotion of the city as a place to visit and do business.
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### **Objective 17.3** To meet the diverse needs of the community and provide access to civic facilities, spaces, and programs.

- Policy 17.3.1** **Civic Facilities**—Provide and enhance recreation opportunities and wellness services to meet the diverse needs of the community through programs delivered at civic facilities and spaces.
- Policy 17.3.2** **Parks and Other Civic Spaces**—Work toward improving access to open space so that all residents live within a 5-minute (400 metre) walk of a park, green space, or other civic space.
- Policy 17.3.3** **Universal Design**—Encourage the incorporation of universal design principles in new buildings, civic facilities, parks, and public realm upgrades.

**Objective 17.4** To improve food security and enhance access to all facets of the local food system, from production and processing to sales and nutrient recovery.

**Policy 17.4.1** **Food Systems**—Continue to support and work toward the goals in Metro Vancouver's Regional Food System Action Plan.

**Policy 17.4.2** **Food Production**—Support the establishment of urban agriculture in White Rock by:

- a) Encouraging and establishing non-commercial community and demonstration gardens where feasible and appropriate in parks, rights-of-way, boulevards, vacant lots, and private developments;
- b) Encouraging the provision of private allotment gardens in new developments;
- c) Encouraging the establishment of small-scale commercial urban food gardens, including the associated sale of the food products;
- d) Encouraging and establishing an informal edible landscaping, with fruit and seed-bearing species, in both public and private lands; and
- e) Considering supporting additional urban agricultural activities such as keeping chickens.

**Policy 17.4.3** **Food Processing**—Support the establishment of community processing facilities such as community kitchens, as well as private sector processing facilities.

**Policy 17.4.4** **Food Sales**—Continue support for a Farmers' Market, and encourage additional grocery store space in the Town Centre.

**Policy 17.4.5** **Nutrient Recovery**—Support the appropriate disposal, recovery, and re-use of organic material.



# Part C Implementation



# 18. Plan Administration & Alignment

## 18.1 Consistency with Other Plans

The City of White Rock has many plans and strategies in place that provide direction for planning and development, engineering, parks and recreation, environmental management, economic development, and more. Most of these plans and strategies already generally align with this OCP, while in other instances there may be gaps in strategic direction. In all cases, updates or creation of new plans or strategies must consider and align with this OCP.

### REQUIRED PLAN UPDATES

~~There are a number of plans and bylaws that have direct impact on the growth and development of the city that should be updated following adoption of the OCP as part of this implementation strategy. The following is a list of plans and bylaws that require a critical review and update to align with the vision and policies of this OCP:~~

- ~~• Zoning Bylaw~~
- ~~• Drainage Master Plan~~
- ~~• Water Master Plan~~
- ~~• Sanitary Sewer Master Plan~~

There are a number of plans and bylaws that have a direct impact on the growth and development of the city that should be updated following the adoption of the OCP as part of this implementation strategy. The following is a list of plans and bylaws that require a critical review and update to align with the vision and policies of this OCP:

- Zoning Bylaw
- Subdivision & Development Servicing Bylaw

## 18.2 Alignment with Financial Decision-Making

One of the most effective implementation tools in the administration of this Plan is through the municipal budget. The City will incorporate a budget that takes a holistic approach to city-building that is driven by the Vision, Guiding Principles, Goals, and Objectives in this OCP.

Strategies and decision-making relating to community amenity contributions and density bonusing provisions must directly align with the vision, principles, goals, and objectives of this OCP. Likewise, development cost charges and capital budgeting approaches will continue moving the City toward a full cost accounting approach, in which different types and locations of growth will better address real costs and value creation.

## 18.3 Development Proposals & OCP Amendments

All development proposals submitted to the City will be required to comprehensively and credibly identify how the proposal facilitates or inhibits the realization of the vision, principles, goals, and objectives of this OCP. Such analysis cannot be selective in the referencing of elements of this OCP, but must be comprehensive in its consideration of the OCP in its totality.

Each application for OCP amendments must clearly demonstrate how the proposal conforms with and will help realize the OCP's vision, principles, goals, and objectives. City staff will provide its own review of proposal alignment with the OCP, ensuring that such reviews do not become formulaic, but rather are candid and critical in their contextual and project-specific consideration.

Development proposals that require OCP amendments will be reviewed by City staff and an initial information report on the proposed OCP amendment will be prepared for Council. Following receipt of the information report, Council may refuse the application for OCP amendment or direct City staff to continue processing the application.

# 19. Monitoring, Evaluation, & Actions

## 19.1 Monitoring & Evaluating Success

Ensuring the successful implementation of the OCP not only requires updates to plans and strategies, but also requires ongoing monitoring and evaluation. Staff and Council should be equipped with the information needed to respond to the evolving context of the community, and to determine whether the OCP vision and goals are being achieved. Monitoring should be kept relatively simple and measurable.

## 19.2 Actions

It is not possible to list all of the actions likely to be undertaken by the City through the life of this OCP. However, Table 3 below identifies priority action items and anticipated time frames. An annual review will outline progress that is being made.

Table 3 Priority Action Items and Timelines

<b>Short Term (1-2 Years)</b>
Develop a new Zoning Bylaw (Policies 11.1.2, 11.2.1, 13.5.5, 15.2.5)
Develop a new Urban Forest Management Plan (Policies 6.2.2, 12.2.2, 15.2.4)
Develop new GHG emission reduction targets (Policy 12.5.1)
Adopt the Province’s Energy Step Code (Policy 12.5.4)
Develop a new Water Management Plan (Policy 16.1.2)
Develop a new Stormwater Management Plan (Policies 16.1.2, 16.2.4)
Develop a new Sanitary Sewer Management Plan (Policy 16.1.2)
Develop road standards (Policy 16.3.1)
<b>Medium Term (3-4 Years)</b>
Develop an expanded wayfinding system (Policies 10.1.1, 10.2.1, 10.3.2, 13.1.10, 13.5.3)
Implement a people movement system between the Waterfront and the Town Centre (Policies 10.1.1, 13.3.5)
Develop public realm design guidelines for Marine Drive (Policy 10.4.3)
Establish outdoor amenity space requirements for multi-unit developments (Policy 11.1.1)
Develop design criteria and establish minimum unit counts for accessible units in new developments (Policy 11.1.2)
Establish an Affordable Housing Reserve Fund (Policy 11.2.1)
Develop a Tenant Relocation Policy (Policy 11.2.3)
Assess municipal infrastructure to determine the level of risk associated with sea level rise (Policy 12.4.1)
Develop a Green Building Strategy (Policy 12.5.3)
<b>Long Term (5+ Years)</b>
Relocate the rail line away from the Waterfront (Policies 10.2.3, 13.4.3)
Conduct a feasibility study for a multi-use recreation facility that includes public art space (Policy 17.2.4)
<b>Ongoing</b>
Construct parking structures to provide additional capacity away from the surface lots on the Waterfront (Policies 10.2.2, 10.3.2)
Monitor the net increase in total secured market rental units and affordable rental units (Policy 11.2.1)
Monitor the area of additional public space in the City (Policies 9.2.1, 9.3.1, 9.3.2, 13.1.1, 13.1.8, 15.1.1/2/3/4/5, 15.2.2, 18.3.2)
Monitor significant improvements to the pedestrian realm (Policies 13.1.1/2/3/4/6/7/8/9, 17.2.3/5/6/8/9)
Monitor significant improvements to the cycling network (Policies 13.2.1/2/3/4/5)
Monitor significant improvements to transit infrastructure (Policy 13.3.1/2/3)

**Table 3.** Priority Action Items and Timelines

**Short Term (1–2 years)**

Update Zoning Bylaw (Policies 10.1.1, 10.1.2, 10.2.1, 10.2.2, 12.5.5, 14.2.5)

Develop an Amenity Cost Charges (ACC) Bylaw in accordance with Bill 46 (Policy 18.2)

Develop Rental Protection Mechanisms (Policies 10.2.2, 10.2.4)

Developing a target non-market and/or affordable rental housing contribution policy (Policy 10.2.1)

Review and update the Density Bonus/Community Amenity Contribution Policy (Policies 7.13.1, 10.2.2, 8.4.3, 18.2)

Review and update the Subdivision & Development Servicing Bylaw (Policies 15.3.1, 15.4.3)

Develop a Utilities Servicing Plan for the North Bluff Road Corridor Area (Policy 15.4.1)

**Medium Term (3–4 years)**

Develop Incentives for Health Uses Policy (Policy 7.10.2)

Review and update the Development Cost Charge Bylaw (Policy 15.4.4)

Develop a Development Cost Charge (DCC) Waivers Policy (Policy 7.10.2)

Develop an expanded wayfinding system (Policies 9.1.1, 9.2.1, 9.3.2, 12.1.10, 12.5.3)

Implement a people movement system between the Waterfront and the Town Centre (Policies 9.1.1, 12.3.6)

Develop public realm design guidelines for Marine Drive (Policy 9.4.3)

Establish outdoor amenity space requirements for multi-unit developments (Policy 10.1.1)

Develop a Tenant Protection Bylaw (Policies 10.2.2, 10.2.5)

Assess municipal infrastructure to determine the level of risk associated with sea level rise (Policy 11.4.1)

Develop a Green Building Strategy (Policy 11.5.3)

Update the Water Master Plan (Policies 15.1.1, 15.1.2)

Update the Sewer Master Plan (Policies 15.1.1, 15.1.2)

Update the Parks Master Plan (Policy 17.3.2)

Develop new GHG emission reduction targets (Policy 11.5.1)

**Long Term (5+ years)**

Develop a Transportation Demand Management Bylaw (Policy 12.3.9)

Relocate the rail line away from the Waterfront (Policies 9.2.3, 12.4.3)

Conduct a feasibility study for a multi-use recreation facility that includes public art space (Policy 16.2.4)

### Long Term (5+ years) cont.

Update the Drainage Master Plan (Policies 11.1.5, 15.1.1, 15.1.2)

Update the Integrated Stormwater Management Plan (Policies 11.1.5, 15.2.4)

Develop a new Urban Forest Management Plan (Policies 5.2.2, 11.2.2, 14.2.4)

### Ongoing

Construct parking structures to provide additional capacity away from the surface lots on the Waterfront (Policies 9.2.2, 9.3.2)

Monitor the net increase in total secured market rental units and affordable rental units (Policies 10.2.1, 10.2.2)

Monitor the area of additional public space in the City (Policies 8.2.1, 8.3.1, 8.3.2, 12.1.1, 12.1.8, 14.1.1/2/3/4/, 14.2.2, 17.3.2)

Monitor significant improvements to the pedestrian realm (Policies 12.1.1/2/3/4/5/6/7/8/9, 16.2.3/5/6/8/9)

Monitor significant improvements to the cycling network (Policies 12.2.1/2/3/4/5/6)

Monitor significant improvements to transit infrastructure (Policies 12.3.1/2/3/4)



## 20. Development Approvals Information

### 20.1 Authority & Purpose

Section 485 of the *Local Government Act* authorizes a local government to specify circumstances in which Development Approval Information may be required within its Official Community Plan. Development Approval Information can include studies and technical information that allow the City to evaluate a development and any potential impacts the development may have on the surrounding neighbourhood and the City as a whole. This information can also be used to ensure that a new development meets the intent of the objectives and policies in the Official Community Plan and meets other City Bylaws. Development approval information may be required for Official Community Plan Amendment, Rezoning, Subdivision, Development Permit, and/or Temporary Use Permit applications.

### 20.2 Required Information

The types of plans and studies that may be required with a development application in the City of White Rock include, but are not limited to, the following:

- Plans indicating proposed land uses, landscaping, and open spaces
- Plans of rental buildings that are proposed for removal to accommodate redevelopment
- Traffic impact assessment that addresses vehicular traffic, transit, cycle and pedestrian routes, garbage and loading, and parking
- Engineering studies identifying infrastructure impacts, requirements, and potential upgrades
- Geotechnical assessment
- Hydrological assessment addressing drainage patterns and stormwater management
- Environmental assessment and mitigation plans
- Health Impact Assessment to determine potential health impacts of proposed developments
- Tree report that addresses tree management associated with the proposed development
- Studies on potential impacts on public facilities and community services
- Assessment of impacts on neighbouring properties and public lands, including view analyses, street profiles, shadow studies, wind studies, and noise studies
- Assessment of economic costs and benefits, including tax impacts, and public infrastructure costs, and potentially a market analysis to demonstrate demand for the development
- Studies required to address other issues that may be identified by White Rock City Council

Reports are to be prepared by an appropriately qualified registered professional.

*Part D* Development Permit  
Area Guidelines

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# 21. Development Permit Areas (Form/Character/ Sustainability)

Guidelines for these designated Development Permit Areas (DPAs) are intended to protect and enhance the quality of the natural and built environment and to enhance the character of White Rock’s history and setting.

## 21.1 Overview/Authority

The Local Government Act authorizes local governments to designate areas in the Official Community Plan for several purposes including the establishment of objectives for the form and character of commercial, industrial, multi-family residential, or intensive residential development, and to achieve objectives for energy and water conservation and the reduction of greenhouse gas emissions. The City designates the following Development Permit Areas to achieve these objectives, pursuant to the sections of the Local Government Act listed in brackets:

- ~~• Town Centre [Section 488(1)(d),(h),(i), and (j)]~~
- ~~• Lower Town Centre [Section 488(1)(d),(h),(i), and (j)]~~
- ~~• Waterfront [Section 488(1)(d),(h),(i), and (j)]~~
- ~~• Multi-Family [Section 488(1)(e),(f),(h),(i), and (j)]~~
- ~~• Neighbourhood Commercial [Section 488(1)(f),(h),(i), and (j)]~~
- ~~• Mature Neighbourhood Houseplex [Section 488(1)(e),(h),(i), and (j)]~~

- Town Centre [Section 488(1) (d), (h), (i), and (j)]
- Town Centre Transition [Section 488(1) (d), (f), (h), (i), and (j)]
- Lower Town Centre [Section 488(1) (d), (h), (i), and (j)]
- Waterfront [Section 488(1) (d), (h), (i), and (j)]
- Health District [Section 488(1)(d), (f), (h), (i), and (j)]
- Multi-Family [Section 488(1) (e), (f), (h), (i), and (j)]
- Neighbourhood Commercial [Section 488(1) (f), (h), (i), and (j)]
- Mature Neighbourhood SSMUH [Section 488(1)(e), (h), (i), and (j)]

~~The properties subject to the above Development Permit Areas are identified in Schedule B. Single family infill subdivisions, created through rezoning and subdivision, where the lot width is less than 12.1 metres (40 feet) are defined as “intensive residential redevelopment” pursuant to Section 488(1)(e). Further, all lands zoned to permit and proposed to be developed for assisted living use are defined as “intensive residential development” and designated as Development Permit Area pursuant to Section 488(1)(e), and the Multi-Family DPA guidelines will apply to such developments. The DPA that applies to a given site may need to be revised with development applications that include amendments to the land use designations included in this Plan. This will ensure the appropriate guidelines are being applied to each new proposed development, as different types and scales of development are contemplated and regulated in each DPA as outlined in the following list:~~

- ~~• Town Centre - large-scale mixed-use and apartment~~
- ~~• Lower Town Centre - moderate-scale mixed-use and apartment~~
- ~~• Waterfront - small-scale mixed-use and apartment~~
- ~~• Multi-Family - mixed-use, apartment, townhouse, and intensive assisted living use (varying scales), intensive single family infill (varying scales)~~
- ~~• Neighbourhood Commercial - small-scale commercial and mixed-use buildings~~
- ~~• Mature Neighbourhood Houseplex - houseplexes and intensive single family infill~~

The properties subject to the above Development Permit Areas are identified in Map B. All lands zoned to permit and proposed to be developed for assisted living use are defined as “intensive residential development” and designated as Development Permit Area pursuant to Section 488(1)(e), and the Multi-Family DPA guidelines will apply to such developments. The DPA that applies to a given site may need to be revised with development applications that include amendments to the land use designations included in this Plan. This will ensure the appropriate guidelines are being applied to each new proposed development, as different types and scales of development are contemplated and regulated in each DPA, as outlined in the following list:

- **Town Centre:** large-scale mixed-use and residential buildings
- **Town Centre Transition:** moderate to large-scale mixed-use and residential buildings
- **Lower Town Centre:** moderate-scale mixed-use and apartment
- **Waterfront:** small-scale mixed-use and apartment
- **Health District:** moderate to large-scale mixed-use buildings that include health and medical-related services, and residential buildings
- **Multi-Family:** mixed-use, apartment, townhouse, and intensive assisted living use (varying scales)
- **Neighbourhood Commercial:** small-scale commercial and mixed-use buildings
- **Mature Neighbourhood SSMUH:** Small-Scale Multi Unit Housing

The following Sections include guidelines for the form, character, and sustainability measures for new development. Conditions of development and exemptions are also outlined. The guidelines will be used by the Planning Department and the Advisory Design Panel in evaluating development applications.

## 21.2 Development Permit Exemptions

The following are exempt from the development permit application approval process:

- Proposals where the intended use is wholly limited to institutional, utility, and/or civic uses
- Interior alterations and/or renovations with no changes to the exterior appearance or design
- Building envelope remediation, so long as there are no changes to exterior appearances or design
- In-kind replacements of missing, worn, or damaged exterior materials, so long as there are no changes to the exterior design or appearance
- Alterations to a façade that result in only minor changes to the form and character of the building
- Applications for individual signs only
- One unit residential and small-scale multi unit housing development up to four (4) dwelling units are exempt from Form and Character Development Permits.





## 21.3 Town Centre Development Permit Area

The objectives of this Development Permit Area are to:

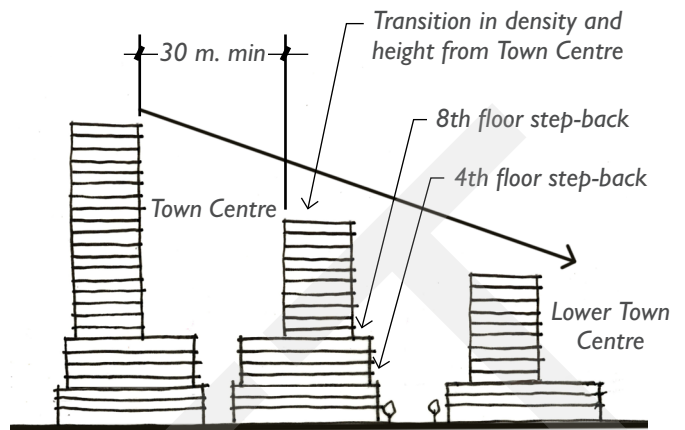
- Reinforce Town Centre as the cultural, commercial, and civic heart of White Rock
- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock



*The Heart of Our City*

## BUILDINGS

- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design. The tallest and densest developments are to be located at the intersection of Johnston and North Bluff Roads, with heights and densities decreasing to the south and west as outlined in Figures 8 and 9. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.
- b) Set buildings back from the property line at least 3 metres, with a further stepping above the second or third floor in mixed use areas and above the fourth floor in residential areas. Consider an additional step back above the seventh floor. Fewer steps in building form may be accepted where the building achieves greater energy efficiency by a streamlined form.
- c) Create visual interest with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations.
- d) Ensure main entrances of residential and mixed-use buildings are level with the sidewalk. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off of the street.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.



**Guidelines a and b:** Decreasing heights and densities moving from the Town Centre to the Lower Town Centre, 30 metre tower separations, and stepping building forms



**Guidelines d, j, and h:** Ground-oriented units, entrances directly off of and level with the sidewalk, natural materials, and private outdoor space



**Guidelines c, d, g and k:** Windows and colour variations along the front elevation, weather protection above the entrance level with the sidewalk, and projecting signage

- g) Maximize transparency in commercial areas through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or other similar material on the ground floor façade of any building facing a street will not be accepted.
- h) Provide common outdoor amenity spaces for residents in mixed use and residential buildings. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each apartment unit with a private outdoor space, such as a balcony or ground level patio, where possible. Provide ground floor units with a semi-private front patio. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.
- i) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- j) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- k) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.



**Guidelines d and e:** Mixed-use development addressing both street frontages, with entrances and weather protection along both streets



**Guideline f:** Facade articulation and narrow store fronts with entrances at regular intervals



**Guideline k:** Commercial projecting signs

## PEDESTRIAN REALM & LANDSCAPE

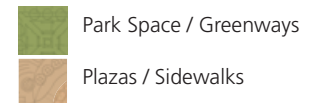
- a) Site new development back from the intersection of Johnston Road and North Bluff Road to create a “Gateway” to the City. Design this space as a plaza and incorporate public art and/or a Town Centre welcome feature or sign.
- b) Create a “Heart” in the Town Centre by setting buildings back on all four corners at the intersection of Johnston Road and Russell Avenue and building a Central Plaza or Park in the block bounded by North Bluff Road, Russell Avenue, Johnston Road, and Foster Street. Enhance these public spaces with public art and opportunities for programmed uses.
- c) Provide continuous commercial frontage along both sides of Johnston Road, and allow additional street-front commercial along North Bluff Road and portions of Thrift and Russell Avenues adjacent to Johnston Road. Bicycle parking adjacent to retail entrances is encouraged.
- d) Improve the public realm along Johnston Road in accordance with the results of the Johnston Road Upgrade. Widen sidewalks throughout the Town Centre (minimum 1.8-2.5 metres), and install ornamental street lamps and street furniture. Provide space for patios adjacent to commercial retail frontages. Curb let-downs are to be designed to accommodate wheelchair and scooter movement.
- e) Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realm that is reflective of the local landscape and heritage.
- f) Site buildings to create outdoor public spaces and through-block walking connections, as these spaces create opportunities for a variety of pedestrian-oriented activities and uses. Special attention should be paid to establishing linear park connections from Russell Avenue north and Johnston Road west to the Central Plaza/Park, and west from the intersection at Russell Avenue and Foster Street to Centennial Park.



