

The Corporation of the
CITY OF WHITE ROCK



Land Use and Planning Committee
AGENDA

Monday, October 19, 2020, 6:00 p.m.

On Tables starting on pg. 525

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

***Live Streaming/Telecast:** Please note that all Committees, Task Forces, Council Meetings, and Public Hearings held in the Council Chamber are being recorded and broadcasted as well included on the City's website at: www.whiterockcity.ca

The City of White Rock is committed to the health and safety of our community. In keeping with Ministerial Order No. M192 from the Province of British Columbia, City Council meetings will take place without the public in attendance at this time until further notice.

T. Arthur, Director of Corporate Administration

Pages

1. CALL TO ORDER (Councillor Trevelyan, Chairperson)

1.1. MOTION TO CONDUCT LAND USE AND PLANNING COMMITTEE MEETING WITHOUT THE PUBLIC IN ATTENDANCE

RECOMMENDATION

THAT the Land Use and Planning Committee:

WHEREAS COVID-19 has been declared a global pandemic;

WHEREAS the City of White Rock has been able to continue to provide the public access to the meetings through live streaming;

WHEREAS holding public meetings in the City Hall Council Chambers, where all the audio/video equipment has been set up for the live streaming program, would not be possible without breaching physical distancing restrictions due to its size, and holding public meetings at the White Rock Community Centre would cause further financial impact to City Operations due to staffing resources and not enable live streaming;

WHEREAS Ministerial Order No. 192 requires an adopted motion in order to hold public meetings electronically, without members of the public present in person at the meeting;

THEREFORE BE IT RESOLVED THAT the Land Use and Planning Committee (including all members of Council) authorizes the City of White Rock to hold the September 14, 2020 meeting to be video streamed and available on the City's website, and without the public present in the Council Chambers.

2. ADOPTION OF AGENDA

RECOMMENDATION

THAT the Governance and Legislation Committee adopt the agenda for October 19, 2020 as circulated.

3. ADOPTION OF MINUTES

5

RECOMMENDATION

THAT the Land Use and Planning Committee adopt the following minutes:

- September 14, 2020; and,
- October 5, 2020.

4. CORPORATE REPORTS

4.1. BOSA PROJECT: MIRAMAR

Councillor Johanson requested this topic be placed on the agenda for discussion.

4.2. UPDATE ON PROPOSED REVISIONS TO COUNCIL POLICIES 511 and 514

13

Corporate report dated October 19, 2020 from the Director of Planning and Development Services titled "Update on Proposed Revisions to Council Policies 511 and 514".

RECOMMENDATION

THAT the Land Use and Planning Committee recommend that Council endorse the proposed revisions to Council Policy 511: Density Bonus Amenity Contribution and Council Policy 514: Tenant Relocation Policy.

4.3. REZONING AND MAJOR DEVELOPMENT PERMIT APPLICATION – 1485 FIR STREET (ZON/MJP 19-009)

49

Corporate report dated October 19, 2020 from the Director of Planning and Development Services titled "Rezoning and Major Development Permit Application – 1485 Fir Street (ZON/MJP 19-009)".

RECOMMENDATION

THAT the Land Use and Planning Committee recommends:

1. That Council give first and second readings to “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 -1485 Fir Street) Bylaw, 2020, No. 2363 as presented, and direct staff to schedule the required Public Hearing;

2. That Council direct staff to resolve the following issues prior to final adoption, if Bylaw No. 2363 is given Third Reading after the Public Hearing:

a) Ensure that all engineering requirements and issues, including dedication of a 5.0 metre by 5.0 metre corner cut on the corner of the site at Fir Street and Russell Avenue, intersection improvements including ‘watch for pedestrian’ signage as well as tactile paving on the northwest and northeast corners of George Lane and Thrift Avenue, and completion of a servicing agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;

b) A Tenant Relocation Plan and adoption of a Housing Agreement Bylaw are finalized; and

c) The consolidation of existing three lots and the demolition of the existing residential building occurs; and

3. That, pending adoption of “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 – 1485 Fir Street) Bylaw, 2020, No. 2363,” Council consider issuance of Development Permit No. 432 for 1485 Fir Street.

4.4. CONSIDERATION OF NEW CITY HALL AND THEATRE/PERFORMANCE FACILITY IN PROPOSED TOWN CENTRE DEVELOPMENT PROJECT

456

Corporate Report dated October 19, 2020 from the Director of Planning and Development Services titled "Consideration of New City Hall and Theatre/Performance Facility in Proposed Town Centre Development Project".

RECOMMENDATION

THAT the Land Use and Planning Committee provide an opportunity at this meeting for the owner of 1513 Johnston Road to discuss with the Committee their intentions to redevelop their property, including the feasibility of incorporating a new City Hall facility with public amenities into the redevelopment.

4.5. CONSIDERATION OF ACTIVE REZONING APPLICATIONS

480

Corporate report dated October 19, 2020 from the Director of Planning and Development Services titled "Consideration of Active Rezoning Applications".

RECOMMENDATION

THAT the Land Use and Planning Committee recommends that Council:

1. Resolve that the zoning amendment application at 1464 Vidal Street

proceed to the next stage in the application review process or be denied;

2. Resolve that the zoning amendment application at 15963 Marine Drive proceed to the next stage in the application review process or be denied;

3. Resolve that the zoning amendment application at 1441 Vidal Street proceed to the next stage in the application review process or be denied;

4. Resolve that the zoning amendment application at 15704 North Bluff Road proceed to the next stage in the application review process or be denied;

5. Resolve that the zoning amendment application at 14401 Sunset Drive proceed to the next stage in the application review process or be denied;

6. Resolve that the zoning amendment application at 14068 North Bluff Road proceed to the next stage in the application review process or be denied;

7. Resolve that the zoning amendment application at 1091 Stayte Road proceed to the next stage in the application review process or be denied;

8. Resolve that the zoning amendment application at 15109 Elm Street proceed to the next stage in the application review process or be denied;

9. Resolve that the zoning amendment application at 1361 Finlay Street proceed to the next stage in the application review process or be denied; and

10. Resolve that the zoning amendment application at 15053 Marine Drive Street proceed to the next stage in the application review process or be denied.

4.6. EARLY REVIEW OF REZONING APPLICATIONS (1363 JOHNSTON ROAD & 1453 STAYTE ROAD)

513

Corporate report dated October 19, 2020 from the Director of Planning and Development Services titled "Early Review of Rezoning Applications (1363 Johnston Road & 1453 Stayte Road)".

RECOMMENDATION

THAT the Land Use and Planning Committee recommends that Council:

1. Resolve that the zoning amendment application at 1363 Johnston Road proceed to the next stage in the application review process or be denied; and

2. Resolve that the zoning amendment application at 1453 Stayte Road proceed to the next stage in the application review process or be denied.

5. CONCLUSION OF THE OCTOBER 19, 2020 LAND USE AND PLANNING COMMITTEE MEETING

PRESENT: Councillor Trevelyan, Chairperson
Mayor Walker
Councillor Chesney
Councillor Johanson
Councillor Kristjanson
Councillor Manning

ABSENT: Councillor Fathers

STAFF: G. Ferrero, Chief Administrative Officer
T. Arthur, Director of Corporate Administration
C. Isaak, Director of Planning and Development Services
G. Newman, Manager of Planning
S. Lam, Deputy Corporate Officer

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Please note you can watch the meeting, as well as previous meetings, online www.whiterockcity.ca/councilmeetings.

Councillor Trevelyan, Chairperson

1. CALL MEETING TO ORDER
The meeting was called to order at 5:45 p.m.

1.1 MOTION TO CONDUCT LAND USE AND PLANNING COMMITTEE MEETING WITHOUT THE PUBLIC IN ATTENDANCE

2020-LU/P-018

It was MOVED and SECONDED

WHEREAS COVID-19 has been declared a global pandemic;

WHEREAS the City of White Rock has been able to continue to provide the public access to the meetings through live streaming;

WHEREAS holding public meetings in the City Hall Council Chambers, where all the audio/video equipment has been set up for the live streaming program, would not be possible without breaching physical distancing restrictions due to its size, and holding public meetings at the White Rock Community Centre would cause further financial impact to City Operations due to staffing resources and not enable live streaming;

WHEREAS Ministerial Order No. 192 requires an adopted motion in order to hold public meetings electronically, without members of the public present in person at the meeting;

THEREFORE BE IT RESOLVED THAT the Land Use and Planning Committee (including all members of Council) authorizes the City of White Rock to hold the September 14, 2020 meeting to be video streamed and available on the City's website, and without the public present in the Council Chambers.

CARRIED

2. ADOPTION OF AGENDA

2020-LU/P-019

It was MOVED and SECONDED

THAT the Land Use and Planning Committee adopt the agenda for September 14, 2020 as circulated.

CARRIED

3. ADOPTION OF MINUTES

a) July 27, 2020 – Land Use and Planning Committee Meeting

2020-LU/P-020

It was MOVED and SECONDED

THAT the Land Use and Planning Committee adopt the minutes of the July 27, 2020 meeting as circulated.

CARRIED

4. INITIAL INFORMATION REPORTS FOR ZONING AMENDMENT APPLICATIONS

Corporate report dated September 14, 2020 from the Director of Planning and Development Services titled "Initial Information Reports for Zoning Amendment Applications".

In response to the Committee's question, staff clarified that if approved, the proposed process would only impact future zoning amendment applications (the process would not be backdated to those already in the system).

2020-LU/P-021

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council give first, second and third reading to "City of White Rock Planning Procedures Bylaw, 2017, No. 2234, Amendment (Initial Information Reports for Zoning Amendments) Bylaw, 2020, No. 2357."

CARRIED

5. APPLICATION FOR ZONING AMENDMENT – 15561 & 15569 OXENHAM AVENUE (ZON/SUB 19-022)

Corporate report dated September 14, 2020 from the Director of Planning and Development Services titled "Application for Zoning Amendment – 15561 & 15569 Oxenham Avenue (ZON/SUB 19-022)".

Discussion ensued and the following comments noted:

- Concerns expressed that the tree removal would pose an environmental implication as there would be a reduction in the tree canopy. Staff clarified that in an development scenario, the trees would likely be removed as the roots have grown and are located in critical zones

2020-LU/P-022

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council give first and second readings to “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RS-4 – 15561/15569 Oxenham Avenue) Bylaw, 2020, No. 235.*”

CARRIED

Councillors Johanson and Kristjanson voted in the negative

2020-LU/P-023

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommend Council:

1. Direct staff to schedule the public hearing for “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RS-4 – 15561/15569 Oxenham Avenue) Bylaw, 2020, No. 2358.*” and
2. Recommend that Council direct staff to resolve the following issues prior to final adoption:
 - a) Ensure that all engineering requirements and issues including servicing agreement completion are addressed to the satisfaction of the Director of Engineering and Municipal Operations; and
 - b) Demolish the existing buildings and structures to the satisfaction of the Director of Planning and Development Services.

CARRIED

Councillor Kristjanson voted in the negative

6.

DRAFT ZONING AMENDMENT BYLAWS AND OFF-STREET RESERVE FUND BYLAWS FOR PROPOSED CR-3A ZONE (BYLAWS 2343, 2344, 2345 and 2346)

Corporate report dated September 14, 2020 from the Director of Planning and Development Services titled “Draft Zoning Amendment Bylaws and Off-Street Reserve Fund Bylaws for Proposed CR-3A Zone (Bylaws 2343, 2344, 2345 and 2346)”.

Discussion ensued and the following comments were noted:

- Residents have expressed concern for changes along Marine Drive
- Expressed support for the proposed amendments regarding parking lots for oddly shaped lots

2020-LU/P-024

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommends that Council reject consideration of the proposed bylaws regarding CR-3A Zone (Bylaws 2343, 2344, 2345, and 2346).

CARRIED

Recognizing the Committee's concern regarding heights, it was suggested that the OCP review completion date be moved forward to the end of 2020. It was noted that the waterfront should be a focus within the discussion, along with heights/protected views. It was also noted that the input of developers should be considered within the height review.

2020-LU/P-023

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommends that Council, after rejecting the proposed CR-3A Zone, accelerate the Official Community Plan review by the end of 2020 and bring forward the waterfront elements of the OCP review, and other topics as noted by the Land Use and Planning Committee.

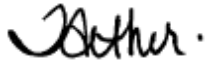
CARRIED

7.

CONCLUSION OF THE SEPTEMBER 14, 2020 LAND USE AND PLANNING COMMITTEE MEETING

The Chairperson declared the meeting concluded at 6:54 p.m.

Councillor Trevelyan
Chairperson



Tracey Arthur, Director of
Corporate Administration

PRESENT: Councillor Trevelyan, Chairperson
Mayor Walker
Councillor Chesney
Councillor Johanson
Councillor Kristjanson
Councillor Manning

ABSENT: Councillor Fathers

STAFF: G. Ferrero, Chief Administrative Officer
T. Arthur, Director of Corporate Administration
J. Gordon, Director of Engineering and Municipal Operations
C. Isaak, Director of Planning and Development Services
S. Lam, Deputy Corporate Officer
G. Newman, Manager of Planning

1. CALL MEETING TO ORDER

The meeting was called to order at 6:10 p.m.

1.1 MOTION TO CONDUCT LAND USE AND PLANNING COMMITTEE MEETING WITHOUT THE PUBLIC IN ATTENDANCE

2020/LU&P/029

It was MOVED and SECONDED

THAT The Land Use and Planning Committee:

WHEREAS COVID-19 has been declared a global pandemic;

WHEREAS the City of White Rock has been able to continue to provide the public access to the meetings through live streaming;

WHEREAS holding public meetings in the City Hall Council Chambers, where all the audio/video equipment has been set up for the live streaming program, would not be possible without breaching physical distancing restrictions due to its size, and holding public meetings at the White Rock Community Centre would cause further financial impact to City Operations due to staffing resources and not enable live streaming;

WHEREAS Ministerial Order No. 192 requires an adopted motion in order to hold public meetings electronically, without members of the public present in person at the meeting;

THEREFORE BE IT RESOLVED THAT the Land Use and Planning Committee (including all members of Council) authorizes the City of White Rock to hold the October 5, 2020 meeting to be video streamed and available on the City's website, and without the public present in the Council Chambers.

CARRIED

2. ADOPTION OF AGENDA

2020/LU&P/030

It was MOVED and SECONDED

THAT the Land Use and Planning Committee adopt the agenda for October 5, 2020 as circulated.

CARRIED

3. ADOPTION OF MINUTES

September 16, 2020

2020/LU&P/031

It was MOVED and SECONDED

THAT the Land Use and Planning Committee adopt the minutes of the September 16, 2020 meeting as circulated.

CARRIED

**4. REQUEST FOR RECONSIDERATION: DENIAL OF TREE REMOVAL PERMIT
15276 COLUMBIA AVENUE**

Corporate report dated October 5, 2020 from the Directors of Planning and Development Services and Engineering and Municipal Operations titled "Request for Reconsideration: Denial of Tree Removal Permit (15276 Columbia Avenue)".

The applicants joined the meeting electronically. The following comments were noted:

- The project is designed to conform with the City's zoning
- The tree was planted by previous owners
- It would be difficult to relocate the tree elsewhere
- BC Hydro can advise if the lines would be unsafe for the tree, and if undergrounding of the wiring could be requested
- Applicant has worked with the neighbour to the east to help protect their view

2020/LU&P/032

It was MOVED and SECONDED

THAT the Land Use and Planning Committee request the City reach out to BC Hydro and inquire if the tree is jeopardizing the power lines, and inquire if the wiring could be installed underground to the house.

CARRIED

2020/LU&P/033

It was MOVED and SECONDED

THAT the Land Use and Planning Committee defer the request for reconsideration of tree removal permit at 15276 Columbia Avenue to a future Committee meeting pending a response from BC Hydro.

CARRIED

5. **APPLICATION FOR MAJOR DEVELOPMENT PERMIT (SIGNAGE)
AMENDMENT – 15177 THRIFT AVENUE**

Corporate report dated October 5, 2020 from the Director of Planning and Development Services titled “Application for Major Development Permit (Signage) Amendment – 15177 Thrift Avenue”.

The following discussion points were noted:

- Support expressed for wayfinding signage, as a way to support businesses
- Concerns expressed regarding lighting in the evening, adding that it could impact quality of life
- For the lighting, a dimmer, or having the lights on a timer was suggested
- Height requirements for signs vary with respect to the message (eg: motorist vs. pedestrian)
- Suggested that there be directional signage to the parking considered

2020/LU&P/034

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council authorize staff to schedule the required Public Meeting for proposed Development Permit No. 429, which includes variances to Sign Bylaw, 2010, No. 1923.

CARRIED

6. **APPLICATION FOR MAJOR DEVELOPMENT PERMIT APPLICATION AND
DEVELOPMENT VARIANCE PERMIT – 14947 BUENA VISTA AVENUE
(MJP/DVP 19-021)**

Corporate report dated October 5, 2020 from the Director of Planning and Development Services titled “Application for Major Development Permit Application and Development Variance Permit – 14947 Buena Vista Avenue (MJP/DVP 19-021)”.

The following discussion points were noted:

- There will be an impact to the view north of the subject property
- The tree is privately owned and within the building envelope
- If the project was not a sub-division, views would still be impacted
- Concerns were expressed regarding the request for height variance

2020/LU&P/035

It was MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that Council direct staff to schedule the public meeting for Development Variance Permit No. 431.

CARRIED

Councillors Johanson and Kristjanson voted in the negative

7. **APPLICATION FOR ZONING AMENDMENT – 14234 MALABAR AVENUE
(ZON/MIP/SUB 19-005)**

Corporate report dated October 5, 2020 from the Director of Planning and Development Services titled “Application for Zoning Amendment – 14234 Malabar Avenue (ZON/MIP/SUB 19-005)”.

The following discussion were noted:

- Neighbours have expressed opposition to the proposed development
- The proposed project looks at ways of preserving single family dwellings
- With the land size, it was suggested that any project could impact surrounding views

2020/LU&P/036

It was MOVED and SECONDED

THAT the Land Use and Planning Committee:

1. Recommend that Council give first and second readings to “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RS-4 – 14234 Malabar Avenue) Bylaw, 2020, No. 2361,*” and
2. Recommend that Council direct staff to schedule the public hearing for “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RS-4 – 14234 Malabar Avenue) Bylaw, 2020, No. 2361.*”


DEFEATED

Councillors Johanson, Kristjanson and Manning voted in the negative

8. **CONCLUSION OF THE OCTOBER 5, 2020 LAND USE AND PLANNING COMMITTEE MEETING**

The Chairperson declared the meeting concluded at 7:11 p.m.

Councillor Trevelyan
Chairperson



Tracey Arthur, Director
Corporate Administration

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: October 19, 2020

TO: Land Use and Planning Committee

FROM: Carl Isaak, Director, Planning and Development Services

SUBJECT: Update on Proposed Revisions to Council Policies 511 and 514

RECOMMENDATION

THAT the Land Use and Planning Committee recommend that Council endorse the proposed revisions to Council Policy 511: Density Bonus Amenity Contribution and Council Policy 514: Tenant Relocation Policy.

EXECUTIVE SUMMARY

On January 27, 2020, the Governance and Legislation Committee directed staff to obtain further information regarding the tax implications for compensation received by tenants affected by redevelopment of their apartments, as well as the possibility of utilizing an annuity or other financial instrument to stretch out the payment of large compensation sums over a period of time. Staff have been in correspondence with several financial institutions, and other government agencies to obtain this information, however not all of the outstanding questions have been fully answered.

The Income Tax Rulings Directorate at Canada Revenue Agency has provided a technical interpretation stating that:

“regarding the lump-sum and top-up supplement amounts, it does not appear that these amounts would constitute income from a source, including social assistance under paragraph 56(1)(u) of the [Income Tax] Act, and therefore would not be included in income and would not be taxable under the Act.”

Further, the Provincial Ministry of Social Development and Poverty Reduction has stated that it is the Ministry’s intent to fully support clients through any tenancy displacement due to redevelopment of rental housing, including working with municipalities to ensure that clients receiving income or disability assistance do not have payments associated with tenancy displacement clawed back. While this has not been formalized in changes to their regulatory structure yet, staff believe that at this time compensation payments would not impact income-tested benefits. The City of Burnaby initiated a resolution at the 2020 Union of British Columbia Municipalities (UBCM) meeting to formalize this arrangement in the applicable legislation (*Residential Tenancy Act*), which was endorsed by UBCM.

The financial institutions that staff have been discussing annuity options with have not been able to finalize and approve products that would be applicable to this scenario. Both annuity options and guaranteed investment fund options have been explored; one of the obstacles in obtaining approval from the institution is that the amounts per tenant household are typically not large enough to meet the institutions minimum threshold for investment (e.g. some want a minimum annuity total of \$50,000, when most compensation may be in the \$10,000-\$20,000 range).

Given the uncertainty around the availability of financial products that would satisfactorily parcel out larger compensation amounts over a period of time, staff recommend that the Tenant Relocation Policy be revised to enable this as an approved method of payment if it becomes available, but not to require it in the event that this is not possible.

PREVIOUS COUNCIL DIRECTION

Resolution # and Date	Resolution Details
January 27, 2020 2020-G/L-004	THAT the Governance and Legislation Committee defers providing direction with respect to Council Policy 511: Density Bonus / Amenity Contribution Policy, Council Policy 514: Tenant Relocation Policy, and Council Policy 516: Renovation and Relocation Policy pending further information with regards to annuity and tax implications to tenants that qualify for compensation.

INTRODUCTION/BACKGROUND

The purpose of this corporate report is to provide Council with an update on the status of outstanding questions regarding proposed amendments to Council Policy 514: Tenant Relocation Policy. A previous corporate report on these proposed revisions was received by the Governance and Legislation Committee on January 27, 2020, and is attached as Appendix B.

As noted in the executive summary, staff recommend that Council Policy 514 be amended to enable, but not require, periodic payments of larger compensation sums, if such financial products are available. Staff have included this provision in the revised draft Council Policy 514 attached as Appendix A. It is recommended that this provision be enabled for compensation amounts larger than \$15,000, that they be able to be paid out annually over a period of three (3) years (e.g. a \$24,000 total sum would be paid as three (3) \$8,000 installments over three (3) years). If the financial instrument used to accomplish this splitting of payments included an interest portion paid to the holder of the benefit above the principal amount of compensation, those amounts would likely be taxable as investment earning income and count towards income-tested benefits. The taxable amounts would not likely be substantial relative to the total compensation, but may reduce the income ultimately received by the tenant household (by marginally impacting income-tested benefits).

A separate corporate report on a development application at 1485 Fir Street is also included in this Land Use and Planning Committee agenda, and if the proposed revisions to Council Policy 511 and Council Policy 514 are endorsed, this application would be subject to the revised policies. The draft density bonus / amenity zoning proposed for the project at 1485 Fir Street incorporates and assumes that the proposed revisions to Council Policy 511 and Council Policy 514 are endorsed by Council.

FINANCIAL IMPLICATIONS

The proposed changes to the Density Bonus / Amenity Contribution Policy and the Tenant Relocation Policy recommended in this corporate report and the previous corporate report dated January 27, 2020 (attached as Appendix B) are not anticipated to affect the operating budget of any City department.

By considering reduced CACs in exchange for greater compensation to tenants, there may be a reduction in the overall level of amenity contributions received. These contributions would

otherwise assist in paying for the capital costs of other amenities that are desired by the community and Council. That being said, enabling a reduction in CACs when associated with affordable housing supports may be viewed as a commitment of Council to support those who may be impacted by redevelopment projects and a community benefit for supporting current residents to stay in their neighbourhoods and close to their social networks.

LEGAL IMPLICATIONS

Council's existing housing policies in the Official Community Plan include the following:

- Policy 11.2.1** New Non-Market and Rental Housing – Support new affordable and rental housing, especially in transit accessible locations, by: (...)
- a. Waiving community amenity contributions for affordable rental housing developments and considering reductions for developments that include secure market rental units; ...
 - f. Requiring a minimum one-to-one replacement of the existing rental dwelling units when an existing rental building is proposed for redevelopment, with the average unit size of the replacement units at least 80% that of the units being replaced;
- Policy 11.2.3** Tenant Relocation – Establish a tenant relocation policy to support those affected by redevelopment of existing rental housing. Plans for relocating existing residents will be the responsibility of the developer.

The OCP policies above, led to the creation of the existing Tenant Relocation Policy. To provide the most current guidance of Council with regard to redevelopment of rental housing and support to existing tenants, to both property owners and the tenants, it is advisable to update the Tenant Relocation Policy in accordance with Council's desired levels of support for tenants.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

At the January 27, 2020 Governance and Legislation Committee meeting, several of the existing residents of a rental building proposed to be redeveloped (1485 Fir Street) spoke regarding the proposed changes to the policies and their concerns regarding redevelopment of the building. If Council moves the development application forward to a public hearing, members of the public including the residents would have an additional opportunity to comment via written or verbal submission on these Council policies as they relate to the redevelopment application.

Staff are in the process of preparing a Housing Needs Report, with the involvement of the Housing Advisory Committee. This report is intended to include public input to document the needs of the community, and public engagement (likely virtual) for this report could include discussion of the Tenant Relocation Policy.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

Not applicable.

CLIMATE CHANGE IMPLICATIONS

There are no direct implications for the City's greenhouse gas emissions or resiliency to climate change impacts foreseen as a result of changes to the policies discussed in this report.

Redevelopment of older buildings can generally result in more efficient energy usage and better utilization of urban land, but the net impact depends on case-by-case factors.

ALIGNMENT WITH STRATEGIC PRIORITIES

The proposed amendments to Council Policies 511 and 514 is not directly related to any of Council's 2018-2022 Strategic Priorities, however, a general Council Policy and Bylaw Review is included in the current list of Immediate Priorities, and a review of these policies has been ongoing since 2019.

Enabling the redevelopment of rental housing to increase the overall rental supply and assisting existing tenants during this process is generally related to the Affordable Housing scope of the Official Community Plan Review, which is a Council Strategic Priority.

OPTIONS / RISKS / ALTERNATIVES

Staff suggest that LUPC proceed with endorsing the recommendation presented in this corporate report to enable a proactive discussion with applicants about the City's expectations when it comes to tenant assistance through a redevelopment process.

LUPC could also direct staff to revise the draft Council Policies, by providing specific direction as it relates to the structure of any tenant assistance scheme.

Alternatively, the Committee, could direct staff to defer bringing these policies to Council until confirmation is received from a financial institution that they are able to provide an annuity or annuity-type product for these tenant compensations. As these products are reviewed at the head offices of large financial institutions, the time frame for this approval may take further time, particularly as these institutions are focused on adjusting to impacts related to the COVID-19 pandemic.

CONCLUSION

This corporate report provides the Land Use and Planning Committee with an update regarding additional information requested for proposed amendments to Density Bonus / Amenity Contribution Policy No. 511 and Tenant Relocation Policy No. 514. Staff have determined that lump-sum compensation income would not likely be considered as taxable income by the Canada Revenue Agency, and that the provincial Ministry of Social Development and Poverty Reduction intends to ensure that clients receiving income or disability assistance do not have payments associated with tenancy displacement clawed back.

The financial institutions with which staff have been discussing annuity options as an alternative to large lump sum payments have not been able to finalize/approve products that would be applicable to this scenario. With this being the case, staff recommend that the Tenant Relocation Policy be revised to enable an annuity option, or a similar financial product, as an approved method of compensation to tenants if the option(s) becomes available, but not to require it in the event that this is not possible.

In the proposed policy updates, the increases in compensation provided directly by the developer and reduced rents below the current policy level, paired with reduced amenity contributions to the City, are meant to enhance the assistance to existing tenants to give them greater opportunity to stay in the community near their social networks and neighbourhoods. These changes would also provide certainty around the financial liabilities related to compensating existing tenants in the event of redevelopment that, in the absence of such detail, might result in a project not being funded by a lender due to concerns regarding an unmeasurable risk. Further amendments to the Density Bonus / Amenity Contribution Policy are intended to provide greater certainty on the City's expectations with regard to applicable amenity contributions, being a benefit to both developers and residents.

Staff recommend that draft Council Policy 511 Density Amenity Contribution Policy (included in Appendix B) and draft Council Policy 514 Tenant Relocation Policy (included as Appendix A) be endorsed.

Respectfully submitted,



Carl Isaak, MCIP, RPP
Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendation of this report.



Guillermo Ferrero
Chief Administrative Officer

Appendix A: Revised Draft Policy 511: Tenant Relocation Policy
Appendix B: Corporate Report dated January 27, 2020, titled "Options for Tenant Assistance During Redevelopment and Renovation"

APPENDIX A

Revised Draft Policy 511: Tenant Relocation Policy

THE CORPORATION OF THE
CITY OF WHITE ROCK
15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



POLICY TITLE: TENANT RELOCATION

POLICY NUMBER: PLANNING AND DEVELOPMENT SERVICES NO. 514

<i>Date of Council Adoption: June 11, 2018</i>	<i>Date of Last Amendment:</i>
<i>Council Resolution Number: 2018-230</i>	
<i>Originating Department: Planning and Development Services</i>	<i>Date last reviewed by the Governance and Legislation Committee: June 11, 2018</i>

Policy:

Purpose

The purpose of the Tenant Relocation Policy is to mitigate the impacts resulting from redevelopment of purpose-built rental apartments on current tenants. This policy requires developers to provide advanced notice and assistance to affected individuals above and beyond what is required by the *Residential Tenancy Act*.

The Official Community Plan (OCP) Policy 11.2.3 is the basis for this policy, stating the City will “establish a tenant relocation policy to support those affected by redevelopment of existing rental housing. Plans for relocating existing residents will be the responsibility of the developer.”

The provisions outlined in the Tenant Relocation Policy also support the OCP objectives of supporting new rental housing and redevelopment of rental housing in order to achieve a mix of housing choices that are appropriate and affordable for residents at various stages of their lives.

Application

This policy applies to all development applications involving the demolition of purpose-built rental housing in a building with three or more units.

This policy does not apply to secondary rental market units, such as single-family residences, secondary suites, coach houses, individual condominium units, or rented structures with less than three self-contained units. Tenants displaced from non-dedicated rental units should refer to the British Columbia Residential Tenancy Act.

Note: Minor variations from the Tenant Relocation Policy to accommodate unique circumstances may be authorized at the sole discretion of the Director of Planning and Development Services. Adherence to the Tenant Relocation Policy does not guarantee development approval from Council.