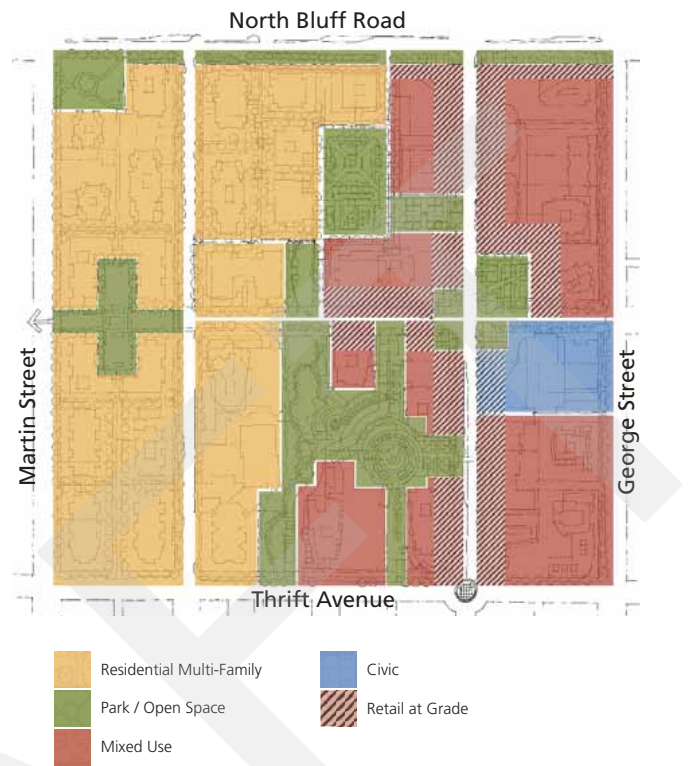
**Guideline a:** Setting buildings back from the corner of North Bluff Road and Johnston Road creates a plaza with public art and a gateway feature or sign



**Guidelines b and f:** Building siting creates a fine-grained network of pathways, parks, and open spaces



- g) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public to private areas.
- h) Increase the quantity, density, and diversity of trees planted in the Town Centre, with both deciduous and coniferous tree species. Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Landscape design should employ CPTED safety principles.
- i) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant and native species requiring minimal irrigation. Use lawn alternatives such as groundcovers or sedums to limit watering requirements and increase biodiversity. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.
- j) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's Integrated Storm Water Management Plan (ISWMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.
- k) Provide sufficient on-site illumination for pedestrian/vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid "light spill" onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.



**Guideline c:** Street-level commercial development focused along Johnston Road, with residential development focused at the west side of the Town Centre



**Guidelines f, h, and i:** Through-block pedestrian connections provide a variety of pedestrian routes, public spaces create opportunities to gather, and trees provide shade

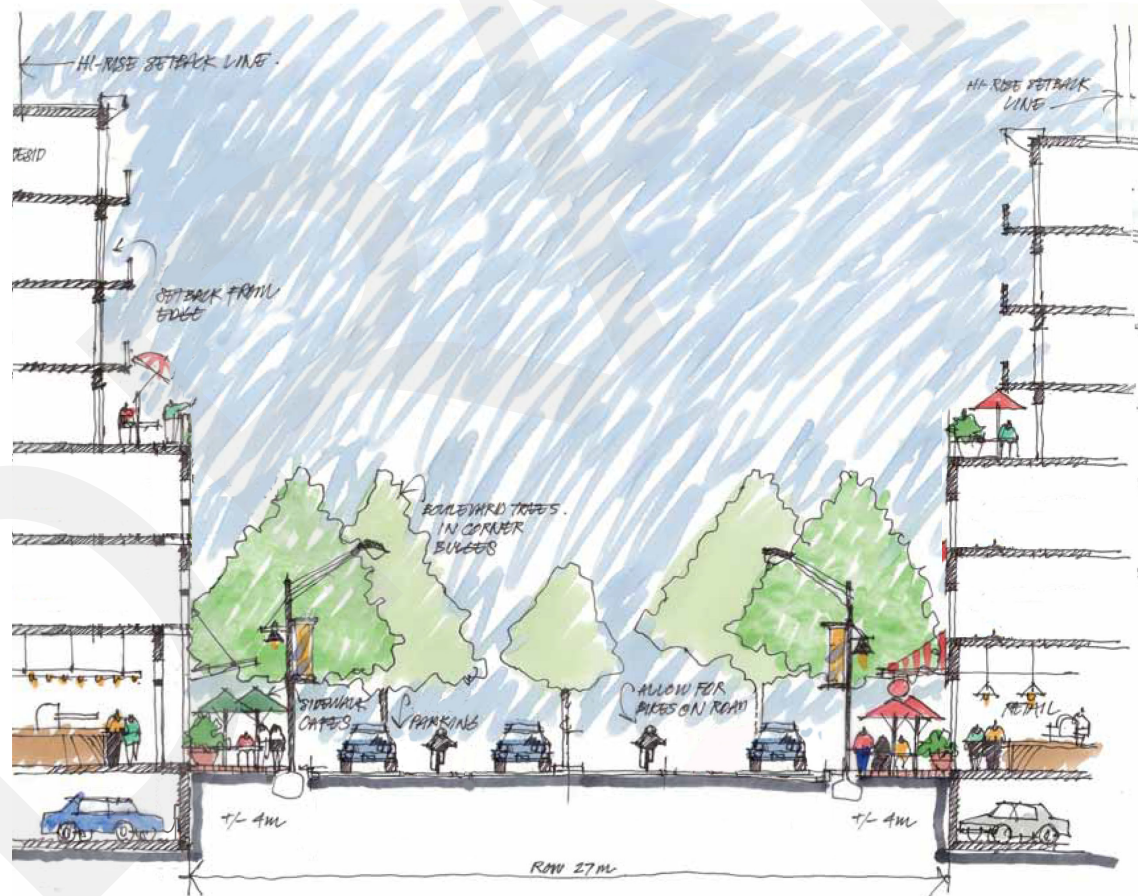
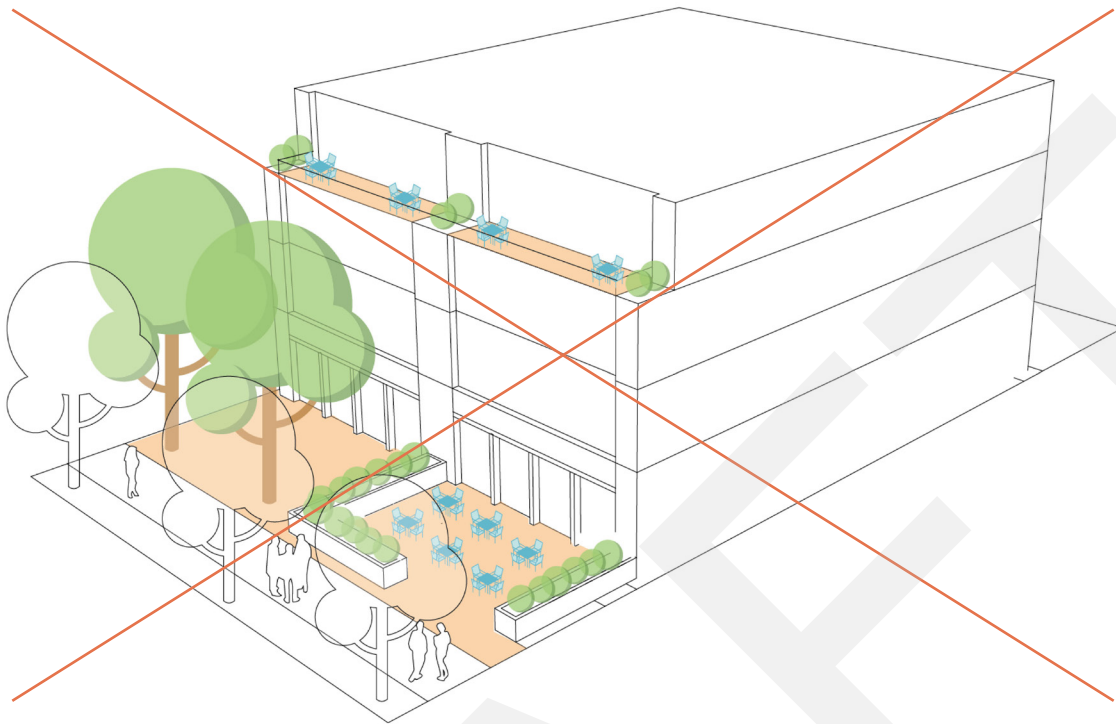


Diagram showing illustrative example of mixed-use building step-backs and pedestrian realm along Johnston Road



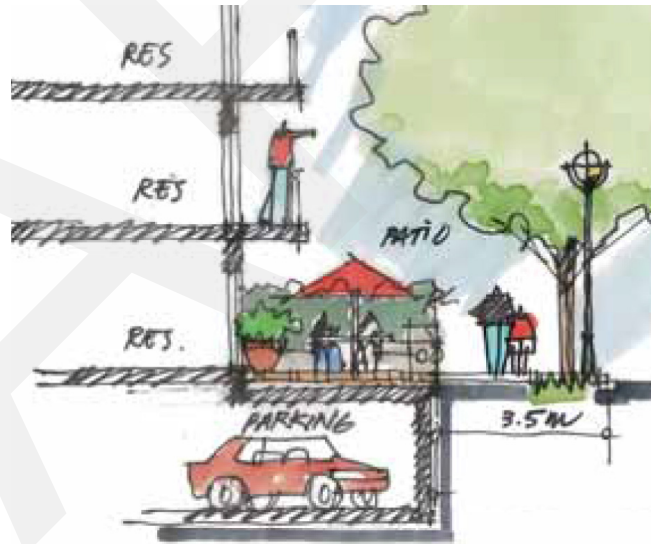
Section showing illustrative example of residential step-backs, parking, pedestrian realm and use of street fronting outdoor amenity spaces

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible, separate from pedestrian entrances. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.



**Guideline a:** Parkade entrance softened by landscaping



**Guideline b:** Below grade, off-street parking

*new!*

## 21.4 Town Centre Transition Development Permit Area

The Objectives of this Development Permit Area are to:

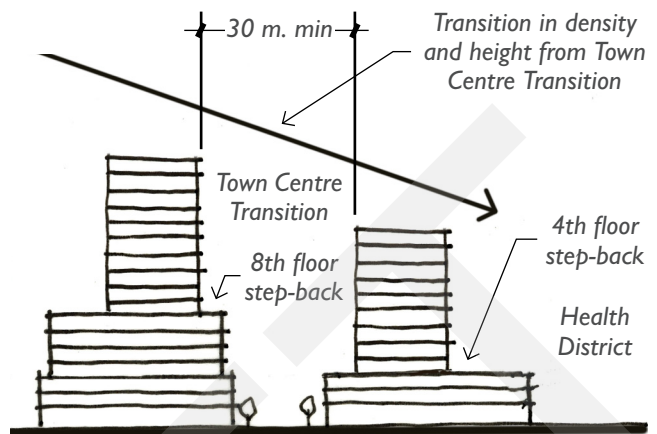
- Reinforce the Town Centre Transition–Mixed Use, and Town Centre Transition–Residential areas as a destination for locally-oriented shops and culture and residential development.
- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water, and reduce GHGs

*A Vibrant Destination*

White Rock  
ELKS  
CLUB

## BUILDINGS

- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design. The tallest and densest developments are to be located along North Bluff Road, with heights and densities decreasing to the south, east and west as outlined in Figures 8 and 9. Vary heights, rooflines, and massing to minimize impacts to solar exposure enjoyed by adjacent buildings and open spaces.
- b) Set buildings back from the property line at least 3 metres, with a further stepping above the second or third floor in mixed-use areas, and above the fourth floor in residential areas. Consider an additional step back above the seventh floor. Tower portions of all buildings should be slim and be set back a minimum of 3 metres from the edge of the podium level to minimize view impacts and shading, and to facilitate a minimum separation of 30 metres between towers. Fewer steps in building form may be accepted where the building achieves greater energy efficiency through a streamlined form.
- c) Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non street-facing elevations shall be treated with the same architectural details as the street-facing elevations.
- d) Ensure main entrances of residential and mixed-use buildings are level with the sidewalk. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off of the street.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-



**Guidelines a and b:** Decreasing heights and densities moving from the Town Centre Transition area to the Health District, 30 metre tower separations, and stepping building forms



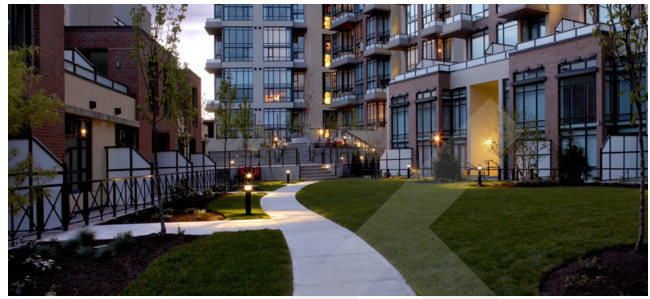
**Guidelines d and e:** Mixed-use development addressing both street frontages, with entrances and weather protection along both streets



**Guideline f:** Facade articulation and narrow store fronts with entrances at regular intervals

scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.

- g) Maximize transparency in commercial areas through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or other similar material on the ground floor façade of any building facing a street will not be accepted.
- h) Provide common outdoor amenity spaces for residents in mixed-use and residential buildings. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each apartment unit with a private outdoor space, such as a balcony or ground-level patio, where possible. Provide ground-floor units with a semi-private front patio. Incorporating green roofs to manage stormwater, reduce the urban heat island effect, and contribute to biodiversity is encouraged.
- i) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- j) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- k) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.



**Guidelines c and h:** Creating visual interest with varied massing, windows, and landscaped areas, and common outdoor space for residents



**Guidelines g and k:** Transparency, mullions and frames, and projecting signs create visual interest at the pedestrian scale

## PUBLIC REALM & LANDSCAPE

- a) Provide continuous commercial frontage along North Bluff Road and along portions of George Street, north of Russell Avenue. Bicycle parking adjacent to retail entrances is encouraged.
- b) Improve the public realm with widened sidewalks throughout the Town Centre Transition -Mixed Use and Town Centre Transition -Residential (minimum 1.8-2.5 metres). Provide space for patios adjacent to commercial frontages. Install ornamental street lamps and street furniture, and provide space for patios adjacent to commercial frontages. Curb let-downs are to be designed to accommodate wheelchair and scooter movement.
- c) Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realms that is reflective of the local landscape and heritage.
- d) Site buildings to create outdoor public spaces and through-block walking connections, creating opportunities for a variety of pedestrian-oriented activities and uses.
- e) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and the urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public to private areas.
- f) Increase the quantity, density, and diversity of trees planted in the Town Centre Transition-Mixed Use and Town Centre Transition-Residential, with both deciduous and coniferous tree species. Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Landscape design should employ CPTED principles.
- g) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought-tolerant and native species requiring minimal irrigation. Use lawn alternatives such as groundcovers or sedums to limit watering requirements and increase biodiversity. The planting of



**Guidelines d, f, and g:** Through-block pedestrian connections provide a variety of pedestrian routes, public spaces create opportunities to gather, and trees provide shade



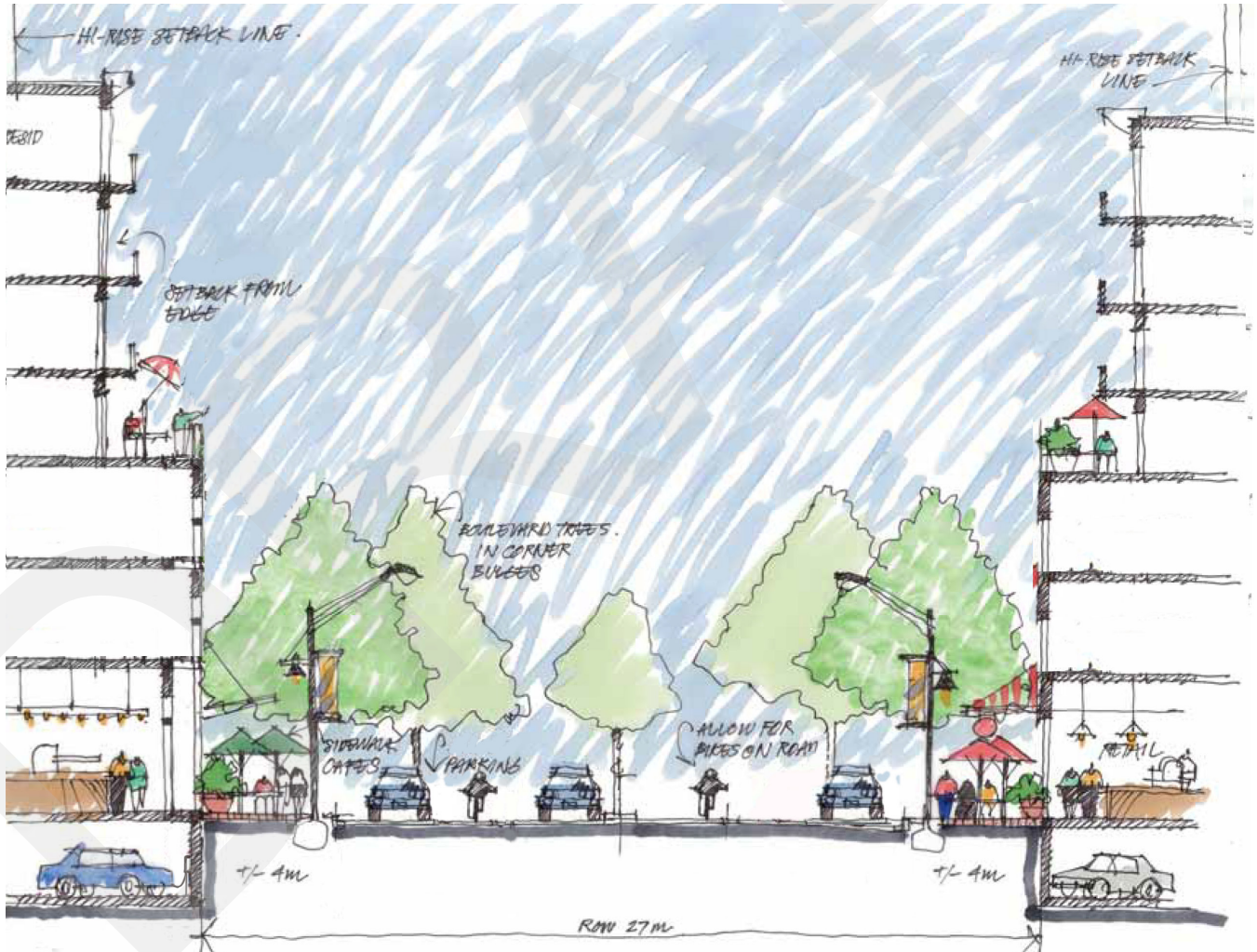
**Guideline e:** Landscaping elements establish a transition from public to private spaces

hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.

- h) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's Integrated Stormwater Management Plan (ISMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.
- i) Provide sufficient on-site illumination for pedestrian/vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid "light spill" onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar power, are encouraged.



**Guideline i:** Duckbill lighting is used to highlight signs and building entrances in retail areas



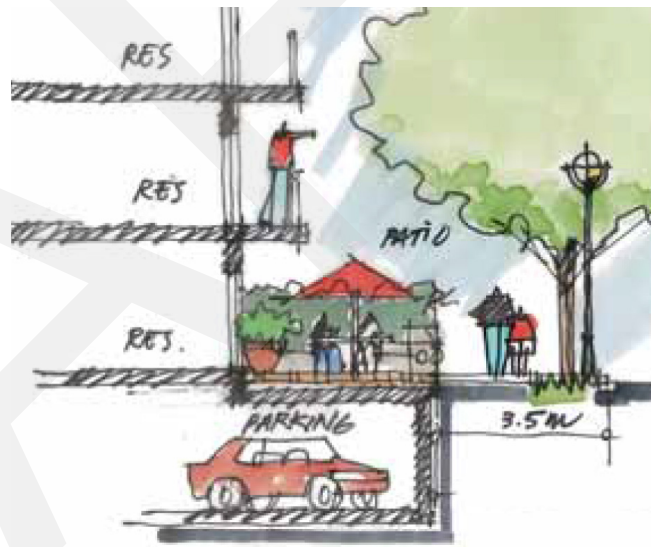
This section illustrates a mixed-use building step-back above the 3rd storey, and continuous street-oriented commercial frontage with weather protection over entrances. Street improvements create a cohesive streetscape with furnishings as well as patio spaces. Short term parking is provided on the street, and long term parking is provided underground.

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible, separate from pedestrian entrances. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building and be designed to dampen noise where required.