Requirements

1. Tenant Relocation Plan

A Tenant Relocation Plan is required as a part of the development application submitted to the City to assist tenants with finding alternate accommodations, and to demonstrate a commitment to providing enhanced communication and support beyond the minimum requirements of the *Residential Tenancy Act*. The Tenant Relocation Plan must be submitted to staff as a part of the development application submission package and will be considered by the Land Use and Planning Committee prior to Council giving First Reading of any associated Official Community Plan Amendment Bylaw, Land Use Contract Discharge Bylaw or Zoning Amendment Bylaw, or considering the issuance of a Development Variance Permit or Development Permit. The Tenant Relocation Plan must meet the following provisions and minimum submission requirements:

1.1. Occupancy Report

Documentation on the existing tenancies must be provided to the City and should include, at a minimum, the following information:

- List of occupied units
- Type of units (number of bedrooms) and current rent levels
- Size of Units
- Vacancy rate for the past year
- Type of tenancy (lease, month to month, fixed term, etc.)
- Start and end date of each tenancy
- Tenants who may require enhanced assistance (mobility issues, seniors, etc.)

The documentation of existing tenancies in the Occupancy Report determines tenant eligibility for assistance. Tenancies active when the Occupancy Report is accepted by the City are eligible for compensation, relocation assistance, and first right of refusal under this policy, and tenancies that begin after do not qualify for assistance other than receiving Notice of Application Submission and a copy of the Tenant Relocation Policy at the start of the tenancy, and any assistance required by the *Residential Tenancy Act*.

1.2. Vulnerable Tenants Approach

The developer may be required to provide additional support for vulnerable tenants (e.g. seniors, persons with disabilities, tenants with low income, mental health issues, etc.). This will be assessed and identified on an individual basis as part of the Occupancy Report.

- (a) Additional financial compensation may be requested for vulnerable tenants;
- (b) The developer is encouraged to partner with Fraser Health Authority or a non-profit organization to provide enhanced support to vulnerable tenants; and
- (c) An approach for vulnerable tenants must be identified within the Tenant Relocation Plan.

1.3. Tenant Communication Strategy

The Tenant Communication Strategy must outline how tenants are to be proactively engaged and notified of the development application process and timeline, any opportunities for input, where to direct enquires, and any applicable tenant resources.

The City recommends engaging tenants as early as possible and tenants should be made aware of any pending development applications to the City. Affected tenants must be engaged in a timely manner throughout the development application process and formal notice should be provided as follows:

- (a) Notice of Application Submission:
 - Notice of a development application submission must be given to existing tenants within two weeks of application receipt by the City. The notice must include a copy of the Tenant Relocation Policy and a Tenant Assistance Package. All new tenancies beginning after the application submission must also be provided with a copy of the Tenant Relocation Policy and made aware of the development application.
- (b) Formal Notice:
 - A minimum of two months formal notice must be given to residents upon issuance of demolition permit.

1.4. Tenant Relocation Coordinator

A dedicated Tenant Relocation Coordinator must be identified as a part of the Tenant Relocation Plan to facilitate direct communication with tenants and provide ongoing assistance on securing suitable replacement accommodations per the provisions identified in Section 1.6. The Coordinator is to be funded by the applicant and have regular hours and contact information where they are available to tenants.

1.5. Compensation

- (a) Financial Compensation must be provided based on length of tenancy and can be in the form of free rent, a lump sum payment, or a combination of the two. Where a lump sum payment would exceed \$15,000, the payment may be made in the form of a financial instrument (e.g. annuity, Guaranteed Investment Fund, etc.) that provides a guaranteed payout over a period of no less than three years, at the choice of the developer and subject to the City being named as an irrevocable beneficiary or otherwise enabled to ensure that changes cannot be made to the term and payments amount. For tenants whose tenancy began 10 years or less prior to the date the Occupancy Report is accepted by the City are to be paid 4 months' rent plus 2 months rent for each full year of tenancy, and tenants whose tenancy began more than 10 years prior to the date the Occupancy Report is accepted by the City are to be paid 14 months' rent plus 1 months' rent for each full year of tenancy, in accordance with the compensation table below. Compensation is to be paid out no later than termination of tenancy; and

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Length of Tenancy	Required Compensation
Up to four (4) years	Three (3) months' rent
Five (5) to nine (9) years	Four (4) months' rent
Ten (10) to fourteen (14) years	Five (5) months' rent
Over fifteen (15) years	Six (6) months' rent

Compensation Table

<u>Length of Tenancy (in years)</u>	<u>Required Compensation</u>
<u>Less than 1</u>	<u>4 months' rent</u>
<u>1</u>	<u>6 months' rent</u>
<u>2</u>	<u>8 months' rent</u>
<u>3</u>	<u>10 months' rent</u>
<u>4</u>	<u>12 months' rent</u>
<u>5</u>	<u>14 months' rent</u>
<u>6</u>	<u>16 months' rent</u>
<u>7</u>	<u>18 months' rent</u>
<u>8</u>	<u>20 months' rent</u>
<u>9</u>	<u>22 months' rent</u>
<u>10</u>	<u>24 months' rent</u>
<u>11</u>	<u>25 months' rent</u>
<u>12</u>	<u>26 months' rent</u>
<u>13</u>	<u>27 months' rent</u>
<u>14</u>	<u>28 months' rent</u>
<u>15</u>	<u>29 months' rent</u>
<u>16</u>	<u>30 months' rent</u>
<u>17</u>	<u>31 months' rent</u>
<u>18</u>	<u>32 months' rent</u>
<u>19</u>	<u>33 months' rent</u>
<u>20</u>	<u>34 months' rent</u>
<u>21</u>	<u>35 months' rent</u>
<u>22</u>	<u>36 months' rent</u>
<u>23</u>	<u>37 months' rent</u>
<u>24</u>	<u>38 months' rent</u>
<u>25</u>	<u>39 months' rent</u>
<u>26</u>	<u>40 months' rent</u>
<u>27</u>	<u>41 months' rent</u>
<u>28</u>	<u>42 months' rent</u>
<u>29</u>	<u>43 months' rent</u>
<u>30, or more</u>	<u>44 months' rent</u>

- (b) The developer must arrange and pay for an insured moving company to relocate existing residents into new accommodation.

1.6. Relocation Assistance

The developer must provide assistance in finding alternate accommodations for displaced tenants as follows:

- (a) Assistance in identifying a minimum of three (3) comparable alternate accommodations, two of which must be located in the City of White Rock or within five (5) kilometres of the City of White Rock's boundary, and one of which must be a purpose-built rental unit;
- (b) Maximum rents for alternate accommodations must not exceed the CMHC rent average for the area unless agreed to by the tenant;
- (c) If the current rent is above the CMHC average rent for the area, then alternate accommodations identified should not exceed the current rental rate; and
- (d) Alternate accommodation options identified must meet the needs of the tenant (e.g. accessible units, pet friendly, family friendly housing, etc.).

A tenant may choose to opt out of receiving assistance in securing alternate accommodations, but will still be entitled to the compensation, rights, and assistance outlined elsewhere in this policy. Written notice from the tenant must be submitted to the City indicating their intention to opt out of receiving relocation assistance.

1.7. First Right of Refusal

The developer must provide all displaced tenants the first right of refusal to live in the new building based on the following options:

- (a) The developer must provide displaced tenants with the first right of refusal to rent a unit in the new development at ~~10%~~rents below starting market rent for a similar unit in the new development for a period of one (1) year, after which rents may increase in accordance with the Residential Tenancy Act. These initial rents for returning tenants must be in accordance with the following table:

<u>Length of Tenancy (in years)</u>	<u>% Below Market Rent</u>
<u>Less than 1</u>	<u>20%</u>
<u>1</u>	<u>21%</u>
<u>2</u>	<u>22%</u>

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<u>3</u>	<u>23%</u>
<u>4</u>	<u>24%</u>
<u>5</u>	<u>25%</u>
<u>6</u>	<u>26%</u>
<u>7</u>	<u>27%</u>
<u>8</u>	<u>28%</u>
<u>9</u>	<u>29%</u>
<u>10 or more</u>	<u>30%</u>

(a) ~~increase in accordance with the Residential Tenancy Act,~~ after which rents may

- (b) The developer must provide displaced tenants with the first right of refusal to purchase a unit in the new development at a discount equal to the value of 12 month rent in the same unit; or
- (c) The developer must provide displaced tenants with the first right of refusal to rent an affordable unit in projects where affordable units have been proposed, provided the tenant meets the eligibility requirements for the affordable unit. These affordable units, rents and specific time period of affordable rents shall be identified in a related Housing Agreement.

2. Tenant Assistance Package

The Tenant Assistance Package is designed for use by tenants and must clearly communicate the developers’ commitment to actively engaging tenants in the development process as outlined in the Tenant Relocation Plan. The Tenant Assistance Plan must be submitted as a part of the development application submission package for staff review and must be distributed to tenants within two (2) weeks of receiving staff approval. The Tenant Assistance Package must at minimum, include the following:

- Timeline and overview of the development application process;
- A commitment by the applicant to provide advanced notifications and updates to tenants on upcoming Public Information Meetings, Council Meetings, and Public Hearings relating to the application;
- Contact information for a dedicated Tenant Relocation Coordinator;
- Methods for advising City staff and Council of concerns;
- Responsibilities of the developer to provide financial compensation (including information on First Right of Refusal);
- Method for requesting additional support by vulnerable tenants;
- A current copy of the British Columbia *Residential Tenancy Act*;
- A copy of the City’s Tenant Relocation Policy;
- Copies of applicable tenant resources. Copies of tenant resources must also be made available on-site, in conspicuous locations within the building. Resources could include:
 - British Columbia Tenant Resource and Advisory Centre (TRAC)
 - Specialized resources for tenants who many need additional assistance

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- Other resources as appropriate

The developer is responsible for providing copies of all written correspondence and evidence that the Tenant Assistance Package has been provided to tenants, to City staff prior to staff bringing forward a report to Council for consideration of the development application.

3. Final Tenant Relocation Report

The Final Tenant Relocation Report must be submitted to City staff prior to issuance of a Demolition or Building Permit for the property. The Final Tenant Relocation Report must provide details for each displaced tenant on the following:

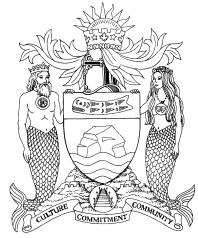
- An update to the outcome on the relocation of displaced tenants, including evidence that tenants have been satisfactorily assisted by the developer in locating alternate accommodations;
- The value of compensation given to each tenant including monetary compensation, free rent, cost of moving, and any additional assistance or services that were provided to the tenant(s).

APPENDIX B

**Corporate Report dated January 27, 2020, titled
“Options for Tenant Assistance During Redevelopment and Renovation”**

(attached separately)

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: January 27, 2020

TO: Governance and Legislation Committee

FROM: Carl Isaak, Director, Planning & Development Services

SUBJECT: Options for Tenant Assistance During Redevelopment and Renovation

RECOMMENDATIONS

THAT the Governance and Legislation Committee:

1. Receive for information the corporate report dated January 27, 2020 from the Director of Planning and Development Services titled “Options for Tenant Assistance During Redevelopment and Renovation;” and
 2. Provide direction on changes to Council Policy 511: Density Bonus / Amenity Contribution Policy, Council Policy 514: Tenant Relocation Policy, and Council Policy 516: Renovation and Relocation Policy.
-

INTRODUCTION

The purpose of this report is to provide the Committee with an analysis of the City’s tenant assistance-related policies in comparison with leading Metro Vancouver municipalities, and to offer an opportunity for the Committee to have a broad discussion regarding how they would like the City to use its authority to balance support for tenants in rental buildings with the need to renew aging buildings through appropriate repairs and redevelopment. This report also introduces, for discussion purposes, proposed changes to the Council policies on amenity contribution (Policy 511), tenant relocation (Policy 514) and renovation and relocation (Policy 516).

On December 16, 2019, Council passed the following resolution:

THAT Council directs staff to bring forward a corporate report regarding City Policies 514 and 516 with a complete comparison of information to those adopted by other municipalities such as the City of Port Coquitlam and the City of Burnaby and outline any possible “gaps” in the City’s noted policies that others may include and, could work for the City of White Rock, to help strengthen Policies 514 and 516. [2019-605]

Previously, at the September 30, 2019 Land Use and Planning Committee (LUPC) meeting, the committee discussion and resolutions regarding the development application at 1485 Fir Street to redevelop an existing rental building indicated that the LUPC is supportive of increasing the supports provided to existing tenants when their buildings are redeveloped, beyond the levels currently mandated by Policy 514. The LUPC resolution indicated that there may be support for reducing the amenity contribution requirements under Policy 511 in order to enhance the support for tenants.

Staff have been in contact with the applicant for 1485 Fir Street to determine the possibility for enhanced tenant support in that instance. Given that these policies would apply to future applications to redevelop rental buildings, it is advisable that Council also update the policies which would also apply to other applications. This would provide appropriate guidance to applicants on the expectations of the City. The 1485 Fir Street applicant has stated that they are supportive of the proposed policy changes in this corporate report.

The primary changes to the policies, which are interrelated, are to:

- meaningfully increase the compensation paid to displaced tenants;
- reduce the rent for returning tenants; and
- reduce the amount of amenity contribution sought by the City in exchange/recognition for the reduced rents and increased compensation.

Other structural changes are proposed to the amenity contribution policy to reflect the current Council Strategic Priorities document, the full list of eligible amenities outlined the Community Amenity Reserve Fund Bylaw, and to introduce a simplified target contribution rate for setting the basis for amenity contribution negotiations in the Town Centre Transition and Waterfront Village land use designations.

The changes proposed to the Density Bonus / Amenity Contribution Policy would be intended to be further updated following analysis of the public input received at the Community Amenity Contribution Forum held on January 20, 2020. The proposed update in this corporate report provides an opportunity for the Committee to discuss and amend the policy in advance to reflect changes that have occurred since the last amendment to the policy in June 2017.

PAST PRACTICE / POLICY / LEGISLATION

White Rock Official Community Plan

The overarching goal for Housing in the Official Community Plan (OCP) is that “The City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives.”

Policy 11.2.1(f) in the Housing Section of the OCP is intended to support new affordable and rental housing, especially in transit-accessible locations, by “requiring a minimum one-to-one replacement of the existing rental dwelling units when an existing rental building is proposed for redevelopment, with the average unit size of the replacement units at least 80% that of the units being replaced.”

Policy 11.2.3 of the OCP is to “establish a tenant relocation policy to support those affected by redevelopment of existing rental housing. Plans for relocating existing residents will be the responsibility of the developer.” Policy 514: Tenant Relocation Policy was adopted in June 2018 in accordance with this OCP Policy.

Existing Council Policies

- The Tenant Relocation Policy (Policy 514) was introduced and adopted by Council on June 11, 2018.
- The current Density Bonus / Amenity Contribution Policy (Policy 511) was originally adopted by Council on April 15, 2013, and last amended on June 28, 2017.
- The Renovation and Relocation Policy (Policy 516) was introduced at the Governance and Legislation Committee meeting on November 4, 2019 and endorsed by Council on November 18, 2019.

Council/Committee Resolutions

At the September 30, 2019 Land Use and Planning Committee meeting, the following motion was passed in relation to a proposed redevelopment of a rental building at 1485 Fir Street:

THAT the Land Use and Planning Committee refers the report back to staff for a revision that permits existing tenants to return to the building after construction at the same rent they are currently paying, subject to the per annum increases; and

THAT the proposed Community Amenity Contributions be reduced further in recognition for keeping the current rent. [2019-LU/P-025]

At the December 16, 2019 Regular Council meeting, the following motion was passed:

THAT Council directs staff to bring forward a corporate report regarding City Policies 514 and 516 with a complete comparison of information to those adopted by other municipalities such as the City of Port Coquitlam and the City of Burnaby and outline any possible “gaps” in the City’s noted policies that others may include and, could work for the City of White Rock, to help strengthen Policies 514 and 516. [2019-605]

Residential Tenancy Act

Section 49 of the *Residential Tenancy Act* provides the conditions under which a landlord may provide notice to end a tenancy and require the unit to be vacant for the landlord’s use of the property.

Section 51 of the *Residential Tenancy Act* outlines the minimum compensation that is payable to a tenant who has received a notice to end a tenancy under the provisions of Section 49.

The above sections of the RTA are included as Appendix D, and the entire RTA is accessible at the following website: http://www.bclaws.ca/civix/document/id/complete/statreg/02078_01.

ANALYSIS

‘Renoviction’ versus ‘Demoviction’

There are two scenarios in which tenants may be required to leave their dwelling unit due to physical changes to the building (per section 49(6) of the *Residential Tenancy Act*):

1. Renovation: where the building alterations are so extensive that it is not possible to live in the unit or be relocated within another unit in the building, and the landlord requires the unit to be vacant; or
2. Demolition: where the building/unit is going to be demolished entirely (typically to allow for the development of a new building on the property).

These reasons for ending a tenancy relate to different City processes and approval requirements, which has implications for the options that the City has to control them.

In the case of renovation for a multi-unit building, typically all that is required is a Building Permit and/or Plumbing Permit, as applicable, and in some cases where the work is only electrical, the City is not directly involved as electrical permits are issued by Technical Safety BC. Building and Plumbing Permits are not discretionary approvals or approved through Council, and if an applicant submits a complete application that is in conformance to the Zoning Bylaw and applicable Code, staff must issue the permit. Any compensation that is owing to tenants as a result of the requirement to vacate the unit, or right of first refusal to return to the renovated unit is solely determined by the *Residential Tenancy Act*. It is possible, though

unlikely, that an application to renovate a building or unit within a building will also require a rezoning.

The Council Policy related to tenant support during renovation of rental units is Policy 516: Renovation and Relocation Policy.

In the case of the demolition of a purpose-built rental building, typically this would involve an application to also develop a new, larger building on the property and therefore include an application to amend the Zoning Bylaw for the property to allow a larger building. Zoning Bylaw approvals are discretionary decisions of Council, and, in conjunction with public input and staff recommendations regarding the application, Council may consider its policies such as a Tenant Relocation Policy when determining if it will approve a rezoning, and to refuse an application to rezone if the proposal does not meet Council's policies. Issues such as compensation, moving expenses, or right of first refusal to return to the unit at a specific rent level can be included in a Tenant Relocation Policy and reviewed as part of negotiations with an applicant during a rezoning process, but would not be specified in advance through another City bylaw. If the City were to apply these residential tenancy requirements through a bylaw instead of through a policy implemented during rezoning negotiations, the bylaw requirements may be found to be outside the authority of a local government.

The Council Policy related to tenant support during demolition (and rezoning) of purpose-built rental buildings is Policy 514: Tenant Relocation Policy.

Comparison with Port Coquitlam's (Bylaw) Approach to Renovation

The City of Port Coquitlam's Business Bylaw includes provisions that require building owners undertaking renovations to a unit that needs to be vacant, to arrange for temporary accommodation for the tenants for the duration of the renovations, and allows the tenant to return to their unit at the same rent they were paying prior to the renovation. The amendment to the Port Coquitlam Business Bylaw that introduced these regulations is attached as Appendix G.

This provision of the Business Bylaw was challenged by a landlord in Port Coquitlam who applied to the Supreme Court to set aside the bylaw, primarily on the basis that the Business Bylaw was outside of the jurisdiction ('ultra vires') of the City of Port Coquitlam and that it is inconsistent with the *Residential Tenancy Act*. The courts have not provide a ruling on the validity of the bylaw because the landlord discontinued their challenge of the bylaw.

As there was no decision from the Supreme Court on this legal challenge, it is possible that another legal challenge to a similar bylaw would occur, with uncertain results. If Council determines that it is interested in pursuing a similar regulation as Port Coquitlam's, it is recommended that Council receive advice on the enforceability, legality, and potential unintended consequences of such a bylaw.

The City's current policy for renovation of rental units (Policy 516: Renovation and Relocation Policy) does not stipulate that building owners must arrange for temporary accommodation for tenants or require that the tenant be returned at the same rent they were paying prior to the renovation. The landlord and tenant relationship under this policy is entirely governed by the *Residential Tenancy Act*, which currently allows that the landlord may determine the rent following renovation, and does not oblige the landlord to arrange for temporary accommodation for tenants.

Policy 516 does task the Building Department with obtaining feedback from the Residential Tenancy Branch (RTB) as to whether the scope of work in a renovation permit would require that a unit be vacant per the Branch's regulations. However, further discussions between staff

and the RTB have indicated that the RTB would not typically review building plans unless a dispute of an eviction notice is filed, and in that case the dispute would be ruled on by an Arbitrator with the RTB. Alternatively, the RTB has a policy guideline for “Ending a Tenancy to Demolish, Renovate, or Convert a Rental Unit to a Permitted Use” which provides a list of examples of common renovations and whether they are likely to require a vacancy or not. This policy guideline is attached as Appendix E.

Staff recommend that if Council is interested in retaining a policy-based approach, that Policy 516 be updated to direct the Building Department to request confirmation of whether a dwelling is currently tenanted at the time of a renovation permit application submission, and at the time of permit issuance send a letter to the tenant with a copy of the RTB policy guideline “Ending a Tenancy to Demolish, Renovate, or Convert a Rental Unit to a Permitted Use.” Further, the letter would include a comment from Building Department staff on whether the type of renovation or repair, according to the examples in the RTB policy guideline, is likely or unlikely to require vacancy of the unit. The same comment would be provided on the renovation permit itself. The purpose of providing this information to the tenant would be to inform them of their rights as a tenant. If they should be considering filing a dispute of any subsequent eviction notice related to the permit, this information would provide them with an early assessment of whether such an eviction is necessary and the process that is required by the RTB (though ultimately it would be up to the RTB Arbitrator to rule on the specific circumstances of the renovation in a dispute). A proposed revision to Policy 516: Renovation and Relocation Policy is attached as Appendix C.

Comparison with Burnaby’s (Policy) Approach to Renovation/Demolition

On December 2, 2019, Burnaby City Council gave approval-in-principle to an update to its Tenant Assistance Policy. The related Burnaby council report is attached to this corporate report as Appendix F.

The Burnaby Tenant Assistance Policy, as updated, applies to purpose-built market rental buildings with five or more units that require rezoning, either for renovation or redevelopment, and would not apply to renovations or redevelopments that do not involve a rezoning (e.g. they only require a Building Permit). As noted on page 4 of the Burnaby report, a local government may only require tenant assistance supplementary to the *Residential Tenancy Act* as a condition of rezoning approval, but not if solely tied to the issuance of a development permit, building permit, or demolition permit. This means that the City of Burnaby applies the Tenant Assistance Policy to rezoning applications, as is also the case with the City of White Rock’s Policy 514: Tenant Relocation Policy.

It is relevant to note that the context and scale of development in Burnaby, which has two rapid transit lines in close proximity to the Metro Core, is different to that found in White Rock. This context affects the financial viability of proponents to overcome the costs of redevelopment while being able to support tenant compensation, relocation and first rights of refusal to return upon project completion at below market rent levels. For example, a sample of the rental redevelopment projects that the Tenant Assistance Policy would apply to (from a Public Hearing agenda on November 19, 2019) include:

- A 35-storey high-rise strata apartment building and a six-storey rental apartment podium
- A 37-storey high-rise strata apartment building and a six-storey rental residential building
- A 43-storey high-rise strata apartment building, townhouses, and a six-storey non-market rental apartment building; and
- A 34-storey, high-rise strata and rental apartment building and a four-storey affordable rental apartment building

The densities (expressed as Floor Area Ratios, or FAR) for these projects in Burnaby are typically more than 6.0 FAR; by comparison, the project at 1485 Fir Street in White Rock is for a six-storey rental apartment building at 2.8 FAR to replace an existing rental apartment building. This would not generate the same profits that a 30+ storey strata residential development with more than double the density could provide towards tenant assistance.

It should also be noted that Burnaby’s revised policy has not been put into effect. Some of these provisions, particularly the details regarding the payment of monthly top-ups to tenants for temporary accommodation during construction of the new building, may prove difficult to implement and verify that the applicant has met their obligations. The mandated monthly contact between tenants and applicants may also be challenging to meet compliance with the *Freedom of Information and Protection of Privacy Act*.

The table below identifies commonalities, differences, and gaps between the City of White Rock’s current Tenant Relocation Policy and the City of Burnaby’s revised Tenant Assistance Policy, for Committee’s consideration.

Table 1:

	White Rock Policy 514 (Current)	Burnaby Tenant Assistance Policy (Approved-In-Principle)										
Application	All development applications (typically rezonings) involving the demolition of purpose-built rental housing in a building with three (3) or more units.	All purpose-built market rental buildings with five (5) or more units that require rezoning either for renovation or redevelopment [i.e. demolition and rebuilding] purposes.										
Eligible Tenants for Right of First Refusal and Compensation	Tenants of the building at the time of submission of the Occupancy Report (i.e. at the time of rezoning application).	Tenants of the building at the time of rezoning application, as well as previous tenants of vacant units (up to 24 months' prior to application, under specified circumstances)										
Length of Final Notice	2 months (the policy was endorsed prior to an update to the RTA that increased the notice length to 4 months; the RTA would supercede City policy)	4 months										
Compensation	<table border="1"> <thead> <tr> <th><i>Length of Tenancy</i></th> <th><i>Compensation</i></th> </tr> </thead> <tbody> <tr> <td>0-4 years</td> <td>three months' rent</td> </tr> <tr> <td>5-9 years</td> <td>four months' rent</td> </tr> <tr> <td>10-14 years</td> <td>five months' rent</td> </tr> <tr> <td>15+ years</td> <td>six months' rent</td> </tr> </tbody> </table>	<i>Length of Tenancy</i>	<i>Compensation</i>	0-4 years	three months' rent	5-9 years	four months' rent	10-14 years	five months' rent	15+ years	six months' rent	Only provided as an alternative to monthly rental top-up (where tenant seeks an exception based on no longer requiring rental accommodation in the area, such as when they intend to purchase a home). The amount is equivalent to the value of the rent top-up for 36 months. Applicant must not enter into compensation agreement until a City attended group tenant meeting has occurred and tenants are to communicate their selected benefit options to the City.
<i>Length of Tenancy</i>	<i>Compensation</i>											
0-4 years	three months' rent											
5-9 years	four months' rent											
10-14 years	five months' rent											
15+ years	six months' rent											

Alternate Accommodation Requirement	Identify 3 comparable units, with 2 within White Rock or 5 kilometres of the City's boundary, and 1 which must be a purpose-built rental unit. Maximum rents must not exceed CMHC rent average for area (unless agreed to by tenant), and must meet needs of tenant (e.g. accessibility, pet friendly, family friendly, etc).	Identify 3 comparable units, ideally within the same quadrant of Burnaby as current unit, and within Metro Vancouver. A tenant may also find their own temporary housing.
Monthly Rental Top-Up During Construction	None. Maximum rents are not to exceed the CMHC rent average for the area (or the current rent, if it is above CMHC rent average) unless agreed to by the tenant	Yes - applicant/landlord to provide top up (pay rent above current rent) for tenant, to a maximum of 30% above CMHC median rents for similar unit or 15% above current rent. Applicant/landlord responsible for providing ongoing top up to tenant and maintaining contact with tenants.
Moving Expenses	Applicant must pay for insured moving company arranged by applicant (at the selection of tenant)	Either flat rate payout (\$900 for studio/1 bedroom, \$1,200 for two-bedroom, and \$1,400 for three bedroom) or insured moving company arranged by applicant (at the selection of tenant)
First Right of Refusal	First right to rent/own in new building, at 10% below market rent or purchase at a discount equal to value of 12 month rent in the same unit	First right to rent a replacement unit in new building, at same rent as current unit (adjusted for permitted allowable rent increases per the RTA)
Accessible Replacement Units	Not specifically required, but implied by applicant requirement to identify their Vulnerable Tenants Approach	Yes - new units must be constructed in accordance with tenant's accessibility needs (where identified in a Household Needs Assessment)
Bonding Requirement	None	Yes - applicant to provide a cost estimate of all tenant assistance benefits to the City prior to final adoption of rezoning, and bonding for the amount (plus 1% administration fee) must be submitted to the City and is released upon the replacement units being occupied.
Communication with Tenants	City recommends engaging tenants early and in a timely manner throughout the development application process, including at a minimum, providing a notice of application and formal notice prior to demolition, and identifying a dedicated tenant relocation coordinator available to provide ongoing assistance to tenants. Public Information Meetings are also required for all rezoning applications, but are not specifically for tenants or focused on tenant assistance aspects of the proposal.	Group Tenant Meeting to be held within 60 days of Council authorizing City staff to work with the applicant (introducing tenant relocation coordinator and Household Needs Assessment form). City staff must be present at meeting. Other regular communication to occur between applicants and tenants.

Household Needs Assessment (Vulnerable Tenants)	Applicants must communicate to all tenants how they can request additional support if necessary (i.e. for vulnerable tenants).	To be submitted within two months of Group Tenant Meeting.
Implementation Guide	No	In process - not currently available
Tenant Relocation Coordinator	Yes	Yes

The major differences between the White Rock and Burnaby tenant assistance policies are:

- Burnaby’s policy also applies to a renovation of a building where a rezoning is required, whereas White Rock’s only applies to the demolition of an existing building and construction of a new rental building, not to renovations;
- Burnaby’s policy allows existing tenants to return to the new building at the current rent levels, whereas White Rock’s policy only requires that the returning tenants pay 10% below market rent (noting that significantly more density, i.e. larger buildings, would likely be required to subsidize rents in a new building to the existing rent levels of an older building);
- Burnaby’s policy requires that an applicant provide existing tenants with a monthly top-up during the construction period to cover the difference in rent between their current rents and the rent in their temporary accommodation (or lump sum compensation), whereas White Rock’s policy does not require a top up; and
- Burnaby’s policy requires the applicant to provide a history of tenancies 24 months prior to the rezoning application submission to address situations where the tenant may have been evicted in bad faith (e.g. in order to avoid compensation), whereas White Rock’s policy does not require this history.

Potential Unintended Consequences Regarding Tenant Assistance Policies and Bylaws

The overarching policy goal for Housing in the OCP is that “the City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives.” While “appropriate” housing is not directly defined in the OCP, a relevant related definition would be the definition for “acceptable housing” used by Canada Mortgage and Housing Corporation (CMHC), which is “housing that is adequate in condition, suitable in size, and affordable.” In addition to the condition, size, and cost criteria for acceptable housing, some residents may also require specialized supports (e.g. accessible features and/or assisted living services) as part of their housing, which adds a further lens to the type of housing that would be appropriate for those individuals.

As the population grows, the existing housing stock ages, and household sizes and needs change. The City’s supply of housing needs to be responsive to these changing circumstances. For detached homes, this can often mean large-scale renovations to add a suite for a family member or a ‘mortgage helper’, or rebuilding a new home that improves the living standards for its occupants. In the case of larger apartment buildings, major repairs can affect a large number of units and be costly, and unless a building owner is able to recover the costs of making upgrades or repairs, or redeveloping, they are not likely to improve the building’s condition.

If the only aim in supporting existing tenants is maintaining affordable costs (i.e. low rents), then keeping existing rental buildings occupied in essentially the condition they are in (i.e. through

recurring renovation and maintenance), rather than allowing redevelopment, is likely the easiest option, though renovations themselves can be cost prohibitive. Policies that make it more costly to redevelop (e.g. through increased financial compensation to tenants or requiring rents to remain the same) are likely to have the affect of making redevelopment less attractive and ultimately having fewer buildings redevelop. While this may keep rents low for some existing tenants, there are a few unintended consequences that may result from these policies which were intended to provide support to tenants. The potential consequences are discussed below.

1) Supply, Vacancy Rates, and Rent Increases:

If no new rental buildings are constructed to increase the supply in the area, and the need for rental housing continues to grow as ownership remains unaffordable, the increased demand will keep vacancy rates low and lead to maximum rent increases for existing tenants (up to the maximum allowed by the *Residential Tenancy Act* on an annual basis). This will also lead to high rents when the units turn over to a new tenant under a new contract. A healthier vacancy rate (e.g. greater than 3%) on a long-term basis resulting from a larger supply of rental units may have the effect of reducing the pressure on rent increases for existing tenants. Reducing supply by making policies that freeze redevelopment of rental buildings (unless offset by increased development on non-apartment sites in White Rock or elsewhere in the area) may have the unintended effect of high rents in the long term.

2) Condition and Maintenance

If landlords are unable to recover the costs of renovating or redeveloping their properties due to restrictive local policies that make it not financially viable, they may decide to reduce the maintenance of their buildings and allow them to deteriorate while waiting for a change in policies that would allow redevelopment. In this case, existing tenants may continue to pay low rents and have housing that is affordable to them but find themselves in below standard conditions because the building is not being properly maintained.

3) Buy Outs and Bad Faith Evictions

If a landlord determines that they want to redevelop, but that having long-term tenants when they apply to rezone is going to be costly due to the requirements of local tenant assistance policies, they may attempt to have existing tenants move out through ‘buy outs’ or ending tenancies in bad faith, leaving units vacant if they feel that the cost of required tenant support is greater than the income from the rental. The unintended consequence of policies that are generous towards existing tenants could be that the same long-term tenants are negatively impacted by a landlord seeking to avoid these costs.

Proposed Amendments to Council Policies 511, 514 and 516

While the main purpose of this corporate report is to provide the Committee with information and offer an opportunity for the Committee to discuss high-level issues and objectives related to tenant support, staff have also prepared a potential approach to revising Council policies, for the Committee’s consideration.

Balancing Fairness to Tenants with Financial Viability of Redevelopment

The current Tenant Relocation Policy requires that a property owner offer units to returning tenants at 10% below market rents. Nonetheless, if the market rent in the new building is twice as high as the existing rent levels, the 10% discount level for returning tenants is still 80% more than the rent they are paying now. For example, if a tenant currently rents a one-bedroom unit for \$1,000 per month and the market rent in a new comparable unit is \$2,000 per month, at a 10%

reduction their rent is \$1,800 per month, \$800 more (80% above) what they are currently paying. Many tenants may not be able to cover the increased rent, particularly those on fixed incomes who have been in the building for a long period of time.

The difference between existing rent levels in a 50+ year old apartment and what the market would be willing to pay for rent in a brand new rental building is significant. The increase in rental revenue helps to make the cost of constructing the new building financially viable. Reducing this income stream permanently to existing rent levels would likely result in the project not moving forward, as lenders would be unwilling to provide loans to finance the construction and operation of the building as the income would not be enough to service the debt. Without knowing how many tenants would be returning and how long they would remain in the new building, the financial impact of maintaining the existing rents would be difficult to predict. However, a moderate level of rent reduction for only the units that are occupied by returning tenants could be feasible, if the remaining new units are able to generate enough rental income to justify the construction of the building.

Proposed Approach to Increase Support for Tenants

One way to provide additional support to all displaced tenants (regardless of whether they choose to return to live in the new building), while also limiting the financial uncertainty around the cost of reduced rents, is to significantly increase the initial lump sum compensation that tenants receive from the developer prior to the termination of the tenancy. Tenants would be able to use this increased compensation to return to the new building and gradually put the funds towards their rent (in effect lowering the rent to current levels), or to choose a new living situation and use the funds to pay for that housing. The proposed amendment to the Tenant Relocation Policy proposes that the compensation be increased between 33% and 733%, with the tenants who have lived in the building the longest receiving proportionately more compensation, as they are more likely to have difficulty paying rent in a new building (or in a new rental agreement at an existing building at current market rents, if they choose not to return to the new building). The length of tenancy is easily quantified, but this approach does not consider other factors such as tenant age, health, income, etc., which would introduce a high level of complexity to the calculation.

While the table below and draft policy contemplates a 'lump-sum' type compensation for tenants, staff are exploring whether it would be possible to structure the compensation as an annuity payment to tenants that gets paid out over a period of time; this has not been done in other jurisdictions but may be a way to mitigate concerns that receiving a large one-time compensation could have problematic outcomes for vulnerable tenants without providing long-term financial support. If further information on the viability of this approach becomes available, staff will bring forward this information to the Committee for consideration.

The compensation table below identifies the current and proposed levels of compensation based on the length of tenancy. For comparison purposes, the City of Vancouver’s compensation amounts for displaced tenants is also included.

Table 2: Tenant Compensation Comparison

Tenancy Length (Years)	Months of Rent Compensation		
	Proposed (WR)	Current (WR)	Vancouver
0	4	3	4
1	6	3	4
2	8	3	4
3	10	3	4
4	12	3	4
5	14	4	5
6	16	4	5
7	18	4	5
8	20	4	5
9	22	4	5
10	24	5	6
11	25	5	6
12	26	5	6
13	27	5	6
14	28	5	6
15	29	6	6
16	30	6	6
17	31	6	6
18	32	6	6
19	33	6	6
20	34	6	12
21	35	6	12
22	36	6	12
23	37	6	12
24	38	6	12
25	39	6	12
26	40	6	12
27	41	6	12
28	42	6	12
29	43	6	12
30	44	6	18*

*Vancouver also requires 24 months rent compensation for tenancies over 40 years

Further, the rents charged to returning tenants are proposed to be reduced from the current level of 10% below market rents to up to a range of 20-30% below market rents, depending on the length of tenancy. The following table outlines the proposed rent levels for returning tenants:

Table 3: Proposed Rent Levels

Length of Tenancy (in years)	% Below Market Rent
Less than 1	20%
1	21%
2	22%
3	23%
4	24%
5	25%
6	26%
7	27%
8	28%
9	29%
10 or more	30%

The combination of increased compensation and reduced rents will allow some existing tenants to return to the new building and live in a brand new unit with enough financial support to continue to pay their rent at current levels for a considerable amount of time. To illustrate this, three hypothetical examples are provided below.

Table 4: Example of Impact of Increased Compensation and Reduced Rents

		Tenant A	Tenant B	Tenant C
A	Length of Tenancy	5 years	10 years	25 years
B	Current Rent (monthly)	\$1,100	\$950	\$850
C	Current Unit Size	950 sq.ft.	900 sq.ft.	650 sq.ft.
D	Compensation	\$15,400 (14 mo.)	\$22,800 (24 mo.)	\$33,150 (39 mo.)
E	Market Rent of Future Unit (monthly) (estimated on \$2.80 per sq.ft.)	\$2,240	\$2,100	\$1,680
F	Future Unit Size	800 sq.ft.	750 sq.ft.	600 sq.ft.
G	Reduced Rent Level (monthly)	\$1,680 (-25%)	\$1,470 (-30%)	\$1,176 (-30%)
H	Length of Time that Compensation Reduces Future Rent to Current Rent = D / (G – B)	26.5 months (2.2 years)	43.8 months (3.7 years)	101.7 months (8.4 years)

Proposed Changes to the Density Bonus / Amenity Contribution Policy

The City typically requires that an amenity contribution be provided for large developments. This contribution provides amenities that offer benefits to the community at large and help mitigate the increased demands on City services. Policy 511 provides Council with the opportunity to consider waiving all or a portion of the applicable amenity contribution for developments that provide either affordable (non-market) or market rental developments, recognizing that these developments offer a needed form of housing which is in itself a form of amenity to the community. The proposed changes to Policy 511 would establish a further reduction (up to 50%) of an applicable amenity contribution for developments where housing is being provided to displaced tenants in accordance with the Tenant Relocation Policy (i.e. compensation has been provided to tenants and reduced rents are available), and where the initial rents for rental replacement units where the tenants are not returning are 10% below market and available for the general public. By connecting a reduction in amenity contributions to the amount of tenant compensation provided by the developer, a developer would have much less incentive to end existing tenancies early or in bad faith to avoid tenant compensation, because if they do not provide the compensation to the tenants they would still be providing a similar contribution to the City.

The displacement of existing residents through the redevelopment of rental properties can have a challenging impact on the community. When strata developments are redeveloped the owners typically receive compensation in the form of proceeds from the sale of their property, but there is not a similar ability for renters to benefit from the redevelopment of their home. This new amenity contribution reduction is in recognition that the applicant has reduced the impact on the community by making it more possible for existing residents to remain in their community.

Other proposed changes to Policy 511 include:

- Establishing a target contribution rate for properties in the ‘Town Centre Transition’ land use designation, with a higher rate (\$430 per square metre over 1.5 FAR/gross floor area ratio) for properties with purpose built rental housing, with the expectation that half the contribution would be reduced through the provision of housing for displaced tenants, and potentially the entire contribution would be waived if the whole development is purpose-built rental housing. A lower rate (\$215 per square metre over 1.5 FAR/gross floor area ratio) would apply to non-rental properties;
- Establishing a target contribution rate for the ‘Waterfront Village’ land use designation, with a higher rate (\$646 per square metre over 1.75 FAR/gross floor area ratio), in recognition of the higher value for waterfront properties and the relatively smaller sites in this area; and
- Removing references to floor area measurements other than gross floor area ratio (i.e. residential/commercial/institutional floor area), to standardize the approach to estimating contribution targets.

Future Amendments to the Density Bonus / Community Amenity Contribution Policy

There was a public Community Amenity Contribution Forum held on January 20, 2020, and it is likely that Council may wish to direct staff to make further changes to the policy at this time. Some of the policy amendments which may be considered include:

- Specifying certain items on the list of eligible amenities
- Reducing the amount of CACs targeted for desired land uses (e.g. hotel space) to increase the viability that such uses may be pursued by the private market.

CLIMATE CHANGE IMPLICATIONS

There are no direct implications for the City's greenhouse gas emissions or resiliency to climate change impacts foreseen as a result of changes to the policies discussed in this report.

Redevelopment of older buildings can generally result in more efficient energy usage and better utilization of urban land, but the net impact depends on case-by-case factors.

BUDGET IMPLICATIONS

The proposed changes to the Density Bonus / Amenity Contribution Policy and the Tenant Relocation Policy in this corporate report are provided for Committee discussion and are not anticipated to affect the operating budget of any City department. By considering reduced CACs in exchange for greater compensation to tenants, there may be a reduction in the overall level of amenity contributions received. These contributions would otherwise assisted in paying for the capital costs of other amenities that are desired by the community and Council.

OPTIONS

The Governance and Legislation Committee may:

1. Receive this report for information and provide direction to staff on changes to Policy 511, Policy 514, and Policy 516;
2. Endorse the proposed changes to Policy 511, Policy 514, and Policy 516;
3. Provide direction to staff on further or alternate changes to Policy 511, Policy 514, and Policy 516; or
4. Direct staff to undertake no further action on these policies at this time.

Staff recommend Option 1, which is incorporated into the recommendations at the beginning of this corporate report.

CONCLUSION

This corporate report provides the Committee with an analysis of the City's tenant assistance-related policies in comparison with leading Metro Vancouver municipalities, and offers an opportunity for the Committee to have a broad discussion regarding how they would like the City to use its authority to balance support for tenants in rental buildings with the need to renew aging buildings through appropriate repairs and redevelopment.

The proposed amendments to Council's Density Bonus / Amenity Contribution Policy, Tenant Relocation Policy, and Renovation and Relocation Policy, provided in this corporate report for discussion purposes, are intended to address previous concerns expressed by the Land Use and Planning Committee that existing tenants would not be able to afford the rents charged if they were to return to the new building. In the proposed Tenant Relocation Policy update, the increases in compensation provided directly by the developer and reduced rents below the current policy level, paired with reduced amenity contributions to the City, are meant to bridge this gap. These changes would also provide certainty around the financial liabilities related to existing tenants that might otherwise result in a project not being funded by a lender. Further amendments to the Density Bonus / Amenity Contribution Policy are intended to provide greater certainty on the City's expectations with regard to applicable amenity contributions. The amendments to the Renovation and Relocation Policy are intended to reflect the resources available from the Residential Tenancy Branch in the event of a landlord making an application to renovate a tenanted rental apartment suite.

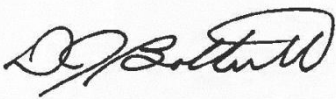
Respectfully submitted,



Carl Isaak, MCIP, RPP
Director, Planning & Development Services

Comments from the Chief Administrative Officer:

I concur with the recommendations of this corporate report.



Dan Bottrill
Chief Administrative Officer

- Appendix A: Proposed Council Policy 511: Density Bonus / Amenity Contribution Policy
- Appendix B: Proposed Council Policy 514: Tenant Relocation Policy
- Appendix C: Proposed Council Policy 516: Renovation and Relocation Policy
- Appendix D: Sections 49 and 51 of the *Residential Tenancy Act* – “Landlord’s notice: landlord’s use of property” and “Tenant’s compensation: section 49 notice”
- Appendix E: Residential Tenancy Policy Guideline 2B: Ending a Tenancy to Demolish, Renovate, or Convert a Rental Unit to a Permitted Use
- Appendix F: City of Burnaby report dated November 27, 2019 titled “Revised Tenant Assistance Policy”
- Appendix G: City of Port Coquitlam Business Amendment Bylaw No. 4116

Note: Attachments B-G removed for the sake of brevity

THE CORPORATION OF THE
CITY OF WHITE ROCK
 15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



POLICY TITLE: DENSITY BONUS / AMENITY CONTRIBUTION
POLICY NUMBER: COUNCIL - 511

Date of Council Adoption: April 15, 2013	Date of Last Amendment: June 28, 2017
Council Resolution Number: 2017-309	
Originating Department: Planning and Development Services	Date last reviewed by the Governance and Legislation Committee: June 12, 2017

Policy:

Purpose

The purpose of density bonus/amenity contribution requirements is to permit an increase in allowable densities in exchange for providing community amenities, consistent with section 482 of the *Local Government Act* which allows Council to establish, within a zoning bylaw, conditions relating to the conservation or provision of amenities, affordable housing and special needs housing. It allows the City to participate in a share of the increase in property values resulting from increases to the allowable densities and provide for amenities that help with the impact of increased development. Variables such as location, land value, lot size, building costs and market conditions affect the feasibility of value increases to the land when greater density is permitted. If these variables provide worthwhile economic gains to a property owner proposing redevelopment of their site, over and above the costs of providing the amenity contribution, then density bonus is a realistic way of acquiring benefit for the community.

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Amenities

Council's 2018-2022 Strategic Priorities highlight several priorities which may be realized partly through the use of amenity contributions. The waterfront in particular is identified as the primary opportunity to drive the local economy and support community initiatives. As White Rock's main attraction, the waterfront is an amenity that serves the City as a whole, and it is the public open space that is most impacted by increased development. Therefore the improvement of the waterfront is the main priority for Council in allocating amenity contributions. Provision of amenities in the Town Centre, Lower Town Centre and surrounding areas, including but not limited to streetscape, public realm and transportation improvements, public art and building floorspace for civic uses, are also considered as a part of this policy.

Section 3.4.178.12.1 of the 2008-2017 Official Community Plan (OCP) outlines the amenity contributions eligible for consideration for the Town Centre Area identifies the need to utilize land use in a manner that provides social, economic and ecological benefits across the City.

The City's Community Amenity Reserve Fund Bylaw, 2017, No. 2190, as amended, identifies a list of eligible amenity contributions, which includes, but is not limited to: for developments located outside of the Town Centre Area and which require rezoning, are similar, with clarification for area of applicability in brackets. Eligible contributions within the Waterfront, Town Centre Area, Lower Town Centre and elsewhere in the community are outlined as follows:

- A building or space within a building for civic uses, including but not limited to office, meeting or convention space (Town Centre Area)
- The provision and improvement of new publicly accessible open space, including a public square and/or pedestrian routes, either through dedication, easement, statutory right-of-way or covenant (all areas; with a preference for the East and West Beach Waterfront Business Areas)
- The improvement of existing publicly accessible open space and/or pedestrian routes (all areas; with a preference for the East and West Beach Waterfront Business Areas)
- Underground publicly accessible parking (Town Centre Area and East and West Beach Waterfront Business Areas)
- Publicly accessible parking (East and West Beach Waterfront Business Areas)
- Outdoor public art in the Town Centre Area located as recommended in the 2011 Town Centre Urban Design Plan (Town Centre Area)
- ~~Outdoor public art elsewhere in the community subject to the review and advice of the City's Public Art Advisory Committee (all other areas)~~
- A transit station, "bus loop" and/or transit shelters (Town Centre Area)
- Special needs or non-market affordable housing (all areas)
- ~~People movement infrastructure~~ Transportation and transit services, including people movement infrastructure (e.g. outdoor escalators, funiculars, or gondola) to link Uptown to the Waterfront (Town Centre and Lower Town Centre areas, all areas)
- Other land
- A park or other public place
- Park improvements, including playgrounds
- Landscaping of City land
- A library, a museum or archive
- An arts and cultural centre
- A child care facility
- A community centre
- A recreation facility
- Heritage conservation

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- City meeting and administration facilities
- A greenhouse gas reduction measure
- A community energy facility
- Similar things that benefit the City and the well being of its community
- ~~the provision of publically accessible open spaces or buildings or spaces within a building for civic uses, to serve the increased population. This could include the possibility of a shared use facility at White Rock Elementary School (e.g. community gym space, art gallery, etc.) (Town Centre, Lower Town Centre Areas)~~
- ~~the provision of public art that reflects the Johnston Road ‘arts and culture corridor’ (Town Centre, Lower Town Centre areas).~~

Process

In the Town Centre, Town Centre Transition, Waterfront Village Area and Lower Town Centre Area land use designations of the Official Community Plan, where the City will has established the zoning that includes the maximum allowable densities both with and without the amenity contribution requirements, or where a new Amenity Zoning Bylaw or Phased Development Agreement is proposed, and proponents for redevelopment will be required to enter into written agreement for deliver an amenity agreement and related section 219 covenant amenity contribution as a condition of development permit approval (when approved by Council for on-site amenities) and prior to the issuance of building permits. Densities are expressed in terms of “Floor Area Ratio” (FAR) which is determined by dividing the gross floor area by the total site area.

For all other areas outside the Town Centre Area and use designations named above where higher densities are permitted in the OCP, redevelopment projects greater than 3 ~~stories~~ storeys in height and/or 1.1 FAR in the Multi-Unit Residential designations, or greater than 3 ~~stories in height and/or 1.75 FAR in the Commercial designations,~~ will require rezoning to comprehensive development (CD) zone an Amenity Zoning Bylaw, and will be required to enter into agreement to establish the requirements for density bonus / amenity contribution prior to final approval of rezoning. ~~Densities are expressed in terms of “Floor Area Ratio” (FAR) which is determined by dividing the residential floor area, commercial floor area, or institutional floor area (as applicable) by the total site area. For these projects, the CD zoning will make it possible to determine the appropriate site densities and building heights on a ease by case basis in accordance with the direction for allowable heights and densities established in the OCP.~~ Further, market research will be required to determine the appropriate amount of density bonus / amenity contribution required, on a project by project basis.

Unless otherwise decided by Council, all amenity contributions will be in the form of payment-in-lieu. A reserve account will be created for deposit of these funds. Funds within this account will only be expended for those types of amenities ~~listed above to be provided in the Waterfront area, the Town Centre area, the Lower Town Centre area, or in other public areas as determined by Council, and~~ identified in the Community Amenity Reserve Fund Bylaw for the benefit of the overall community. Where Council has agreed to accept the amenity contribution to be developed on-site in conjunction with the redevelopment proposal, the specific amenities to be provided will be determined through discussion and negotiation

between the City and the proponent. When it is agreed that the amenities are to be provided on-site, public access to the amenity will be secured through written agreement or covenant registered prior to issuance of a building permit, ~~approval in the Town Centre and Lower Town Centre areas and may require the submission of financial securities acceptable to the City. Outside of the Town Centre and Lower Town Centre areas, public access to the amenity will be secured through written agreement or covenant registered prior to final approval of the rezoning.~~

Determination of Amenity to be Provided

Amenity contributions are required for every development:

- a) In the Town Centre, Waterfront Village Area and Lower Town Centre Area ~~land use designations~~ for developments exceeding three (3) ~~stories~~ storeys in height and/or 1.75 FAR; ~~and~~
- b) ~~In the Town Centre Transition land use designation for developments exceeding four (4) storeys in height and/or 1.5 FAR; and~~
- c) For every rezoning outside of the ~~Town Centre Area and Lower Town Centre Area~~ land use designations named above for proposed developments exceeding three (3) ~~stories~~ storeys in height, ~~and/or 1.1 FAR in the Multi-Unit Residential designations, and three (3) stories in height and/or 1.75 FAR in the Commercial designations.~~

~~Note: In the Town Centre and Lower Town Centre Areas, FAR is calculated based gross floor area as defined in the zoning bylaw. For rezonings outside of the Town Centre and Lower Town Centre Area, FAR is calculated based on residential floor area, commercial floor area, or institutional floor area, as defined in the zoning bylaw, or a combination of the above if applicable.~~

Exemption of Above Ground Parkade Floorspace

Above ground parking floorspace areas, when contained within an above ground enclosed building, and not utilized for pay parking purposes or other revenue generating purpose, are exempt from providing amenity contributions.

Amenity Contribution Approach

1. Within the Town Centre Area ~~land use designation~~, amenity contributions will be determined by using the following target rate as a guide in identifying the equal share of the increase in property value resulting from an increase in allowable density:
 - \$0 for the 1st 1.75 FAR;
 - \$430 per m² for FAR of 1.75 to 5.4.
2. Within the Lower Town Centre Area ~~land use designation~~, amenity contributions will be determined by using the following target rate as a guide in identifying the equal share of the increase in property value resulting from an increase in allowable density:

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- \$0 for the 1st 1.75 FAR;
- \$323 per m² for FAR of 1.75 to 3.5.

3. Within the Town Centre Transition land use designation, amenity contributions will be determined by using the following target rate as a guide in identifying the equal share of the increase in property value resulting from an increase in allowable density:

a) For properties containing three or more units of purpose-built rental housing:

- \$0 for the 1st-1.5 FAR;
- \$430 per m² for FAR of 1.5 to 3.5.

b) For properties without purpose-built rental housing:

- \$0 for the 1st-1.5 FAR;
- \$215 per m² for FAR of 1.5 to 3.5.

4. Within the Waterfront Village land use designation, amenity contributions will be determined by using the following target rate as a guide in identifying the equal share of the increase in property value resulting from an increase in allowable density:

- \$0 for the 1st-1.75 FAR;
- \$646 per m² for FAR of 1.75 to 2.0.

3-5. For every rezoning outside of the Town Centre Area and Lower Town Centre as noted land use designations named above, the amenity contribution required will be determined for the entire project, on a project by project basis, based on the advice and recommendations of a qualified market research consultant specific to that project. Proponents will be required to submit the market research report at the time of application submission, and the City reserves the right to commission a 2nd report from an alternate consultant to determine the amenity contribution for that project.

In establishing the value of a proposed amenity, hard costs, soft costs and land costs will be considered. Eligible costs for on-site amenities, when approved by Council, therefore include:

- i) Hard Costs – all material and labour costs for the construction of the amenity;
- ii) Soft Costs – all fees and costs for the construction of the amenity; and
- iii) Land Costs – eligible only where the ownership of the land containing the amenity is transferred or dedicated to the City.

To determine the value of the on-site amenity, a 3rd party appraisal will be required.

Affordable Housing

Secured non market and market rental housing, as well as other forms of affordable housing, are considered community amenities as they provide for a more diverse range of housing types, tenures and rent or price levels that are available to White Rock residents. On this basis, and on upon the recommendations of the City’s 2016 Rental Housing Task Force, the City will consider waiving or reducing community amenity contributions for these types of development

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applications, on a project-by-project basis, through using the following approaches as a guide in determining appropriate amenity contribution reductions.

Consideration of Amenity Contribution Reductions

1. Affordable Rental Floorspace:

- Affordable rental housing developments are those in which at least 30% of the units are owned or managed by non-profit groups and designed to be affordable for low and moderate income households.
- Consider waiver of up to 100 percent of applicable community amenity contribution for affordable rental floorspace, subject to this floorspace being secured by relevant legal agreements, and a review of the relevant development application and its merits in providing an affordable rental housing amenity that benefits the community. Projects will also be evaluated in way that considers how the proposed non or below market rental unit rates relate to what is currently available on the housing market, as determined by local and sub-regional housing market and household income indicators.

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2. Displaced Tenant Housing:

- Displaced Tenant Housing consists of residential units within a new development which are intended to provide housing at below market costs for existing tenants who are displaced through the redevelopment of purpose-built rental housing.
- Consider waiving up to a maximum of 50 percent of applicable amenity contribution for displaced tenant housing where the owner has:
 - provided compensation to displaced tenants in accordance with Council Policy 514: Tenant Relocation Policy;
 - has committed, through a Housing Agreement Bylaw, to offering displaced tenants with the first right of refusal to return in accordance with Council Policy 514: Tenant Relocation Policy; and
 - has committed, through a Housing Agreement Bylaw, that where a displaced tenant does not return to the building, that the initial maximum rent for that unit in the new development be no less than 10% below starting market rent for a similar unit for a period of one (1) year, after which rents may increase in accordance with the Residential Tenancy Act.

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2.3. Secured Market Rental Floorspace:

- Secured market rental housing developments are those designated for rental purposes only and protected with a covenant, lease agreement, or housing agreement registered against title for the life of the building.
- Consider waiving up to a maximum of 50 percent of applicable amenity contribution for secured market rental floorspace, subject to this floorspace being secured by relevant legal agreements, and a review of the relevant development application and its merits in providing a secured market rental housing amenity that benefits the community.

- In combination with the waiver of amenity contributions for Displaced Tenant Housing, up to 100 percent of the applicable community amenity contribution may be waived where a new development consists entirely of secured market rental housing and displaced tenants are provided with compensation and first right of refusal in accordance with Council Policy 514: Tenant Relocation Policy.

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3.4. Amenity contribution reductions may also be considered for other types of affordable housing applications, on the condition that the proposals demonstrate the ability to provide rental, home ownership and/or other tenure models and dwelling units that are rented or purchased at rates below what is currently available on the housing market, as determined by local and sub-regional housing market and household income indicators, and continue to be offered at below market rates for a time period specified by the City of White Rock and secured by relevant legal agreements. A market research/housing consultant may be utilized in determining potential amenity contribution reductions, on a project by project basis, and consistent with the approach described above regarding the determination of amenity contribution value.

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: October 19, 2020

TO: Land Use and Planning Committee

FROM: Carl Isaak, Director, Planning and Development Services

SUBJECT: Rezoning and Major Development Permit Application – 1485 Fir Street
(ZON/MJP 19-009)

RECOMMENDATIONS

THAT the Land Use and Planning Committee recommends:

1. That Council give first and second readings to “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 -1485 Fir Street) Bylaw, 2020, No. 2363 as presented, and direct staff to schedule the required Public Hearing;
 2. That Council direct staff to resolve the following issues prior to final adoption, if Bylaw No. 2363 is given Third Reading after the Public Hearing:
 - a) Ensure that all engineering requirements and issues, including dedication of a 5.0 metre by 5.0 metre corner cut on the corner of the site at Fir Street and Russell Avenue, intersection improvements including ‘watch for pedestrian’ signage as well as tactile paving on the northwest and northeast corners of George Lane and Thrift Avenue, and completion of a servicing agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;
 - b) A Tenant Relocation Plan and adoption of a Housing Agreement Bylaw are finalized; and
 - c) The consolidation of existing three lots and the demolition of the existing residential building occurs; and
 3. That, pending adoption of “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 – 1485 Fir Street) Bylaw, 2020, No. 2363,” Council consider issuance of Development Permit No. 432 for 1485 Fir Street.
-

EXECUTIVE SUMMARY

The Land Use and Planning Committee (LUPC) received a corporate report dated July 8, 2019 from the Director of Planning and Development Services, titled “Initial OCP Amendment Application Report – 1485 Fir Street (19-009 OCP/ZON/MJP).” The application at the time required an increase in gross floor area ratio (or ‘FAR’) density above what was permitted in the Official Community Plan (OCP) which would have required an OCP amendment and did not provide the number of three-bedroom units (10%) required in the OCP.

Council subsequently directed staff to work with the applicant on a revised application that did not require an OCP amendment. There was also discussion at the LUPC meeting regarding the adequacy of the applicant’s Tenant Relocation Plan. A subsequent report dated September 30,

2019 from the Director of Planning and Development Services, titled “Information Report Update and Revised Tenant Relocation Plan – 1485 Fir Street (ZON/MJP 19-009)” was prepared and provided a brief update including the applicant’s enhanced Tenant Relocation Plan and an overview of the changes to the form of the development which was revised to not require an OCP amendment and proceeded as a rezoning and major development permit application.

A separate corporate report on proposed revisions to Council Policy 511: Density Bonus / Amenity Contribution and Council Policy 514: Tenant Relocation Policy, is included earlier in the Land Use and Planning Committee agenda and would have an impact on this development application.