**Guideline a:** Parking entrances are designed to be discrete and softened by landscape



**Guideline b:** Below grade, off-street parking

## 21.5 Lower Town Centre Development Permit Area

The objectives of this Development Permit Area are to:

- Reinforce the Lower Town Centre as a destination for locally-oriented shops and culture
- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Promote views to the water



Local Life and Culture

## BUILDINGS

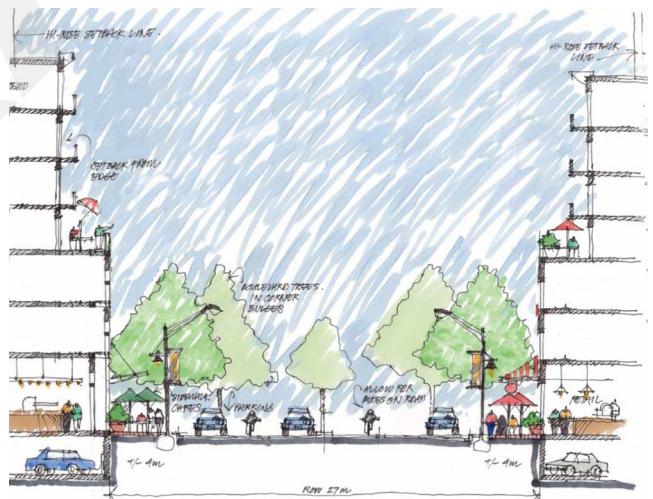
- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design, with height transitions as outlined in Figure 9. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.
- b) Set buildings back from the property line at least 3 metres to provide enough space for front patios next to the sidewalk in mixed-use areas and landscaped front gardens in residential areas. A further setback above the second or third floor along Johnston Road is also required. Tower portions of all buildings should be slim and be set back a minimum of 3 metres from the edge of the podium level to minimize view impacts and shading, and to facilitate a minimum separation of 30 metres between towers.
- c) Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations.
- d) Ensure main entrances of residential and mixed-use buildings are level with the sidewalk. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off of the street.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.



**Guidelines a and e:** Addressing both street frontages and stepping down and back to create a smoother transition to the adjacent, lower density residential area



**Guidelines d and e:** Mixed-use development addressing both street frontages, with entrances and weather protection along both streets

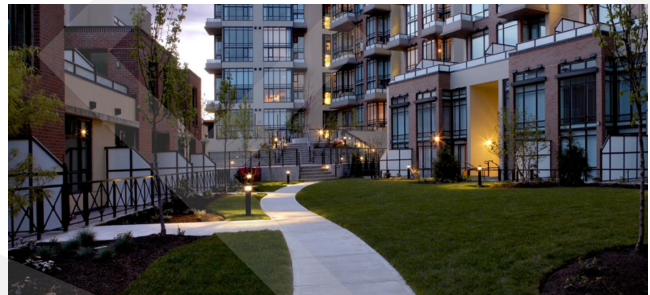


**Guidelines a, b and h:** Buildings are set back from the property line to allow for patio spaces at the sidewalk, with additional stepping above to create outdoor spaces and lessen view and shadowing impacts

- g) Maximize transparency in commercial areas through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or other similar material on the ground floor façade of any building facing a street will not be accepted.
- h) Provide common outdoor amenity spaces for residents in mixed use and residential buildings. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each apartment unit with a private outdoor space, such as a balcony or ground level patio, where possible. Provide ground floor units with a semi-private front patio. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.
- i) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- j) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- k) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.



**Guideline f:** Facade articulation and narrow store fronts with entrances at regular intervals



**Guidelines c and h:** Creating visual interest with varied massing, windows, and landscaped areas, and common outdoor space for residents



**Guidelines g and k:** Transparency, mullions and frames, and projecting signs create visual interest at the pedestrian scale

## PUBLIC REALM & LANDSCAPE

- a) Provide continuous commercial frontage along Johnston Road and Pacific Avenue. Bicycle parking adjacent to retail entrances is encouraged.
- b) Improve the public realm with widened sidewalks throughout the Lower Town Centre (minimum 1.8-2.5 metres). Install ornamental street lamps and street furniture and provide space for patios adjacent to commercial frontages. Curb let-downs are to be designed to accommodate wheelchair and scooter movement.
- c) Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realm that is reflective of the local landscape and heritage.
- d) Site buildings to create outdoor public spaces and through-block walking connections, creating opportunities for a variety of pedestrian-oriented activities and uses. Special attention should be paid to establishing destination or gathering space at Five Corners. Enhance these public spaces with public art and opportunities for programmed uses.
- e) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public to private areas.
- f) Increase the quantity, density, and diversity of trees planted in the Lower Town Centre, with both deciduous and coniferous tree species. Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Landscape design should employ CPTED principles.



**Guidelines b and d:** Wide sidewalks and an outdoor public space with seating and dining areas

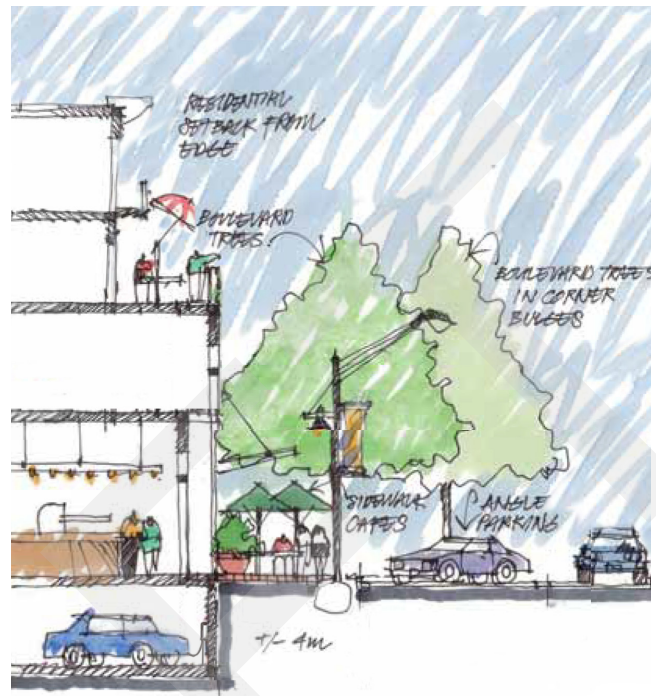


**Guideline i:** Duckbill lighting is used to highlight signs and building entrances in retail areas



**Guideline e:** Landscaping elements establish a transition from public to private spaces

- g) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant and native species requiring minimal irrigation. Use lawn alternatives such as groundcovers or sedums to limit watering requirements and increase biodiversity. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.
- h) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's Integrated Storm Water Management Plan (ISWMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.
- i) Provide sufficient on-site illumination for pedestrian/vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid "light spill" onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.



This section shows a mixed-use building step-back above the 2nd storey, and continuous street-oriented commercial frontage with weather protection over entrances. Street improvements create a cohesive streetscape with furnishings as well as patio spaces. Short term parking is provided on the street, and long term parking is provided underground.



This section illustrates 4-storey residential apartments. Entrances are level with the sidewalk and ground floor units are ground-oriented with semi-private patio spaces defined by low shrubs and low fences. Residential parking is provided underground. Shared rooftop amenity spaces and balconies are encouraged.

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible, separate from pedestrian entrances. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.



**Guideline a:** Parking entrances are designed to be discrete and softened by landscape

## 21.6 Waterfront Development Permit Area

The objectives of this Development Permit Area are to:

- Reinforce the Waterfront as a seaside village unique within the region
- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Enhance the unique waterfront environment



*The Soul of Our Community*

## BUILDINGS

- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.
- b) Set buildings back from the property line at least 1.5-2.5 metres to provide enough space for front patios next to the sidewalk in mixed use areas and landscaped front gardens in residential areas. Provide further step back above the second floor. The ground floor in residential developments along Marine Drive shall be designed as flex-space for potential future use as retail or office space.
- c) Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations.
- d) Ensure main entrances of residential and mixed-use buildings are level with the sidewalk. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off of the street.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.



**Guidelines b and d:** Patio spaces contribute to making the Waterfront a full day destination, with entrances level with the sidewalk to support a barrier free environment



**Guidelines b, c, and d:** Vibrant accent colours help create a welcoming streetscape, with a streetside patio under awnings

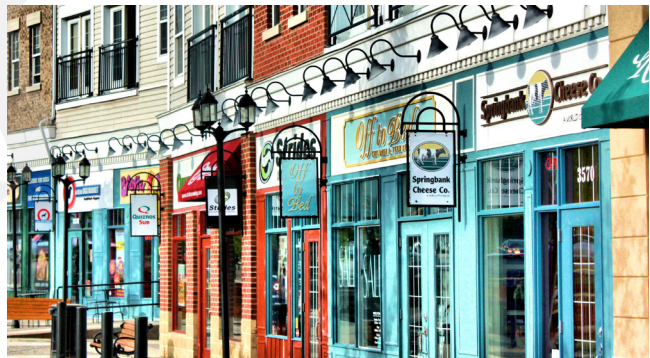


**Guideline j:** Natural materials with rich colours and bold patterns as accents

- g) Maximize transparency in commercial areas through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or other similar material on the ground floor façade of any building facing a street will not be accepted.
- h) Provide common outdoor amenity spaces for residents in mixed-use and residential buildings. Shared roof decks with views to the waterfront and other amenities are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each apartment unit with a private outdoor space where possible. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.
- i) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- j) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- k) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.



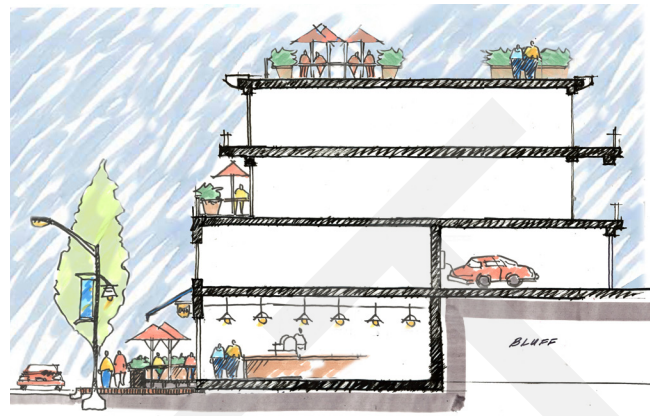
**Guidelines d and k:** Varied massing creates visual interest, entrances are level with the sidewalk, and signage contributes to the vibrant character of the architecture and setting



**Guidelines g and k:** Projecting signs and windows with mullions and frames create visual interest

## PUBLIC REALM & LANDSCAPE

- a) Provide continuous commercial frontage along Marine Drive between Oxford and Foster Streets and between Balsam and Maple Streets. Residential developments in other areas along Marine Drive should incorporate flex spaces at grade level that will allow for future conversion for use as retail or office space. Bicycle parking adjacent to retail entrances is encouraged.
- b) Improve the public realm with widened sidewalks along Marine Drive (minimum 1.8 metres). Install ornamental street lamps and street furniture and provide space for patios adjacent to commercial frontages. Curb let-downs are to be designed to accommodate wheelchair and scooter movement.
- c) Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realm that is reflective of the local landscape and heritage.
- d) Site buildings to enhance potential public spaces in unopened road ends (Hill Walks) and north-south pedestrian connections to and from the Waterfront. Enhance these public areas with public art and opportunities for programmed uses.
- e) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public to private areas.
- f) Plant both deciduous and coniferous tree species, and ensure all trees are planted with sufficient soil volume using soil cells where appropriate. Incorporate diverse native shrub layers below trees to intercept stormwater. Landscape design should employ CPTED safety principles.
- g) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's Integrated Storm Water Management Plan (ISWMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.



**Guidelines a and b:** Commercial frontages create a destination and add to the vibrancy of Marine Drive, with widened sidewalks improving the pedestrian experience



**Guidelines a and b:** Continuous commercial frontage with awnings and seating areas, and widen sidewalks



**Guideline c:** Public art should reflect and build upon a sense of place

- h) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.
- i) Provide sufficient on-site illumination for pedestrian/ vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid “light spill” onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.



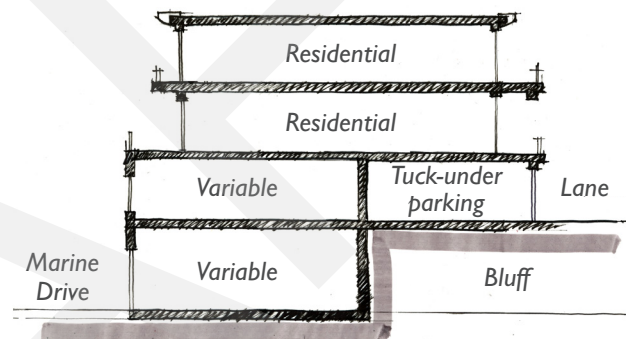
**Guideline h:** Areas not covered by paving are landscaped, with a diverse, native, drought tolerant shrub layer

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible, separate from pedestrian entrances. Avoid vehicular access from Marine Drive unless no other option is available. If a parkade or garage entrance faces a street, it should be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade or garage entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.



**Guideline a:** Parking entrances are designed to be discrete and softened by landscape



**Guideline a:** Build into bluff to create second storey tuck-under parking where possible

new!

## 21.7 Health District Development Permit Area

The objectives of this Development Permit Area are to:

- Support health-related and compatible land uses, including clinics, wellness centres, supportive housing, childcare centres and allied health services.
- Promote walkability, accessibility, and inclusive design for all ages and abilities.
- Enhance the public realm with spaces that support physical and mental well-being.
- Integrate sustainable design and passive energy strategies.
- Foster a cohesive district identity through consistent architectural and landscape character.
- Ensure development contributes to a safe, welcoming, and resilient health-focused community.



Community Wellness

## BUILDINGS

- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design. Locate the tallest and densest developments near key intersections or transit hubs within the Health District, with heights decreasing to the east as outlined in Figure 9.
- b) Set buildings back from the property line at least 3 metres, with a further stepping above the second or third floor in mixed-use areas and above the fourth floor in residential areas. Consider an additional step back above the seventh floor unless streamlined forms improve energy performance.
- c) Create visual interest with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non-street-facing elevations shall be treated with the same architectural details as the street-facing elevations.
- d) Ensure main entrances of residential and mixed-use buildings are level with the sidewalk. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off the street.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.
- g) Maximize transparency in commercial areas through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or similar material on the ground floor façade of any building facing a street will not be accepted.



**Guideline c:** Visual interest created with a mix of colours, materials, and textures

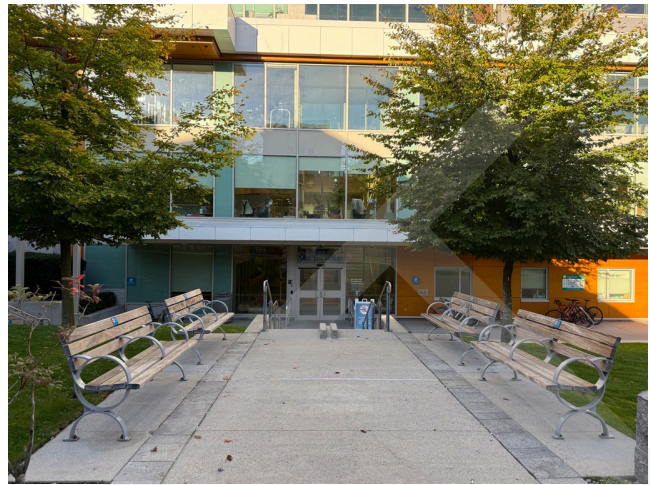


**Guidelines d and e:** Building entrances are level with the sidewalk and weather protected with architectural detailing along street frontages and walkways



**Guideline g:** Extensive use of glazing with character to create visual interest

- h) Provide common outdoor amenity spaces for residents in mixed-use and residential buildings. Shared roof decks with gardens are encouraged. Incorporate dining and seating areas, outdoor cooking facilities, play areas, communal vegetable gardens, and appropriate landscaping. Provide each apartment unit with a private outdoor space, such as a balcony or ground-level patio, where possible. Provide ground-floor units with a semi-private front patio. Incorporating green roofs to manage stormwater, reduce the urban heat island effect, and contribute to biodiversity is encouraged.
- i) Follow passive solar design principles and orient buildings to maximize views and solar collection. Design roofs to optimize winter solar gain and control summer solar gain with shading devices or overhangs. Maximize passive ventilation and cooling through building orientation.
- j) Incorporate west coast design elements using natural materials such as brick, stone, concrete, exposed heavy timber, and/or steel. Use rich natural tones reflecting the landscape, with brighter colours only as accents.
- k) Integrate commercial signage with the building and/or landscaping. Signage shall be pedestrian-scaled, coordinated across developments, and compatible with adjacent properties. Use natural materials and projecting signs where possible.



**Guideline j:** Natural tones and materials to reflect the surrounding landscape



**Guideline k:** Pedestrian-scaled, projecting signage coordinated between developments

## PEDESTRIAN REALM & LANDSCAPE

- a) Site new development to create welcoming gateways to the Health District. Design these spaces as plazas and incorporate public art and district signage.
- b) Create central gathering spaces or plazas near key intersections to serve as the 'Heart' of the Health District. Enhance these spaces with public art and opportunities for health-related programming.
- c) Provide continuous active frontages along key corridors, with commercial and health-supportive uses. Bicycle parking adjacent to entrances is encouraged.
- d) Improve the public realm with widened sidewalks (minimum 1.8–2.5 metres), ornamental lighting, street furniture, and patio spaces. Ensure curb let-downs accommodate wheelchairs and scooters.
- e) Ensure consistency in street trees, plant materials, and public art to create cohesive streetscapes. Reflect the district's health and wellness identity.
- f) Site buildings to create outdoor public spaces and through-block walking connections. Establish linear park connections to nearby green spaces and health facilities.
- g) Use light-coloured reflective paving materials to reduce heat absorption. Landscape all non-built areas and use planting to transition between public and private spaces.
- h) Increase the quantity, density, and diversity of trees using native species. Ensure sufficient soil volume and incorporate shrub layers for stormwater interception. Apply CPTED principles for safety.
- i) Select trees and plants that support passive solar gain, ventilation, and cooling. Use drought-tolerant and native species. Avoid hedges adjacent to sidewalks unless screening utility areas.
- j) Incorporate Low Impact Development techniques for stormwater management, such as bioswales, cisterns, and permeable paving. Use narrower lanes and porous asphalt where appropriate.
- k) Provide sufficient on-site lighting for safety and visibility. Highlight entrances and avoid light spill. Use renewable energy-powered lighting systems where feasible.



**Guideline b:** Central gathering spaces with public art promoting health-related topics



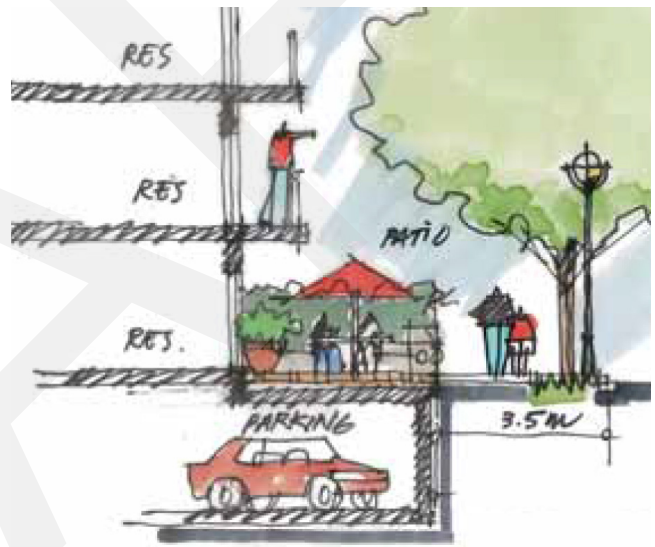
**Guideline c:** Bicycle parking near building entrances

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible, separate from pedestrian entrances. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.



**Guideline a:** Parking entrances are designed to be discrete and softened by landscape



**Guideline b:** Below grade, off-street parking



## 21.8 Multi-Family Development Permit Area

The objectives of this Development Permit Area are to:

- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock



*Life Near the Centre*

## BUILDINGS

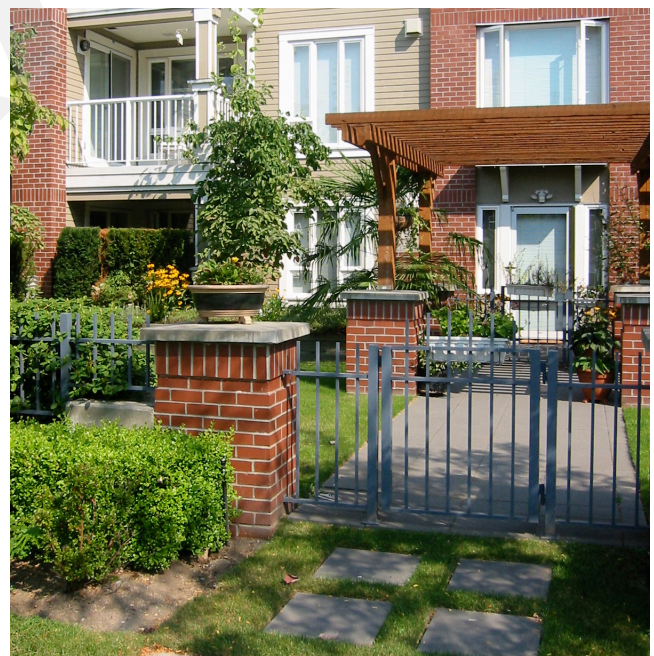
- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design, with height transitions as outlined in Figure 9 in applicable areas. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.
- b) Set buildings back from the property line at least 3 metres to provide enough space for gardens and shade trees in the front yard. Include a further step back above the fourth floor.
- c) Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Townhouse developments are encouraged to provide for individuality from site to site and unit to unit, and to vary the front set-back between units. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations.
- d) Ensure the main entrances of residential apartment buildings are level with the sidewalk to create a barrier free environment for aging in place. Townhouses may have elevated patios and entrances. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances. Residential units on the ground floor should be ground-oriented.
- e) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- f) Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.