The application has been further revised to incorporate changes that follow the endorsement from the Governance and Legislation Committee to the Tenant Relocation Plan, discussed in the sections below. The proposal for 1485 Fir Street now presents a six-storey, 80-unit building, for which all units would be rental units. The rezoning, if approved, would create a Comprehensive Development (CD) zone largely designed to implement the height and density allowed within the Official Community Plan. A major development permit for form and character, energy and water conservation and the reduction of greenhouse gases is also required. Location and ortho photo maps of the subject property are attached as Appendix C.

PREVIOUS COUNCIL DIRECTION

Resolution # and Date	Resolution Details
LUPC July 8, 2019 2019-LU/P-022	<p>THAT the Land Use and Planning Committee:</p> <ol style="list-style-type: none"> 1. Receives for information the corporate report dated July 8, 2019 from the Director of Planning and Development Services, titled "Initial OCP Amendment Application Report- 1485 Fir Street (19-009 OCP/ZON/MJP);" and 2. Recommends that Council refuse the OCP amendment application, and direct staff to work with the applicant on a revised rezoning and Major Development Permit application, for a secured rental housing development that includes a reduced FAR (2. 8 gross floor area ratio consistent with the OCP), and amended building and site design.
LUPC September 30, 2019 2019-LU/P-025	<p>THAT the Land Use and Planning Committee refers the report back to staff for a revision that permits existing tenants to return to the building after construction at the same rent they are currently paying, subject to the per annum increases permitted by the province; and</p> <p>THAT the proposed Community Amenity Contributions (CACs) be reduced further in recognition for current tenants being able to keep their current rent amounts.</p>
2019-LU/P-026	<p>THAT the Land Use and Planning Committee receives for information the corporate report dated September 30, 2019 from the Director of Planning and Development Services, titled “Information Report Update and Revised Tenant Relocation Plan – 1485 Fir Street (ZON/MJP 19-009).”</p>

INTRODUCTION/BACKGROUND

White Rock Official Community Plan 2017, No. 2220 (OCP) designates the subject property as ‘Town Centre Transition’, characterized by residential uses that provide a gradual height transition between the Town Centre area and surrounding lower density single-family neighbourhoods. Building heights in the Town Centre Transition area are encouraged to develop within the range presented in Figure 10 of the OCP. For the subject site, this is shown as a continuum between 18 storeys at North Bluff Road and 6 storeys at Thrift Avenue, suggesting that between ~6-10 storeys would be a supportable transitional height at this location.

Under OCP Policy 8.2.3, properties in the Town Centre Transition area including 1485 Fir Street, are identified as being eligible for additional density (up to 40% above the base density) where at least half this additional floor area is dedicated to and secured as residential rental units. The base density for this property is 2.0 FAR, therefore the total maximum density permitted, including the rental bonus density, is 2.8 FAR, of which 0.4 FAR would need to be comprised of rental units. There is no additional bonus available for projects that consist entirely of rental units. Policy 11.2.1(f) requires that a minimum one-to-one replacement of existing rental units be provided when an existing rental building is proposed for redevelopment, with an average unit size of the replacement units at least 80% of the units being replaced. The proposal for 1485 Fir Street would consist of a six-storey, 80-unit building, for which all units would be rental units; the size and number of units is sufficient to satisfy the replacement requirements of the OCP.

The development is subject to a Major Development Permit being within the ‘Multi-Family’ Development Permit Area (DPA). The DPA Guidelines, outlined in Section 22.6 of the OCP have been applied to the proposal to ensure the form and character of the development fits within the established character of the neighbourhood. The project has been reviewed by City staff and the City’s Advisory Design Panel. Staff believe the rezoning to be consistent with the applicable policies of the OCP and the City’s Multi-Family DPA Guidelines. The following sections give greater merit to the factors considered in evaluating this proposal.

ANALYSIS

Current Zoning and Land Use Context

The subject property is located at 1485 Fir Street, on the corner of Fir Street and Russell Avenue (see Appendix C for Location Map and Ortho Photo). The property is occupied by a 25-unit rental apartment building (“The Firs;” building address of 1475 Fir Street) which was constructed in 1965. The existing building is located on three separate parcels and straddles the shared property lines. The subject properties are currently zoned ‘RM-2 Medium Density Multi-Unit Residential Zone’, which permits townhouse or apartment complexes with a 10.7 metre (35.1 feet) maximum height.

The subject site is surrounded by a mix of commercial, institutional and residential uses. To the west across a lane is St. John’s Presbyterian Church and Daycare Centre, to the north across Russell Avenue is a three storey office building (Russell Professional Building), and to the south and east are existing multi-unit residential buildings (one storey building on the east side of Fir Street, and three storey buildings to the south).

Previous Design Proposals

The initial report titled "Initial OCP Amendment Application Report- 1485 Fir Street (19-009 OCP/ZON/MJP);" on July 8, 2019 to the Land Use and Planning Committee (see Appendix D) included an overview of a new development application submitted on May 9, 2019, for a proposed development with a total of 84 rental residential units in a six (6) storey building. The proposed density for the apartment site exceeded the OCP maximum density by 0.53 FAR (3.23

FAR proposed; 2.8 FAR allowed). Council subsequently directed staff to work with the applicant on a revised application that did not require an OCP amendment (i.e. that did not exceed the maximum density in the OCP).

Following Council’s direction, the applicant submitted drawings for the rezoning and development permit application on August 15, 2019. An additional report on September 30, 2019 confirmed that the new proposal did not exceed the maximum density allowed in the OCP and therefore did not require an OCP amendment. The major changes that were proposed included:

- Reducing the amount of floor area density and increasing the number of three-bedroom units so that an OCP amendment is no longer necessary;
- Building massing was addressed by recessing the fifth and sixth storeys of the building to reduce the total floor area and the visual impact of the building height;
- Lot coverage was decreased to below 50%;
- Balconies were added to the homes along Fir Street; and
- The outdoor play area was relocated to the front of the building along Fir Street instead of in the rear along George Lane.

Table 1 below provides a summary of changes to the site statistics from the two previous proposals, in comparison to the current proposal. Design changes that have occurred following receipt of the last information report will be discussed in the sections that follow. Of note, there have been no changes to the number of units or building height and only minor reductions to lot coverage and floor area. Parking has been further reduced to 108 spaces, representing a 10% reduction to the required 120 spaces.

Table 1: Comparison of Original Development Proposal Statistics, Second Revised Proposal, and Current Proposal

	Original Proposal (May 9, 2019)	Revised Proposal (August 15, 2019)	Current Proposal (October 19, 2020)
Number of Units	84 (all secured rental)	80 (all secured rental)	80 (all secured rental)
Gross Floor Area	6,586.9 m ² (70,900.4 ft ²)	5,706.7 m ² (61,426.8 ft ²)	5,700 m ² (61,356.85 ft ²)
Floor Area Ratio (Gross)	3.23	2.8	2.8
Lot Coverage	56%	49.9%	48.7%
Height (to top of roof)	Six storeys (18.9 metres)	Six storeys (18.9 metres)	Six storeys (18.9 metres)
Parking Spaces	115 (1.37 per unit)	112 (1.4 per unit)	108 (1.35 per unit)

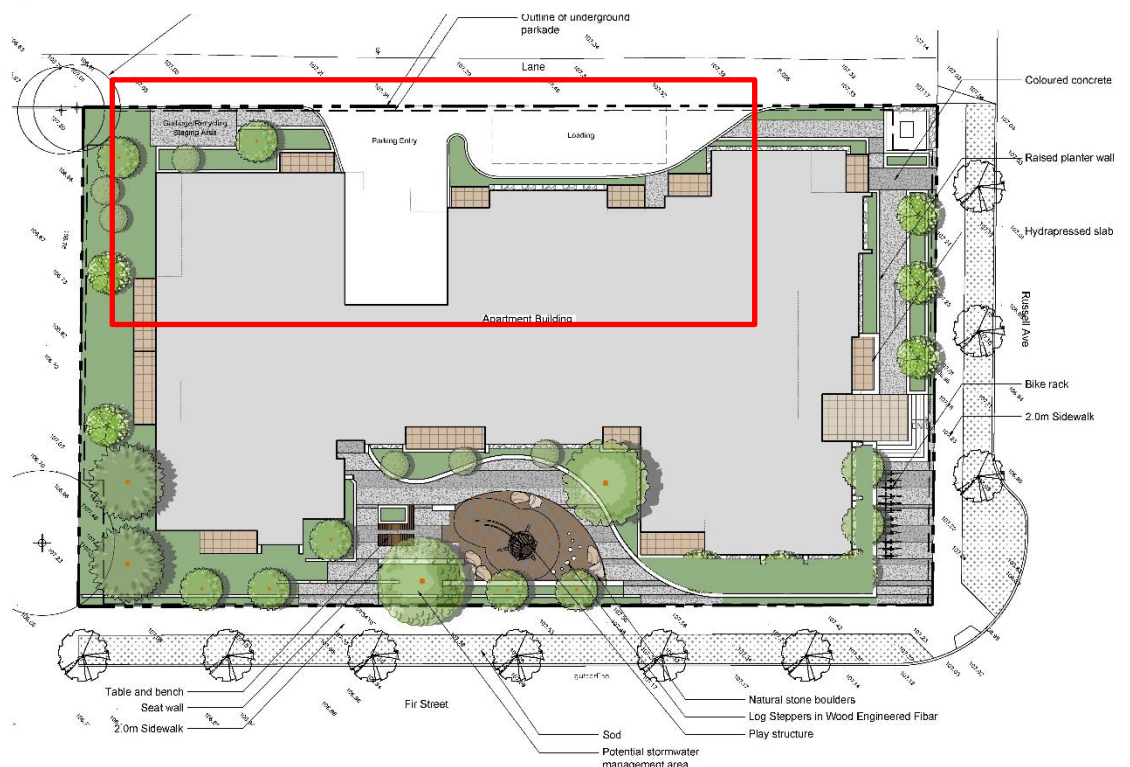
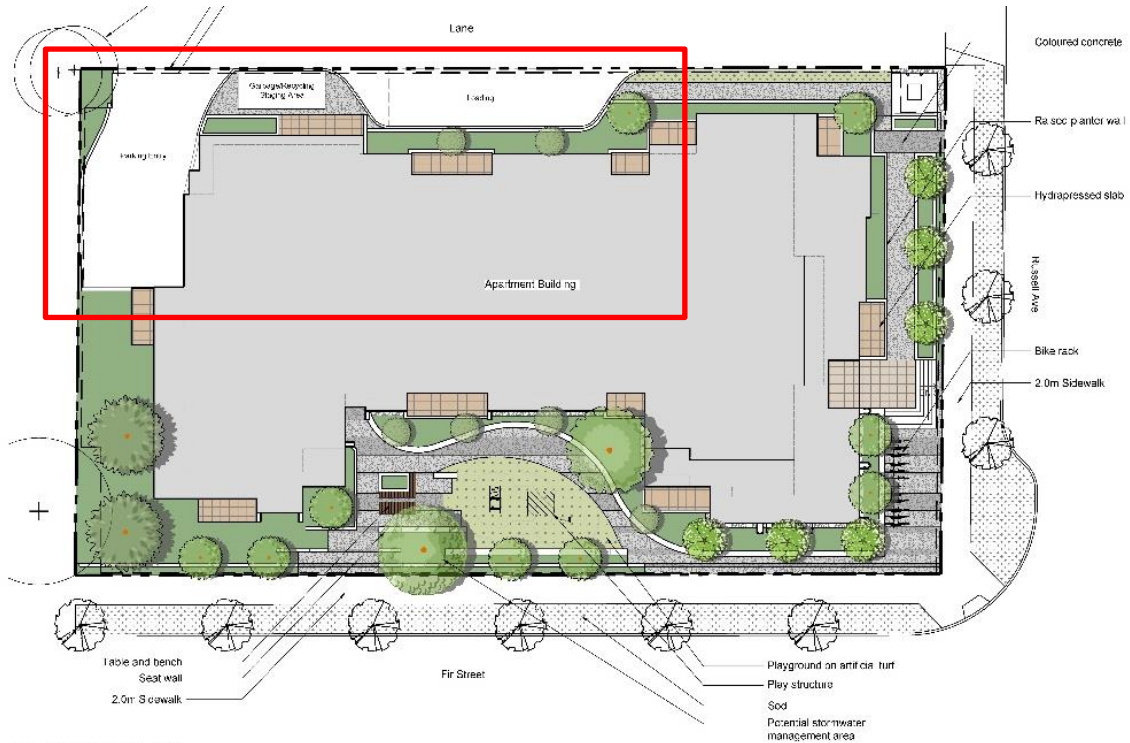
Current Proposal

The current development proposal would include a total of 80 units within a six-storey residential rental building. Unit sizes proposed range between 420 ft² – 520 ft² for a studio, 540 ft² – 625 ft² for a one-bedroom, 680 ft² – 990 ft² for a two-bedroom, and 980 ft² – 1010 ft² for a three-bedroom unit. It is important to note that the proposal now conforms to various elements of the OCP’s “Family-Friendly” housing policies, with 40 percent of the units containing either two or three bedrooms (32 units) and 12.5 percent of the units having three bedrooms (10 units).

Much of the design reflects the proposal in the information report presented to the LUPC from September 30, 2019 included as Appendix E. The major changes reflected in the current proposal pertain to the location of the parkade access which has been relocated to the middle of the site along George Lane, and a slight change in the configuration of the loading bay (see Figure 1). Access through an internal corridor has also been provided from the loading bay to the main elevator, so residents who are moving can easily access the building. A revision in the type of

play structure proposed in the communal courtyard area has also been accommodated in order to provide a more naturalized feel to the space with a ‘tree house’ structure instead of a more traditional playset; this latter revision stems from the feedback of the Advisory Design Panel. The revised parkade access location accommodates more green space on the south end of the site to provide a buffer between the proposed development and the existing property to the south.

Figure 1: Site Plan Comparison between September 30, 2019 version and Current Proposal



Public Information Meeting and Public Feedback

The applicant (Billard Architecture) held a public information meeting (PIM) on December 12, 2019, at ThirdSpace Community Café, Unit 1 - 1381 George Street) from 5:30 p.m. - 7:00 p.m. Approximately five-hundred and fifty (550) letters were circulated notifying owners within 100 metres of the subject property of the proposal. The meeting was also advertised in consecutive publications of the Peace Arch News in advance of the PIM. Appendix F to this report includes the PIM sign-in sheet, completed comment forms, and PIM summary submitted by the applicant. There was a total of 25 paper feedback forms submitted and 11 emails written to staff pertaining to the proposal. A total of 19 of the respondents were in favor of the application, 12 were in opposition of the proposal, and 2 were undecided about the proposal.

Support for the proposal was outlined through comments relating to:

- The rental aspect of the project;
- Elevators in the proposed building helping tenants/visitors to overcome mobility issues;
- Financial issues with maintaining the existing building and requirement for higher density on the site in order to meet the costs of owning the building; and
- The benefit that a new building and amenity space would provide to existing and new tenants as well as the surrounding neighbourhood considering the existing building is 60 years old.

Major concerns that were brought up during the meeting included the following:

- Compromised views for existing residents with the increase in building height;
- Several comments related to the increase in traffic congestion related to the proposed development and how City infrastructure will handle this;
- Not enough greenspace proposed on the site;
- Concerns with the proposed architectural style of the building;
- Concern expressed by existing building residents about the loss of their homes;
- Rental rates being too high to afford; and
- Many of the existing tenants being elderly and it being difficult for them to find alternative housing at an affordable rate.

Planning Review

As noted, the original proposal has undergone a series of revisions to address early concerns expressed by Council as well as feedback received through the PIM. The design has also been modified in response to technical issues identified by City staff and feedback received from the City's Advisory Design Panel (ADP). The project is now consistent with the OCP's Town Centre Transition policies. These policies contemplate development in the form of multi-unit residential buildings transitioning in height from 18 storeys at North Bluff Road down to six (6) storeys at Thrift Avenue. The following sections describe details of the proposal and key land use planning considerations made in preparing the staff recommendation outlined in this report.

The proposed multifamily building is rectangular in shape and is situated in the middle of the subject site. Building setbacks are greatest along Russell Avenue (north) and the residential lands to the south, being approximately 5 metres (16 feet) in width. Setbacks along Fir Street (east) and George Lane (west) are slightly less at approximately 3 metres (10 feet) in width. The lands within the yard setbacks are to be programmed with a mixture of trees and shrubs to ensure

adequate privacy and screening for neighbouring residents, and to help create a pleasant interface between the building and pedestrian realm / streetscape.

Since the original submission, the massing of the building has been stepped back on the fifth and sixth levels to reduce the impact of the structure as experienced at the ground level (i.e., opening up pedestrian views to the sky) while also reducing the impact of shadows on abutting properties. Further, the project has been enhanced with the creation of an outdoor amenity space on the east portion of the site, formerly situated off the laneway along the western side of the property. This amenity space offers an open, publicly-visible, play area for young children and a space for residents to enjoy the outdoors; this design enhancement is becoming increasingly important in light of the on-going COVID-19 pandemic and efforts to support social distancing while enabling access to private open-air green spaces. Finally, access to the parkade, space for loading, and a space for garbage and recycling pick-up, has been situated off of George Lane, being the western limit of the property. This design helps lessen breaks in the pedestrian realm (sidewalk) while helping to “hide” the operational needs of the project.

Table 2 below provides a comparison of the existing and proposed zoning standards tied to the property and project. As noted, the CD Zone is largely intended to implement the height and density permissions contemplated in the OCP.

Table 2: Existing Zoning Provisions versus Proposed Zoning

Existing Zoning Provisions: RM-2 Medium Density Multi-Unit Residential	Proposed Zoning Provisions: CD 64 - Comprehensive Development Zone
Permitted Uses	Permitted Uses
Townhouse or apartment complexes with densities not exceeding 50 units per acre	Multi-unit residential use with accessory home occupation use
Number of Dwelling Units	Number of Dwelling Units
25 units (50 units / 0.4 hectares) Existing Lot Area: 2,036 m ²	80 dwelling units (10 three-bedroom units, 22 two-bedroom units, 41 one-bedroom units, and 7 studio units)
Minimum Lot Requirements	Lot Dimensions
Lot Width: 18.0 m (59.04 ft) Lot Depth: 30.5 m (100.4 ft) Lot Area: 742.0 m ² (7,986.82 ft ²)	Lot Width: 34.48 m (113.12 ft) Lot Depth (averaged): 59.04 m (193.69 ft) Lot Area: 2,036 m ² (21,917 ft ²)
Lot Coverage	Lot Coverage
45% 916 m ² (9,962.9 ft ²)	48.7% 991 m ² (10,667 ft ²)
Gross Floor Area	Gross Floor Area
1.1 times the lot area 2,240 m ² (24,109 ft ²)	2.8 times the lot area 5,700 m ² (61,357 ft ²)
Building Height	Building Height
10.7 m (35.1 ft) for principal buildings	Six Storeys – 18.9 metres to top of parapet measured from average natural grade (62 feet) Geodetic height: 126.49 metres top of parapet and 129.2 metres top of elevator shaft

Minimum Setback	Setback
Front Lot Line: 6 m (19.68 ft)	Front Lot Line: 3.47 m (11.38 ft)
Rear Lot Line: 6 m (19.68 ft)	Rear Lot Line: 3.08m (10.1 ft)
Interior Side Lot Line: 5.0 m (16.4 ft)	Interior Side Lot Line: 5.25 m (17.22 ft)
Exterior Side Lot Line: 3.8 m (12.47 ft)	Exterior Side Lot Line: 5.05 m (16.57 ft)

Public Realm and Streetscape Improvements

The project includes short-term bicycle parking at the pedestrian entrance and an extended sidewalk / queuing space at the corner of Russell Avenue and Fir Street. These measures lessen the need for private automobile use while supporting improved overall pedestrian safety, respectively. Further, plantings are proposed along the sidewalk to, over time, support the growth of a tree canopy along streets. These measures support the objectives and policies of Section 13.1 of the OCP as they relate to “Transportation + Mobility”.

The dedication of land has been sought by the City’s Engineering and Operations Department to enable the creation of improvements to the City’s boulevard (e.g., additional on-street parking, sidewalks, street tree planting, etc.) thereby contributing to a more “complete” street. Efforts to design streets for all users can reduce collision rates (particularly for vulnerable road users, such as pedestrians and cyclists), better support adjacent land uses, support shifts to sustainable transportation methods of travel (walking, cycling, and transit), and improve the quality of the street as a positive space that is a destination and thoroughfare where residents, visitors, and passersby can feel safe.

Multi-Family DPA Guidelines

The applicant has submitted a response to the Multi-Family Development Permit Area Guidelines, which are applicable to the proposal pursuant to OCP Policy 22.1. The response to the guidelines is attached as Appendix G. Staff consider the submitted response to be in conformance with the Development Permit Guidelines. Figure 2 below provides a rendering of the current proposal, the form and character of which remains largely the same as the previous proposal considered in the report dated September 30, 2019.



Figure 2: Rendering of the Proposal from the corner of Russell Avenue and Fir Street Looking Southwest

The applicant has adequately identified how the proposed development meets the development permit guidelines by providing the following key aspects:

- a) A transition from high-rise buildings in the Town Center neighbourhood with much of the density located on the first to fourth level. The upper levels are then recessed back on all four sides of the building to reduce shadow and view impacts on neighbouring sites.
- b) Repetitive architectural details continue around all elevations of the building to create visual interest at all angles. The front entrance is clearly indicated with linear framework to create a vibrant space for residents and pedestrians to gather and connect in a safe, comfortable environment that is fully accessible with an integrated gradual access from the sidewalk.
- c) Natural materials will be used on the building's exterior including brick, exposed red cedar, fiber cement and natural metals such as aluminum. Natural tones are incorporated into the project to reflect the natural landscape with one dominate accent colour.
- d) The public realm will be improved by providing sidewalks and a boulevard that is 2m wide. An extra wide front entrance pathway is provided for bicycles, wheelchairs and scooters as well as a curb let-down at the intersection. Planting along the street fronting property lines will be provided to deter pedestrians from accessing the property on the grass and provide privacy from private patios that face the street.
- e) A light-coloured roof that is low in albedo will be used to reduce heat and energy efficient light fixtures will be used to conserve energy. Water efficient plumbing fixtures along with an abundance of zero-irrigation landscaping will be used to conserve water. A stormwater management plan will be set in place to alleviate heavy flooding from rainfall due to climate change.

Advisory Design Panel Review

During the Advisory Design Panel (ADP) meeting on July 21, 2020, the panel recommended that the application for the development proposal at 1485 Fir Street be referred to Council once the applicant had the opportunity to consider comments pertaining to the following items (see Appendix H for related ADP meeting minutes):

- a) Stormwater management plan must go to the Engineering Department – efforts to minimize the amount of stormwater going to the storm system;
Design Response: The stormwater is managed in a combined strategy between the civil and landscape designs. Raised planter beds are provided with soil to absorb water for uptake by the plant material. The excess water is collected in drains that connect to a storage tank located inside the parkade. The stormwater tank is sized so as to retain water and slowly release this water into the municipal storm sewer.
- b) Rooftop to be designed to reduce solar gain;
Design Response: It was confirmed that the roof would be light coloured and low albedo to reduce heat.
- c) Efforts to increase the number of electrical charging stations
Design Response: The addition of 12 electrical charging stations was incorporated into the design of the parkade.

- d) Efforts to increase the number of accessible parking spaces
Design Response: One more accessible parking space was incorporated into the design of the parkade for a total of three parking spaces
- e) Design of the children’s play space – naturalization of the space
Design Response: The playground was naturalized by using a form inspired by a tree house with earth toned materials and wood grain panels. Feature boulders were also introduced to blend the playground to the surrounding landscape treatment.

Staff believe the applicant has provided a satisfactory response to the comments noted above.

Tree Management

The Arborist Report prepared by Woodridge Tree Consulting Arborists Ltd. identifies that a total of three (3) “protected trees”, being those subject to City of White Rock Tree Management Bylaw, 2008, No. 1831, within the site area. One of the trees is a City tree and the other two are off-site trees. The Report recommends that all trees be retained as they are in good condition.

City staff have reviewed the recommendations of the Project Arborist and are comfortable with their retention subject to the posting of securities (i.e., \$9,500) for the three (3) offsite trees as required by the Tree Management Bylaw. Twenty-seven (27) trees are proposed as part of the development. Appendix B includes the proposed landscape plan which will be further reviewed upon receipt of an application for a Tree Management Permit (TMP), likely to accompany a future request for demolition of the existing building.

Traffic Study Review

The applicant has submitted a Traffic Study that analyses existing traffic volumes at the intersection of Fir Street and Russell Avenue. The peak traffic conditions (weekday morning and afternoon hours) for four different time frames – 2019 (existing), 2022 (full build-out), 2027 (5 years after build-out), and 2045 (the end of future timeframe for the White Rock OCP). A summary of the expected trip generation is shown in Table 3: Daily Trip Generation Statistics below:

Table 4: Daily Trip Generation Statistics

Peak Periods	Inbound Traffic	Outbound Traffic
Morning Hours: 8:00 a.m. to 9:00 a.m.	8 (28%)	21 (72%)
Afternoon Hours: 3:00 p.m. to 4:00 p.m.	22 (61%)	14 (39%)

The proposed development is estimated to add a total of 21 additional trips in the morning and 26 additional trips in the evening, which takes the total number from each category above (29 and 36 inbound and outbound trips respectively) and subtracts the number of existing trips made by residents in the current building. No major traffic issues are expected along this length of Russell Avenue and Fir Street. No major intersection improvements are proposed as a result of the study, however, additional ‘watch for pedestrian’ signage is suggested at the corner of George Lane and Thrift Avenue as well as tactile paving on the northwest and northeast corners of this intersection. The traffic study is attached as Appendix I.

Parking Standards and Requested Variance

The total number of required parking spaces for the proposed development equates to 120 spaces. A total of 96 spaces would be provided for residents and 24 parking spaces for visitors, totalling 108 spaces. This would be a 10% total reduction to the requirements of the Zoning Bylaw. CTS Traffic Consultants analyzed the peak parking demand using the Institute of

Transportation Engineers (ITE) Parking Generation Manual 5th Edition. Using representational data in the mid-rise multi-family category, the consultants estimated that 80 dwelling units would require 1.31 spaces per dwelling unit, or a total of 105 spaces to meet the peak average demand. The 108 spaces proposed for the development exceeds the estimated peak travel demand by three spaces. To supplement the request for a 10% parking variance, residents would be provided with a \$100.00 compass card to encourage the use of public transportation, with several routes located in close proximity to the development; the nine (9) routes include the 321, 345, 351, 354, 361, 362, 363, 375, and 531.

Further to the information provided above, under Zoning Bylaw No. 2000, a maximum of 40% of the stalls can be provided as small car spaces. The development is proposing a total of 33 small car spaces, equivalent to 30% of the total spaces. A total of two handicapped spaces are required as part of the proposed development. A total of three handicapped spaces will be provided, all located near the elevator. Electric vehicle (EV) charging is required as per Zoning Bylaw No. 2000, with a requirement of one charging station per every 10 parking spaces. The development proposes a total of 36 EV stations, significantly over the 18-space requirement. In consideration of the recommendations of the parking analysis prepared by CTS Traffic Consultants and the information noted above, City staff are supportive of the requested variance to parking.

Tenant Relocation Plan

In the original Tenant Relocation Plan, the applicant proposed to give the option to existing tenants to return to the new building at rents that would be 10% below the market rent that was being charged at that time for other similar units, which is consistent with the City's Tenant Relocation Policy No: 514. Due to concerns that were raised during the discussion at LUPC regarding the large increase that even 10% below market rent would be for existing tenants, the applicant then offered to reduce the rents charged to returning tenants to 20% below projected market rent for the building, which was estimated at \$2.80 per square foot. The rents at this rate for returning residents would have been \$2.24 per square foot, equating to:

- \$1,232 for an average size (550 square foot) one-bedroom unit
- \$1,859 for an average size (830 square foot) two-bedroom unit

Since this approach was discussed the City's Governance and Legislation Committee has advanced further discussions regarding potential amendments to Policy No. 514. These amendments were presented in a report to the Committee on January 27, 2020 titled "Options for Tenant Assistance During Redevelopment and Renovation". Stemming from this meeting, the developer has agreed to revise the Tenant Relocation Plan further to align with the draft amendments as presented in the noted report.

Per the draft policy amendments, for existing tenants compensation would be provided on a sliding scale dependent on the length of tenancy (number of years) of the resident. For example, someone who has lived in the building for 10 years and is currently paying \$1,540 for a one-bedroom unit would receive \$36,960 to be used towards alternative housing costs (i.e., 24 months times the monthly rent rate). Note that at the time of preparing this report alternative mechanisms to control the disposition of monies to tenants (i.e. via an annuity or Guaranteed Investment Fund or alternative) are under review and not yet solidified by way of amendments to the policy, and are discussed further in a previous corporate report on this LUPC agenda. That said, there exist opportunities to implement such controls by way of a Housing Agreement Bylaw if Council directs staff to further advance the review of this proposal.

For those who choose to move back into the building, the developer has also agreed to follow the recommendations of the January 27, 2020 report by significantly reducing the rate of the initial rent for returning tenants. The rental rates in the new building would be in accordance with the table below, outlining a rate between 21-30% below market depending on the length of tenancy.

Table 3: Length of Tenancy and Application Rent Reduction

Length of Tenancy (Years)	% Below Market Rent
1	21%
2	22%
3	23%
4	24%
5	25%
6	26%
7	27%
8	28%
9	29%
10 or more	30%

Based on a market rental rate of \$2.80 per square foot, the rents for returning residents at the rates identified above would equate to:

- Between \$1,078 (30%) and 1,216 (21%) for a one-bedroom unit (550 square foot); and
- Between \$1,627 (30%) and \$1,835 (21%) for two-bedroom unit (830 square foot)

These rents would be inclusive of a parking space and hot water, and following occupancy the owner would be permitted to increase rents in accordance with the annual increases regulated by the Residential Tenancy Branch. Finally, if a tenant did not opt to move back into the building, this unit would be available to a new tenant at a 10% reduced rental rate which would be formalized in the Housing Agreement Bylaw. Please see the financial section below for a discussion of how the Community Amenity Contribution could be reduced or waived to support the rental compensation, rent reduction, and reduced 10% rental rate if a tenant does not opt to move back into the building.

Amenity Contribution

Policy 511 provides Council with the opportunity to consider waiving all or a portion of the applicable amenity contribution for developments that provide either affordable (non-market) or market rental developments, recognizing that these developments offer a needed form of housing which is in itself a form of amenity to the community. The target contribution rate for properties in the ‘Town Centre Transition’ land use designation is a rate of \$430 per square metre over 1.5 FAR/gross floor area ratio as per the recommendations in the Governance and Legislation Committee Report dated January 27, 2020 (and discussed in the separate corporate report on this Land Use and Planning Committee agenda). The expectation would be that the full contribution could be reduced through the provision of housing for displaced tenants as well as the whole development being offered as purpose-built rental housing. Staff are supportive of this approach based on the additional compensation and rate of below market rent proposed for returning tenants as identified above.

Housing Agreement

The Housing Agreement Bylaw is the formal binding agreement between the Developer and the City that regulates and secures the rental rates based on the recommendations and discussion provided above. The Housing Agreement will be finalized pending the completion of of third

reading of the associated bylaw. All 80 residential units would be secured as rental in perpetuity through the Housing Agreement Bylaw. Additional controls tied to the disposition of monies to support tenant relocation may also be incorporated into the terms of a future agreement.

FINANCIAL IMPLICATIONS

The Rezoning and Major Development Permit, if approved, will not result in any additional costs to the City. Development cost charges will apply to the redevelopment.

Previously and in accordance with existing Council Policy 511: ‘Density Bonus / Amenity Contribution’, a community amenity contribution of \$922,000 would have been anticipated based on the target rates for the Town Centre (this site is in close proximity to the Town Centre and a similar target rate was considered appropriate). This rate would be increased under the proposed changes considered by the Governance and Legislation Committee report submitted on January 27, 2020 titled “Options for Tenant Assistance During Redevelopment and Renovation”, to a rate of \$430 square foot over 1.5 FAR.

As the project proposed a FAR of 2.8, the total contribution would equate to \$1,137,780 (i.e., Additional floor area from 1.5 to 2.8 {[lot area x 2.8] – [lot area x 1.5]} = 2,646 m² x \$430). The proposed changes to Policy 511 would establish a further reduction (up to 50%) of an applicable amenity contribution as the housing would be provided to displaced tenants in accordance with the Tenant Relocation Policy (i.e. compensation being provided to tenants and reduced rents are available), and where the initial rents for rental replacement units where the tenants are not returning are 10% below market and available for the general public. Council Policy 511 currently allows a reduction of up to 50% of an applicable amenity contribution for secured market rental floor space, which would amount to \$568,873 and could be further waived up to 100%.

LEGAL IMPLICATIONS

A Housing Agreement Bylaw would be prepared for Council’s consideration, based on the applicable provisions in Council Policy 511: Density Bonus / Amenity Contribution and Council Policy 514: Tenant Relocation, as directed by Council. A draft of this Housing Agreement Bylaw would be made available as part of the materials available prior to the Public Hearing.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

This application has received a Public Information Meeting, and if Council provides 1st and 2nd reading to the draft zoning amendment bylaw, the public would have an opportunity to comment on this application via a Public Hearing.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The Rezoning and Major Development Permit applications were circulated to internal City departments and comments requiring a response / resolution by the proponent have been addressed.

CLIMATE CHANGE IMPLICATIONS

The application will enable the intensification of the ‘Town Centre Transition’ designation, thereby lessening the demand for outward sprawl otherwise necessary to accommodate growth. The applicant has also proposed several initiatives to address climate change, which include the following:

- Water, electric and gas will be individually metered to increase self-imposed conservation.
- Landscaping includes a variety of permeable surface areas and decreases consumption of irrigation water by the use of native, drought resistant planting.
- Lighting and plumbing fixtures to be energy/water efficient as well as the provision of Energy Star® rated appliances.
- High efficiency windows and doors with effective blinds will be preinstalled.
- Materials used in construction or finishing such as cabinets and floors will be made from renewable resources and sourced locally where possible
- Flooring, paint and other finishes will be non-toxic with low volatile organic compounds (VOCs)

ALIGNMENT WITH STRATEGIC PRIORITIES

The proposal is generally aligned with the Corporate Vision established as part of Council’s Strategic Priorities, particularly with respect to supporting a community where people can live, work and play in an enjoyable atmosphere.

OPTIONS / RISKS / ALTERNATIVES

As an alternative to the staff recommendation provided at the outset of this corporate report (to move the application forward to Public Hearing), Council may alternately:

1. Reject “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 – 1485 Fir Street), 2020, No. 2363” and Development Permit No. 432; or
2. Defer consideration of “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 – 1485 Fir Street), 2020, No. 2363” and Development Permit No. 432 pending further information to be identified.

Staff recommend proceeding with the application to Public Hearing, which is incorporated into the recommendations of this corporate report.

CONCLUSION

The proposal for an 80-unit rental building at 1485 Fir Street is consistent with the objectives and policies of the ‘Town Centre Transition’ OCP land use designation and Development Permit Area Guidelines. Staff consider the proposed changes to the six-storey multi-unit residential building as improvements to the design and its impact on the surrounding neighbourhood from the original OCP amendment proposal, and have brought forward a draft Zoning Amendment Bylaw and draft Development Permit to move the application forward to a Public Hearing. The Tenant Relocation Plan requirements of the proposal would provide additional compensation and reduced rental rates outlining an additional benefit to those residents impacted by the redevelopment proposal. The proposed variance to parking is minor and supported by a rigorous analysis by the consultant. Staff recommend that the Zoning Amendment Bylaw be given first

and second reading, and that a Public Hearing be scheduled to receive additional input from the community on the proposal.

Respectfully submitted,



Carl Isaak, MCIP, RPP.
Director of Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

- Appendix A: Draft Zoning Amendment Bylaw No. 2363
- Appendix B: Draft Development Permit No. 432
- Appendix C: Location and Ortho Photo Maps
- Appendix D: LUPC Report "Initial OCP Amendment Application Report- 1485 Fir Street (19-009 OCP/ZON/MJP)" dated July 8, 2019
- Appendix E: LUPC Report "Information Report Update and Revised Tenant Relocation Plan 1485 Fir Street (ZON/MJP 19-009)" dated September 30, 2019
- Appendix F: Public Information Meeting Sign-in Sheet, Comment Forms, and Summary
- Appendix G: DPA Guidelines Response Table
- Appendix H: ADP Minutes dated July 21, 2020
- Appendix I: CTS Traffic Study dated November 25, 2019

APPENDIX A

Draft Zoning Amendment Bylaw No. 2363

(Attached Separately)

**The Corporation of the
CITY OF WHITE ROCK
BYLAW No. 2363**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled ENACTS as follows:

1. THAT Schedule C of the *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended by rezoning the following lands:

Lot 16 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-931
(1485 Fir Street)

Lot 17 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-965
(1485 Fir Street)

Lot 18 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-981
(1485 Fir Street)

as shown on Schedule "1" attached hereto, from the 'RM-2 Medium Density Multi-Unit Residential Zone' to 'CD-64 Comprehensive Development Zone (1485 Fir Street).'

2. THAT *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended:

(1) by adding to the Table of Contents for 'Schedule B (Comprehensive Development Zones)', Section 7.64 CD-64 Comprehensive Development Zone';

(2) by adding the attached Schedule "2" to 'Schedule B (Comprehensive Development Zones)' Section 7.64 CD-64 Comprehensive Development Zone'.

3. This bylaw may be cited for all purposes as "*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-64 – 1485 Fir Street) Bylaw, 2020, No. 2363*".

Public Information Meeting held this 12th day of December, 2019

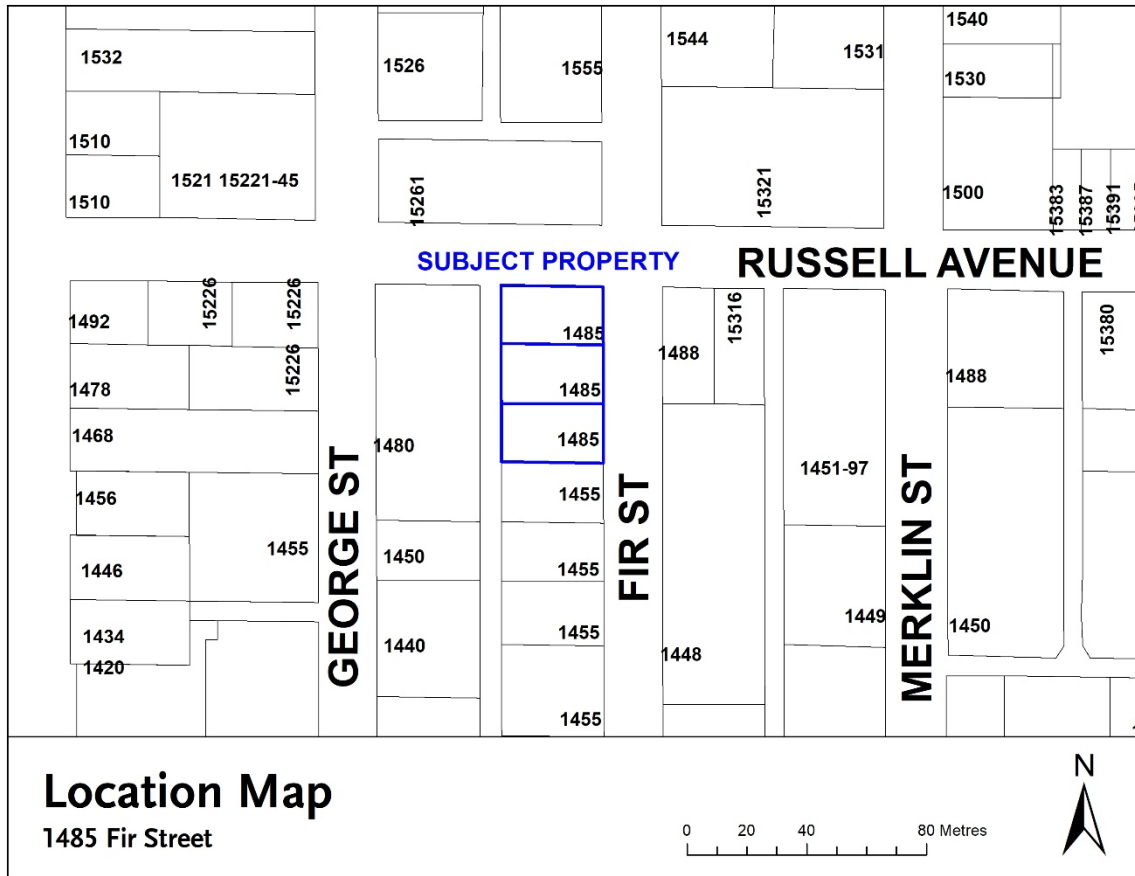
Read a first time this day of , 2020

Read a second time this	day of	, 2020
Considered at a Public Hearing this	day of	, 2020
Read a third time this	day of	, 2020
Adopted this	day of	, 2020

Mayor

Director of Corporate Administration

Schedule "1"



7.64 CD-64 COMPREHENSIVE DEVELOPMENT ZONE

INTENT

The intent of this zone is to accommodate the development of a multi-unit residential building on a site of approximately 2,036 square metres, with the provision of affordable housing and a housing agreement bylaw in accordance with section 482 of the *Local Government Act*.

1. Permitted Uses:

- (1) *multi-unit residential use; and*
- (2) *accessory home occupation use in accordance with the provisions of section 5.3 and that does not involve clients directly accessing the principal building*

2. Lot Coverage:

- (a) For *multi-unit residential uses*, lot coverage shall not exceed 49%

3. Maximum Base Density:

The following base density regulation applies generally for the zone:

Maximum *residential floor area* shall not exceed 1.1 times the lot area, and maximum *gross floor area* shall not exceed 1.5 times the lot area.

4. Maximum Increased Density:

Despite section 7.64.3, the reference to the maximum *gross floor area* of “1.5 times the lot area” is increased to a higher density of a maximum of 5,700 m² (61,356.85 ft²) of *gross floor area* (2.8 FAR; or gross floor area ratio) and 80 apartment dwelling units where a housing agreement has been entered into and filed with the Land Title Office to secure eighty (80) dwelling units as rental tenure for the life of the building.

5. Building Height:

- (a) The *principal buildings* for *multi-unit residential uses*, inclusive of elevator shafts, stair housing, and all mechanical equipment, shall not exceed a *height* of 129.2 metres geodetic; and
- (b) *Ancillary buildings and structures* for *multi-unit residential uses* shall not exceed a height of 5.0 metres from *finished grade*.

6. Siting Requirements:

- (a) Minimum setbacks for *multi-unit residential uses* are as follows:
 - (i) Setback from north lot line = 5.05 metres
 - (ii) Setback from south lot line = 5.25 metres
 - (iii) Setback from west lot line = 3.08 metres

(iv) Setback from east lot line = 3.47 metres

- (b) *Ancillary structures* may be located on the subject property in accordance with the Plans prepared by Billard Architecture dated August 11, 2020 that are attached hereto and on file at the City of White Rock, with the exception that no *ancillary buildings* or *structures* are permitted within a 1.0 metre distance from a lot line

7. Parking:

Parking for *multi-unit residential uses* shall be provided in accordance with Sections 4.14 and 4.17, with the minimum number of spaces required as follows:

- (a) A minimum of ninety-six (96) spaces shall be provided for residents of the *multi-unit residential use*;
- (b) A minimum of twenty-four (24) spaces shall be provided for visitors and marked as “visitor”;
- (c) A minimum of three (3) of the required one hundred and eight (108) spaces shall be provided as accessible parking spaces, shall be clearly marked, and shall have a minimum length of 5.5 metres. Of the three accessible parking spaces, one space shall be provided as a van-accessible loading space with a minimum width of 2.8 metres, and the other two spaces shall have a minimum width of 2.5 metres, provided that the three parking spaces have a shared or non-shared access aisle with a minimum width of 1.5 metres; and
- (d) The minimum height clearance at the accessible parking spaces and along the vehicle access and egress routes from the accessible parking spaces must be at least 2.3 metres to accommodate over-height vehicles equipped with a wheelchair lift or ramp.

8. Bicycle Parking:

Bicycle parking shall be provided in accordance with Section 4.16, with the minimum number of spaces required as follows:

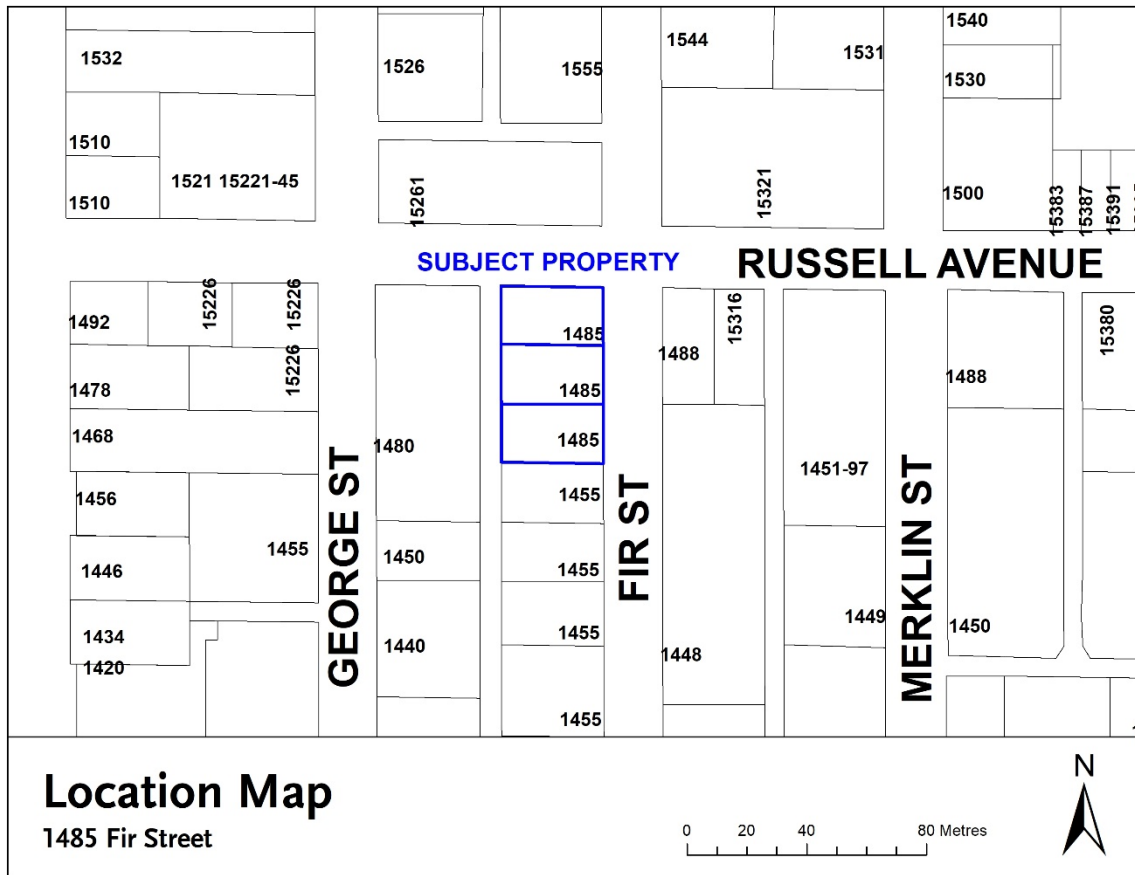
- (a) A minimum of 94 Class I spaces shall be provided; and
- (b) A minimum of 16 Class II spaces shall be provided

9. Loading:

- (a) One loading space shall be provided for a *multi-unit residential use* in accordance with Section 4.15

10. General:

Development in this zone that includes the additional (bonus) density referred to in Section 4 shall substantially conform to the Plans prepared by Billard Architecture dated August 11, 2020 that are attached hereto and on file at the City of White Rock



DR

THE CLIENT'S RESPONSIBILITY IS TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AUTHORITIES. THE ARCHITECT ASSUMES NO LIABILITY FOR SUCH PERMITS AND APPROVALS. THE ARCHITECT'S RESPONSIBILITY IS TO PROVIDE THE CLIENT WITH ALL INFORMATION NECESSARY FOR THE CLIENT TO OBTAIN SUCH PERMITS AND APPROVALS.

THE ARCHITECT'S DESIGN IS BASED ON THE INFORMATION PROVIDED BY THE CLIENT. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY INACCURACIES OR OMISSIONS IN THE INFORMATION PROVIDED BY THE CLIENT.

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THE ARCHITECT'S DESIGN IS SUBJECT TO THE CHANGES REQUIRED BY THE CLIENT. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY CHANGES TO THE DESIGN REQUIRED BY THE CLIENT.

NO.	DATE	REVISION
11	13.08.2023	ISSUE FOR PERMIT
10	13.05.2023	2P 42008
9	30.11.2021	REVISED SET OF DRAWINGS
8	09.09.2021	ISSUE FOR PERMIT
7	22.07.2021	ISSUE FOR PERMIT
6	15.07.2021	ISSUE FOR PERMIT
5	11.04.2021	ISSUE FOR PERMIT
4	11.04.2021	ISSUE FOR PERMIT
3	11.04.2021	ISSUE FOR PERMIT
2	11.04.2021	ISSUE FOR PERMIT
1	11.04.2021	ISSUE FOR PERMIT



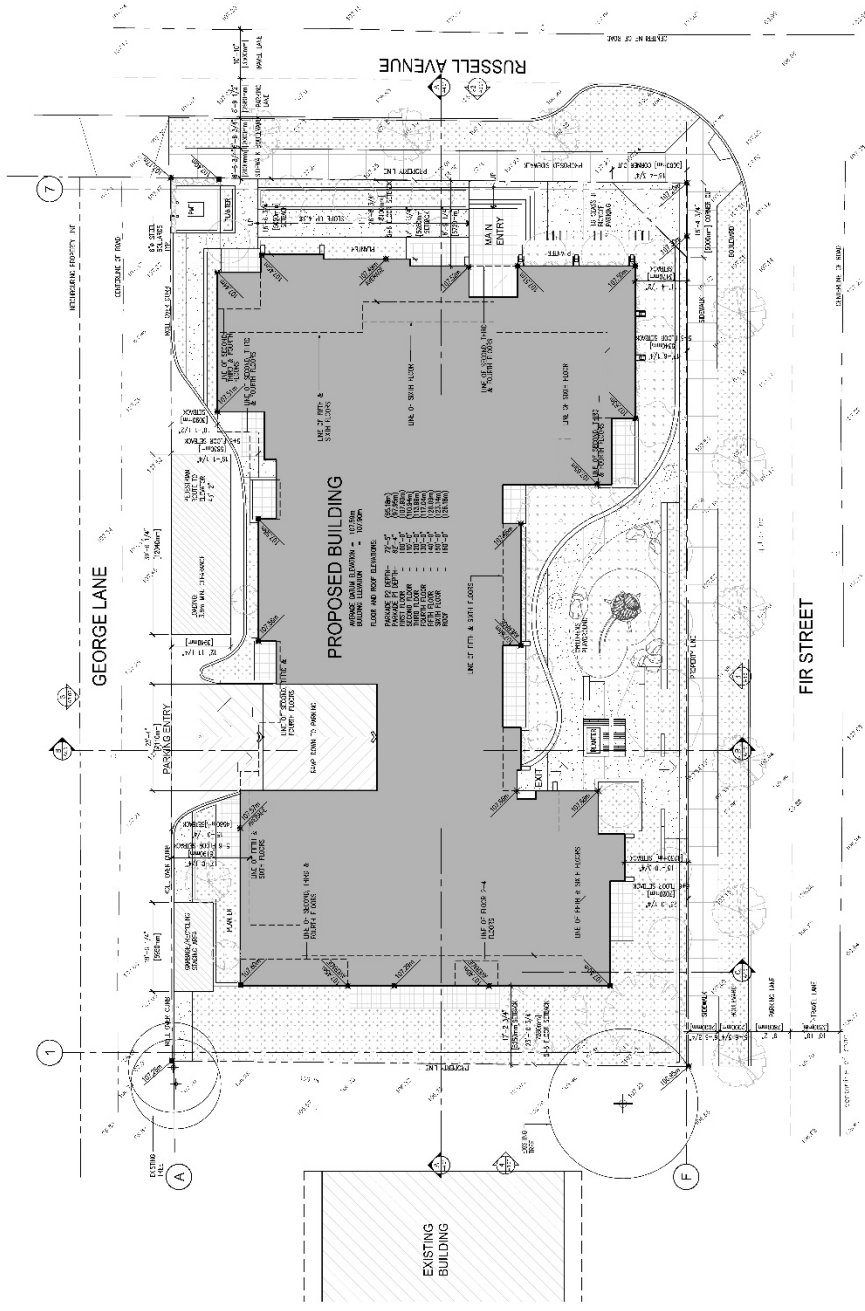
Bilard Architecture
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 info@bilardarchitecture.ca
 www.bilardarchitecture.ca

WHITEBIRCH APARTMENTS
 1465 FIR STREET
 WHITEROCK, BC

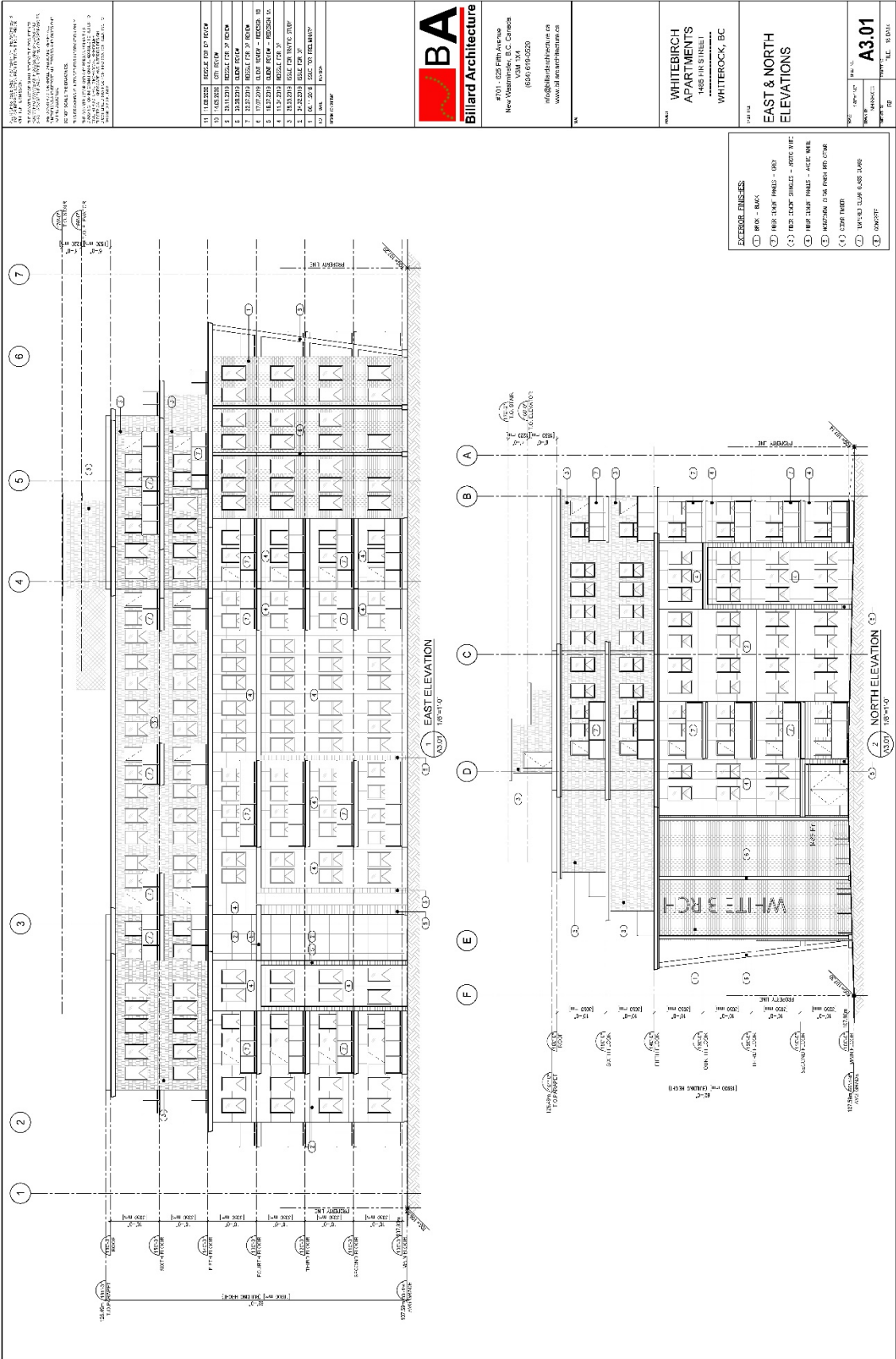
SITE PLAN

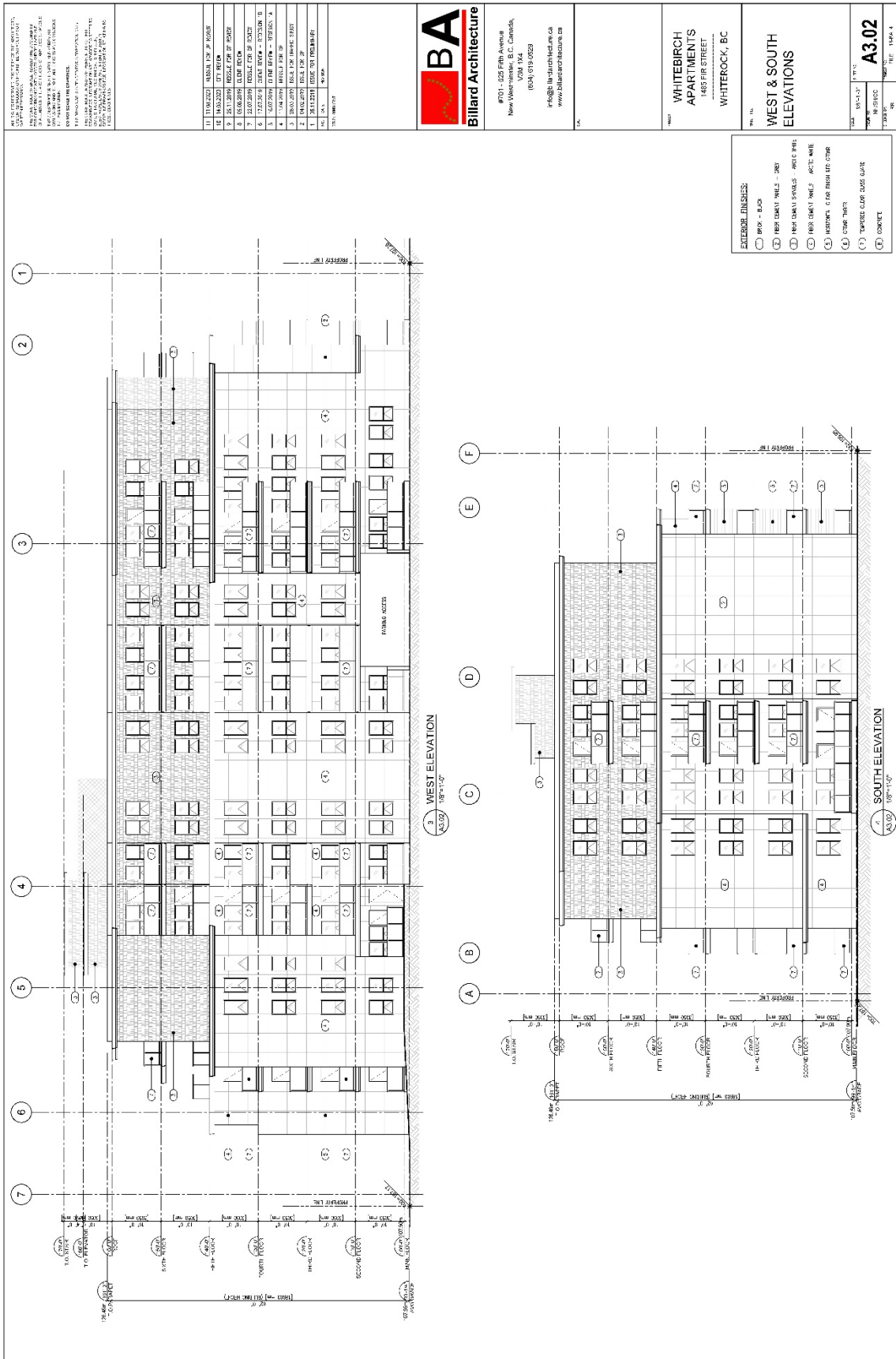
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1 SITE PLAN
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WHITE BIRCH APARTMENTS
 1400 6th Street
 WHITEROCK, BC

WEST & SOUTH ELEVATIONS

A3.02

DATE: 11/18/22
 DRAWN BY: M-S/BCD
 CHECKED BY: T-BCD
 SCALE: 1/8" = 1'-0"



KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1, 2 (LD-01)	EXISTING TREE (fresh)
	3 (LD-02)	PROPOSED TREE (fresh)
	4 (LD-03)	PROPOSED TREE (fresh)
	5 (LD-04)	PROPOSED TREE (fresh)
	6 (LD-05)	PROPOSED TREE (fresh)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
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	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)



TREES	BOTANICAL COMMON NAME	ZONE	SIZE	QTY.	REMARKS
	Acacia saligna / Black Wattle	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Alnus incana / Common Alder	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Crataegus monogyna / Hawthorn	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Quercus robur / Common Oak	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Prunella vulgaris / Blackberry	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Malva sylvestris / Garden Mallow	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Malva sylvestris / Garden Mallow	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Malva sylvestris / Garden Mallow	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Malva sylvestris / Garden Mallow	2B	4.5m	1	Urban landscaping, sunny, low, common tree
	Malva sylvestris / Garden Mallow	2B	4.5m	1	Urban landscaping, sunny, low, common tree

KEY	REF.	DESCRIPTION
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	3 (LD-04)	GRAVEL (60% BTP)
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	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
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	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
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	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
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	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

KEY	REF.	DESCRIPTION
	1 (LD-02)	GRAVEL (60% BTP)
	2 (LD-03)	GRAVEL (60% BTP)
	3 (LD-04)	GRAVEL (60% BTP)
	4 (LD-05)	GRAVEL (60% BTP)

APPENDIX B

Draft Development Permit No. 432

(Attached Separately)

**THE CORPORATION OF THE
CITY OF WHITE ROCK**



DEVELOPMENT PERMIT NO. 432

1. Development Permit No. 432 is issued to 1062822 B.C Ltd. as the owner and shall apply only to ALL AND SINGULAR those certain parcels or tracts of land and premises situate, lying and being in the City of White Rock, in the Province of British Columbia, and more particularly known and described as:

Legal Description:

Lot 16 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-931
(1485 Fir Street)

Lot 17 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-965
(1485 Fir Street)

Lot 18 Section 11 Township 1 New Westminster District Plan 15362
PID: 001-331-981
(1485 Fir Street)

As indicated on Schedule A, including a consolidation of these three lots.