**Guidelines a and e:** Addressing both street frontages and stepping down and back to create a smoother transition to the adjacent, lower density residential area



**Guidelines a and c:** Materials, roof lines and front entrances create visual interest and soften transitions between multi-family development areas and single family areas

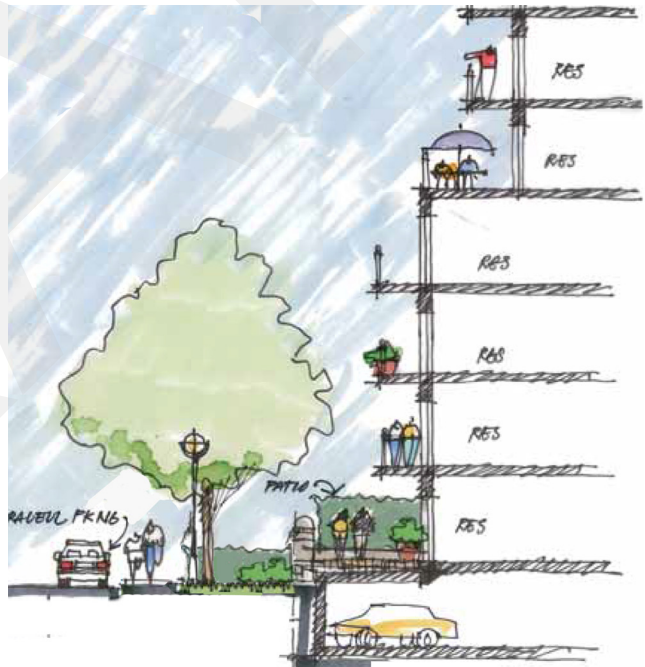


**Guideline d:** Ground-oriented residential units

- g) Provide shared outdoor amenity spaces for residents in mixed-use and residential buildings. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each residential unit with a private outdoor space where possible. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.
- h) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- i) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- j) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.
- k) Blocks of side-by-side townhouses are limited to a maximum of eight contiguous units. Lot consolidation to allow for street-fronting townhouse developments are encouraged.



**Guidelines c and i:** Massing and natural materials are varied to break up the building front and create visual interest



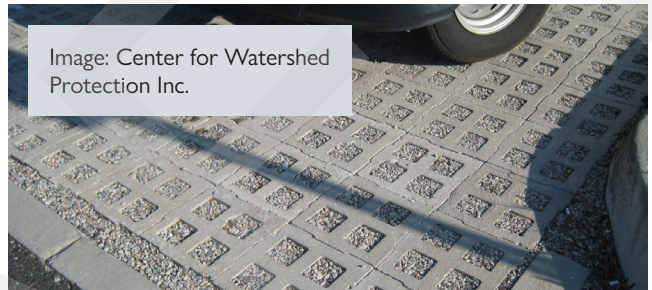
**Guidelines b and g:** Section showing apartment stepped apartment building form and outdoor amenity spaces

## PUBLIC REALM & LANDSCAPE

- a) Improve the public realm with widened sidewalks (minimum 1.8 metres). Plant street trees and design curb let-downs to accommodate wheelchairs and scooters.
- b) Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realm that is reflective of the local landscape and heritage.
- c) Site buildings to create through-block walking connections. These will create opportunities for a variety of pedestrian-oriented activities and a finer-grained street grid. Special attention should be paid to establishing a linear park connection between the Town Centre and Centennial Park. Enhance these public spaces with public art and opportunities for programmed uses.
- d) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public, to semi-public, to private areas.
- e) Increase the quantity, density, and diversity of trees planted. Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Projects should be designed to allow for the retention of large, mature, healthy trees, and landscape design should employ CPTED safety principles.
- f) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.



**Guidelines c and h:** A pedestrian pathway through a development, with lighting



**Guideline c:** Permeable pavers



**Guideline d:** Areas not covered by paving are landscaped, with a diverse, native, drought tolerant shrub layer

- g) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's ISWMP. This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.
- h) Provide sufficient on-site illumination for pedestrian/ vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid "light spill" onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.



**Guideline g:** On-site, natural stormwater management

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate parkade entrances at the rear or side of buildings where possible and separate from pedestrian entrances. Vehicular access from North Bluff Road will only be considered when alternative access is not available. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Use a single internal vehicular access for townhouse developments where possible, with a shared parkade or individual garages. Provide landscaped areas between garages in townhouse developments that have multiple direct vehicular accesses from the street.
- c) Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.
- d) Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- e) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.



**Guideline a:** Parking entrances are designed to be discrete and softened by landscape

## 21.9 Neighbourhood Commercial Development Permit Area

The objectives of this Development Permit Area are to:

- Encourage complete neighbourhoods with attractive, comfortable, well-connected, pedestrian-oriented environments
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock

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& CORNER



Neighbourhood Hubs

## BUILDINGS

- a) Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.
- b) Ensure commercial space is located in a highly visible location on the ground floor. Residential and retail entrances shall be clearly distinguished between, and weather protection shall be provided over all entrances and along all commercial frontages. Residential units on the ground floor should be ground-oriented with entrances directly off of the street.
- c) Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations. Ensure all main entrances are level with the sidewalk.
- d) Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.
- e) Optimize opportunities for creating through-block walking connections during site planning, and provide walkways, stairs, and other pedestrian pathways where possible.
- f) Maximize transparency in the commercial portion of the development through extensive use of glazing at grade level, and consider use of mullions and frames to create character and visual interest. Reflective glass or other similar material on the ground floor façade of any building facing a street will not be accepted.



**Guidelines b and c:** Retail and residential entrances and uses are clearly distinguished through entry treatment and architectural articulation



**Guidelines b and d:** Mixed-use development addressing both street frontages, with entrances and weather protection along both streets



**Guidelines b, g, and i:** Ground-oriented units, entrances directly off of and level with the sidewalk, natural materials, and private outdoor space

- g) Provide common outdoor amenity spaces for residents where possible. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping. Provide each apartment and/or townhouse unit with a semi-private outdoor space, such as a balcony or ground level patio, where possible. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.
- h) Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.
- i) Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.
- j) Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.



**Guideline j:** Commercial projecting signs are scaled to the pedestrian and are designed to complement the architecture of the building



Image: Brew Books

**Guideline h:** Transparency in commercial spaces

## PUBLIC REALM & LANDSCAPE

- a) Site buildings to create a gathering or patio space on the property. These spaces will create opportunities for a variety of retail-oriented activities and local gathering spaces. Retractable awnings, pergolas, or other overhead structures for weather protection are encouraged for outdoor seating areas. Enhance these public spaces with public art and opportunities for programmed uses. Use landscaping to ensure privacy of residential units and outdoor spaces from public outdoor amenity spaces associated with the retail use.
- b) Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public to private areas.
- c) Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Projects should be designed to allow for the retention of large, mature, healthy trees, and landscape design should employ CPTED principles.
- d) Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.
- e) Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City's ISWMP. This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.
- f) Provide sufficient on-site illumination for pedestrian/vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid "light spill" onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.



**Guideline a:** Outdoor patio seating areas defined by landscape edges and screened from adjacent residential uses



**Guidelines c and d:** Areas not covered by paving are landscaped, with a diverse, native, drought tolerant shrub layer



**Guideline f:** Lighting is used to highlight signs and building entrances while avoiding light-spill onto residential uses

## PARKING & FUNCTIONAL ELEMENTS

- a) Locate vehicular accesses at the rear or side of buildings where possible, separate from pedestrian entrances. Provide vehicular access from the lane or a single shared driveway from the street when no lane exists. If a parkade or garage entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade or garage entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.
- b) Provide off-street parking below grade or enclosed within a building where possible, with the exception of some visitor parking spaces short-term commercial parking spaces. Ensure buildings are accessible from parkades for those with mobility impairments.
- c) Provide sufficient space for garbage, recycling, and composting within parkades where possible. These areas are to be located so that they are convenient for users and accessible for waste/recycling/compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.
- d) Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.




**Guideline a:** Parking entrances are designed to be discrete and softened by landscape



## 21.10 Mature Neighbourhood SSMUH Development Permit Area

The objectives of this Development Permit Area are to:

- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment
- Ensure the compatibility of infill development (i.e. small-scale multi-unit housing) within established neighbourhoods
- Enhance quality of life
- Conserve energy and water and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock



*Our Residential Neighbourhoods*

## SITE CONTEXT

- a) Neighbourhood Connectivity—Design the site to enhance the pedestrian connections in the area. Site buildings to create through-block walking connections where appropriate. Incorporate shared pedestrian accesses where possible to minimize impervious areas.
- b) Streetwall Continuity—Avoid visually impermeable fencing or other monolithic features along publicly visible edges of the site.
- c) Natural Features—Integrate the development with existing natural features topography and vegetation.
- d) Habitat Connection and Preservation—Connect new landscaped areas and fragmented habitat to networks of open space and larger habitat corridors wherever possible.
- e) Climate and Comfort—Maximize the benefits of sun exposure to public open spaces, nearby buildings, and dwelling units, while providing some shade for respite from heat. Limit building element projections into setback areas, streets, and amenity areas to protect solar access.



**Guideline a:** Neighbourhood connectivity



**Guideline b:** Streetwall continuity

## SITE LAYOUT & LANDSCAPE

- a) Relationship to Grade—Limit the height and use of retaining walls, particularly along street frontages, parks, open spaces, ravines and other areas of the public realm. Site and orient buildings in a way that respects and works with topographical features.
- b) Hierarchy of Spaces—Define the spaces that are public from those that are private with elements such as: grade changes, fencing, landscaping, and other features.
- c) Private Common Spaces—Integrate usable private outdoor common spaces into the site layout for gardens and other activities that promote sociability and neighbourliness between residents. Provide generous and well-designed open spaces that balance screening for privacy, while maintaining sightlines to the street and attractive interfaces with the surrounding spaces.
- d) Walking Connections—Connect main entrances to small-scale multi-unit housing units to public sidewalks, trails, and parking areas.
- e) Separate Access—Consider separate accesses for small-scale multi-unit housing units that are located on corner lots or that have street and lane accesses.
- f) Rear Access—For rear-facing units, clearly identify the location of the rear entrance with a walkway that incorporates a landscape border, distinct surface treatments, and/or an entry trellis or gate.
- g) Vehicle Access—Minimize paved areas with narrow, shared vehicular accesses. Separate accesses are considered for house-plexes that are located on corner lots or that have street and lane accesses.
- h) Safe Vehicle Movement—Consider the design and layout of driveways to ensure safe and efficient vehicle maneuvering.
- i) On-Site Parking—Promote the use of on-site parking and garages over street parking by providing adequately sized parking spaces for residents' vehicles at or above minimum Zoning Bylaw requirements, ensuring they are well-designed and easily accessible.
- j) Siting Parking—Provide access to parking from a secondary street or lane, wherever possible. Incorporate parking into the natural landscape where feasible to reduce the need for lot grading.



**Guideline b:** Hierarchy of spaces



**Guideline c:** Private common spaces

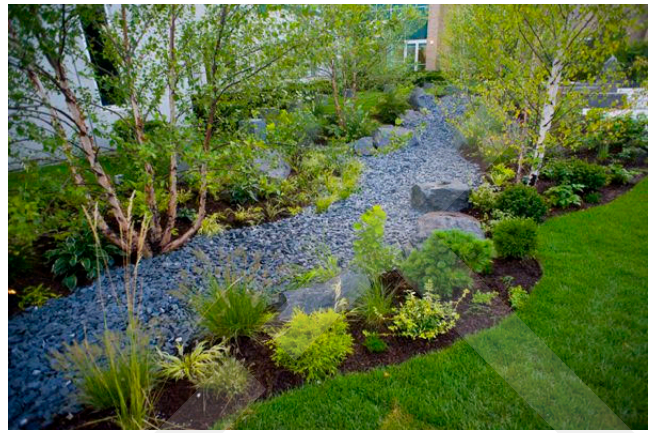


**Guideline e:** Separate access



**Guideline i:** On-site parking using pervious materials

- k) Parking & Pervious Area—Minimize paved areas with narrow, shared vehicular accesses.
- l) Stormwater Management—Site design should incorporate stormwater runoff mitigation features. This may include elements like raingardens, landscape strips for parking areas and permeable surface treatment, that are consistent with approved city policies and practices.
- m) Low Impact Development—Use Low Impact Development Techniques for stormwater management, where appropriate, in accordance with the City’s Integrated Storm Water Management Plan (ISWMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Design for narrower lanes, access roads, and driveways.
- n) Tree Health & Preservation—Preserve existing trees especially those that are large, mature, and healthy wherever possible and incorporate them into the landscape design. Ensure all new trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater.
- o) Tree Selection—Select tree species that will maximize passive solar gain, natural ventilation, and natural cooling. Prioritize the selection of native tree species such as Western Red Cedar, Common Douglas Fir, and Bigleaf Maple.
- p) Plant Selection (Smart Landscaping)—Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species.
- q) Shrubs and Hedges—Use only small shrubs, ornamental and turf grasses, herbaceous perennials, and groundcovers on city property and in areas within 2.0 m from the sidewalk, street curb, or road edge to maintain clear sightlines. Avoid the use of tall hedges, especially in areas adjacent to public property.
- r) Passive Solar Design—Follow passive solar design principles for the orientation and siting of buildings. Maximize passive ventilation and passive cooling through building orientation.



**Guidelines l and o:** Stormwater management and tree selection



**Guideline q:** Shrubs and hedges

- s) Cool Materials—Where impermeable materials are used, select light coloured reflective paving materials such as white asphalt or concrete for paths and driveways to reduce heat absorption and urban heat island effect.
- t) Signage Clarity—Provide visible and legible signage identifying building addresses at all entrances in a colour contrasting with the building, and either illuminated in periods of darkness or reflective for easy visibility at night. When building addresses are not visible from the street frontage, use directional address signs.
- u) Mailbox Siting—Consider the siting of mailboxes during site design so that a front-loading lockbox is easily accessible for mail delivery and complies with Canada Post standards.
- v) Space for Waste—Provide sufficient on-site space for garbage, recycling, and composting where appropriate. These areas are to be located so that they are convenient for users and accessible for waste/ recycling/compost collection and removal.



**Guideline s:** Cool materials

## BUILDINGS

- a) Varied Configurations—Consider alternatives to the traditional side-by-side and mirror-image house-plexes, such as front/rear and top/bottom layouts. Avoid repetition of plex-house designs and incorporate variations in massing from one building to the next and throughout a neighbourhood.
- b) Visual Interest—Create visual interest with a variety of cladding colours, materials and architectural details that break up the mass of the building and give each dwelling unit in a small-scale multi-unit housing development its own visual identity. Open verandas and peaked roofs are encouraged.
- c) Roof Design—Ensure that roof elements do not dominate the building, particularly on larger buildings. Where possible, minimize the visual impact of rooftop railings, screens, and accesses.
- d) Roof Orientation & Rooflines—Orient the slope of the roof in the same direction as the natural slope of the lot. Avoid roof and dormer pitches steeper than 4/12 for a gentle slope that helps protect views.
- e) Decks, Balconies & Patios—Create meaningful, private outdoor environments that respect the privacy of surrounding neighbours and are sensitive to the local context through features like decks, balconies, and groundfloor/rooftop patios.
- f) Weather Protection—Incorporate weather-protection to all main entrances. This may take the form of overhangs, recesses, or awnings. A covered entry is not required to have posts or a traditional porch appearance and should fit with the architectural style of the development.
- g) Garage Design and Placement—Ensure that garages do not dominate the front face of a building. If a garage faces a street, design it to be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and architectural emphasis. Use landscaping to screen and soften the appearance of a garage.



**Guideline b:** Visual interest



**Guidelines c and d:** Roof design, orientation, and rooflines



**Guideline g:** Garage design and placement

- h) Stairs—Design outdoor stairs with similar materials as the main building and integrate them with the building's architecture so they do not dominate the face of a building. Encourage stairs with offset runs and wider landings to break up the run's length. Use plantings and furnishings on the landings to create visual interest.
- i) Efficient Homes—Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer.
- j) Building Material—Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Avoid vinyl siding and stucco for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accent



**Guideline h:** Stairs

## 22. Development Permit Areas (Environmental)

Guidelines for the following designated Development Permit Areas (DPAs) are intended to protect the natural environment, ecosystems, and biological diversity in White Rock.

### 22.1 Overview & Authority

The *Local Government Act* authorizes local governments to designate areas in the Official Community Plan for several purposes including the protection of the natural environment, its ecosystems and biological diversity. Pursuant to section 488(1)(e)(h) (i)(j) of the *Local Government Act*, the City designates the following Development Permit Areas to achieve objectives for the protection of the natural environment and its ecosystems and biological diversity:

- Environmental (Floodplain) Development Permit Area
- Environmental (Watercourse) Development Permit Area
- Environmental (Ravine Lands and Significant Trees) Development Permit Area

The properties subject to the above Environmental Development Permit Areas are identified on Map C. The following Sections outline the information that is required to be submitted by a property owner or applicant prior to any development on a property, including rezoning, subdivision, demolition, excavation, tree removal and construction. Conditions of development and exemptions are also outlined. All definitions in the Province's Riparian Areas Regulation are applicable for the Environmental (Watercourses) Development Permit Area.

Variations- Council may, as part of a development permit, vary the property line setback or building height requirements of the Zoning Bylaw where it can be demonstrated that such variance is needed to facilitate the development proposal while attempting to satisfy the objective of preserving steep slopes, significant trees or environmentally sensitive features. Innovative site designs which are in harmony with existing healthy, mature trees and other natural features are encouraged.

### 22.2 Development Permit Exemptions

The following activities occurring in an Environmental Development Permit Area shall be exempt from the development permit application process:

1. Emergencies—all actions to resolve emergency situation must be reported immediately to the Engineering and Municipal Operations and Development Services departments, as well as the appropriate Federal/Provincial authorities. A Development Permit is not required to authorize procedures to prevent, control, or reduce flooding, erosion, or other immediate threats to life or public or private property, including:
  - a) Emergency actions for flood protection, erosion protection, and clearing of obstructions;
  - b) Emergency works to prevent, repair, or replace public utilities;
  - c) Clearing of an obstruction from a culvert or drainage flow;
  - d) Repairs to safety fences; and
  - e) Removal of a hazardous tree that presents a danger to the safety of persons or is likely to damage public or private property, as determined by an ISA Certified Tree Rick Assessor. A tree cutting permit will be required prior to removal.
2. Public Works and Services—the construction, repair and maintenance of works by the City or its authorized agents and contractors are exempt from the formal development approval process. However, development within Riparian Assessment Areas must be completed in accordance with a Riparian Areas Assessment.
3. Re-establishment of the Streamside Protection and Enhancement Area by planting native vegetation.

4. Removal of invasive non-native vegetation, provided that a vegetation management plan, prepared by a Qualified Environmental Professional, is provided to and approved by the City.
5. Existing approved developments, including previously approved Development Permits or variances still in effect that were approved prior to adoption of these policies. Modifications to these permits or variances may necessitate a new application.
6. Repairs and renovations to existing buildings and structures on existing foundations that involve no changes to the footprint of the building or structure.

## 22.3 Environmental (Floodplain) Development Permit Area

The purpose of this Development Permit Areas is to minimize damage in areas identified as having the potential for flood risk. To achieve this, the underside of the floor systems for habitable space in all buildings for residential occupancy shall be above the identified flood levels. All other development, including floor space ancillary to residential occupancies (such as parking areas) and floor space for commercial occupancies, may be permitted below the identified flood levels, provided that all electrical and mechanical equipment must be located above the identified flood levels. A Development Permit is not required for construction in accordance with these requirements. Development that is not consistent with these requirements is subject to the following conditions:

1. The applicants must submit stamped and sealed drawings for the building foundation from a qualified professional engineer. The foundation must be designed to prevent damage to habitable space in the event of a flood event.
2. The applicants must offer and enter into a covenant under Section 219 of the Land Title Act and agree to save harmless the City of White Rock in the event of damage to goods and materials by flood waters.
3. The applicants must submit a report from a qualified professional engineer indicating that the land may be used safely for the use intended, and that electrical and mechanical equipment can be safely located below the identified flood level (where applicable).
4. Exemptions may be considered on a site-by-site basis for repairs and minor alterations to existing buildings and structures, including minor additions which increase the building footprint by less than 10% or relocate habitable areas above the calculated flood level.
5. The applicants must submit a plan showing the Environmental Development Floodplain Permit Area in accordance with OCP Map C, along with any protected trees or environmental features on site, and detailing satisfactory building envelopes, with consideration of setback requirements, access, parking and circulation, and existing/proposed rights-of-way, easements, and restrictive covenants. The plan must be reviewed and signed by the applicant and all consultants or qualified professionals completing components of the application.