2. Development Permit No. 432 is issued pursuant to the authority of Sections 490 and 491 of the *Local Government Act, R.S.B.C. 2015, Chapter 1* as amended, the "White Rock Official Community Plan Bylaw, 2017, No. 2220" as amended, and in conformity with the procedures prescribed by the "City of White Rock Planning Procedures Bylaw, 2017, No. 2234" as amended.
3. The terms, conditions and guidelines as set out in "White Rock Official Community Plan Bylaw, 2017, No. 2220" as amended, that relate to the "Multi-Family Development Permit Area" shall apply to the area of land and premises hereinbefore described and which are covered by this Development Permit.
4. Permitted Uses of Land, Buildings and Structures

Land, buildings, and structures shall only be used in accordance with the provisions of the "CD-64 Comprehensive Development Zone" of the "White Rock Zoning Bylaw, 2012, No. 2000" as amended.

5. Dimensions and Siting of Buildings and Structures on the Land

All buildings and structures to be constructed, repaired, renovated, or sited on said lands shall be in substantial compliance with the Plans prepared by Billard Architecture and VDZ Landscape Architecture attached hereto in accordance with the provisions of Section 491 of the *Local Government Act*:

Schedule B	Site Plan
Schedule C	Building Elevations
Schedule D	Renderings
Schedule E	Landscaping Plans

These Plans form part of this development permit.

6. Terms and Conditions:

- a) The applicant shall enter into a Servicing Agreement to provide frontage improvements and on-site works and services in accordance with Section 506 of the *Local Government Act* and to the acceptance of the Director of Engineering and Municipal Operations;
- b) The applicant shall provide landscaping for the development in substantial compliance with the Landscape Plans (Schedule E) to the acceptance of the Director of Planning and Development Services and the Director of Engineering and Municipal Operations;
- c) The permittee must also submit an estimate for the cost of landscaping, along with securities in the amount of \$188,000.00 (125% of the cost of landscaping) to the City prior to the issuance of a building permit;
- d) Rooftop mechanical equipment shall be screened from view to the acceptance of the Director of Planning and Development Services; and
- e) The hydro kiosk is to be located on site to the acceptance of the Director of Planning and Development Services.

7. In the interpretation of the Development Permit all definitions of words and phrases contained in Sections 490 and 491 of the *Local Government Act, R.S.B.C. 2015, Chapter 1* as amended, and the “White Rock Official Community Plan Bylaw, 2017, No. 2220”, as amended, shall apply to this Development Permit and attachments.

8. Where the holder of this Permit does not obtain the required building permits and commence construction of the development as outlined in this Development Permit within two years after the date this Permit was authorized by Council, the Permit shall lapse, unless the Council, prior to the date the Permit is scheduled to lapse, has authorized further time extension of the Permit.

9. This permit does not constitute a subdivision approval, a tree management permit, a demolition permit, or a building permit.

Authorizing Resolution passed by the Council for the City of White Rock on the _____ day of _____, 20__.

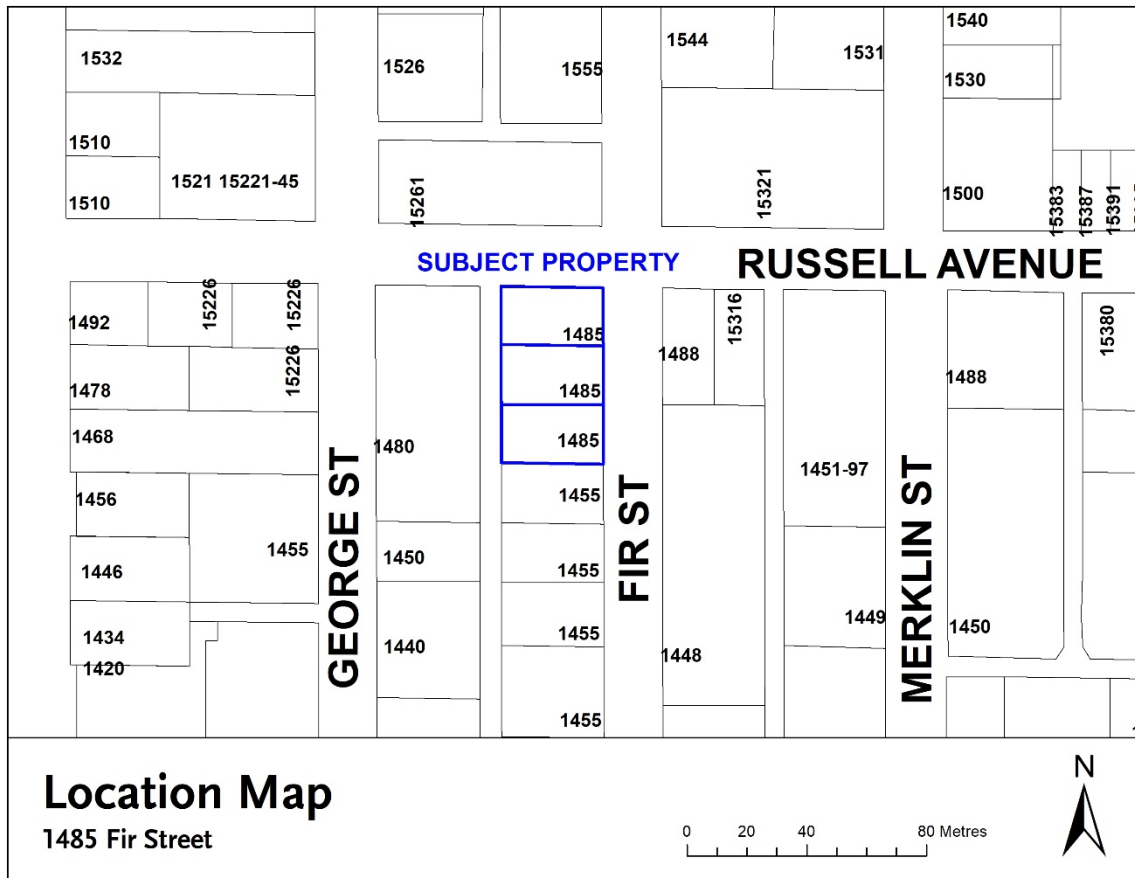
This development permit has been executed at White Rock, British Columbia on the _____
day of _____ 20__.

The Corporate Seal of THE CORPORATION
OF THE CITY OF WHITE ROCK was hereunto
affixed in the presence of:

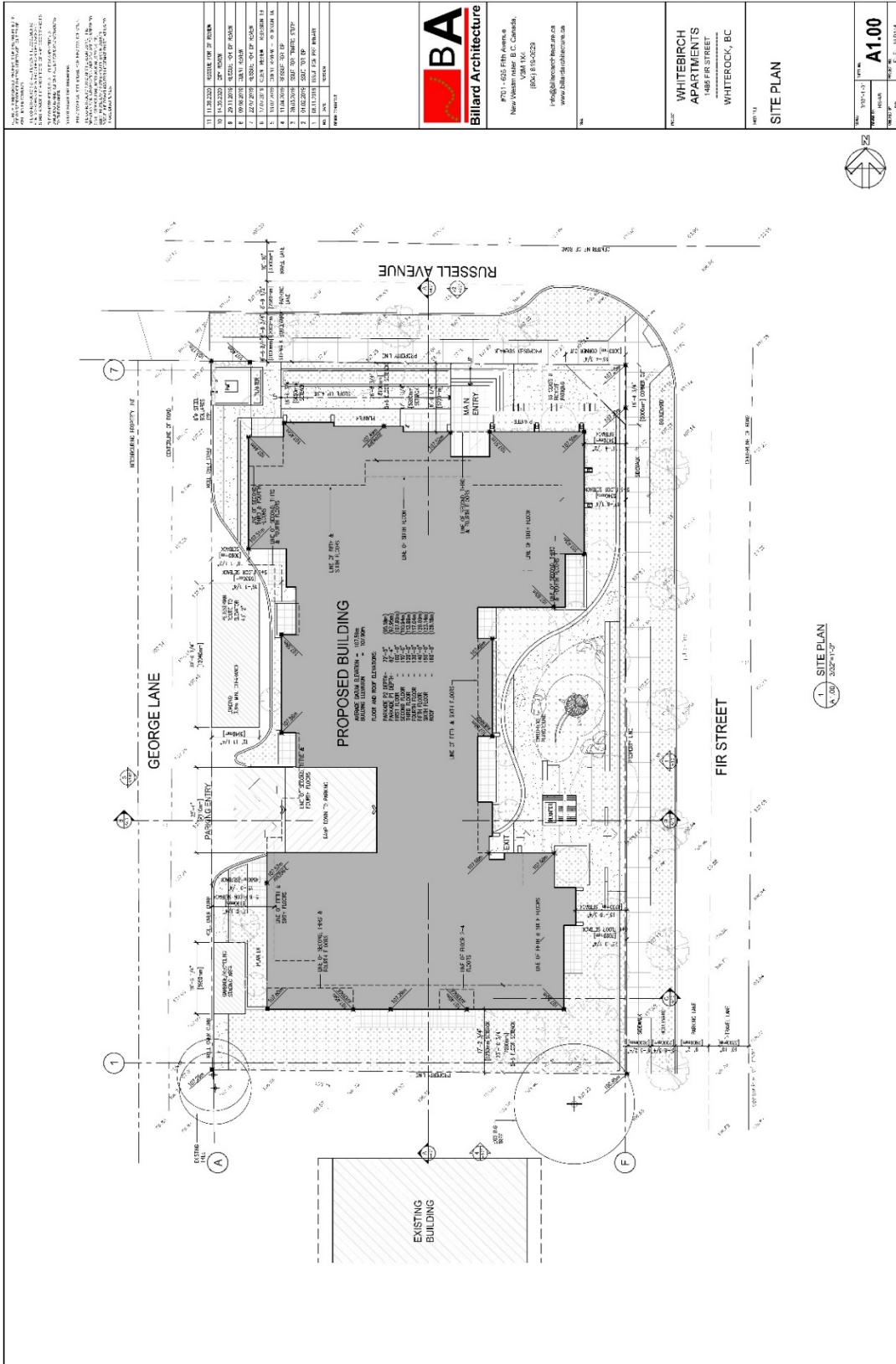
Mayor
Authorized Signatory

Director of Corporate Administration
Authorized Signatory

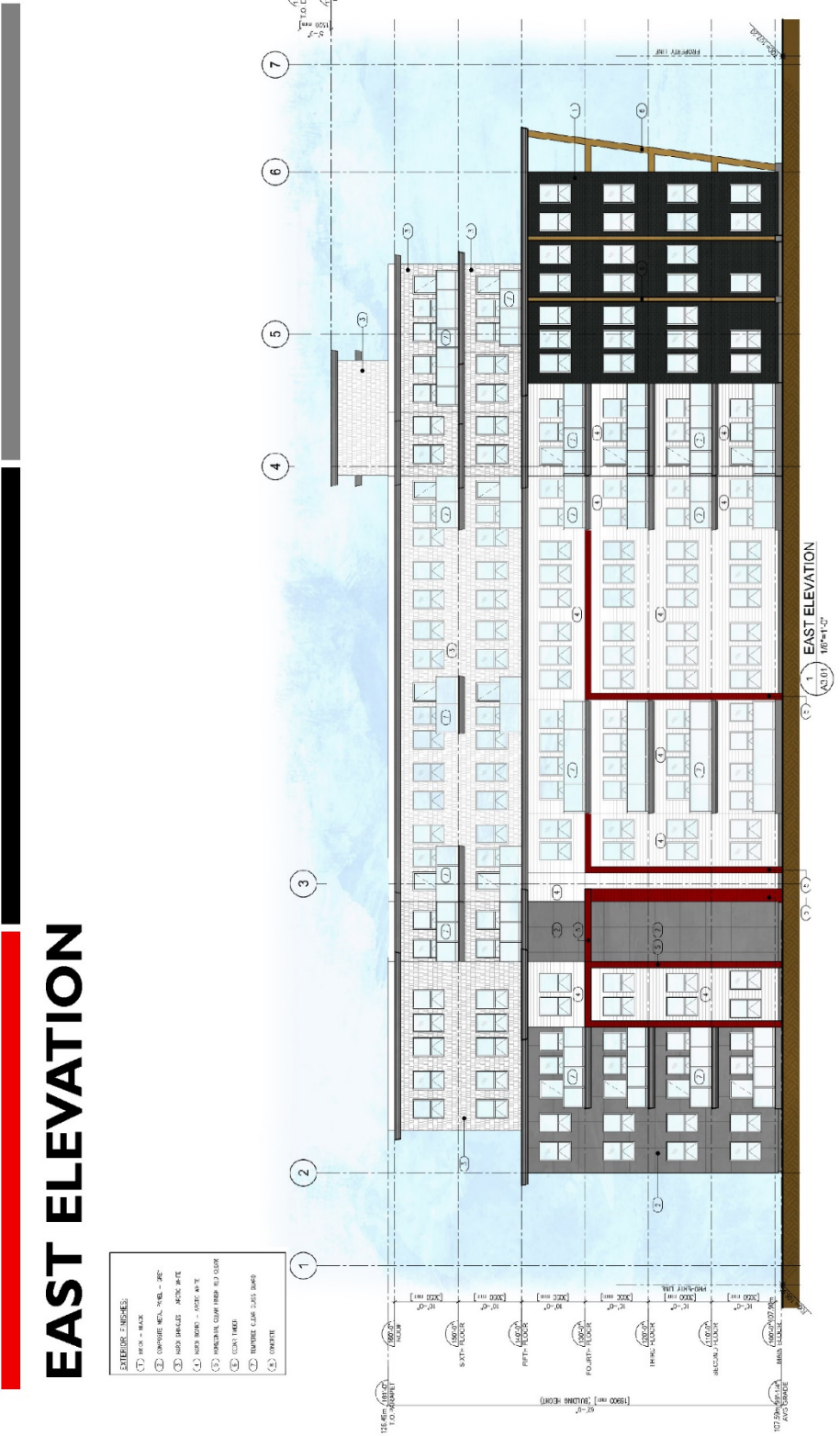
Schedule A – Location Map



Schedule B – Site Plan



Schedule C – Elevations



11

WEST ELEVATION

EXTERIOR FINISHES:

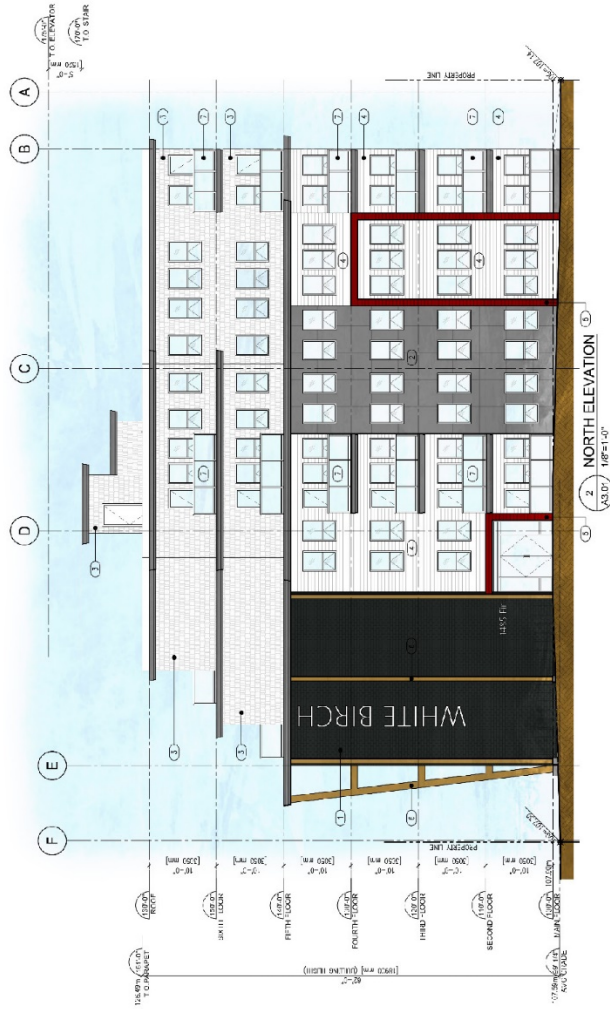
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(2)	CONCRETE
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(99)	SMOOTH BRICK - WHITE
(100)	SMOOTH BRICK - WHITE



NORTH ELEVATION

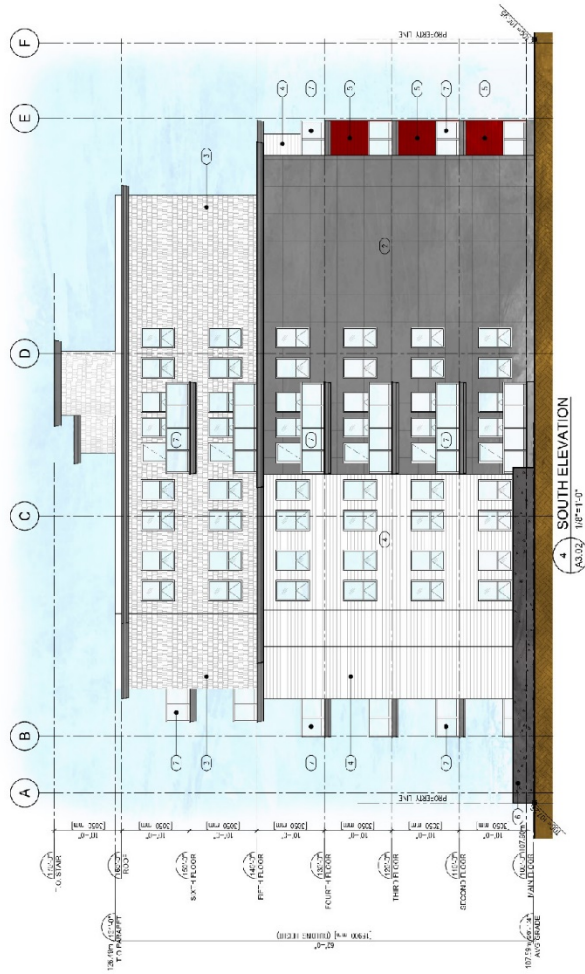
EXTERIOR FINISHES:

1	BRICK - BRICK
2	COMPOSITE WOOD, WOOD - BRK
3	WOOD SHAPES, WOOD W/TE
4	WOOD SHAPES - WHITE W/TE
5	HORIZONTAL CLUM FISH RD. CLUM
6	CONC. TRUSS
7	TERRAZZO CLAS CLAS STAIRS
8	CONCRETE



SOUTH ELEVATION

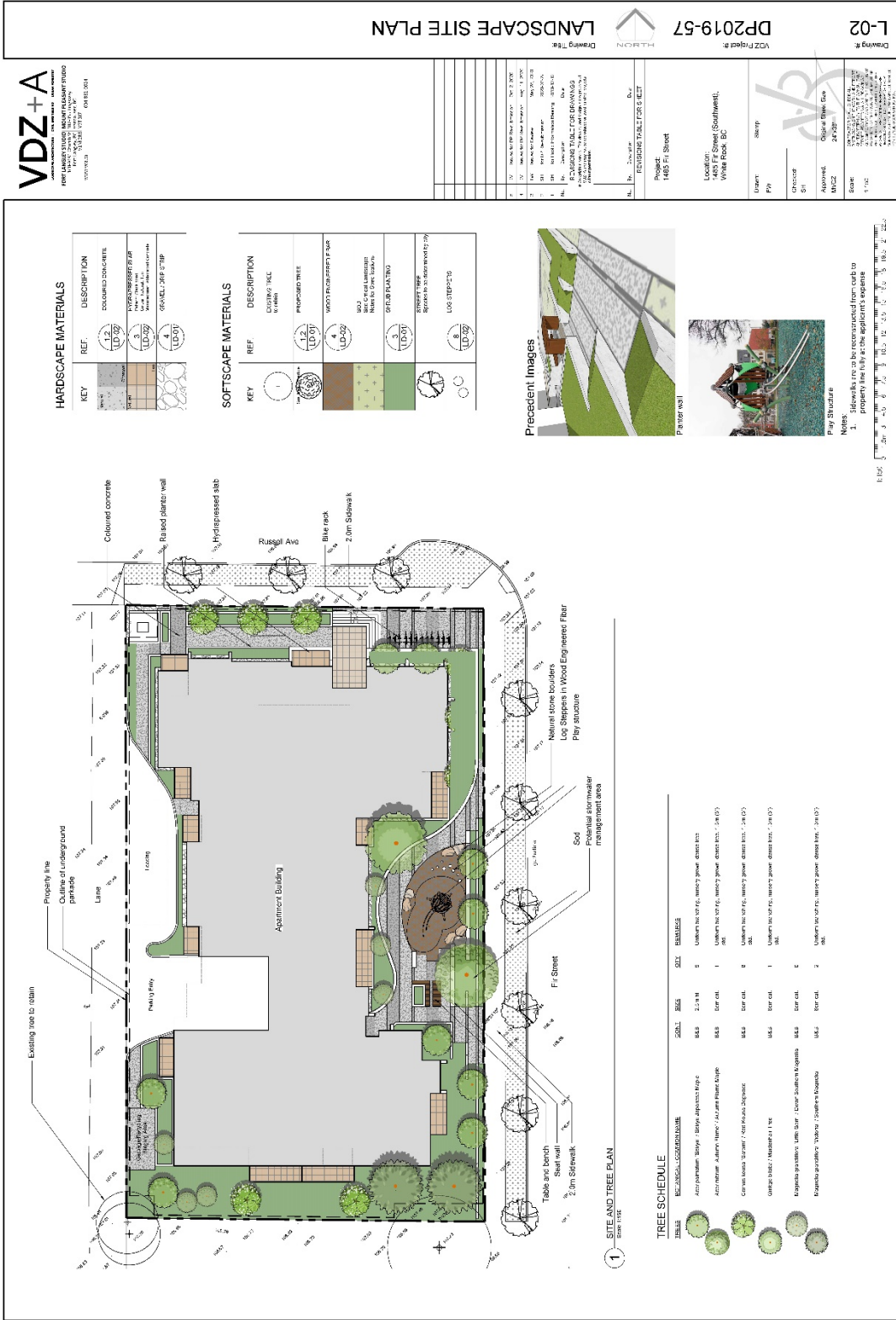
EXTERIOR FINISHES:	
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(3)	INS. BRICKS - WHITE W/TE
(4)	INS. BRICKS - BROWN W/TE
(5)	INDUSTRIAL GUM BRK - RD. GRAY
(6)	CLAY TILE
(7)	TEXTURE CLAY SLATE SHIP
(8)	CONCRETE



Schedule D – Renderings



Schedule E – Landscape Plans



1 CONCRETE ON SLAB
Scale 1:20

2 CONCRETE JOINTS
Scale 1:20

3 HYDRO-PRESSED SLAB ON STRUCTURAL SLAB
Scale 1:10

4 FIBER ON SLAB
Scale 1:10

5 TABLE AND BENCH
Scale 1:20

6 PLAY STRUCTURE - TREE HOUSE
Scale 1:10

7 FEATURE BOULDER
Scale 1:20

8 LOG STEPPERS ON SLAB
Scale 1:10

9 HYDRO-PRESSED SLAB ON STRUCTURAL SLAB
Scale 1:10

10 LOG STEPPERS ON SLAB
Scale 1:10

11 FIBER ON SLAB
Scale 1:10

12 LOG STEPPERS ON SLAB
Scale 1:10

13 HYDRO-PRESSED SLAB ON STRUCTURAL SLAB
Scale 1:10

14 LOG STEPPERS ON SLAB
Scale 1:10

VDZ+A
VANDER ZANDER ARCHITECTS
1485 FIR STREET, SUITE 100
VANCOUVER, BC V6Z 1R7
TEL: 604.681.1234
WWW.VDZ+A.COM

REVISION TABLE FOR SHEET

NO.	DATE	DESCRIPTION
1	2019.05.01	ISSUED FOR PERMIT
2	2019.05.15	REVISED PERMIT COMMENTS
3	2019.06.01	REVISED PERMIT COMMENTS
4	2019.06.15	REVISED PERMIT COMMENTS
5	2019.07.01	REVISED PERMIT COMMENTS
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100	2023.07.01	REVISED PERMIT COMMENTS

Project: 1485 Fir Street
Location: White Rock, BC
Drawn: PW
Checked: SA
Approved: M/Z
Scale: AS SHOWN
Sheet: 24/29

VDZ Project # DP2019-57
Drawing # LD-02
Drawing Title: DETAILS

VDZ+A
 VANDER ZANDER DESIGN ARCHITECTS
 1485 FIR STREET, SUITE 100
 VANCOUVER, BC V6Z 1S7
 TEL: 604.681.1111
 WWW.VDZ+A.COM

VDZ Project #
DP2019-57

Drawing Title
DETAILS

Drawing #
LD-01

No.	Rev.	Description	Date
1	01	Issue for Information	01.11.2019
2	02	Issue for Review	01.11.2019
3	03	Issue for Construction	01.11.2019
4	04	Issue for Construction	01.11.2019
5	05	Issue for Construction	01.11.2019
6	06	Issue for Construction	01.11.2019
7	07	Issue for Construction	01.11.2019
8	08	Issue for Construction	01.11.2019
9	09	Issue for Construction	01.11.2019
10	10	Issue for Construction	01.11.2019

REVISIONS TABLE FOR SHEET

Project: 1485 Fir Street

Location: 1485 Fir Street (Southwest), West Block, BC

Drawn: TW
 Checked: SM
 Approved: VZD
 Scale: AS SHOWN

1 TREE PLANTING ON SLAB
Scale: 1/8"

NOTE:

1. Backfill up to be loosened and dropped to the bottom of the planting hole at string line, etc. to be removed.
2. All trees shall be removed prior to planting.
3. All trees shall be single stem unless specified otherwise.

2 TREE PLANTING IN RAISED PLANTER
Scale: 1/8"

NOTE:

1. Remove all string line, job tags from site.
2. All trees shall be removed prior to construction.
3. All trees shall be removed prior to construction.
4. O.C. spacing per planting plan.

3 SHRUB ON SLAB
Scale: 1/8"

NOTE:

1. Remove all string line, job tags from site.
2. All shrubs shall be removed prior to construction.
3. All shrubs shall be removed prior to construction.
4. O.C. spacing per planting plan.

4 DRIP STRIP
Scale: 1/8"

NOTE:

1. Building wall refer to Architects detail.
2. O.C. concrete wall, 25mm chamfered edge. Standard finish, waterproof as per manufacturer's typical detail.
3. 100mm sand drain (see per specification).
4. Drainage hole, install as per manufacturer's typical detail.
5. Structural slab, Minimum protection board, shown for removal only refer to Architects detail.
6. Concrete slab, shown for removal only, refer to Architects detail.
7. Protection board, Refer to Architects detail.
8. 25mm round river rock.

5 PLANTER WALL ON SLAB
Scale: 1/8"

NOTE:

1. Concrete wall to be in slab with Architect and Structural Engineer.
2. All walls over 200mm height to be Structurally Engineered.
3. All walls over 200mm height to be Structurally Engineered.
4. All walls over 200mm height to be Structurally Engineered.
5. All walls over 200mm height to be Structurally Engineered.
6. All walls over 200mm height to be Structurally Engineered.

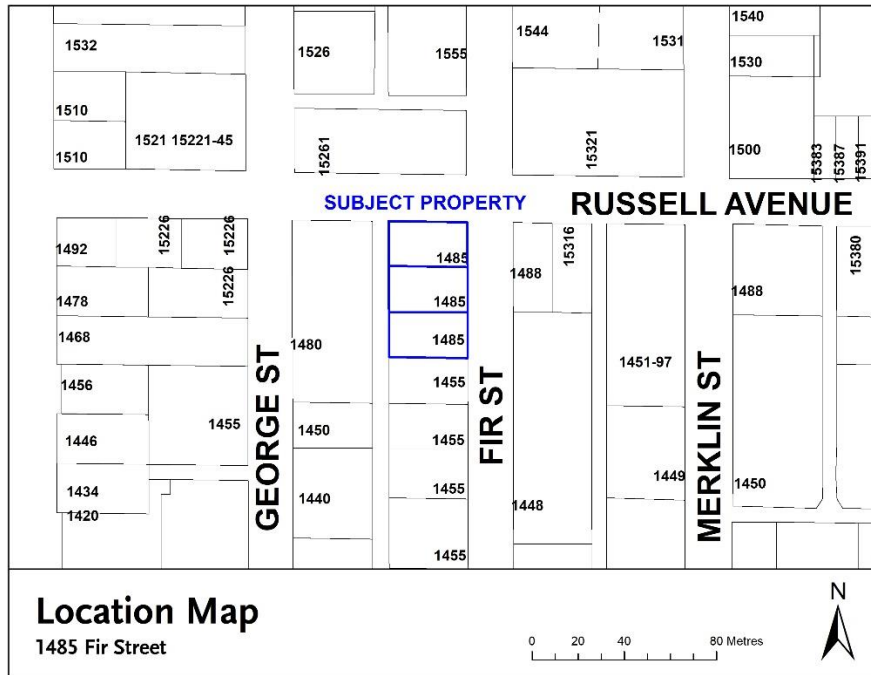
6 STAIRS WITH HANDRAIL
Scale: 1/8"

NOTE:

1. Steps to have light broom finish.
2. Expansion joints require seal of slope, which continuous adjacent to cheek prevent moisture infiltration.
3. Refer to Geotechnical report for concrete subgrade requirements.

2024/05/28 09:11:11 AM C:\Users\jordan\OneDrive\Documents\432 - 1485 Fir Street\Drawings\LD-01.dwg

APPENDIX C
Location and Ortho Photo Maps

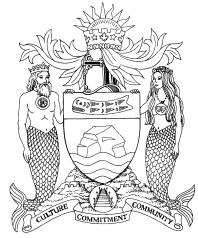


APPENDIX D

**LUPC Report "Initial OCP Amendment Application Report- 1485 Fir Street (19-009
OCP/ZON/MJP)" dated July 8, 2019**

(Attached Separately)

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 8, 2019

TO: Land Use and Planning Committee

FROM: Carl Johannsen, Director of Planning and Development Services

SUBJECT: Initial OCP Amendment Application Report – 1485 Fir Street (19-009 OCP/ZON/MJP)

RECOMMENDATIONS

THAT the Land Use and Planning Committee:

1. Receive for information the corporate report dated July 8, 2019 from the Director of Planning and Development Services, titled “Initial OCP Amendment Application Report – 1485 Fir Street (19-009 OCP/ZON/MJP);” and
 2. Recommend that Council refuse the OCP amendment application, and direct staff to work with the applicant on a revised rezoning and Major Development Permit application, for a secured rental housing development that includes a reduced FAR (2.8 gross floor area ratio consistent with the OCP), and amended building and site design.
-

EXECUTIVE SUMMARY

An Official Community Plan (OCP) Amendment application has been received regarding a development proposal at 1485 Fir Street, which is the civic address for the land occupied by an existing three storey rental apartment building known as “White Birch Apartments” and addressed as 1475 Fir Street.

This proposal consists of redeveloping the site to replace the existing 25 unit building with a six (6) storey rental apartment building with 84 rental dwelling units. This exceeds the OCP minimum requirement of a 1:1 replacement policy by 59 units. As required by the Council Policy 514: Tenant Relocation Policy, the applicant has provided tenants with a Notice of Redevelopment and Tenant Assistance Package, which is attached to this report as Appendix D.

The application is requesting an amendment to the OCP to permit a density 15% higher than the maximum allowed, with a proposed gross Floor Area Ratio (FAR) density of 3.23 exceeding the maximum 2.8 FAR currently allowed in the OCP.

The proposed apartment development also does not meet the minimum percentage of three bedroom units required under OCP Policy 11.1.1 (10% minimum three bedroom and 35% either two or three bedroom), instead providing 7% (six units) out of the total 84 units as three bedroom and 37% (25 units) as either two or three bedroom.

While staff support the expansion of the rental housing supply that this project would help provide, staff do not support the proposed FAR increase beyond the maximum in the OCP and the resulting bulkiness of the proposed design.

Achieving the required minimum percentage of three bedroom units can be a financial challenge for projects as these larger units typically generate lower revenue per square foot; this financial obstacle is particularly relevant for rental apartment proposals which have historically been less profitable than strata residential development and therefore unable to compete with strata developers to acquire new sites. Staff recommend that as part of the “Improving Housing Affordability” topic within the OCP Review, that Council consider reducing the amount of three bedroom units required for rental apartment buildings to 5%.

This report sets out options for consideration by the Land Use and Planning Committee, in terms of giving direction to staff on how this application should be managed moving forward. These options include:

- 1) Committee refusing the OCP amendment related to this proposal and directing staff to work with the applicant on a revised rezoning application that is consistent with the current OCP FAR for these properties (2.8 gross floor area ratio) and includes a refined building design; or
- 2) directing staff to continue to process the entire proposal in its current form, including the OCP amendment, with the next step being a Public Information Meeting to be hosted by the Applicant.

INTRODUCTION

The Planning and Development Services Department has received an OCP Amendment application for 1485 Fir Street. This corporate report provides initial, high-level staff analysis and commentary on this application, for the Land Use and Planning Committee’s (LUPC) information.

Staff seek feedback from the LUPC on whether this OCP Amendment application should be:

- Refused and the rezoning application referred back to staff, with direction from the LUPC to staff regarding suggested revisions to the rezoning application; or
- Moved forward in its current form.

The proposed development is for a rental residential buildings that is six (6) storeys in height, with a proposed FAR of 3.23. The proposal includes 84 residential dwelling units (all of which would be secured as rental for the life of the building). The orthophoto and location maps are included as Appendix A of this corporate report, and the applicant’s drawing package is included as Appendix C (including site plan, conceptual massing drawings, and commentary on the relationship with City OCP policies).

PAST PRACTICE / POLICY / LEGISLATION

The City’s OCP (*White Rock Official Community Plan, 2017, No. 2220*) sets out land use, density, height and other policy directions for new development applications.

In the Land Use chapter of the OCP, under policy 8.2.3, properties in the Town Centre Transition area including 1485 Fir Street are identified as being eligible for additional density (up to 40% above the base density) where at least half this additional floor area is dedicated to and secured as residential rental units. The base density for this property is 2.0 FAR, there for the total maximum density permitted, including the rental bonus density, is 2.8 FAR, of which 0.4 FAR would be required to be for rental units. There is no additional bonus available for projects that consist entirely of rental units.

Building heights in the Town Centre Transition area are encouraged to develop within the range presented in Figure 10 of the OCP; for the subject site, this is shown in a continuum between 18

storeys at North Bluff Road and 6 storeys at Thrift Avenue, suggesting that between 6-8 storeys would be a supportable height on this block.

Policy 11.2.1(f) requires that a minimum one-to-one replacement of existing rental units be provided when an existing rental building is proposed for redevelopment, with an average unit size of the replacement units at least 80% of the units being replaced.

The new OCP also includes policy regarding OCP Amendment applications. According to Section 19.3 (page 76) OCP Amendment applications are to be reviewed by staff and an initial information report on the proposal presented to Council for review and feedback to staff. As stated in the OCP, Council may then refuse the application or direct City staff to continue processing it. Council may also refer it back to staff with specific direction.

This approach provides the Committee an opportunity to provide direction on OCP Amendment applications, prior to these applications being presented at a Public Information Meeting and proceeding through the application process (i.e. Advisory Design Panel, consultation with potentially affected groups, preparation of Amendment Bylaws, Public Hearings, etc.), as set out in the Planning Procedures Bylaw and Policy 512: Official Community Plan Consultation.

ANALYSIS

Existing Land Use Context

There are three separate parcels that form 1485 Fir Street (the existing building straddles the shared property lines) and the subject properties are currently zoned ‘RM-2 Medium Density Multi-Unit Residential Zone’ which permits townhouse or apartment complexes with a 10.7 metre (35.1 feet) maximum height.

On the outside edge of the Town Centre area, the subject site is surrounded by a mix of commercial, institutional and residential uses. To the west of the subject site across a lane is St. John’s Presbyterian Church and Daycare Centre, to the north across Russell Avenue is a three storey office building (Russell Professional Building), and to the south and east are existing multi-unit residential buildings (one storey building on the east side of Fir Street, and three storey buildings to the south).

Proposed Development

The subject properties are 0.50 acres (2,036 square metres; 21,917 square feet) in overall size. In terms of OCP land use the subject properties are in the ‘Town Centre Transition’ designation, which allows multi-unit residential uses in low-rise to high-rise buildings. The base density for this property is 2.0 FAR, and the total maximum density permitted including the rental bonus density (40% above the base density) is 2.8 FAR, of which 0.4 FAR would be required as rental floor area. A summary of development statistics are provided in Table 1 below:

Table 1: Development Proposal Statistics

	Original Proposal
Land Area	0.50 acres (2,036.m ²)
Total Number of Units	84
Residential Floor Area (Net)	5,825.6 m ² (62,706.1 ft ²)
Gross Floor Area	6,586.9 m ² (70,900.4 ft ²)
Density - Floor Area Ratio (Gross)	3.2347 (rounded to 3.23 for this report)
Lot Coverage	56%
Height	18.9 m (62.0 ft)
Residential Parking Spaces	115 (1.37 per unit)
Loading Spaces	1

A rendering of the proposed development is included below as Figure 1, the proposed site plan is included as Figure 2, and a more detailed drawing package is available in Appendix C.

Figure 1: Rendering



Figure 2: Proposed Site Plan (Main Floor Plan)



Proposed FAR in Relation to the OCP

The proposed OCP Amendment application involves an FAR of 3.23. Staff do not support the proposed FAR, which is 0.43 FAR above the maximum FAR available of 2.8.

FAR is calculated by using these measurement methods in the White Rock Zoning Bylaw:

- 1) General ‘gross floor area’ measurement: includes the sum total of floor areas of each storey in a building, and excludes community amenity spaces. The Zoning Bylaw defines this approach for buildings in commercial, mixed-use and public use zones.
 - This measurement approach results in a ‘gross FAR’ number. The gross FAR approach also reflects the overall massing and bulk of a building.
- 2) General ‘net floor area’ measurement: the Zoning Bylaw allows this approach for buildings in multi-family residential zones, and includes the sum total of floor areas of each storey but excludes unenclosed balconies, common stairwells, elevator shafts, common corridors, recreation and amenity areas and above grade enclosed parking areas.
 - This measurement approach results in a ‘net FAR’ number.
 - A net FAR number can typically be 10 to 20 percent lower than the gross FAR number for the same building, primarily due to removing the ‘circulation floor space’ (i.e. hallways, stairwells, elevator shafts) areas on each building floor plate or storey, and in some cases, removing above ground enclosed parking floor area contained within the same building. The net FAR approach does not reflect the overall massing and bulk of the building, and typically only includes saleable or leasable floor space.

The measurement of density in the OCP is always based on the **gross FAR** and is intended to regulate the overall massing and bulk of a building, whereas the Zoning Bylaw utilizes both gross and net approaches, depending on the zone.

Staff note that the base density allowed in the OCP without providing rental housing on these properties is 2.0 FAR. Since a replacement of the existing rental units is required it is likely that at least approximately 1.0 FAR on this site would be provided as rental which enables the bonus density provisions of up to 2.8 FAR, leaving the potential for approximately 1.8 FAR as either rental or strata. The development does not include any strata and instead proposes that the entire building be secured market rental units.

The applicant has provided an amendment rationale for the apartment site (attached as Appendix B) and has described the relationship with the proposal and other OCP objectives in their drawing package attached as Appendix C.

Should Council wish to advance this particular application at the currently proposed density (3.23 FAR) on the basis that it will make rental development more viable, it is recommended that staff also be directed to include in the “Improving Housing Affordability” topic within the OCP Review a proposal that would allow up to a larger density bonus (e.g. 50-65% above base density instead of the current 40%) for sites within the Town Centre Transition area that are proposed as entirely market rental buildings.

Apartment Design Commentary

Staff consider that the proposed mid-rise six (6) storey building, at 3.23 FAR and a lot coverage of 56%, presents a bulky/boxy form that would have an imposing and somewhat monolithic or institutional presence in the neighbourhood. Comparatively, a six (6) storey building at the 2.8 FAR allowed in the OCP would have an approximate lot coverage of 47%. While it is possible to

reduce the bulky/boxy effect of the building through revisions to exterior materials and architectural details that break up the mass of the building and therefore increase the visual interest and residential feel, the size of the building itself contributes to the bulkiness of the massing, and lack of articulation and recessing needed to provide relief and transitions from the street and adjacent properties.

By comparison, the OCP allows six (6) storey buildings on sites identified for affordable rental developments at a maximum density of 2.5 FAR, which is considered appropriate to allow for a design with adequate setbacks (i.e. reduced lot coverage of approximately 42% creating open space at the sidewalk level) and terracing (to reduce the perceived height impact of upper floors) of a six storey building.

On the subject property, the permitted OCP density increases to 2.8 FAR, which is close to the density at which a strata concrete tower would be a viable form of development on a large enough site. However, due to the rental replacement requirements and the applicant's interest in providing an entirely market rental development, the cost of concrete construction would likely not be financially viable.

Staff would consider it appropriate for the building to be redesigned to a density of 2.8 FAR, within the maximum allowed in the OCP for this property, which could provide for:

- reduced lot coverage (increased open space at the ground level) closer to or less than 50%
- deeper setbacks for the upper levels (reducing the perceived height impact), such as stepping levels 4 to 6 at the corner of Russell and Fir and on the south property line
- private function patios for residents on the 1st-4th floors facing the street (open balconies are not included in FAR, but are encouraged in the Development Permit Area guidelines)
- increasing the depth of the building's "bays" to break up the massing

Additional suggested design revisions, which do not relate directly to building size, would be:

- varying the fenestration (windows and doors) in size and/or colours (of muntin bars), which are currently the same across the entire building, to accent the residential character of the building
- emphasize the vertical elements to provide balance to the building's overall size
- consider potential for the reorientation/relocation of the outdoor amenity space to provide relief to the massing impact
- providing architectural details on portions of the west façade which are currently blank
- increase roofline variability to provide interest and punctuation
- consider a low maintenance landscaped (planted) edge on the fourth level roof to soften the appearance of the building and create a horizontal break in the building

Should Council direct staff to work with the applicant on a revised design within the maximum density permitted in the OCP or proceed with the current application, these design considerations could receive further feedback from the Advisory Design Panel.

The current parking ratio for the proposal is 1.37 spaces per unit, which is less than the typical Zoning Bylaw requirement of 1.5 spaces per unit. However, noting that the property is near bus stops and routes in the Town Centre, and that OCP policy 11.2.1(f) states that "Council will consider reviewing parking requirements to determine the extent to which they can be relaxed for nonmarket and rental housing within walking distance (i.e. 400-800 metres of real travel

distance) of frequent transit service and/or commercial areas,” staff would consider supporting the parking reduction for this rental housing proposal, subject to confirmation of the proposed Transportation Demand Management measures proposed by the applicant (pre-loaded Compass Cards for residents, etc.)

Tenant Relocation Plan

The applicant has submitted a complete tenant relocation plan. The associated Notice of Redevelopment and Tenant Assistance Package, which was distributed to tenants on May 17, 2019, is attached to this report as Appendix D. The information contained in the Notice is considered to be consistent with the Tenant Relocation Policy. However, staff note that a dedicated tenant coordinator is to be retained and is not intended to be the developer/owner. Staff have not undertaken an in-depth analysis of the tenant relocation plan, as the proposed application is beyond the OCP FAR and direction on this is required first as this may affect the proposed number of units and potential tenant approach. Staff note that the current low vacancy rate for rental apartments may make locating alternative accommodation challenging.

The LUPC may consider discussing the tenant relocation plan at this time, or defer discussion to a later corporate report / LUPC meeting according to direction provided by Committee.

OPTIONS

While staff support the proposed market rental tenure of the project, staff do not support the proposed OCP Amendment in its current form.

Increasing permitted OCP densities on a site-specific basis will likely lead to future requests for similar OCP amendments, as prospective purchasers will ‘bid’ higher for the land on the basis of an anticipated increase in density. Staff do not believe that the densities in the approved OCP need to be increased in order to accommodate the projected increases in population, however if Council is interested in further incentivizing the construction of new rental apartments, staff recommend that additional density only be considered for projects that consist entirely of secured rental units.

This being said and based on the above analysis, the LUPC can consider these options, amongst other feedback, in directing how staff should manage this application moving forward:

1. Refuse the OCP amendment aspect of this proposal and refer back to staff to work with the applicant to revise their rezoning and major development permit application to be consistent with the maximum FAR for the property (2.8 FAR maximum). This also involves refining the apartment design as discussed in the report, which would have the likely effect of reducing the FAR below 2.8 FAR; or
2. Staff continue to process the entire proposal in its current form, with the next step being referral to external agencies and internal departments, a Public Information Meeting, followed by review by the Advisory Design Panel.

If the OCP amendment application is refused per Option #1 above which is also the recommendation of this corporate report, the applicant would be refunded a portion of their application fees for the OCP amendment application and the existing rezoning and major development permit applications which were applied for concurrently would remain open. In order to proceed with the rezoning and major development permit applications the applicant would need to submit revised designs that do not require amendments to the OCP (i.e. within the allowable density, and meeting the minimum 10% three bedroom unit requirements).

Additional Considerations

Should this proposed application move forward, staff note there are additional considerations that the applicant will need to meet and that the LUPC should be aware of, including:

- requests to exceed the OCP should have a clear public benefit beyond 1:1 replacement and additional rental space as contemplated in the OCP. This additional benefit could be secured below market rental units for vulnerable/existing tenants;
- the market rental residential tenure of the building would be required to be secured by way of a Housing Agreement and related bylaw as rental for the life of the building;
- as a market rental project, this development may be eligible for a reduction of Community Amenity Contributions (CAC) up to 50% of the targeted value;
- the OCP and Zoning Bylaw requires new buildings to include one (1) electric vehicle charging station and one (1) ‘rough in’ for every ten (10) parking spaces (the applicant is proposing 24 electric vehicle charging stations, which is slightly more than the 23 stations that would be required with the proposed 115 off-street parking spaces); and
- noting that stormwater and sanitary servicing master plans are currently being developed to guide development-related upgrades to these services (and a water master plan was recently approved), and that these master plans are based on FARs in the current OCP, it is important to note that increasing the FAR on this property and potentially other properties may undermine the basis of these servicing plans, and require significant additional servicing upgrades and funding.

CONCLUSION

The Planning and Development Services Department has received an OCP Amendment application for 1485 Fir Street. While staff support the rental housing component, staff do not support the proposal in its current form, primarily due to the FAR being over what is identified in the OCP. Staff seeks feedback from the Land Use and Planning Committee on whether this OCP Amendment application should be:

- Refused and the rezoning application referred back to staff for revisions within the allowable density in the OCP and design refinements to the building as discussed in this report, with direction from the LUPC to staff regarding suggested revisions to the application; or
- Moved forward in its current form.

Respectfully submitted,



Carl Johannsen, MCIP, RPP
Director of Planning and Development Services

Comments from the Chief Administrative Officer:

I concur with the recommendations of this corporate report.



Dan Bottrill
Chief Administrative Officer

Note: Attachments removed for brevity

Appendix A: Location and Ortho Photo Maps

Appendix B: Applicant's Official Community Plan Amendment Rationale Letter

Appendix C: Drawing Package

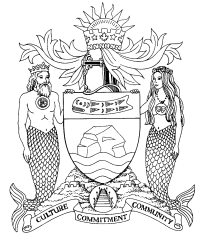
Appendix D: Applicant's Notice of Redevelopment and Tenant Assistance Package

APPENDIX E

LUPC Report “Information Report Update and Revised Tenant Relocation Plan 1485 Fir Street (ZON/MJP 19-009)” dated September 30, 2019

(Attached Separately)

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: September 30, 2019

TO: Land Use and Planning Committee

FROM: Carl Isaak, Director of Planning and Development Services

SUBJECT: Information Report Update and Revised Tenant Relocation Plan – 1485 Fir Street (ZON/MJP 19-009)

RECOMMENDATION

THAT the Land Use and Planning Committee receive for information the corporate report dated September 30, 2019 from the Director of Planning and Development Services, titled “Information Report Update and Revised Tenant Relocation Plan – 1485 Fir Street (ZON/MJP 19-009).”

INTRODUCTION

The Land Use and Planning Committee (LUPC) received a corporate report dated July 8, 2019 from the Director of Planning and Development Services, titled “Initial OCP Amendment Application Report – 1485 Fir Street (19-009 OCP/ZON/MJP).” The application at the time required an increase in gross floor area ratio (or ‘FAR’) density above what was permitted in the Official Community Plan (OCP) and did not provide the number of three-bedroom units (10%) required in the OCP, and would have required an OCP amendment.

Council subsequently directed staff to work with the applicant on a revised application that did not require an OCP amendment. There was also discussion at the LUPC meeting regarding the adequacy of the applicant’s Tenant Relocation Plan. This corporate report provides a brief update including the applicant’s enhanced Tenant Relocation Plan and an overview of the changes to the form of the development proposal application which now does not require an OCP amendment and will now proceed as a rezoning and major development permit application. Location and ortho photo maps of the subject property are attached as Appendix A. The revised Notice of Redevelopment and Tenant Assistance Package (components of the Tenant Relocation Plan) is attached as Appendix B. The corporate report from July 8, 2019 is attached as Appendix C, for LUPC’s information.

PAST PRACTICE / POLICY/LEGISLATION

OCP Land Use and Policy

The OCP land use designation for the subject properties is ‘Town Centre Transition.’ The City’s OCP (White Rock Official Community Plan, 2017, No. 2220) sets out land use, density, height and other policy directions for new development applications.

In the Land Use chapter of the OCP, under policy 8.2.3, properties in the Town Centre Transition area including 1485 Fir Street are identified as being eligible for additional density (up to 40% above the base density) where at least half this additional floor area is dedicated to and secured as residential rental units. The base density for this property is 2.0 FAR, therefore the total maximum density permitted, including the rental bonus density, is 2.8 FAR, of which 0.4 FAR would be required to be for rental units. There is no additional bonus available for projects that consist entirely of rental units.

Building heights in the Town Centre Transition area are encouraged to develop within the range presented in Figure 10 of the OCP; for the subject site, this is shown in a continuum between 18 storeys at North Bluff Road and 6 storeys at Thrift Avenue, suggesting that between 6-8 storeys would be a supportable height on this block.

Policy 11.2.1(f) requires that a minimum one-to-one replacement of existing rental units be provided when an existing rental building is proposed for redevelopment, with an average unit size of the replacement units at least 80% of the units being replaced.

ANALYSIS

Existing Land Use Context

There are three separate parcels that form 1485 Fir Street (the existing building straddles the shared property lines) and the subject properties are currently zoned 'RM-2 Medium Density Multi-Unit Residential Zone' which permits townhouse or apartment complexes with a 10.7 metre (35.1 feet) maximum height.

On the outside edge of the Town Centre area, the subject site is surrounded by a mix of commercial, institutional and residential uses. To the west across a lane is St. John's Presbyterian Church and Daycare Centre, to the north across Russell Avenue is a three storey office building (Russell Professional Building), and to the south and east are existing multi-unit residential buildings (one storey building on the east side of Fir Street, and three storey buildings to the south).

Previous Proposal

The July 8, 2019 report to the Land Use and Planning Committee included an overview of a new development application submitted on May 9, 2019, for a proposed development with a total of 84 rental residential units in a six (6) storey building

The proposed density for the apartment site exceeded the OCP maximum density by 0.53 FAR (3.23 FAR proposed; 2.8 FAR allowed).

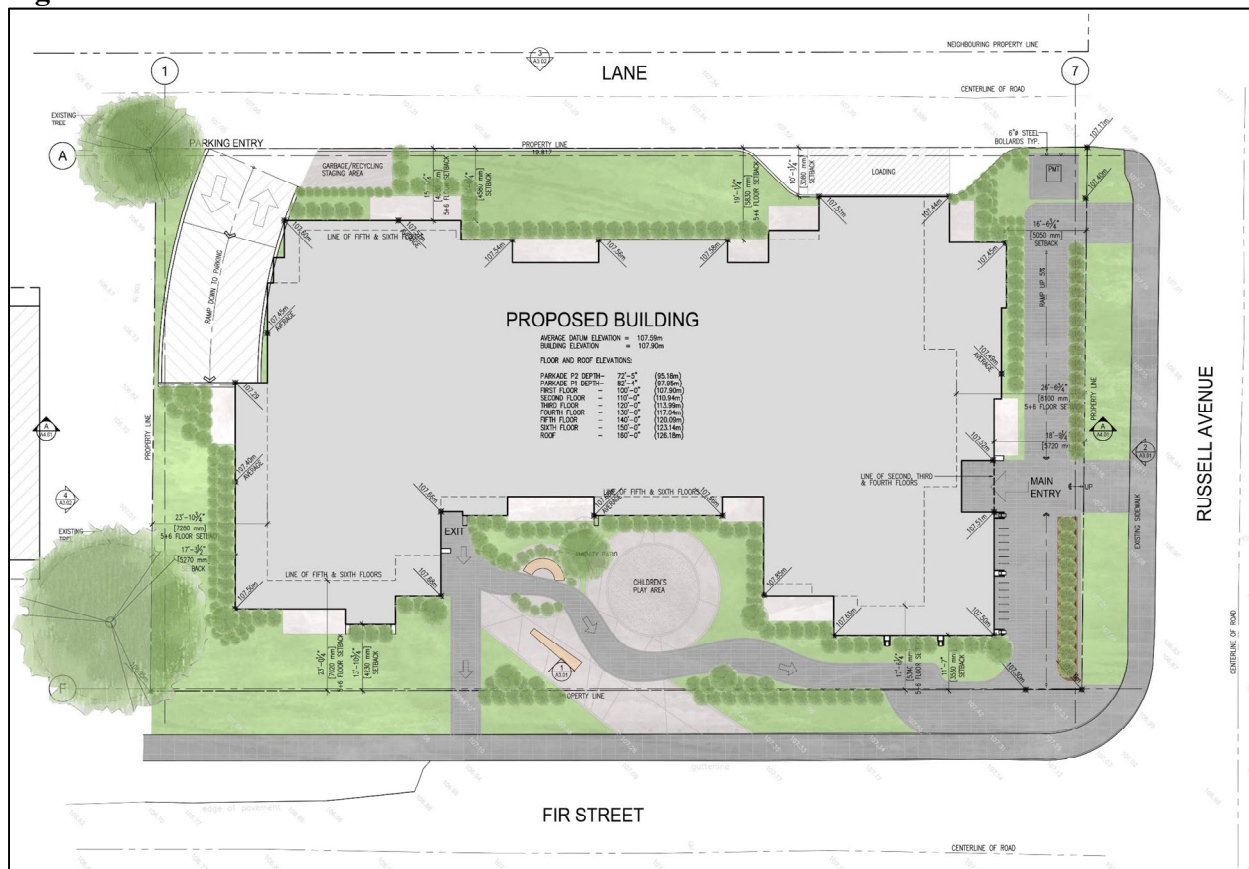
Council subsequently directed staff to work with the applicant on a revised application that did not require an OCP amendment (i.e. that did not exceed the maximum density in the OCP).

Revised Proposal

Following Council's previous direction to work with staff on a revised application that did not require an OCP amendment, the applicant has submitted drawings for the rezoning and development permit application on August 15, 2019. The new proposal does not exceed the maximum density allowed in the OCP and therefore does not require an amendment to the OCP. As the subject properties' current zoning is RM-2, an amendment to the zoning bylaw ('rezoning') would be required to allow the proposal, as well as a development permit to regulate the form and character of the development. Appendix D of the report provides a table outlining the changes in development statistics from the original application to the revised application.

A site plan of the revised proposal is included below as Figure 2, with an enlarged version of the same site plan included as Appendix E to this report.

Figure 1: Site Plan



Revisions to Tenant Relocation Plan

In the original Tenant Relocation Plan, the applicant proposed to give the option to existing tenants to return to the new building at rents that would be 10% below the market rent that was being charged at that time for other similar units, which is consistent with the City’s Tenant Relocation Policy No: 514. Due to concerns that were raised during the discussion at LUPC regarding the large increase that even 10% below market rent would be for existing tenants, the applicant has offered to reduce the rents charged to returning tenants to 20% below projected market rent for the building, which is estimated at \$2.80 per square foot. The proposed initial rents for returning residents would be \$2.24 per square foot, equating to:

- \$1,232 for an average size (550 square foot) one-bedroom unit
- \$1,859 for an average size (830 square foot) two-bedroom unit

These rents would be inclusive of a parking space and hot water, and following occupancy the owner would be permitted to increase rents in accordance with the annual increases regulated by the Residential Tenancy Branch.

In addition to the cost of the foregone revenue from the below market rents for existing tenants, the applicant has estimated that the total cost of the reduced rents and other forms of assistance to tenants would be \$145,000 (\$90,000 for compensation to tenants, \$23,000 for moving expenses, \$15,000 for moving assistance, and \$16,000 for a tenant relocation coordinator).

The applicant has also identified that the previous owner of the subject property, who also have another rental apartment in White Rock, have offered to relocate existing tenants to their building during the construction period as their units become vacant.

The applicant has also designated a new Tenant Relocation Coordinator instead of personally acting as the Tenant Relocation Coordinator.

Revisions to Architectural Drawings

The applicant has made several revisions to the drawing package in response to the comments provided in the corporate report dated July 8, 2019, attached as Appendix C. These revisions reduce the amount of floor area density and increase the number of three-bedroom units so that an OCP amendment is no longer necessary. The changes also begin to address the massing and design issues noted in the report. Some of the more significant design changes include:

- Decreasing the lot coverage to below 50%
- Increasing the setbacks on the upper floors to reduce the visual impact of the height
- Adding balconies onto the homes along Fir Street
- ‘Flipping’ the outdoor play area / courtyard to Fir Street (from the lane)

The design will likely have further changes through public feedback and Advisory Design Panel review, prior to being brought forward to Land Use and Planning Committee. Figures 2 and 3, showing the revised and original renderings of the building from the corner of Fir and Russell, are provided for comparison purposes.