## 22.4 Environmental (Ravine Lands & Significant Trees) Development Permit Area

The purpose of this Development Permit Areas is to ensure the protection of healthy, mature stands of trees which are recognized for the important function they serve for the local eco-system. All development within the Environmental (Ravine Lands and Significant Trees) Development Permit Area is subject to the following conditions:

1. The applicants must submit a report from a Certified Arborist that identifies all trees on the subject property and adjacent lands that meet the minimum size requirements outlined in the City's Tree Management Bylaw. The report must indicate the species, health, structure, and size of each protected tree, and include a tree protection plan and a tree placement plan. Retention of mature, healthy trees and native vegetation and ground cover is a priority.
2. The applicants must submit an Erosion and Sediment Control Plan that shows how potential impacts to sensitive areas and nearby watercourses will be mitigated.

3. The applicants may be required to submit a geotechnical assessment, prepared by a Registered Geotechnical Engineer, in accordance with the current edition of the Guidelines for Legislated Landslide Assessments for Proposed Residential Development in British Columbia. Registration of a restrictive covenant pursuant to Section 219 of the Land Title Act may be required.
4. The applicants must submit a plan showing the Environmental Development Ravine Lands and Significant Trees Permit Area in accordance with OCP Map C, along with any steep slopes, protected trees, tree protection measures, replacement trees and any other environmental features protected on site, and detailing satisfactory building envelopes, with consideration of setback requirements, access, parking and circulation, existing/proposed rights-of way, easements, and restrictive covenants. The plan must be reviewed and signed by the applicant and all consultants or qualified professionals completing components of the application.

## 22.5 Environmental (Watercourse) Development Permit Area

The purpose of this Development Permit Area is to protect and enhance riparian areas adjacent to streams and watercourses. All definitions in the Province's Riparian Areas Regulation are applicable, and all development within the Environmental (Watercourse) Development Permit Area is subject to the following conditions:

1. The applicants must submit a Riparian Areas Regulation Assessment, prepared by a Qualified Environmental Professional (QEP), as defined in the Provincial Riparian Areas Regulation. Confirmation that the Assessment has been received and accepted by the Ministry of Forest, Lands, and Natural Resource Operations is also required. Development must be consistent with the findings of the Riparian Areas Assessment, including but not limited to the Streamside Protection and Enhancement Area (SPEA).
2. The applicants must submit a tree assessment report, prepared by a Certified Arborist, that includes a tree protection/replacement plan. Retention of mature, healthy trees as well as native vegetation and ground cover is a priority. A vegetation management plan is required for planting proposed within a Riparian Assessment Area. Tree removal within a SPEA will only be permitted if the tree has been identified as a hazardous tree by a BC Danger Tree Assessor.
3. The applicants must submit an Erosion and Sediment Control Plan that shows how the adjacent watercourses and associated drainages will be protected during construction, in accordance with the Riparian Areas Assessment.
4. The applicants must submit a geotechnical assessment, prepared by a Registered Geotechnical Engineer, in accordance with the current edition of the Guidelines for Legislated Landslide Assessments for Proposed Residential Development in British Columbia. Registration of a restrictive covenant pursuant to Section 219 of the Land Title Act may be required.
5. The applicants may be required to provide securities to prevent an unsatisfactory landscaping condition, an unsafe condition and/or damage to the natural environment as a result of a contravention to a condition in the Development Permit. The amount of securities will be equal to the estimated cost of rehabilitating and/or restoring the environmental area. The securities may be withheld by the City in the event that the Riparian Assessment Area is damaged due to a contravention of a Development Permit. The applicant(s) may be required to submit a cost estimate of required landscaping from a qualified professional to assist in the determination of the amount of securities required.
6. The applicants may be required to submit a post-construction report, prepared by a QEP, that confirms that all development, including but not limited to the planting of vegetation, has taken place as required in the Riparian Areas Assessment. A report from a Certified Arborist regarding the planting of required replacement trees may also be required.
7. The applicants must submit a plan showing Environmental Watercourse Development Permit Area in accordance with OCP Map C, along with the protected watercourse, SPEA as determined by a qualified environmental professional, steep slopes, and any other environmental features protected on site, erosion and sediment control measures, and detailing satisfactory building envelopes, with consideration of setback requirements, access, parking and circulation, existing/proposed rights-of-way, easements, and restrictive covenants. The plan must be reviewed and signed by the applicant and all consultants or qualified professionals completing components of the application.
8. The City may require that adequate public liability insurance in the amount of \$5,000,000 be provided, with the City of White Rock as an "additional named insured."

Appendix A Regional Context  
Statement

new!



**Metro 2050  
Implementation Guideline  
Regional Context Statement Template**

July 2025

# Regional Context Statement Checklist

This checklist is for staff use, not to be included in the Regional Context Statement.

## Mapping Requirements

In addition to completing the template below, an RCS must contain a parcel-based map that identifies the six regional land use designations, the Urban Containment Boundary, and the regional overlays within the Member Jurisdiction whenever applicable.

- Regional Land Use Designations
  - General Urban
  - Industrial
  - Rural
  - Employment
  - Agricultural
  - Conservation and Recreation
- Regional Overlays
  - Urban Centres
  - Frequent Transit Development Areas
  - Trade-Oriented Lands Overlay
  - Natural Resource Areas Overlay (Maintained by Metro Vancouver)
- GIS digital file for Metro Vancouver

## Population, Dwelling Unit and Employment Projections Requirements

- [Section 1.1.9\(b\)](#) of *Metro 2050* requires Member Jurisdictions adopt RCSs that “provide Member Jurisdiction population, dwelling unit, and employment projections, with reference to guidelines contained in Table 1, and demonstrate how local plans will work towards accommodating the projected growth within the Urban Containment Boundary in accordance with the regional target of focusing 98% of residential growth inside the UCB.” An RCS should illustrate efforts to align the OCP with the Member Jurisdiction and regional projections over time. RCSs should indicate growth projections for the same time periods as indicated in *Metro 2050*: i.e. to 2030, 2040, and 2050.
- [Section 1.2.23\(a\)](#) of *Metro 2050* requires Member Jurisdictions adopt RCSs that “provide dwelling unit and employment projections that indicate the Member Jurisdiction’s share of planned growth and contribute to achieving the regional share of growth for Urban Centres and Frequent Transit Development Areas as set out in Table 2.”
- [Section 6.2.7](#) of *Metro 2050* provides Member Jurisdictions with the flexibility to amend OCP land use designations (or equivalent) without amending the RCS under certain circumstances. This language must be included in the RCS to permit this flexibility.

## Official Community Plan Consistency with Metro 2050

- Demonstrate how the OCP or equivalent contributes to reaching the regional federation’s targets using the Regional Context Statement template included on the following pages.

# METRO 2050 REGIONAL CONTEXT STATEMENT

## Metro 2050 Targets

In the following section please outline the Official Community Plan policies (or equivalent) that demonstrate your Member Jurisdiction’s contribution to reaching the regional federation’s Regional Growth Strategy targets. Please also provide supplementary information on how these policies, along with any other applicable plans or strategies, will work together to contribute to meeting these regional targets. For the environmental and housing targets, Metro 2050 recognizes that the targets are regional in scale and long term to the year 2050; Member Jurisdiction local context is expected.

<b>Metro 2050 Regional Targets</b>		
<b><u><a href="#">Goal 1 Create a Compact Urban Area Targets</a></u></b>		
<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>
<p>1.1.9 b) Provide Member Jurisdiction population, dwelling unit, and employment projections, with reference to guidelines contained in Table 1, and demonstrate how local plans will work towards accommodating the projected growth within the Urban Containment Boundary in accordance with the regional target of <b>focusing 98% of residential growth inside the Urban Containment Boundary</b></p>	<p>The Urban Containment Boundary is shown on Map 1 – The Land Use Plan identifies White Rock as completely located within the Urban Containment Boundary (UCB); therefore, one hundred percent (100%) of all growth will be within the UCB. <b>See Map 1 land use designations.</b></p> <p><b>See Section 6.0 Growth Management.</b> Objective 6.1 states that the City’s intent is to manage growth in a way that is consistent with the Regional Growth Strategy.</p> <p><b>See Section 7.0: Land Use</b> specific polices that guide the management of development in the City.</p> <p><b>Population, Dwelling Units and Employment Projections (estimates) for White Rock (2050), based on Metro Vancouver Growth Projections (medium growth):</b></p> <p><b>Population</b></p> <ul style="list-style-type: none"> <li>• Year 2021: 21,939</li> <li>• Year 2050: 32,560</li> </ul> <p><b>Dwelling Units</b></p> <ul style="list-style-type: none"> <li>• Year 2021: 11,000</li> <li>• Year 2050: 31,662</li> </ul> <p><b>Total Employment</b></p> <ul style="list-style-type: none"> <li>• Year 2021: 7,730</li> <li>• Year 2050: 11,830</li> </ul>	<p>The OCP includes the findings from the North Bluff Road Corridor Plan.</p> <p>Updates to the OCP policies to include Small-Scale Multi-Unit Housing forms have increased the potential for the creation of housing in the City.</p> <p>White Rock does not have a Frequent Transit Development Area; therefore, only Urban Centre Policies are applicable.</p>

<p>1.2.23 a) provide dwelling unit and employment projections that indicate the Member Jurisdiction’s share of planned growth and <b>contribute to achieving the regional share of growth for Urban Centres and Frequent Transit Development Areas as set out in <a href="#">Table 2</a></b> (Dwelling Unit and Employment Growth Targets for Urban Centres and Frequent Transit Development Areas)</p> <p>Regional Targets for Residential Growth by Location:</p> <ul style="list-style-type: none"> <li>• <b>All Urban Centre Types: 40%</b></li> <li>• <b>Frequent Transit Development Areas: 28%</b></li> </ul> <p>Regional Targets for Employment Growth by Location:</p> <ul style="list-style-type: none"> <li>• <b>All Urban Centre Types: 50%</b></li> <li>• <b>Frequent Transit Development Areas: 27%</b></li> </ul>	<p>White Rock’s Municipal Town Centre is shown on Map 1, and Urban Centre mirrors the Municipal Town Centre.</p> <p>With the incorporation of the <i>North Bluff Corridor Plan</i> into the OCP, it is anticipated that the Town Centre Transition Areas will support and contribute to the Metro 2050 target.</p>	<p>White Rock does not have a Frequent Transit Development Area; therefore, only Urban Centre Policies are applicable. All of the projected growth for White Rock will be located within the Urban Containment Boundary, and focused in the Urban Centre and adjacent transition areas.</p>
<p>1.2.23 b) ii) include policies and actions for Urban Centres and Frequent Transit Development Areas that: <b>focus and manage growth and development in Urban Centres and Frequent Transit Development Areas consistent with guidelines set out in <a href="#">Table 3</a></b> (Guidelines for Urban Centres and Frequent Transit Development Areas) and demonstrate how that growth will contribute to the Urban Centre and Frequent Transit Development Area targets set out in <a href="#">Table 2</a> and <a href="#">Action 1.2.12</a></p> <p><i>1.2.12 Implement the strategies and actions of the Regional Growth Strategy that contribute to regional targets as shown on Table 2 to:</i></p> <ul style="list-style-type: none"> <li>a) <b>focus 98% of the region’s dwelling unit growth to areas within the Urban Containment Boundary;</b></li> <li>b) <b>focus 40% of the region’s dwelling unit growth and 50% of the region’s employment growth to Urban Centres; and</b></li> <li>c) <b>focus 28% of the region’s dwelling unit growth and 27% of the region’s</b></li> </ul>	<p>Section 7.0 outlines White Rock’s 'Town Centre' land use designation. The 'Town Centre' permits mixed use development and the highest densities considered in the plan.</p> <p><i>Part B: Policies</i> of the OCP provides the detailed policy framework to guide growth that supports the Regional Growth Strategy, which contributes to regional targets.</p> <p>White Rock does not have a Frequent Transit Development Area, so all of the projected growth for White Rock will be located within the Urban Containment Boundary, which is supported through the policies of the OCP, with significant development in the Urban Centre.</p>	<p>The City of White Rock’s Town Centre is part of the Semiahmoo Municipal Town Centre, an ‘Urban Centre’ in the Regional Growth Strategy, which extends into the City of Surrey. The overall development of the Semiahmoo Municipal Town Centre will require consultation with the City of Surrey.</p> <p>The Town Centre area is identified as the focus for the majority of future growth over the life of this Official Community Plan and is noted as the City’s centre for cultural, civic, economic, and public life in the City.</p> <p>The City’s North Bluff Road Corridor Plan has been incorporated into this OCP, with the highest growth expected in the Town Centre and along the North Bluff Road Corridor.</p>

<i>employment growth to Frequent Transit Development Areas.</i>		
<b><u>Goal 3 Protect the Environment, Address Climate Change, and Respond to Natural Hazards Targets</u></b>		
<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>
<p>3.2.7 a) identify local ecosystem protection and tree canopy cover targets, and demonstrate how these targets will contribute to the regional targets in Action 3.2.1:</p> <ul style="list-style-type: none"> <li>• increase the area of lands protected for nature <b>from 40% to 50% of the region’s land base by the year 2050;</b> and</li> <li>• increase the total regional tree canopy cover within the Urban Containment Boundary <b>from 32% to 40% by the year 2050.</b></li> </ul>	<p>The City of White Rock’s goal for environmental management is to preserve and protect natural areas, features, and habitat, and encourage responsible use of resources through sustainable development. The OCP contains a policy that will contribute to the increase in the protection of land for nature and also contribute to the regional tree canopy within the Urban Containment Boundary. These include <i>Guiding Principle 6</i> of the OCP that supports increasing the quality and amount of green spaces within White Rock, enhancing tree canopy, and protecting natural resources and ecological areas.</p> <p>Goal 7 of Environmental Management – Encourages the preservation and protection of natural areas, features, and habitat, and encourages the responsible use of resources through sustainable development.</p> <p>Section 6.0 Growth Management – Objective 6.1 states that growth will be managed in a way that is consistent with the Regional Growth Strategy.</p> <p>Policy 12.2.2 Tree Preservation contributes to preserving and protecting mature, healthy trees throughout the city.</p>	<p>The City’s Environmental Management objectives include the following:</p> <ul style="list-style-type: none"> <li>• protection of the ecological integrity of watercourses and Semiahmoo Bay;</li> <li>• protect and expand habitat and natural areas in the city;</li> <li>• to protect environmental and human health.</li> <li>• mitigate risk of natural disasters and climate change</li> </ul> <p>The City developed an Environmental Strategic Plan. This plan provides a framework and detailed implementation plan linking broad environmental objectives with ongoing City operations, services and policy and provides leadership for the community on environmental sustainability. The plan provides nine broad objectives outlined below, and an implementation strategy describing specific goals, actions, timelines, relative costs and ways of measuring progress in preserving the natural beauty and characteristics that help define White Rock. City operations, policies such as the Official Community Plan, Parks Master Plan and Strategic Transportation Plan, and commitments to climate change action were considered during Plan development. The nine broad objectives are:</p>

		<ol style="list-style-type: none"> <li>1. Protect and Enhance Semiahmoo Bay</li> <li>2. Manage Rainwater Runoff and Sanitary Waste Appropriately</li> <li>3. Preserve the Drinking Water Source Land</li> <li>4. Protect Wildlife Habitat</li> <li>5. Protect and Enhance the Tree Canopy</li> <li>6. Eliminate Cosmetic Pesticide Use</li> <li>7. Reduce Solid Waste and Promote Recycling Air</li> <li>8. Promote Energy Efficiency and GHG Reduction in City Facilities and Operations</li> <li>9. Promote Energy Efficiency and GHG Reduction in the Community.</li> </ol> <p>The City has implemented tree management measures. The purpose of tree management is to ensure the preservation, protection, and enhancement of the City's natural environment - namely, the preservation and protection of mature, healthy trees in the community. Tree management is guided and regulated by two key documents:</p> <ul style="list-style-type: none"> <li>• Tree Protection, Canopy Enhancement and Management on City lands - Council Policy 611.</li> <li>• Tree Protection Bylaw, 2021 No. 2407.</li> </ul>
<p>3.3.7 a) identify how local land use and transportation policies will contribute to meeting the <b>regional greenhouse gas emission reduction target of 45% below 2010 levels by the year 2030 and achieving a carbon neutral region by the year 2050;</b></p>	<p>White Rock land use designation policies provide an opportunity to increase density in the Town Centre area, which is close to transit and services, and will contribute to the community's overall emissions reduction.</p> <p>The OCP includes policies that encourage active transportation as well as encourage the use of transit, which aims to reduce the</p>	<p>The White Rock's zoning bylaw contains requirements for electric vehicle charging requirements for multi-family development, bicycle parking standards and options for transportation demand management at the redevelopment stage.</p> <p>The City's OCP also includes the creation of a Transportation Demand</p>

	<p>reliance on private automobiles thus leading to reduced emission.</p> <p>Objective 12.5 - To conserve energy and reduce greenhouse gas emissions.</p> <p>Policy 12.5.2 Development Permit Areas – Implement DPA guidelines in Part D relating to energy conservation and greenhouse gas emissions reductions.</p>	<p>Management bylaw to encourage transit use and reduce the reliance on automobiles.</p> <p>The City has developed a Community Climate Action Plan.</p>
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**Goal 4 Provide Diverse and Affordable Housing Choices Target**

<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>
<p>4.2.7 a) indicate how they will, within their local context, contribute toward the regional target of having <b>at least 15% of newly completed housing units built within all Urban Centres, Frequent Transit Development Areas, and Major Transit Growth Corridors combined, to the year 2050, be affordable rental housing units</b> (recognizing that developing affordable rental housing units in transit-oriented locations throughout the urban area is supported)</p>	<p>All new housing units built will be located within the Urban Containment Boundary. Affordable rental units are identified in the OCP as an important part of housing in White Rock. The following supports the building of affordable rental units, which will contribute to the regional target of having at least 15% of newly completed housing units built within all Urban Centres.</p> <p>Policies 7.1.3, 7.2.3, 7.3.3, and 7.10.3 provide opportunities to increase density with affordable rental units being included in development proposals.</p>	<p>Through the incorporation of the <i>North Bluff Corridor Plan</i>, greater opportunities for affordable housing are expected to be realized through density bonuses provisions for land use designation in the Urban Centre.</p>

## Goal 1

<b>Metro 2050 <u>Goal 1: Create a Compact Urban Area</u></b>		
<p><i>Describe how the OCP and other supporting plans and policies contribute to this Goal:</i></p> <p>White Rock's OCP update 2025 was focused on expanding residential opportunities throughout the city with varying densities from higher density in the Town Centre and lower density in Mature Neighbourhoods.</p>		
<b>Goal 1 Targets</b>		
<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>
<p>1.1.9 b) Provide Member Jurisdiction population, dwelling unit, and employment projections, with reference to guidelines contained in Table 1, and demonstrate how local plans will work towards accommodating the projected growth within the Urban Containment Boundary in accordance with the regional target of <b>focusing 98% of residential growth inside the Urban Containment Boundary</b></p>	<p>The City of White Rock is completely located within the Urban Containment Boundary (UCB); therefore, one hundred percent (100%) of all growth will be within the UCB.</p>	<p>Objective 6.1 in the OCP states the City's intent is to manage growth in a way that is consistent with the Regional Growth Strategy.</p>
<p>1.2.23 a) provide dwelling unit and employment projections that indicate the Member Jurisdiction's share of planned growth and <b>contribute to achieving the regional share of growth for Urban Centres and Frequent Transit Development Areas as set out in <a href="#">Table 2</a></b> (Dwelling Unit and Employment Growth Targets for Urban Centres and Frequent Transit Development Areas)</p> <p>Regional Targets for Residential Growth by Location:</p> <ul style="list-style-type: none"> <li>• <b>All Urban Centre Types: 40%</b></li> <li>• <b>Frequent Transit Development Areas: 28%</b></li> </ul> <p>Regional Targets for Employment Growth by Location:</p> <ul style="list-style-type: none"> <li>• <b>All Urban Centre Types: 50%</b></li> <li>• <b>Frequent Transit Development Areas: 27%</b></li> </ul>	<p><i>See response in Targets section</i></p>	<p><i>See response in Targets Section</i></p>
<p>1.2.23 b) ii) include policies and actions for Urban Centres and Frequent Transit Development Areas that: <b>focus and manage growth and development in Urban Centres and Frequent Transit Development Areas consistent with</b></p>	<p><i>See response in Targets Section</i></p>	<p><i>See response in Targets Section</i></p>

<p><b>guidelines set out in Table 3</b> (Guidelines for Urban Centres and Frequent Transit Development Areas) and demonstrate how that growth will contribute to the Urban Centre and Frequent Transit Development Area targets set out in <a href="#">Table 2</a> and <a href="#">Action 1.2.12</a></p> <p><i>1.2.12 Implement the strategies and actions of the Regional Growth Strategy that contribute to regional targets as shown on Table 2 to:</i></p> <p><b>a) focus 98% of the region’s dwelling unit growth to areas within the Urban Containment Boundary;</b></p> <p><b>b) focus 40% of the region’s dwelling unit growth and 50% of the region’s employment growth to Urban Centres; and</b></p> <p><b>c) focus 28% of the region’s dwelling unit growth and 27% of the region’s employment growth to Frequent Transit Development Areas.</b></p>		
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**Strategy 1.1: Contain urban development within the Urban Containment Boundary**

	Section	Policy	Applicable OCP Policies
<b>Policy 1.1.1.9</b>	<b>Adopt Regional Context Statements that:</b>		
	a)	Depict the Urban Containment Boundary on a map, generally consistent with the Regional Land Use Designations map (Map 2)	The City of White Rock is located entirely within the Urban Containment Boundary - see Map 1.
	b)	Provide Member Jurisdiction population, dwelling unit, and employment projections, with reference to guidelines contained in Table 1, and demonstrate how local plans will work towards accommodating the projected growth within the Urban Containment Boundary in accordance with the regional target of focusing 98% of residential growth inside the Urban Containment Boundary	<i>See response in Targets section</i>
	c)	Include a commitment to liaise regularly with Metro Vancouver Liquid Waste Services and Metro Vancouver Water Services to keep them apprised of the scale and timeframe of major development plans as well as specific plans to separate combined sewers	White Rock owns its own water system. White Rock commits to liaising with Metro Vancouver regarding liquid waste services.
	d)	Integrate land use planning policies with local and regional economic development strategies, particularly in the vicinity of the port and airports, to minimize	There are no ports or airports located within or adjacent to the City boundaries.

		potential exposure of residents to environmental noise and other harmful impacts	
<b>Strategy 1.2: Focus growth in Urban Centres and Frequent Transit Development Areas</b>			
<b>Policy 1.2.23</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
	<b>Adopt Regional Context Statements that:</b>		
	a)	provide dwelling unit and employment projections that indicate the Member Jurisdiction’s share of planned growth and contribute to achieving the regional share of growth for Urban Centres and Frequent Transit Development Areas as set out in Table 2 (Dwelling Unit and Employment Growth Targets for Urban Centres and Frequent Transit Development Areas)	<i>See response in Targets section</i>
	b)	include policies and actions for Urban Centres and Frequent Transit Development Areas that:	The City of White does not have a Frequent Transit Development Area; thus, only Urban Centre policies are applicable.
	i)	identify the location, boundaries, and types of Urban Centres and Frequent Transit Development Areas on a map that is consistent with the guidelines set out in Table 3 (Guidelines for Urban Centres and Frequent Transit Development Areas) and Map 4	See Map 1 for the location of the Urban Centre.
	ii)	focus and manage growth and development in Urban Centres and Frequent Transit Development Areas consistent with guidelines set out in Table 3 (Guidelines for Urban Centres and Frequent Transit Development Areas) and demonstrate how that growth will contribute to the Urban Centre and Frequent Transit Development Area targets set out in Table 2 and Action 1.2.12	<i>See response in Targets section</i>
	iii)	encourage office development to locate in Urban Centres through policies, economic development programs, or other financial incentives	Objective 7.1 and Policy 7.1.1 support a concentrated mix of multi-unit residential and commercial.  Objective 8.1 and Policy 8.1.1 aim to attract office and employment-generating uses that reinforce the Town Centre as the primary employment hub.
	iv)	support modal shift by establishing or maintaining reduced residential and commercial parking requirements in Urban Centres and FTDA and consider the use of parking maximums	The OCP supports and encourages alternative automobile uses for all people within White Rock.  Policies 10.2.1 through 10.2.2 encourage review of parking standards for New Non-Market and Affordable Rental Housing

		and secured Market Rental Housing.
v)	consider the identification of appropriate measures and neighbourhood plans to accommodate urban densification and infill development in Urban Centres, Frequent Transit Development Areas, and, where appropriate, Major Transit Growth Corridors in a resilient and equitable way (e.g. through community vulnerability assessments, emergency services planning, tenant protection policies, and strategies to enhance community social connectedness and adaptive capacity)	<p>The City has a Tenant Relocation Policy in effect, and the OCP lists the development of a Tenant Protection Bylaw as a future action item.</p> <p>Objective 10.1 and Policies 10.1.1, 11.1.2 support expanding housing choices for existing and future residents and increasing diversity of housing types for a variety of household sizes, incomes, tenures, needs, and preferences.</p> <p>Objective 10.2 and policies 10.2.1 through 10.2.5 support rental housing and a range of non-market housing options and needs along the housing spectrum.</p>
vi)	consider support for the provision of childcare spaces in Urban Centres and Frequent Transit Development Areas	Policies 7.13.3 encourage childcare use in areas with higher concentrations of housing, including in multi-family and mixed-use areas.
vii)	consider the implementation of green infrastructure	Policies 11.1.5 and 15.2.5 encourage the use of green infrastructure.
viii)	focus infrastructure and amenity investments (such as public works and civic and recreation facilities) in Urban Centres and Frequent Transit Development Areas, and at appropriate locations within Major Transit Growth Corridors	<p>Objective 8.2 and Policies 8.2.1 through 8.2.4 support the establishment of civic and community uses around the Town Center, which is located within the Urban Centre.</p> <p>Policy 14.1.4 supports a Town Square at Russell Avenue and Johnston Road, to strengthen a community focal point in the heart of the Town Centre.</p>
ix)	support the provision of community services and spaces for non-profit organizations	<p>Objective 17.1 supports the effective delivery of essential community and emergency services.</p> <p>Policy 17.1.1 encourages the continued support and</p>

		coordination with and between local community service providers, including Sources Community Resource Centres, to optimize programs available to the residents of White Rock.
x)	consider, where Urban Centres and Frequent Transit Development Areas overlap with Employment lands, higher density forms and intensification of commercial and light industrial	Does not apply.
xi)	take appropriate steps to avoid or mitigate the negative health impacts of busy roadways on new or redeveloped residential areas	Objective 11.3 and Policies 11.3.1 through 11.3.3 encourage the protection of environmental and human health.
c)	Include policies for General Urban lands that:	See below.
i)	identify General Urban lands and their boundaries on a map generally consistent with Map 2	See Map 1.
ii)	exclude new non-residential Major Trip- Generating uses, as defined in the Regional Context Statement, from those portions of General Urban lands outside of Urban Centres and Frequent Transit Development Areas and direct new non-residential Major Trip-Generating uses to Urban Centres and Frequent Transit Development Areas	Does not apply.
iii)	encourage infill and intensification (e.g. row houses, townhouses, mid-rise apartments, laneway houses) in appropriate locations within walking distance of the Frequent Transit Network;	Objective 6.1 and Policy 6.1.1 support the management of growth in a way that is consistent with the Regional Growth Strategy.
iv)	encourage neighbourhood-serving commercial uses	Policy 6.4.2 supports Neighbourhood-Serving Retail in specific areas within the City to service local needs.  Objective 7.9 and Policy 7.9.1 provide the land use details to support neighbourhood commercial uses.

	d)	with regards to Actions 1.2.15 and 1.2.23 c) ii), include a definition of “non-residential Major Trip Generating uses” that includes, but is not limited to, the following uses: office or business parks, outlet shopping malls, post-secondary institutions, and large-format entertainment venues	Objective 8.1 and Policy 8.1.1 support White Rock to attract office and employment-generating uses that reinforce the Town Centre as the primary employment hub.  Policy 13.2.2 encourages White Rock to work with the Business Improvement Association to improve and promote retail and amenity areas, including along Marine Drive and Johnston Road, by focusing on the city as a “full day destination” and on the waterfront as a “year-round destination”. Explore the possibility of expanding programming on the waterfront and actively promote the development of new hotel space and related uses in this area.
	e)	consider the identification of new Frequent Transit Development Areas in appropriate locations within Major Transit Growth Corridors, as part of the development of new or amended area or neighbourhood plans, or other community planning initiatives	Does not apply.
	f)	consider long-term growth and transportation planning coordination with adjacent municipalities, First Nations, TransLink, and Metro Vancouver for transit corridors that run through or along two or more adjacent jurisdictions	Policy 13.3.2 encourages White Rock to work with the City of Surrey, the City of Blaine, and the Semiahmoo First Nation to promote economic development opportunities.

**Strategy 1.3: Develop resilient, healthy, connected, and complete communities with a range of services and amenities**

Policy 1.3.7	Section	Policy	Applicable OCP Policies
	Adopt Regional Context Statements that:		

	a)	support compact, mixed-use, transit, walking, cycling and rolling-oriented communities	<p>Objective 6.3 and Policies 6.3.1 and 6.3.2 try to maximize the proportion of residents who can access transit and meet daily needs within walking distance of home, and to reinforce a “City of Centres” with the Town Centre, Lower Town Centre, and Waterfront Village as the focal points of public, economic, and cultural life.</p> <p>Objective 6.4 and Policies 6.4.1 and 6.4.2 support new housing choices and the ability of residents to meet more daily needs within walking distance of home within Mature Neighbourhoods.</p> <p>Objective 12 and policies 12.1.1 through 12.1.11 encourage the improvement of safety, comfort, convenience, and enjoyment of walking in White Rock, encouraging it as the first choice for short trips</p> <p>Objective 12.2 and policies 12.2.1 through 12.2.6 support the improvement of the safety, comfort, convenience, and enjoyment of cycling within and through White Rock.</p> <p>Objective 12.3 and policies 12.3.1 through 12.3.9 support the utilization of land use and design tools, and work with transit service providers, to improve the frequency, comfort, and convenience of transit use.</p>
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	b)	locate and support community, arts, cultural, recreational, institutional, medical/health, social service, education and child care facilities, and local serving retail uses in Urban Centres or areas with good access to transit	<p>Policy 7.13.3 encourages childcare uses in areas with higher concentrations of housing, including in multi-family and mixed-use areas.</p> <p>Objective 16.1 and Policies 16.1.1 and 16.1.2 support the protection and reinforce White Rock’s heritage.</p> <p>Objective 16.2 and Policies 16.2.1 through 16.2.10 support and reinforce cultural inclusivity, cultural involvement, and the arts and artistic expression in White Rock.</p> <p>Objective 17.1 and Policies 17.1.1 through 17.1.5 support the effective delivery of essential community and emergency services.</p> <p>Objective 17.3 and Policies 17.3.1 through 17.3.2 aim to meet the diverse needs of the community and provide access to civic facilities, spaces, and programs.</p>
	c)	provide and encourage public spaces and other place-making amenities and facilities (e.g. community gardens, playgrounds, gathering places, etc.) in new and established neighbourhoods, for all ages, abilities, and seasons, to support social connections and engagement	<p>Policy 17.3.2 works towards improving access to open space so that all residents live within a 5-minute (400 metre) walk of a park, green space, or other civic space.</p> <p>Policy 17.3.3 encourages the incorporation of universal design principles in new buildings, civic facilities, parks, and public realm upgrades.</p> <p>Policy 17.4.2 supports the establishment of urban agriculture in White Rock.</p>
	d)	respond to health and climate change-related risks by providing equitable access to:	<p>Objective 17.4 and policies 17.4.1 through 17.4.5 support the improvement of food security and enhance access to all facets of the local food system, from production and processing to sales and nutrient recovery.</p>

i)	recreation facilities	Objective 17.2 and Policies 17.3.1 and 17.3.2 encourage access to civic facilities, spaces and programs to meet the diverse needs of the White Rock.
ii)	green spaces and public spaces (e.g. parks, trails, urban forests, public squares, etc.)	Objective 17.2 and policies 17.3.1 and 17.3.2 encourage access to civic facilities, spaces and programs to meet the diverse needs of the White Rock.
iii)	safe and inviting walking, cycling, and rolling environments, including resting spaces with tree canopy coverage, for all ages and abilities	Objective 9.1 and Policy 9.1.1 support the improvement of comfortable and convenient access to the Waterfront.  Objective 9.2 and policies 9.2.1 through 9.2.3 encourage the improvement as well as the comfort, safety, and delight of pedestrian spaces at the Waterfront.
e)	support the inclusion of community gardens (at-grade, rooftop, or on balconies), grocery stores and farmers' markets to support food security, and local production, distribution and consumption of healthy food, in particular where they are easily accessible to housing and transit services	Objective 17.4 and policies 17.4.1 through 17.4.5 support the improvement of food security and enhance access to all facets of the local food system, from production and processing to sales and nutrient recovery.
f)	consider, when preparing new neighbourhood and area plans, the mitigation of significant negative social and health impacts, such as through the use of formal health and social impact assessment methods in neighbourhood design and major infrastructure investments	Social and health impacts were considered in the creation of the North Bluff Corridor Plan, which resulted in the development of the Health District land use designation, which is found in <i>Section 7. Land Use</i> .
g)	provide design guidance for existing and new neighbourhoods to promote social connections, universal accessibility, crime prevention through environmental design, and inclusivity while considering the impacts of these strategies on identified marginalized members of the community	Objective 7.13 - To utilize land use efficiently and in a manner that provides social, economic, and ecological benefits across the city.
h)	consider where appropriate, opportunities to incorporate recognition of Indigenous and other cultures into the planning of Urban Centres, FTDA's, and other local centres	Objective 16.1 and policies 16.1.1 and 16.1.2 support the protection and reinforcement of White Rock's heritage.

Strategy 1.4: Protect Rural lands from urban development			
	Section	Policy	Applicable OCP Policies
Policy 1.4.3	Adopt Regional Context Statements that:		
	a)	identify Rural lands and their boundaries on a map generally consistent with Map 2	Not applicable. Metro 2050 does not identify Rural lands within the City of White Rock.
	b)	limit development to a scale, form, and density consistent with the intent for the Rural land use designation, and that is compatible with on-site sewer servicing	Not applicable. Metro 2050 does not identify Rural lands within the City of White Rock.
	c)	specify the allowable density and form, consistent with Action 1.4.1, for land uses within the Rural regional land use designation	Not applicable. Metro 2050 does not identify Rural lands within the City of White Rock.
	d)	prioritize and support agricultural uses within the Agricultural Land Reserve, and where appropriate, support agricultural uses outside of the Agricultural Land Reserve	Not applicable. Metro 2050 does not identify agricultural land, nor is there any Agricultural Land Reserve within the City of White Rock.
	e)	support the protection, enhancement, restoration, and expansion of ecosystems identified on Map 11 to maintain ecological integrity, enable ecosystem connectivity, increase natural carbon sinks and enable adaptation to the impacts of climate change	Objective 11.1 and policies 11.1.1 through 11.1.7 support the protection of the ecological integrity of watercourses and Semiahmoo Bay.  Objective 11.2 and 11.2.1 through 11.2.4 policies support the protection and expansion of habitat and natural areas in the city.  The land identified with ecosystems and uses on <i>Metro 2050</i> Map 11 is located within a Development Permit intended to protect the natural environment, ecosystems, and biological diversity.

## Goal 2

<b>Metro 2050 <u>Goal 2: Support a Sustainable Economy</u></b>			
<p><i>Describe how the OCP and other supporting plans and policies contribute to this Goal:</i></p> <p><i>The OCP contains policies as described below, which supports economic development within the City, and Metro 2050 Goal 2: Support a Sustainable Economy. The draft economic development strategic plan supports a prosperous and diverse economic base through policies, practices and initiatives enabling the City to be a desirable place to live, work, visit, shop and invest.</i></p>			
<b>Strategy 2.1 Promote land development patterns that support a diverse regional economy and employment opportunities close to where people live</b>			
<b>Policy 2.1.10</b>	Section	Policy	Applicable OCP Policies
	<b>Adopt Regional Context Statements that:</b>		
	a)	include policies to support appropriate economic activities, as well as context-appropriate built form for Urban Centres, Frequent Transit Development Areas, Industrial lands, and Employment lands	Policies 6.1.1, 6.3.1, 6.4.3 and 7.7 support appropriate economic activities and buildings that are context appropriate.
	b)	support the development and expansion of large-scale office and retail uses in Urban Centres, and lower-scale uses in Frequent Transit Development Areas through policies such as: zoning that reserves land for commercial uses, density bonus provisions to encourage office development, variable development cost charges, and/or other incentives	Policy 7.7.2 details the density and heights.
c)	discourage the development and expansion of major commercial uses outside of Urban Centres and Frequent Transit Development Areas and that discourage the development of institutional land uses outside of Urban Centres and Frequent Transit Development Areas	Land use designations allow for the management of future development in the City. Future growth is to be focused on the Town Centre as well as the adjacent Town Centre Transition areas and the Lower Town Centre.	
<b>Strategy 2.2 Protect the supply and enhance the efficient use of Industrial land</b>			
<b>Policy 2.2.9</b>	Section	Policy Text	Applicable OCP Policies
	<b>Adopt Regional Context Statements that:</b>		
	a)	identify the Industrial and Employment lands and their boundaries on a map generally consistent with Map 7	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
b)	identify Trade-Oriented lands, if applicable, with a defined set of permitted uses that support inter-regional, provincial, national, and international trade (e.g. logistics, warehouses, distribution centres, transportation and intermodal terminals) and location needs (e.g. large and flat sites, proximity to highway, port, or rail infrastructure) on a map consistent with the	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.	

	goals in the Regional Growth Strategy. Strata and/or small lot subdivisions on these lands should not be permitted	
c)	include policies for Industrial lands that:	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
i)	consistently define, support, and protect Industrial uses, as defined in Metro 2050, in municipal plans and bylaws, and ensure that non-industrial uses are not permitted	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
ii)	support appropriate and related accessory uses, such as limited-scale ancillary commercial spaces, and caretaker units	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
iii)	exclude uses that are not consistent with the intent of Industrial lands and not supportive of Industrial activities, such as medium and large format retail uses, residential uses, and stand- alone office uses, other than ancillary uses, where deemed necessary	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
iv)	encourage improved utilization and increased intensification/densification of Industrial lands for Industrial activities, including the removal of any unnecessary municipal policies or regulatory barriers related to development form and density	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
v)	review and update parking and loading requirements to reflect changes in Industrial forms and activities, ensure better integration with the surrounding character, and reflect improvements to transit service, in an effort to avoid the over- supply of parking	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
vi)	explore municipal Industrial strategies or initiatives that support economic growth objectives with linkages to land use planning	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
vii)	provide infrastructure and services in support of existing and expanding Industrial activities	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
viii)	support the unique locational and infrastructure needs of rail-oriented, waterfront, and trade-oriented Industrial uses	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
ix)	consider the preparation of urban design guidelines for Industrial land edge planning, such as interface designs, buffering standards, or tree planting, to minimize potential land use conflicts between Industrial and sensitive land uses, and to improve resilience to the impacts of climate change	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
x)	do not permit strata and/or small lot subdivisions on identified Trade-Oriented lands	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.

d)	include policies for Employment lands that:	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
i)	support a mix of Industrial, small scale commercial and office, and other related employment uses, while maintaining support for the light Industrial capacity of the area, including opportunities for the potential densification/intensification of Industrial activities, where appropriate	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
ii)	allow large and medium format retail, where appropriate, provided that such development will not undermine the broad objectives of the Regional Growth Strategy	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
iii)	support the objective of concentrating larger- scale commercial, higher density forms of employment, and other Major Trip-Generating uses in Urban Centres, and local-scale uses in Frequent Transit Development Areas	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
iv)	support higher density forms of commercial and light Industrial development where Employment lands are located within Urban Centres or Frequent Transit Development Areas, and permit employment and service activities consistent with the intent of Urban Centres or Frequent Transit Development Areas, while low employment density and low transit generating uses, possibly with goods movement needs and impacts, are located elsewhere	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
v)	do not permit residential uses, except for: <ul style="list-style-type: none"> <li>• an accessory caretaker unit; or</li> <li>• limited residential uses (with an emphasis on affordable, rental units) on lands within 200 m of a rapid transit station and located within Urban Centres or Frequent Transit Development Areas, provided that the residential uses are located only on the upper floors of buildings with commercial and light Industrial uses, where appropriate and subject to the consideration of municipal objectives and local context.</li> </ul>	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
e)	include policies to assist existing and new businesses in reducing their greenhouse gas emissions, maximizing energy efficiency, and mitigating impacts on ecosystems	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.
f)	include policies that assist existing and new businesses to adapt to the impacts of climate change and reduce their exposure to natural hazards risks, such as those identified within the Regional Growth Strategy (Table 5)	Not applicable. Metro 2050 does not identify Industrial and Employment lands within the City of White Rock.

Strategy 2.3 Protect the supply of agricultural land and strengthen Agricultural viability		
Section	Policy	Applicable OCP Policies
<b>Adopt Regional Context Statements that:</b>		
a)	specify the Agricultural lands within their jurisdiction, denoting those within the Agricultural Land Reserve, on a map generally consistent with Map 8	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
b)	consider policies and programs that increase markets and the distribution of local food in urban areas to strengthen the viability of agriculture and increase availability of local food for all residents	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
c)	include policies that protect the supply of Agricultural land and strengthen agriculture viability including those that:	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
i)	assign appropriate land use designations to protect Agricultural land for future generations and discourage land uses on Agricultural lands that do not directly support and strengthen Agricultural viability	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
ii)	encourage the consolidation of small parcels and discourage the subdivision and fragmentation of Agricultural land	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
iii)	support climate change adaptation including: <ul style="list-style-type: none"> <li>• monitoring storm water, flooding, and sea level rise impacts on Agricultural land,</li> <li>• implementing flood construction requirements for residential uses, and</li> <li>• maintaining and improving drainage and irrigation infrastructure that support Agricultural production, where appropriate and in collaboration with other governments and agencies</li> </ul>	Policy 15.2.4 encourages an integrated stormwater management approach.  Policy 14.2.5 encourages the use of green infrastructure.
iv)	protect the integrity of Agricultural land by requiring edge planning along the Urban Containment Boundary and adjacent to Agricultural operations through activities such as screening, physical buffers, roads, or Development Permit area requirements	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock, nor is it adjacent to any agricultural lands.
v)	demonstrate support for economic development opportunities for Agricultural operations that are farm related uses, benefit from close proximity to farms, and enhance primary Agricultural production as defined by the <i>Agricultural Land Commission Act</i>	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.

Policy 2.3.12

	vi)	align policies and regulations, where applicable, with the Minister's Bylaw Standards and Agricultural Land Commission legislation and regulations	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.
<b>Policy 2.3.13</b>	<b>Section</b>	<b>Policy</b>	<b>Supplementary Information</b>
		In partnership with other agencies and organizations, support agricultural awareness and promote the importance of the agricultural industry, the importance of protecting agricultural land, and the value of local agricultural products and experiences	Not applicable. Metro 2050 does not identify Agricultural lands within the City of White Rock.

### Goal 3

<b>Metro 2050 <u>Goal 3: Protect the Environment, Address Climate Change, and Respond to Natural Hazards</u></b>			
<p><i>Describe how the OCP and other supporting plans and policies contribute to this Goal:</i></p> <p><i>The City's OCP has policies that protect the environment, address Climate Change, and respond to Natural Hazards. The City's Tree Protection Bylaw includes policies to support the protection and preservation of tree canopies. The City's Integrated Transportation and Infrastructure Master Plan (ITIMP) has policies that will also help contribute to the City's environmental goals by reducing transportation-related greenhouse gas emissions by encouraging sustainable forms of transportation, such as walking, cycling, and transit and promoting emerging technologies, such as non-polluting electric vehicles.</i></p>			
<b>Goal 3 Targets</b>			
<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>	
3.2.7 a) identify local ecosystem protection and tree canopy cover targets, and demonstrate how these targets will contribute to the regional targets in Action 3.2.1: <ul style="list-style-type: none"> <li>increase the area of lands protected for nature <b>from 40% to 50% of the region's land base by the year 2050</b>; and</li> <li>increase the total regional tree canopy cover within the Urban Containment Boundary <b>from 32% to 40% by the year 2050</b>.</li> </ul>	See response in Targets section.	See response in the Target section.  The City has a Tree Protection Bylaw that applies to land that is under development and where trees are to be removed. A replacement ratio applies that aims to increase the overall Tree Canopy in the city, which will contribute to the Regional Tree Canopy.  The City intends to develop an Urban Forest Management Plan, which will help develop city targets to contribute to the Regional Tree Canopy Target.	
3.3.7 a) identify how local land use and transportation policies will contribute to meeting the <b>regional greenhouse gas emission reduction target of 45% below 2010 levels by the year 2030</b> and achieving a carbon neutral region by the year 2050;	See response in Targets section.	See response in Targets section.	
<b>Strategy 3.1: Protect and enhance Conservation and Recreation lands</b>			
<b>Policy 3.1.9</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
	<b>Adopt Regional Context Statements that:</b>		
	a)	identify Conservation and Recreation lands and their boundaries on a map generally consistent with Map 2	See Map 1
	b)	include policies that support the protection and enhancement of lands with a Conservation and Recreation land use designation, which may include the following uses:	See below

i)	drinking water supply areas	Policy 15.1.3 ensures drinking water supply and distribution are managed to safeguard public health, protect the environment, and provide an adequate supply for a growing population.
ii)	environmental conservation areas	Objective 11.1 is to protect the ecological integrity of watercourses and Semiahmoo Bay, which is supported by Policies 11.1.1 through 11.1.7.
iii)	wildlife management areas and ecological reserves	Policy 11.1.1 supports the protection and enhances the aquatic environment of Semiahmoo Bay.
iv)	forests	Policy 11.2.2 supports preserving and protecting mature, healthy trees throughout the City.  The OCP identifies as a future action item the development of Urban Forest Management Plan.
v)	wetlands (e.g. freshwater lakes, ponds, bogs, fens, estuarine, marine, freshwater, and intertidal ecosystems)	Policy 11.1.4 continues to support shoreline restoration measures and the protection of the foreshore from erosion.
vi)	riparian areas (i.e. the areas and vegetation surrounding wetlands, lakes, streams, and rivers)	Policy 11.2.1 supports the protection of ecologically sensitive areas and retains natural landforms in White Rock, including bluffs and ravines.
vii)	ecosystems not covered above that may be vulnerable to climate change and natural hazard impacts, or that provide buffers to climate change impacts or natural hazard impacts for communities	Policy 11.2.1 supports the protection of ecologically sensitive areas and retains natural landforms in White Rock, including bluffs and ravines.
viii)	uses within those lands that are appropriately located, scaled, and consistent with the intent of the designation, including: <ul style="list-style-type: none"> <li>○ major parks and outdoor recreation areas;</li> <li>○ education, research and training facilities, and associated uses that serve conservation and/or recreation users;</li> <li>○ commercial uses, tourism activities, and public, cultural, or community amenities;</li> <li>○ limited agricultural use, primarily soil-based; and</li> <li>○ land management activities needed to minimize vulnerability / risk to climate change impacts</li> </ul>	Objective 14.3 and policy 14.3.1 support protecting the riparian, foreshore, and aquatic habitat and biodiversity in the operation of City parks and open spaces.
c)	Include policies that:	

	i)	protect the integrity of lands with a Conservation and Recreation regional land use designation from activities in adjacent areas by considering wildland interface planning, and introducing measures such as physical buffers or development permit requirements	The city has Development Permit Areas to achieve objectives for the protection of the natural environment and its ecosystems, and biological diversity.
	ii)	encourage the consolidation of small parcels, and discourage subdivision and fragmentation of lands with a Conservation and Recreation regional land use designation.	Generally, the City's conservation lands are consolidated. The city does not have policies that support the fragmentation of these parcels.

**Strategy 3.2: Protect, enhance, restore, and connect ecosystems**

	Section	Policy Text	Applicable OCP Policies
<b>Policy 3.2.7</b>	<b>Adopt Regional Context Statements that:</b>		
	a)	identify local ecosystem protection and tree canopy cover targets, and demonstrate how these targets will contribute to the regional targets in Action 3.2.1	<i>See the Targets Section</i>
	b)	refer to Map 11 or more detailed local ecological and cultural datasets and include policies that:	See below.
	i)	support the protection, enhancement, and restoration of ecosystems through measures such as land acquisition, density bonusing, development permit requirements, subdivision design, conservation covenants, land trusts, and tax exemptions	The City has Development Permit Areas to achieve objectives for the protection of the natural environment and its ecosystems, and biological diversity.
	ii)	seek to acquire, restore, enhance, and protect lands, in collaboration with adjacent member jurisdictions and other partners, that will enable ecosystem connectivity in a regional green infrastructure network	<p>Policy 12.1.1 supports the protection and enhancement of the aquatic environment of Semiahmoo Bay.</p> <p>Policy 12.1.2 supports the protection and enhancement of riparian areas and buffers adjacent to watercourses.</p> <p>Policy 12.1.4 supports the continuing shoreline restoration measures and the protection of the foreshore from erosion.</p> <p>Policy 12.2.4 supports the enhancement and restoration of ecological links between existing natural areas, increasing connections between isolated habitats.</p>
	iii)	discourage or minimize the fragmentation of ecosystems through low impact development practices that enable ecosystem connectivity	The city has Development Permit Areas to achieve objectives for the protection of the natural environment and its ecosystems, and biological diversity.

iv)	indicate how the interface between ecosystems and other land uses will be managed to maintain ecological integrity using edge planning, and measures such as physical buffers, or development permit requirements.	The City has Development Permit Areas to achieve objectives for the protection of the natural environment and its ecosystems, and biological diversity.
c)	Include policies that:	See below.
i)	support the consideration of natural assets and ecosystem services in land use decision-making and land management practices	Policies 11.1.2, 11.1.3, 11.2.2, 11.2.3 and 11.2.4 speak to the protection of the natural assets ecosystem services in land use decision-making and land management practices.
ii)	enable the retention and expansion of urban forests using various tools, such as local tree canopy cover targets, urban forest management strategies, tree regulations, development permit requirements, land acquisition, street tree planting, and reforestation or restoration policies, with consideration of resilience	Policies 14.2.3 and 14.2.4 speak to the protection and enhancement of trees and increasing opportunities to protect trees.
iii)	reduce the spread of invasive species by employing best practices, such as the implementation of soil removal and deposit bylaws, development permit requirements, and invasive species management plans	The following Development Permit Areas, which are sections 21.6,21.8,21.9,21.10, provide guidelines that address and discourage the use of plant material that is considered an invasive species.
iv)	increase green infrastructure along the Regional Greenway Network, the Major Transit Network, community greenways, and other locations, where appropriate, and in collaboration with Metro Vancouver, TransLink, and other partners	Objective 8.3 and Policy 8.3.1 support the strengthening of permeability of the Town Centre, and the integration of open spaces and connections for walking and cycling.  Objective 11.2 and Policy 11.2.4 support enhancement and expansion of habitat and natural areas in the City.
v)	support watershed and ecosystem planning, the development and implementation of Integrated Stormwater Management Plans, and water conservation objectives.	Policy 12.1.3 Watershed Management – Approve only uses, programming, and infrastructure adjacent to the waterfront that are compatible with the Boundary Bay WMA’s management objective of providing priority to fish, wildlife, and foreshore habitats.

**Strategy 3.3: Advance land use, infrastructure, and human settlement patterns that reduce energy consumption and greenhouse gas emissions, create carbon storage opportunities, and improve air quality**

Policy 3.3.7	Section	Policy	Applicable OCP Policies	
	<b>Adopt Regional Context Statements that:</b>			
	a)	identify how local land use and transportation policies will contribute to meeting the regional greenhouse gas emission reduction target of 45% below 2010 levels by the year 2030 and achieving a carbon neutral region by the year 2050	<i>See response in Targets section</i>	
	b)	identify policies, actions, incentives, and / or strategies that reduce energy consumption and greenhouse gas emissions, create carbon storage opportunities, and improve air quality from land use, infrastructure, and settlement patterns, such as: <ul style="list-style-type: none"> <li>• existing building retrofits and construction of new buildings to meet energy and greenhouse gas performance guidelines or standards (e.g. BC Energy Step Code, passive design), the electrification of building heating systems, green demolition requirements, embodied emissions policies, zero-carbon district energy systems, and energy recovery and renewable energy generation technologies, such as solar panels and geexchange systems, and zero emission vehicle charging infrastructure; and</li> <li>• community design, infrastructure, and programs that encourage transit, cycling, rolling and walking</li> </ul>	Policy 11.5.5 supports on-site Energy Generation.  Policy 11.5.6 supports opportunities to explore the implementation of a district energy system.  Policies 12.1.1, 12.2.1, 12.2.2 and 12.2.4 speak to enhancing and creating connectivity through pedestrian networks, greenways and bicycle networks.  Policies 12.3.1 through 12.3.6 support systems and the built environment to improve access to transit.	
c)	focus infrastructure and amenity investments in Urban Centres and Frequent Transit Development Areas, and at appropriate locations along Major Transit Growth Corridors	Objective 15.4 and Policies 15.4.1 through 15.4.4 support the development of plans and standards to ensure there is appropriate infrastructure capacity for development for new and existing development within the North Bluff Coordinator area, which includes the Urban Centre.		

**Strategy 3.4 Advance land use, infrastructure, and human settlement patterns that improve resilience to climate change impacts and natural hazards**

Policy 3.4.5	Section	Policy	Applicable OCP Policies	
	<b>Adopt Regional Context Statements that:</b>			
	a)	include policies that minimize risks associated with climate change and natural hazards in existing communities through tools such as heat and air quality response plans, seismic retrofit policies, and flood-proofing policies	Policy 12.4.2 relates to floodproofing buildings.	

	b)	include policies that discourage new development in current and future hazardous areas to the extent possible through tools such as land use plans, hazard-specific Development Permit Areas, and managed retreat policies, and where development in hazardous areas is unavoidable, mitigate risks	The following policies are to protect and mitigate risk for new development from current hazards: Policy 12.1.2 Riparian Buffers; Policy 12.2.1 Natural Landforms; Policy 12.2.2 Tree Preservation; Policy 12.4.2 Flood Damage Protection  Section 23 details the requirements for Development Permits Areas (Environmental) that are intended to protect the natural environment, ecosystems, and biological diversity in White Rock.
<b>Policy 3.4.6</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
		Incorporate climate change and natural hazard risk assessments into planning and location decisions for new municipal utilities, assets, operations, and community services.	Section 11 and the policies under this section incorporate climate change and natural hazard risk assessment.
<b>Policy 3.4.7</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
		Integrate emergency management, utility planning, and climate change adaptation principles when preparing land use plans, transportation plans, and growth management policies.	The following policies 11.1.3, 11.1.4, and 11.4.1 include environmental and resilience implications with respect to decision making and the built environment.
<b>Policy 3.4.8</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
		Adopt appropriate planning standards, guidelines, and best practices related to climate change and natural hazards, such as flood hazard management guidelines and wildland urban interface fire risk reduction principles.	Policy 12.4.2 relates to flood hazard management.

## Goal 4

<b>Metro 2050 <u>Goal 4: Provide Diverse and Affordable Housing Choices</u></b>			
<p>Describe how the OCP and other supporting plans and policies contribute to this Goal:  Refer to the applicable policies below, which reference the City's Interim Housing Needs Report and Affordable Housing Strategy, which addresses the needs of the diverse population in the city.</p>			
<b>Goal 4 Targets</b>			
<b>Policy with Target</b>	<b>Applicable OCP Policies</b>	<b>Supplementary Information</b>	
<p>4.2.7 a) indicate how, within the local context, contribute toward the regional target of having <b>at least 15% of newly completed housing units built within all Urban Centres and Frequent Transit Development Areas combined, to the year 2050, be affordable rental housing units</b> (recognizing that developing affordable rental housing units in transit-oriented locations throughout the urban area is supported)</p>	<p>See response in Targets section.</p>	<p>See response in Targets section.</p>	
<b>Strategy 4.1 Expand the supply and diversity of housing to meet a variety of needs</b>			
<b>Policy 4.1.8</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
	<b>Adopt Regional Context Statements that:</b>		
	a)	<p>indicate how you will work towards meeting estimated future housing needs and demand, as determined in their housing needs report or assessment</p>	<p>The OCP policies contain policies that specifically address the Housing Needs as identified in the <i>2024 Interim Housing Needs Report</i>, which is contained in Section 10 of the OCP.</p> <p>The OCP indicates ongoing work that will be undertaken to increase housing choices.</p>
	b)	<p>articulate how local plans and policies will meet the need for diverse (in tenure, size, and type) and affordable housing options</p>	<p>The Goal for OCP Section 10 <i>Housing</i> states, "The City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives."</p>
	c)	<p>identify policies and actions that contribute to the following outcomes</p>	<p>See below.</p>
i)	<p>increased supply of adequate, suitable, and affordable housing to meet a variety of needs along the housing continuum</p>	<p>Objective 10.1 supports expanding housing choices for existing and future residents and increases diversity of</p>	

			housing types for a variety of household sizes, incomes, tenures, needs, and preferences.
	ii)	increased supply of family-friendly, age-friendly, and accessible housing	Policy 10.1.1 supports the increase in family-friendly housing.  Policy 10.1.3 supports housing choices everywhere through the land use designations.
	iii)	increased diversity of housing tenure options, such as attainable homeownership, rental, co-op housing, rent-to-own models, and cohousing	Objective 10.2 - To support rental housing and a range of non-market housing options and needs along the housing spectrum.
	iv)	increased density and supply of diverse ground-oriented and infill housing forms in low-density neighbourhoods, such as duplex, four-plex, townhouse, laneway/coach houses, and apartments, particularly in proximity to transit	Policy 6.4.1 supports increased density in the form of secondary suites, accessory dwelling units, semi-detached residential buildings, rowhouse residential buildings and houseplexes.
	v)	integration of land use and transportation planning such that households can reduce their combined housing and transportation costs	Policy 10.2.1 speaks to supporting new affordable and rental housing, especially in transit-accessible locations.
	vi)	increased social connectedness in multi-unit housing	Section 22 Development Permit Areas (Form/Character/Sustainability) deals with on-site connections through design.
	vii)	integrated housing within neighbourhood contexts and high-quality urban design	Section 22 Development Permit Areas (Form/Character/Sustainability) speaks to the design of new residential development for multi-family and intensive single-family infill.
	viii)	existing and future housing stock that is low carbon and resilient to climate change impacts and natural hazards	Policies 11.5.3, 11.5.4, and 11.5.5 speak to the inclusion of green building strategy, energy step code and on-site energy generation as part of land use decisions and planning that is related to the built environment.
<b>Policy 4.1.9</b>	<b>Section</b>	<b>Policy</b>	<b>Supplementary Information</b>
	<b>Prepare and implement housing strategies or action plans that:</b>		
	a)	are aligned with housing needs reports or assessments, and reviewed or updated every 5-10 years to ensure that housing strategies or action plans are based on recent evidence and responsive to current and future housing needs	Please see White Rock's Interim Housing Needs Report (October 2024) and White Rock Affordable Housing Strategy (2024-2034).

b)	are based on an assessment of local housing market conditions, by tenure, including assessing housing supply, demand, and affordability	Please see White Rock’s Interim Housing Needs Report (October 2024) and White Rock Affordable Housing Strategy (2024-2034).
c)	identify housing priorities, based on the assessment of local housing market conditions, household incomes, changing population and household demographics, climate change and natural hazards resilience, and key categories of local housing need, including specific statements about special needs housing and the housing needs of equity-seeking groups	Please see White Rock’s Interim Housing Needs Report (October 2024) and White Rock Affordable Housing Strategy (2024-2034).
d)	identify implementation measures within their jurisdiction and financial capabilities, including actions set out in Action 4.1.8	Please see White Rock’s Interim Housing Needs Report (October 2024) and White Rock Affordable Housing Strategy (2024-2034).

**Strategy 4.2 Protect tenants and expand, retain, and renew rental housing supply**

Section	Policy	Applicable OCP Policies
<b>Adopt Regional Context Statements that:</b>		
a)	indicate how they will, within their local context, contribute toward the regional target of having at least 15% of newly completed housing units built within all Urban Centres and Frequent Transit Development Areas combined, to the year 2050, be affordable rental housing units (recognizing that developing affordable rental housing units in transit-oriented locations throughout the urban area is supported)	<i>See response in Targets section</i>
b)	articulate how local plans and policies will mitigate impacts on renter households, particularly during redevelopment or densification of Urban Centres and Frequent Transit Development Areas	Policy 10.2.3 speaks to tenant relocation and protection and policies 10.2.1 and 10.2.2 speak to preserving and protecting existing rental stock.
c)	identify the use of regulatory tools that protect and preserve rental housing	Policy 10.2.3 speaks to tenant relocation and protection.
d)	identify policies and actions that contribute to the following outcomes:	Policy 10.2.3 speaks to tenant relocation and protection and policies 10.2.1 and 10.2.2 speak to preserving and protecting existing rental stock.
i)	increased supply of affordable rental housing in proximity to transit and on publicly-owned land	Policy 10.2.1 supports new affordable non-market and rental housing, especially in transit-accessible locations.
ii)	increased supply of market and below-market rental housing through the renewal of aging purpose-built rental housing and prevention of	Policy 10.2.2 explores the preservation and protection of existing rental

**Policy 4.2.7**

		net rental unit loss	housing stocks through redevelopment. Policy 10.2.3 speaks to policies for tenant relocation.
	iii)	protection and renewal of existing non-market rental housing	Policy 10.2.2 explores the preservation and protection of existing rental housing stocks through redevelopment.
	iv)	mitigated impacts on renter households due to renovation or redevelopment, and strengthened protections for tenants	Policy 10.2.5 supports following the tenant relocation policy that details the requirement to support tenants who are being affected by a redevelopment of their rental residence building.
	v)	reduced energy use and greenhouse gas emissions from existing and future rental housing stock, while considering impacts on tenants and affordability	Objective 11.5 and Policy 11.5.1 encourage energy conversations and reductions of greenhouse gas emissions, and further undertake a study to establish updated greenhouse gas emissions targets, including city-wide targets as well as targets for buildings, transportation, and solid waste.
	<b>Section</b>	<b>Policy</b>	<b>Supplementary Information</b>
<b>Policy 4.2.8</b>	<b>Prepare and implement housing strategies or action plans that:</b>		
	a)	encourage the supply of new rental housing and mitigate or limit the loss of existing rental housing stock	See White Rock’s 2021 Housing Needs Report and 2024 Interim Housing Needs Report, as well as the Affordable Housing Strategy (2024 – 2034).
	b)	encourage tenant protections and assistance for renter households impacted by renovation or redevelopment of existing purpose-built rental housing	See White Rock’s 2021 Housing Needs Report and 2024 Interim Housing Needs Report, as well as the Affordable Housing Strategy (2024 – 2034).
	c)	cooperate with and facilitate the activities of Metro Vancouver Housing under Action 4.2.2	See White Rock’s 2021 Housing Needs Report and 2024 Interim Housing Needs Report, as well as the Affordable Housing Strategy (2024 – 2034).
<b>Strategy 4.3 Meet the housing needs of lower income households and populations experiencing or at risk of homelessness</b>			
<b>Policy 4.3.7</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
	<b>Adopt Regional Context Statements that:</b>		
	a)	indicate how they will collaborate with the Federal Government, the Province, and other partners, to assist in increasing the supply of permanent, affordable, and supportive housing units	Policy 10.2.1 supports opportunities for new affordable and rental housing

	b)	identify policies and actions to partner with other levels of government and non-profit organizations in order to create pathways out of homelessness and contribute to meeting the housing and support needs of populations experiencing or at risk of homelessness	and look that could include partners with other levels of government.
<b>Policy 4.3.8</b>	<b>Section</b>	<b>Policy</b>	<b>Supplementary Information</b>
	<b>Prepare and implement housing strategies or action plans that</b>		
	a)	identify opportunities to participate in programs with other levels of government to secure additional housing units to meet the housing needs of lower income households	See White Rock’s 2021 Housing Needs Report and 2024 Interim Housing Needs Report, as well as the Affordable Housing Strategy (2024 – 2034).
	b)	identify strategies to increase community acceptance and communicate the benefits of affordable and supportive housing development	
c)	are aligned with or integrate plans to address homelessness, and identify strategies to reduce the total number of households that are in core housing need and populations experiencing or at risk of homelessness		

## Goal 5

<b>Metro 2050 <a href="#">Goal 5: Support Sustainable Transportation Choices</a></b>			
<p><i>Describe how the OCP and other supporting plans and policies contribute to this Goal:</i></p> <p><i>Please refer to the policies below. These policies are based on the City's Integrated Transportation and Infrastructure Master Plan, which contains a specific objective (objective 3.2) to promote sustainable modes of transportation such as walking, cycling, and transit</i></p>			
<b>Strategy 5.1 Coordinate land use and transportation to encourage transit, multiple-occupancy vehicles, cycling and walking</b>			
<b>Policy 5.1.14</b>	<b>Section</b>	<b>Policy</b>	<b>Applicable OCP Policies</b>
	<b>Adopt Regional Context Statements that:</b>		
	a)	identify land use and transportation policies and actions to encourage a greater share of trips made by transit, shared mobility options, cycling, walking, and rolling	<p>Policies 12.1.1 through 12.2.6 encourage and support active transportation.</p> <p>Objective 12.3 and Polices 12.3.1 through 12.3.9 encourage the use of transit.</p>
	b)	support the development and implementation of transportation demand management strategies, such as: parking pricing and supply measures, transit priority measures, end-of-trip facilities for active transportation and micro-mobility, and shared mobility services	Policy 12.3.9 details transportation demand management strategies requirements as part of all new developments.
	c)	manage and enhance municipal infrastructure in support of transit, multiple-occupancy vehicles, cycling, walking, and rolling	Policies 12.2.1 through 12.2.6 support infrastructure to support transit, cycling, walking and rolling.
	d)	support the transition to zero-emission vehicles	Policy 12.5.4 details White Rock's transition to electric vehicles.
e)	support implementation of the Regional Greenway Network and Major Bikeway Network, as identified in Map 10	<p>Policies 14.1.2, 14.2, 14.2.1 and 14.2.2 support the implementation of regional greenways.</p> <p>Policy 12.2.2 supports a major bikeway network through the local bikeway network.</p>	

	f)	support implementation of local active transportation and micro-mobility facilities that provide direct, comfortable, all ages and abilities connections to the Regional Greenway Network, Major Bikeway Network, transit services, and everyday destinations	The OCP Policy 9.3.1 supports connectivity through the creation of pedestrian pathways and the creation of greenway connections, which will support the Regional Greenway Network.
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**Strategy 5.2 Coordinate land use and transportation to support the safe and efficient movement of vehicles for passengers, goods, and services**

	Section	Policy	Applicable OCP Policies
<b>Policy 5.2.6</b>	<b>Adopt Regional Context Statements that:</b>		
	a)	identify routes on a map for the safe and efficient movement of goods and service vehicles to, from, and within Urban Centres; Frequent Transit Development Areas; Major Transit Growth Corridors; Industrial, Employment, and Agricultural lands; ports; airports; and international border crossings	Please see Map 2.
	b)	identify land use and related policies and actions that support the optimization and safety of goods movement via roads, highways, railways, aviation, short sea shipping, and active transportation	<p>Objective 13.4 - To enable the efficient delivery of goods to local businesses in White Rock.</p> <p>Policy 13.4.1 Deliveries – Establish delivery times during off-peak hours in areas where there is congestion with goods delivery.</p> <p>Policy 13.4.2 Truck Routes – Maintain existing dedicated Truck Routes in White Rock’s Street and Traffic Bylaw.</p>
	c)	support the development of local and regional transportation system management strategies, such as the provision of information to operators of goods and service vehicles for efficient travel decisions, management of traffic flow using transit priority measures, coordinated traffic signalization, and lane management	<p>Objective 13.5 and Policies 13.5.1 to 13.5.5 provide support for enabling the safe movement of vehicles and effectively managing parking supply.</p> <p>Objective 13.4 and Policies 13.4.1 to 13.4.3 relate to the efficient delivery of goods to businesses in White Rock. Policy 13.4.2 notes that the City’s goods are delivered on dedicated Truck Routes as identified in the City’s Street and Traffic Bylaw, which functions as the City’s Goods Movement Network map</p>

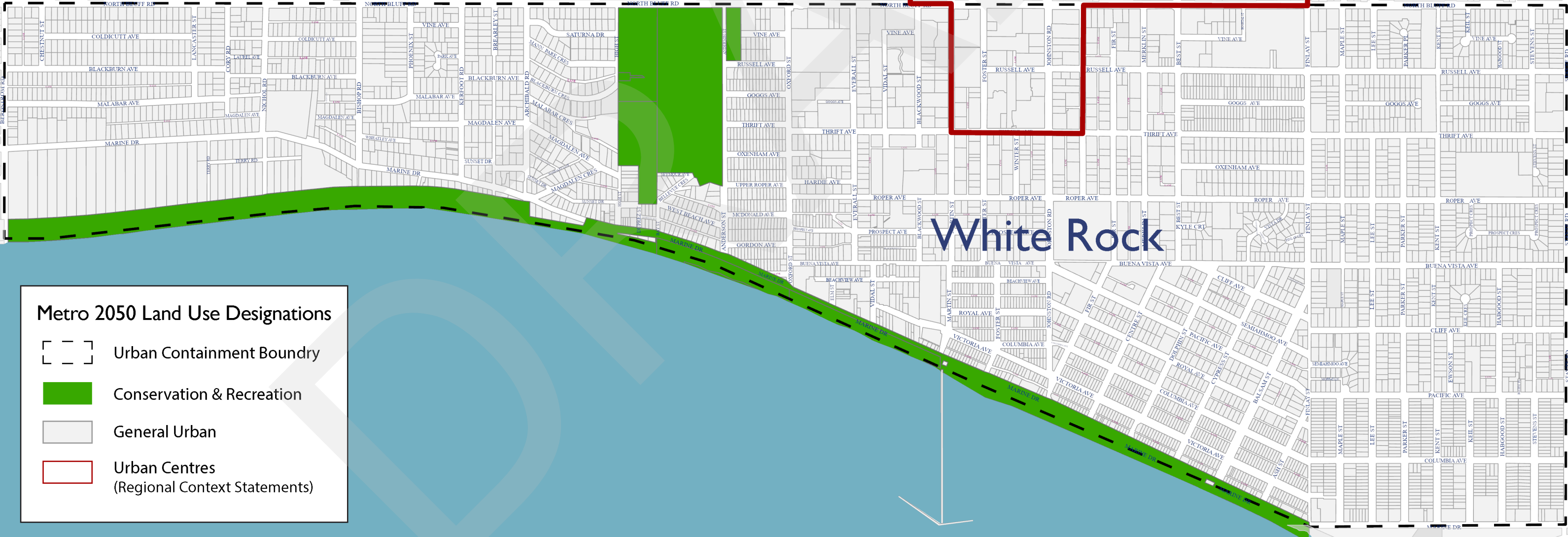
	d)	identify policies and actions that support the protection of rail rights-of-way, truck routes, and access points to navigable waterways in order to reserve the potential for goods movement	Section 9 Waterfront, Objective 9.3 and Policy 9.3.1 support the protection of access points to navigable waterways.
	e)	identify policies and actions to mitigate public exposure to unhealthy levels of noise, vibration, and air pollution associated with the Major Road Network, Major Transit Network, railways, truck routes, and Federal / Provincial Highways	Policy 12.5.1 supports the reduction of Greenhouse Gas Emissions, which will mitigate public exposure to air pollution.
	f)	identify policies and actions that anticipate the land and infrastructure requirements for goods movement and drayage, such as truck parking, zero-emission vehicle charging infrastructure, and e-commerce distribution centres, and mitigate any negative impacts of these uses on neighbourhoods	Policy 13.1.4 Rail Crossings – Explore opportunities to upgrade and improve the safety of rail crossings.

## Metro 2050 Implementation

*Metro 2050* includes some policies that can be used if the Member Jurisdiction includes language permitting them, either below or in statements elsewhere in the RCS.

	Policy	Member Jurisdiction Response
<b>Policy 6.2.7</b>	<p>A Member Jurisdiction may include language in its Regional Context Statement that permits amendments to the municipality's Official Community Plan to adjust the boundaries of regional land use designations within the Urban Containment Boundary, as follows:</p> <p>a) the Member Jurisdiction may re-designate land from one regional land use designation to another regional land use designation, only if the aggregate area of all proximate sites so re-designated does not exceed one (1) hectare;</p> <p>b) notwithstanding section 6.2.7 (a), for sites that are greater than one (1) hectare and less than three (3) hectares in area, the Member Jurisdiction may redesignate land:</p> <ul style="list-style-type: none"> <li>• from Industrial to General Urban regional land use designation, if the site is contiguous with an Industrial site and the developable portion of the site will be predominantly within 150 metres of an existing or approved rail rapid transit station; or</li> <li>• from Industrial to Employment regional land use designation if the developable portion of the site will be predominantly within 250 metres of an existing or approved rail rapid transit station,</li> </ul> <p>provided that:</p> <ul style="list-style-type: none"> <li>• the re-designation does not impede rail, waterway, road, or highway access for industrial uses; and</li> <li>• the aggregate area of all proximate sites so re-designated does not exceed three (3) hectares;</li> </ul> <p>c) the aggregate area of land affected by all re-designations under section 6.2.7 (a) and (b) together cannot exceed two (2) percent of the Member Jurisdiction's total lands within each applicable regional land use designation as of July 29, 2011.</p>	<p><i>The City of White Rock acknowledges that these policy provisions are within our Regional Context Statement so that they may be permitted as required.</i></p>
<b>Policy 6.2.8</b>	<p>A Member Jurisdiction may include language in its Regional Context Statement that permits amendments to its Official Community Plan to adjust the boundaries of Urban Centres and Frequent Transit Development Areas, provided such boundary adjustments meet the guidelines set out in Table 3 (Guidelines for Urban Centres and Frequent Transit Development Areas) of the Regional Growth Strategy.</p>	<p><i>The City of White Rock acknowledges that this policy provision is within our Regional Context Statement so that they may be permitted as required.</i></p>

# Appendix A — Map 1

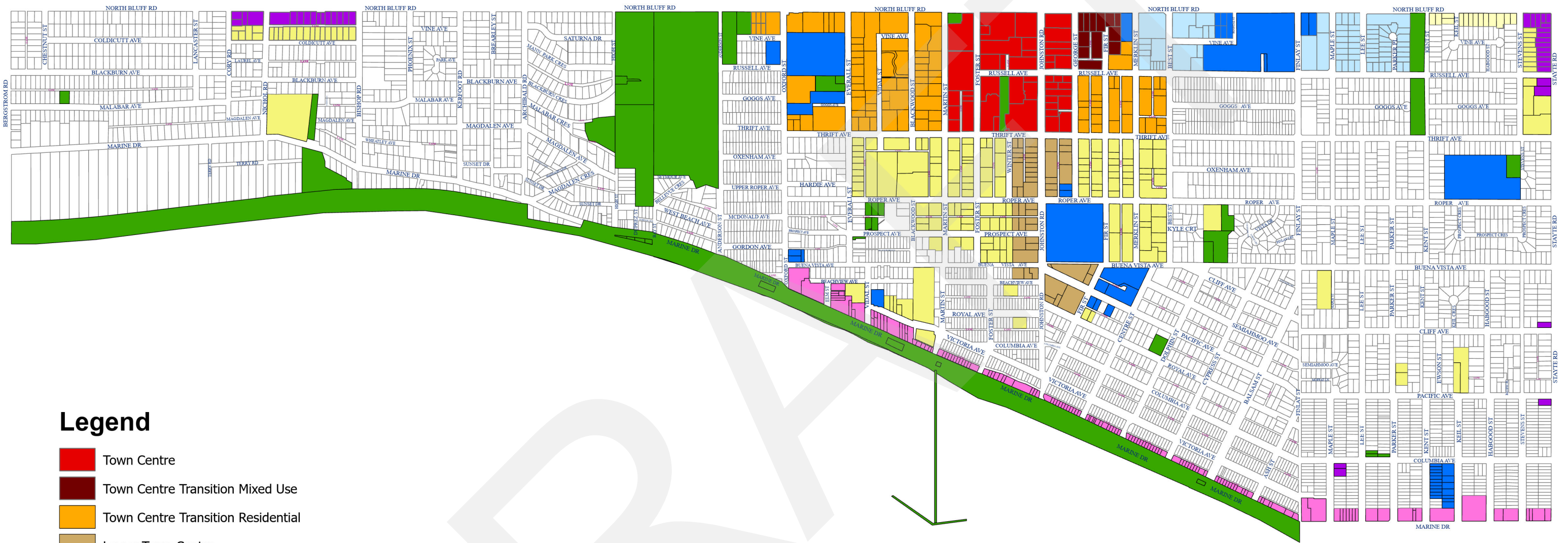


Appendix B Maps

new!

DRAFT



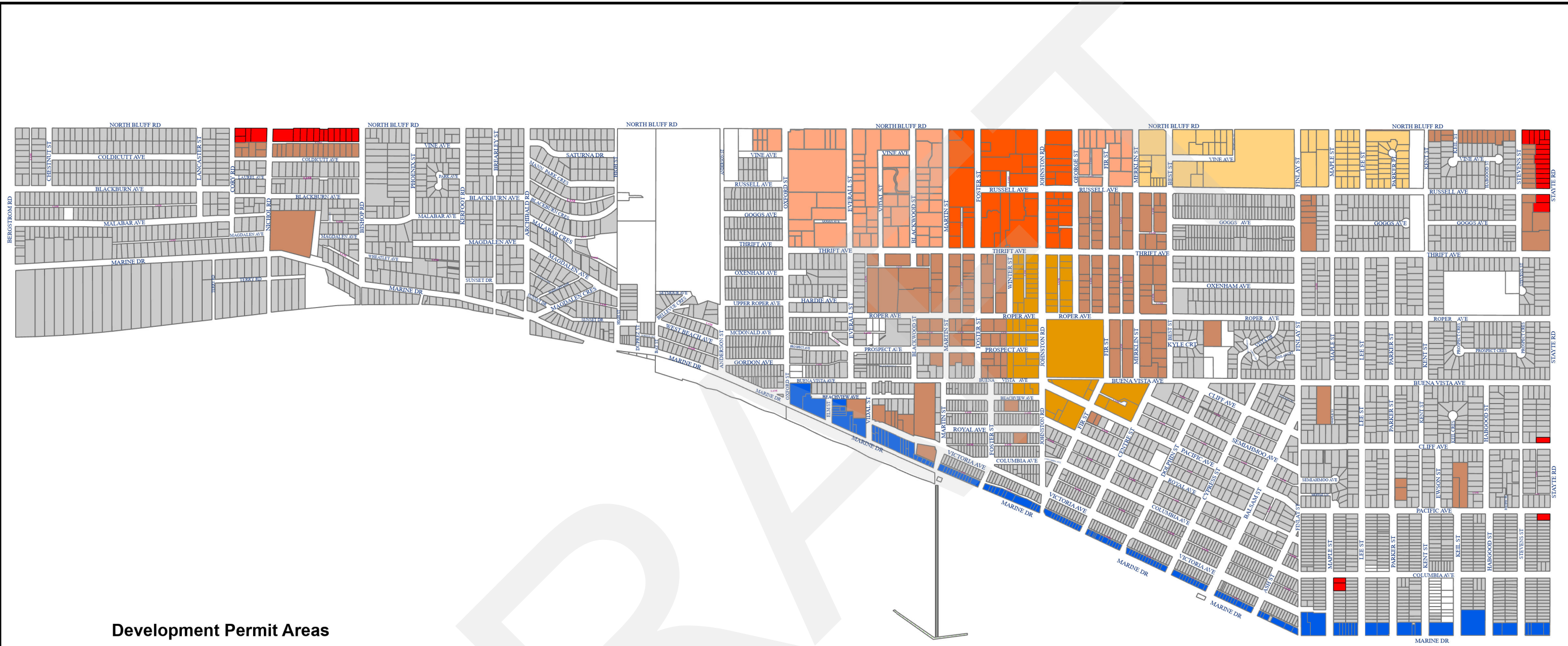


## Legend

- Town Centre
- Town Centre Transition Mixed Use
- Town Centre Transition Residential
- Lower Town Centre
- Health District
- Waterfront Village
- Urban Neighbourhood
- North Bluff East
- Mature Neighbourhood
- Neighbourhood Commercial
- Institutional and Utility
- Open Space & Recreation

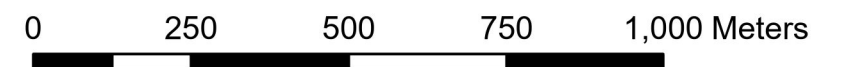


0 250 500 750 1,000 Meters

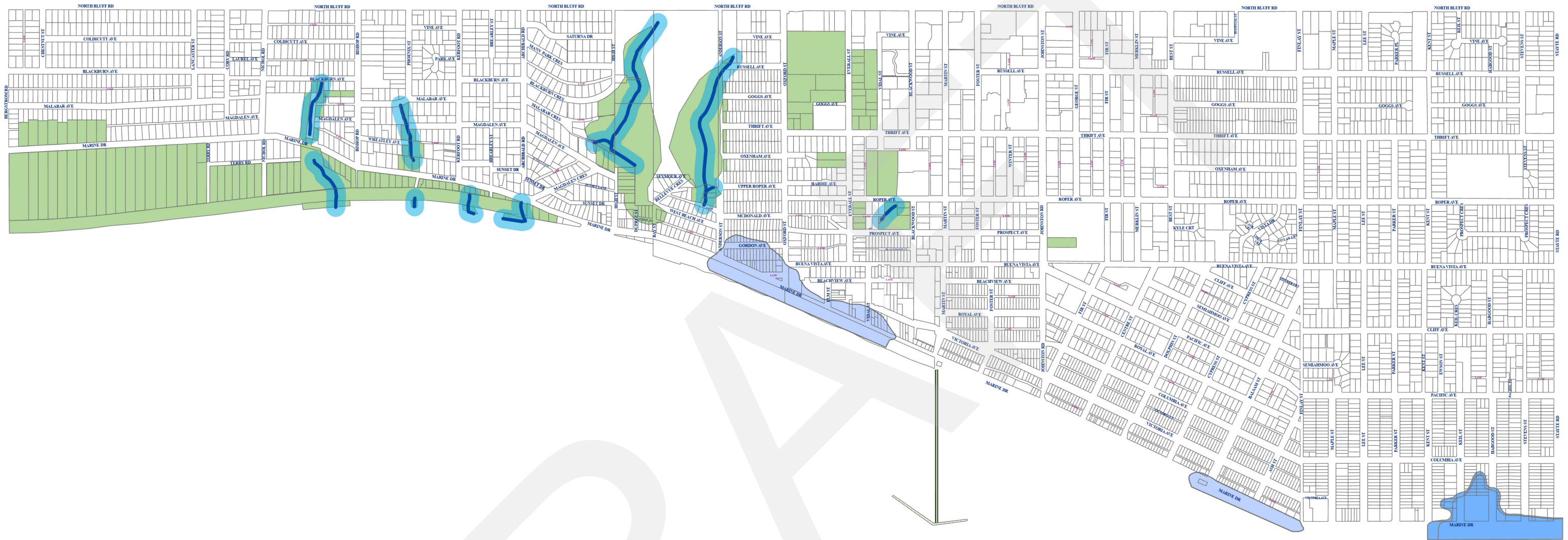


**Development Permit Areas**

- Mature Neighbourhood SSMUH
- Town Centre
- Town Centre Transition
- Lower Town Centre
- Neighbourhood Commercial
- Multi-family
- Waterfront
- Health District



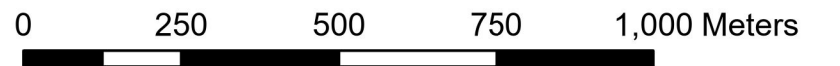
**Map B—Form and Character Development Permit Areas**



**Legend**

- Environmental (Ravine Lands and Significant Trees) Development Permit Area
- Environmental (Watercourse) Development Permit Area
- Environmental (Floodplain) Development Permit Areas**
- Ocean Floodplain - 3.90m Contour Level
- Campbell River Floodplain - 4.60m Contour Level (Includes Ocean Flood plain)
- Parcel

Data Source: City of White Rock  
 Note: Locations of Floodplains and Watercourses shown on this map are approximate



**Map C—Environmental Development Permit Areas**