Figure 2: Revised Rendering (from Russell Avenue and Fir Street)



Figure 3: Previous Rendering (from Russell Avenue and Fir Street)



Next Steps

Consistent with the process for a Zoning Bylaw amendment and Major Development Permit application (outlined in Schedules H and L of Planning Procedures Bylaw No. 2234), the following are the next steps for the application:

1. The application materials will be circulated to internal departments for comment, as well as to staff at the Surrey School District (this is already underway).
2. The applicant will install development notification signs on the property, and a public information meeting hosted by the applicant and attended by staff will be scheduled to allow residents an opportunity to provide early input on the proposal.
3. An Advisory Design Panel will be held meeting to receive advice and direction on the form and character of the proposed development.

A detailed corporate report for a future LUPC meeting to consider this application will be prepared upon completion of the technical and public review processes.

BUDGET IMPLICATIONS

Further details regarding the Development Cost Charges associated with the project will be brought forward in the detailed corporate report noted above.

In accordance with Council Policy 511: ‘Density Bonus / Amenity Contribution’, a community amenity contribution of \$922,000 would be anticipated based on the target rates for the Town Centre (this site is in close proximity to the Town Centre and a similar target rate is considered appropriate), and Council may consider reducing the amenity contribution target based on the provision of rental housing.

Council Policy 511 currently allows a reduction of up to 50% of an applicable amenity contribution for secured market rental floorspace, which would amount to \$461,000 based on the above noted target.

CONCLUSION

As a follow-up to a previous OCP amendment application information report, the applicant has revised the density of the proposal to below the maximum 2.8 gross floor area ratio (FAR) for

this rental apartment building, consistent with the OCP, and the application no longer requires an OCP amendment. This report is provided to Council for information regarding the revised proposal which includes a zoning bylaw amendment and Major Development Permit application. A detailed corporate report regarding this application will be provided to LUPC for consideration upon completion of the technical and public review processes.

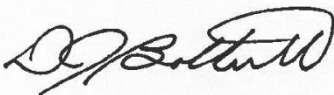
Respectfully submitted,



Carl Isaak, MCIP, RPP
Director of Planning and Development Services

Comments from the Chief Administrative Officer:

This corporate report is provided for information.



Dan Bottrill
Chief Administrative Officer

Note: Attachments removed for brevity

- Appendix A: Location and Ortho Photo Maps
- Appendix B: Revised Notice of Redevelopment and Tenant Assistance Package
- Appendix C: Corporate Report dated July 8, 2019 titled “Initial OCP Amendment Application Report – 1485 Fir Street (19-009 OCP/ZON/MJP)”
- Appendix D: Comparison of Original Development Proposal Statistics with Revised Proposal
- Appendix E: Renderings and Landscape Site Plan

APPENDIX F

Public Information Meeting Sign-in Sheet, Comment Forms, and Summary

(Attached Separately)

PUBLIC INFORMATION MEETING
1485 FIR SREET
REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009
DECEMBER 12, 2019

THIRDSPEACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
1.	Elena	1475 - 204	
2.	B.H. Inu	14728 Upper Ropa Ave	V4B 2C9
3.	Pohly Pactor	101 - 1475 FIR ST.	
4.	Michelle McCallum	1544 Fir -	
5.	PAT PETRALA	15020N. BLUFF RD	V4B 5A4
6.	Ron Reid	1487 Merklin	V4B 4C9
7.	GEORGE WARTTIG	1475 FIR ST	V4B 4B7
8.	William King	1475 FIR ST.	✓
9.	ELIZABETH WARTTIG	147 FIR ST	✓
10.	Celine Chudow	1475 FIR ST	✓
11.	CHUCK BRYANT	1475 FIR ST	V4B-4B5
12.	Edgar DAVIS	1455 FIA ST	V4B 4B5
13.	PORAN POKOTOWSKI	1448 FIR ST	V4B 4B4
14.	Reg Nash	1481 Merklin ST.	V4B 4C4
15.	Inés Quiroga	1475 Fir ST.	V4B 4B5
16.	M.R. Becker	1475 Fir. #105	"

PUBLIC INFORMATION MEETING
 1485 FIR SREET
 REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009
 DECEMBER 12, 2019

THIRDSPACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
17.	ISABEL STORAR	1448 FIR ST	V4B 4B4
18.	EMILIE BELL	1497 MERKLIN ST	V4B 4C4
19.	EMILIE BELL	1493 Myrtle St	
20.	Ed Brennan	1527 SCOTT	
21.	Liz Doucette	#1544 Fir Street	V4B 4B7
22.	Ray Doucette	1544 KIR STREET	V4B 4B7
23.	Anthony Manning	WR City Hall	
24.	IAN MIDDLETON		
25.	Phil Childers	Delta	V4M 2K7
26.	Fran MacLennan	White Rock	V4B 4B4
27.	Bob Dandson	Vancouver	V6J 2A6
28.	NELSON de AMARAL	Richmond	V7A 1G7
29.	KEN GUTHRIE	1521 GEORGE	V4B 4A4
30.			
31.			
32.			

PUBLIC INFORMATION MEETING
1485 FIR SREET
REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009
DECEMBER 12, 2019

THIRDSPACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
33.	Barton Jessup	302-1544 Fir St.	V4B4B7
34.	SAM DRAND	14835 MARINE DR.	V4B1C1
35.	MOTI BALI	14933 Beachview Ave	V4B1P2
36.	SHERRY SOOLE	1491 MERKIN ST.	V4B9C4
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PUBLIC INFORMATION MEETING FEEDBACK FORM

Rezoning & Major Development Permit Proposal

Application No. 19-009 – 1485 Fir Street

5:30 PM to 7:00 PM, December 12, 2019

Please note that your completed feedback form will be disclosed to the public and presented to Mayor and Council as part of the information package attached to this development proposal application. Any personal information or commentary you provide on this document will form part of the public record.

Please provide your name and address below: (optional)

Name:

G. WARTTIG

Address:

1475 FIR ST.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

TOTAL FARCE .

RIDICULOUS .

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

PUBLIC INFORMATION MEETING FEEDBACK FORM

Rezoning & Major Development Permit Proposal

Application No. 19-009 – 1485 Fir Street

5:30 PM to 7:00 PM, December 12, 2019

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Please provide your name and address below: (optional)

Name:

Moti Bali

Address:

14933 Beachview Ave. White Rock

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I highly recommend the building. In White Rock we have over 60% old age personnel who have difficulty climbing steps. This will be the first building in over 30 years to have elevators to help the elderly and help people have affordable housing facility.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

ELIZABETH WARTTIC

Address:

1475 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

NOT ALLOWED TO SPEAK

COMPLETE FARCE

WE ARE NOT ALLOWED TO SPEAK

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

M. R. Becker

Address:

#105 - 1475 Fir St.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

The building/property owner has an ambition to redevelop his property; it would be 'daffy' of me to oppose his wishes. He has assured me that all obligations placed on him by the province & the City of White Rock will be observed. M. Becker

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name: ELAINE BELL

Address: 1497 MERKLIN

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

I AM WORRIED ABOUT SO MUCH MORE TRAFFIC IN OUR AREA WITH TWO STORIES OF UNDERGROUND PARKING + 6 STORIES. HAD THE BUILDING BEEN 3/4 STORIES, I PROBABLY WOULD HAVE SUPPORTED THE PROPOSAL.

HAVING MORE RENTAL PLACES IS IMPORTANT BUT WHITE ROCK ~~IS~~ HAS BEEN BURDENED WITH CONSTRUCTION SITES FOR SEVERAL YEARS NOW. PRESENTLY IT'S NOT THAT LIVEABLE

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

DORSEN POROTOWSKI

Address:

#313-1448 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I UNDERSTAND ZONINGS WILL BE CHANGED TO ALLOW THIS BUILDING TO GO AHEAD. WILL THAT EFFECT ALL OLDER BUILDINGS IN A CLOSE PROXIMITY?? IF SO I AM OPPOSED. I ~~HE~~ LIVE ON FIR ST ACROSS FROM PROPOSED BUILDING. I UNDERSTOOD THIS MEETING WOULD ALLOW QUESTIONS. A WASTE OF TIME

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Reg Nash

Address:

1491 Mendelin St. White Rock V4B 4C4

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I oppose the proposal, as it is directly inline with my sunsets and I think it will invade my privacy.

No thank you to this project, at the proposed height.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

LILLIAN KING

Address:

102-1475 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

Mahdi is REPRESENTATIVE for what Company why ALL THE SECRETS WE SHOULD PROTECT LOW RENTAL HOUSING NOT KNOCK THEM DOWN SO SOME UNKNOWN CO. CAN BUILD APT TO MAKE MONEY. I THINK THIS LOOKING IN TO FIND OUT WHO THESE PEOPLE ARE

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

SHERRY SOOLE

Address:

1491 MERKLIN ST.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- NOT ENOUGH GREEN SPACE (NO TREES)
- TOO HIGH BLOCKS SKY & SUN !!!
- TOO MUCH MORE TRAFFIC CONGESTION
- UGLY DESIGN - AGGRESSIVE FRONT THAT DOESN'T BLEND WITH THE NEIGHBOURHOOD.
- UNENVIRONMENTAL TO WASTE A SUBSTANTIAL BUILDING - & SEND ALL THE ^{QUALITY} MATERIAL TO THE JOM
- WHY DO THE RULES COMMUNITY PLAN KEEP CHANGING?

Thank you for your participation. If you have any questions, please contact the following:

- MY HOME WILL BE DEVALUED AS IT IS SUBMERGED INTO DARKNESS!

I want to contact the CITY ...

I want to contact the APPLICANT ...

Athena von Hausen
Planner, City of White Rock
Tel: (604)-541-2159
Email: avonhausen@whiterockcity.ca

Robert Billard
Billard Architecture Inc.
Tel: (604)-619-0529
Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

CHUCK BRYANT

Address:

202-1475 Fir St

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I UNDERSTAND WHY NEW RENTAL UNITS ARE NECESSARY. THE PROBLEM ~~IS~~ IS THAT THE RENT WILL BE TOO HIGH. BEING ON A FIXED INCOME I CAN'T AFFORD IT UNLESS SOME OF THE UNITS ARE SUBSIDIZED

Thank-you.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

McCallum Michelle ~

Address:

1544 Fir

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

if more rentals needed, if this site may be controversial, some would be less = sites like 1544 Fir would be more suitable - Maybe with an 8 Storey, in the middle of complexes and only 21 owners to deal with, who can relocate easily with the price paid per unit - No one would complain - Owners are ready for offers - if interested, or another investor, contact FIONA at

Thank you for your participation. If you have any questions, please contact the following: 604 831-0099

I want to contact the CITY ...

Athena von Hausen
Planner, City of White Rock
Tel: (604)-541-2159
Email: avonhausen@whiterockcity.ca

I want to contact the APPLICANT ...

Robert Billard
Billard Architecture Inc.
Tel: (604)-619-0529
Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Address:

R. Rodde
1493 Meeklin
W.R.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I'm very concerned.
I feel the Building is too high,
The design doesn't contribute to our
Town's Homeless!
I also feel it is too high!
I hope it isn't Built -

I wish you'd move your ideas
to a different City -

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name: ISABEL STEURER

Address: 304-1448 Fir St. 604 789 8600

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- ① This development doesn't represent a gain of 55 apartments, but the loss of 25 affordable ones, and I have a hard time supporting that.
- ② I would really like council to consider the impact of construction on the immediate neighbourhood, not only in terms of parking, but also noise & emissions (i.e. heavy equipment idling). We have already put up with a lot uptown White Rock in the last couple of years. Please consider existing residents while welcoming new ones.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Liz Doucette

Address:

#106-1544 Fir Street
White Rock, BC V4B 4B7

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- There is no enough water if there is a fire to go around as if now.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

Concerns about more traffic congestion in area originally zoned residential

Concerns about adequate to serve a growing population in WRock in light of this proposed development

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

IAN MIDDLETON

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

THIS COMMUNITY DESPERATELY NEEDS
NEW RENTAL SUPPLY. THE CURRENT
SUPPLY IS OUT DATED AND DOES NOT
SUPPORT THE COMMUNITY.

THIS PROPOSAL IS MODEST IN SIZE AND
WILL HELP SUPPORT THE LACK OF SUPPLY
WE NEED TO REMEMBER THIS BUILDING
WILL BE HERE FOR 50+ YEARS WHICH ^{WILL} ~~WILL~~
HELP SUPPORT THE POPULATION GROWTH.

PLEASE APPROVE! WE NEED IT!

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

Barbara + Bjorn Holm

Address:

14728 Upper Reper Avenue.

What is your position on the development proposal application?

(Please circle your preferred response)

SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

We owned the White Birch - 1485 Fir Street and due to the high cost of running and maintaining the building had to sell. All tenants were told that it was sold as a development property & they all understood that. The costs - property taxes, insurance, heat, hot water, maintenance & repair require a higher density for it be to remotely viable. White Rock needs new

Thank you for your participation. If you have any questions, please contact the following:

Rental Buildings

I want to contact the CITY ...

Athena von Hausen
Planner, City of White Rock
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Email: avonhausen@whiterockcity.ca

I want to contact the APPLICANT ...

Robert Billard
Billard Architecture Inc.
Tel: (604)-619-0529
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Please provide your name and address below: (optional)

Name:

Barton Jessup

Address:

302-1544 Fir St.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I think higher density and corresponding more green space is a sound overall principle. I would like this project to be higher - for example an FAR of 4 to 5. If the lower mainland continues to have increased population (as anticipated), then housing towers are necessary to preserve parks and agricultural land.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Harold M. DIXON

Address:

1022 Pacific Pl. Delta

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

The project seems to be "right sized" with 80 suites, in a desirable location.

The mix of suites offered will allow for a variety of family configurations.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I support the project, I believe more rental housing is required in White Rock.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

JAN MAC LENNAN

Address:

13986 BLACKBURN AVE, WH, TO ROCK

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- MORE RENTAL HOUSING NEEDED.

- NEIGHBOURHOOD NEEDS UPDATING

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

PAT PETRACA

Address:

15020 N. BLUFF

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

How will this "Market" complex serve the needs of the CARLE Community of diverse income levels & comfortable homes. Cubicals/Urban micro squished spaces at inflated prices. The builder needs to work with Fed/CMHC + Provincial + NonProfits to enable inclusive better choices. Sterile Brutalists design is financially advantageous for builders profits yet and Zip to

Thank you for your participation. If you have any questions, please contact the following:

Community
Feedback

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

PUBLIC INFORMATION MEETING FEEDBACK FORM

Rezoning & Major Development Permit Proposal

Application No. 19-009 – 1485 Fir Street

5:30 PM to 7:00 PM, December 12, 2019

Please note that your completed feedback form will be disclosed to the public and presented to Mayor and Council as part of the information package attached to this development proposal application. Any personal information or commentary you provide on this document will form part of the public record.

Please provide your name and address below: (optional)

Name:

Pauline Paton

Address:

101-1475 FIR ST. WHITE ROCK, B.C.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I have lived at 1475 FIR ST.
FOR 9 YEARS.

I realize this building is aging
& I am happy to move into a
500 sq. ft. 1 bedroom. My current
rent is 1029.⁰⁰. Market value
is my concern. \$1200.⁰⁰ is my
max for rent as I am a senior
on a budget. I am not proposed
to a new building. East.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: GEORGE WARTTIG.

Address: 1475 FIR ST.

What is your position on the development proposal application?

(Please circle your preferred response)

~~I SUPPORT the proposal.~~

I am ~~UNDECIDED~~ on the proposal.

I ~~OPPOSE~~ the proposal.

I OPPOSE THE PROPOSAL

Please provide your comments in the box below:

WE UNDERSTOOD THIS 'PUBLIC' MEETING WAS FOR US TO HAVE OUR SAY ABOUT THE PROJECT. WE ARE LOSING OUR HOMES! I AM FRANKLY DISGUSTED.

Thank you for your participation. If you have any questions, please contact the following:

G. Warthi

I want to contact the CITY ... ✓	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

SHARAD DHAND

Address:

14835 MARINE DR.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

This is exactly the kind of development this city needs.

This is NOT a condo project - it is a RENTAL building. This is very much needed as all the current rental buildings are very old with no elevators, which is terrible for seniors as well as all others. I strongly support this project. It is very good for the future of White Rock.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Ines Quiroga

Address:

207 - 1475 - Fir St. White Rock.

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

This is an Open House - Gallery
Not a public meeting for discussion
& questions.

Thank you.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: rbillard@billardarchitecture.ca

From: [Carl Isaak](#)
To: [Athena von Hausen](#)
Subject: FW: 1075/1085 Fir Street, White Rock, BC
Date: Wednesday, May 22, 2019 2:24:00 PM

Ms. Brearley has emailed Carl J on several occasions and this email is the most recent and detailed correspondence from her regarding her concerns with redevelopment of the 1485 Fir Street (building addressed as 1475 Fir Street) property where she lives.

From: Elizabeth Brearley <elizabethbrearley@hotmail.com>
Sent: Sunday, May 19, 2019 10:45 AM
To: Darryl Walker <DWalker@whiterockcity.ca>; David Chesney <DChesney@whiterockcity.ca>; Helen Fathers <HFathers@whiterockcity.ca>; Erika Johanson <EJohanson@whiterockcity.ca>; Scott Kristjanson <SKristjanson@whiterockcity.ca>; Anthony Manning <AManning@whiterockcity.ca>; Christopher Trevelyan <CTrevelyan@whiterockcity.ca>; Carl Johannsen <CJohannsen@whiterockcity.ca>; Carl Isaak <CIsaak@whiterockcity.ca>
Subject: Re: 1075/1085 Fir Street, White Rock, BC

To His Worship the Mayor and White Rock City Councillors:

We, the tenants of the above property are aware that the owner of 1062822 BC Ltd. has submitted an OCP Amendment, a Zoning Bylaw Amendment and a Major Development Permit Application for the above property, which is a 25 unit rental property, not 24 as mentioned in the application. The mailing address is 1475, not 1485 as stated in the application. I understand this is an error on the part of City Hall.

We sent you a letter and a signed petition by the residents on February 11th 2019 stating our concerns and dismay at being evicted from our homes.

In the information given to us on May 14th 2019, by Mahdi Heidari on behalf of 1062822 BC Ltd., we would like to point out the following mis-information:

- The building has 25 suites not 24
- It has a state of the art heating system
- All windows were replaced with double-glazed high quality windows
- Blinds have been replaced
- New carpets have been installed in all suites
- Light fixtures and electrical outlets have been replaced in all suites

- WiFi is installed in the building for tenants use
- Telus installed optic fibre throughout the building last year

This building is not derelict and has been well maintained by the previous owners over the years.

Many of the tenants have lived here for over 20 years and are in their seventies and eighties. No one is on welfare. We are a very quiet and respectable community. There are no drugs or smoking in the building. There are 4 suites on the 3rd floor and they are occupied by young working adults. Rents are between \$800 and \$1150 per month. Since 1062822 BC Ltd. took over the building in November 2018, 1 tenant has died and 2 have moved into care homes. These suites have been re-rented at \$1,100 and \$1,300 per month. The new owners appear to be letting the building slide into disrepair. We are determined not to let this happen, so now we, the tenants, are now maintaining the building, cleaning the hallways, laundry room and cutting the grass at no cost to the owners. How can these owners morally do this to us? We are happy community that look out for each other. We are all stressed to the max with this hanging over our heads. Where will we go? As you know, market rents are astronomical and not affordable by this community.

Please do not let these greedy developers, who are not familiar with the area, throw us out of our homes. We do not know who they are. We do not know if the money is coming from off-shore and we do not know if the profits will be sent offshore!! They are hiding behind a numbered company.

We hope this information will help you in determining your consideration at the Land Use and Planning Committee.

We invite you all to come and visit the building to see for yourselves what a great community we have here. We will be happy to show you around.

With much respect and best regards,

Elizabeth Brearley-Warttig (on behalf of the tenants of 1475 Fir Street, White Rock, BC)

Tel: 778-294-0647

April 3, 2019

City of White Rock
15322 Buena Vista Avenue
White Rock, BC
V4B 1Y6

Attn: Carl Johannsen,
Director of Planning and Development

Dear Sir:

Re: 1475 Fir Street, White Rock, BC

Our building was sold to a Vancouver developer last fall. It is our understanding that the developer is planning to tear down the building. My husband and I are the caretakers of this rental building and we, along with the rest of the tenants, are concerned for the welfare of the tenants. This building is solid and well maintained. It is not a "slum". Most of the tenants are elderly, with low incomes. We are a close community that takes care of each other.

We are well aware of your Policy No. 514 (Tenant Relocation).

Why tear down a perfectly good building, for some greedy Vancouver developer?

Other properties that are being developed within the community are on land that was not occupied by residents of White Rock and no one lost their homes.

I enclose a letter that I have written to the Mayor and Council, signed by all the residents. Two councillors have responded.

We are prepared to do whatever it takes to keep our homes.

Best Regards,
Elizabeth Warttig - Suite 104 Tel: 778-294-0647 elizabethbreatley@hotmail.com

Residents of 1475 Fir Street, White Rock, BC V4B 4B5			
Suite No.	Name	Signature	Telephone
101	Pauline Paton	<i>Pauline Paton (70)</i>	604-990-0320
102	Lillian King (88)	<i>Lillian King</i>	
103	Peggy Best	<i>Peggy Best</i>	604-379-7997
104	George Warttig (68)	<i>George Warttig</i>	778 968 1947
104	Elizabeth Warttig	<i>E. Warttig (72)</i>	778-294-0647
105	Maria Tajaro (77)	<i>Mariano Tajaro</i>	
106	Mike Becker	<i>M. Becker</i>	604.791.2537
107	Sadie Hadley	<i>Sadie Hadley (87)</i>	604-536-8559.
108	Judy Belanger	<i>J. Belanger</i>	604-536-3342
109	Dan Jarvis	<i>Dan Jarvis</i>	
109	Sharon Jones	<i>Sharon Jones</i>	
201	Tony Brugger	<i>T. Brugger</i>	604-531-8490
201	Mrs. Brugger	<i>A. Brugger</i>	"
202	Charles Bryant	<i>Charles Bryant</i>	604-535-4634
203	Julia Rachev	<i>J. Rachev</i>	604-733-3349
204	Elena Udot (65)	<i>E. Udot</i>	604 778 - 545 8024
205	Chris McRae	<i>Chris McRae</i>	778-835-9699
206	Cindy Olynyk	<i>Cindy Olynyk</i>	604-536-9880
207	Leona Burnell	<i>ALZEMER</i>	
208	Sharon Wallace		
209	Jordon Wall Cameron	<i>Jordon Cameron</i>	604-836-7332
210	B. Intile		
211	Celine Chidlow	<i>Celine Chidlow (85)</i>	604-536-4749
212	Jillian Andrews	<i>Jillian Andrews</i>	604-536-0886
301	Maria Simpson	<i>Maria Simpson</i>	778-552-3926
302	Ken Romaniuk	<i>Ken Romaniuk</i>	604-626-7976
303	Neil McEathron (86)	<i>N.E. McEathron</i>	778 232 5976
304	Don Mooney	<i>Don Mooney</i>	604 771-1796

From: [Sadie Hadley](#)
To: [Planning](#)
Subject: tenant
Date: Tuesday, May 21, 2019 3:31:59 PM

I am a tenant at 1485 Fir st. in White Rock which has been bought by developers. I have lived here over 11 years and am 87 yrs. old because an development permit application has been made I have put my name in a senior residence with a 6-12 month waiting list. My question is if a space comes available am I still eligible for the Tenant Relocation package or is it only after the developers have City approval and permits are approved. Thanks Sadie

From: [Maret Erickson](#)
To: [Athena von Hausen](#)
Subject: 1485 Fir Street, White Rock, B.C.
Date: Friday, December 13, 2019 1:46:43 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Athena

I was unable to attend the public information meeting held on December 12, 2019. This is to advise that I support the project planned for 1485 Fir Street, White Rock, B.C.

Maret Erickson

From: [Elizabeth Brearley](#)
To: [Athena von Hausen](#)
Subject: Public Information Meeting December 12, 2019
Date: Friday, December 13, 2019 11:12:34 AM
Attachments: [Public Meeting Dec-12-2019.odt](#)
[IMG 20191213 0001.pdf](#)
[Letter to Johanssen Apr-3-2019.odt](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Von Hausen:

My name is Elizabeth Warttig and my husband and I attended the above meeting last evening. We mistakenly thought that it was a meeting that we could speak to. Many of the residents of 1475 Fir Street did not attend because they intended that I speak for them. Therefore I am attaching the letters and petition that was signed by all the residents last February. Your department may already have a copy. The Mayor and Council received a copy of this petition and letter also in February.

I did not see you at the meeting, or I would have given you the letters and the petition at that time. We are very opposed to this development. My comments to the planning department on behalf of the tenants of 1475 Fir Street, are below.

Regards, Elizabeth

My name is Elizabeth Warttig and I am representing the residents of 1475 Fir Street, who signed a petition in February of this year to protest the eviction of 30 people from this building. It was presented to the White Rock City council, along with a letter and forwarded to the planning department. We live in sound affordable housing. This building is not derelict and we respectfully ask that you do allow this unknown developer to evict us and tear down a perfectly sound building. I have researched the rental market in the White Rock South Surrey area for a year now and the average rents are much higher than those we are paying now. In fact they have increased considerably in November. The average for 1 bedroom is now \$1,500 and for a 2 bedroom the average is \$2,200. Burnaby has recently passed a revised residents assistance policy, which asks the developer to top up rents for tenants that

have been evicted and allow them back to the new building at the same cost of the rents that they are currently paying. I would suggest that the City of White Rock make that amendment to their policy.

We are a close knit community who look after each other. Many are elderly tenants who have lived there for 20 years. Some tenants are struggling young people on minimum wages. We cannot afford the so called market rents. Some of us have been subject to harassment and intimidation by the representative for our building, which has been reported to the Residential Tenancy Branch. It has been very traumatic and upsetting for the tenants. 17 new highrises have been slated for construction in White Rock. Those that have already been completed are struggling to fill them.

Everyone is stressed by the uncertainty of where they will go. As you are aware there is no affordable housing in White Rock and I urge you to consider this and do not have us evicted from our homes.

Thank you.

From: [Barbara Holm](#)
To: [Athena von Hausen](#)
Subject: Development Project 1485 Fir Street
Date: Sunday, December 15, 2019 3:14:09 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Athena,

Thank you for listening at the information meeting December 12th.

As I outlined to you White Rock will soon be facing significant problems due to the ageing infrastructure of virtually all the rental buildings that date primarily from the 60's. They are expensive to run and maintain - lacking modern energy efficient building envelopes, windows, pipes and wiring that are expensive to replace and also to maintain. The small number of apartments relative to the size of the lots they are built on make the property taxes and other fixed costs, heat, water, gas, recycling, garbage quite exorbitant even before the high cost of maintenance is added. Just maintaining heat in suites in winter with 60 year old equipment (despite new boilers etc) had become a major, very time consuming, challenge - the great majority of plumbing contractors are not even familiar with these issues - even if OEM parts are available - which frequently they are not meaning that it is necessary to substitute with after market parts. These areas of concern will grow exponentially with ageing.

There are many significant advantages to dedicated rental buildings in comparison to condo's where tenants are at the mercy of individual owners and tend to be shorter term in most case.

I was concerned about some of the misconceptions and misunderstandings that many of the tenants from the building had. From money laundering and shady accounts to a total lack of trust and understanding of the commitments required by the developer in order for him to attempt to redevelop the site was misinformed and rather scary. We explained to all tenants in the building that we could no longer manage to deal with the maintenance ourselves and it would have been too expensive to hire a management company with the already high overhead costs and this would have been reflected in much lower maintenance levels.

I do not want to just ramble here so please contact me if you, Carl, or anyone else have any questions.

My husband and I feel that a dedicated rental building on such a convenient location would be a tremendous asset to the community and a huge benefit to tenants who would have the convenience of a safer modern building with all that new technology has to offer.

Kind Regards,
Barbara Holm
604 535 3585

From: [Mahmoud Mahmoud](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca; [MobileMe](#)
Subject: Support for Proposed Rental Development Project: 1485 Fir St, White Rock
Date: Monday, December 16, 2019 5:55:23 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Attention: Athena von Hausen

Dear Ms. von Hausen,

I am writing in support of the above-referenced Rental Development project that is being proposed by C2C Construction. I understand that you will accept email communications from those who were unable to attend the Public Meeting that was held on the project last Thursday.

As someone who visits White Rock frequently, I understand that the City of White Rock has close to zero rental vacancy. Therefore, I am in support of any development which promotes rental accommodation for those who may wish to live in your beautiful municipality, but cannot afford to buy there. To that end, it seems very obvious that any developer that wishes to build rental accommodation in White Rock should be supported by the municipality.

I understand that the proposed project at 1485 Fir Street will be replacing an almost 60 year-old building into a brand new 80-unit fully rental property. I also understand that most of the current rental buildings in White Rock are old and poorly maintained, with no elevator, ramp or other amenities which are needed by the community's senior citizens. Given the location of the site to White Rock's town centre, it escapes me as to what barriers the municipality sees in approving this project. In the circumstances, I respectfully ask that the city support this fully rental development to address the current rental shortage in White Rock.

I'm copying the project architect, Mr. Robert Billard, so that he is aware of the support that this member of the public wishes to lend this worthy rental development project.

Yours truly,

Mahmoud Mahmoud, PhD, FEC
mahmoudmahmoud@icloud.com

From: [Michelle Guy](#)
To: [Athena von Hausen](#)
Subject: 1485 Fir Street
Date: Monday, December 16, 2019 8:29:05 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear City of White Rock,

I write to express my support of this project. I was not able to attend the information meeting with council due to other commitments but thought I would send in my two cents, for what it is worth.

White Rock is a beautiful area and my mother, retired, very much would like to live there. She has a decent pension but does not have the asset base that would allow her to buy. As a result she rents and likely always will. That does not limit her ability to contribute to the local economy of coffee shops and local artists.

We have looked for a place for her and have only found either dated rental housing or basement suites. I was excited to see a proposal that might well meet her long term needs.

I do hope that City Council will see the wisdom in encouraging modern new builds for middle income people who are not able or interested to buy homes but would still prove to be valuable and desirable members of your community.

Michelle

From: [Parastoo Shirazi](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir St white Rock
Date: Monday, December 16, 2019 6:15:53 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Avon Hausen,

Regrettably I was not able to attend the public information meeting for 1485 Fir St project in White Rock, yet I would like to express my support for the below reasons:

Most rental buildings in White Rock are older developments. The current building is 60 years old, it is of great timing to have the old building replaced with a new rental building.

White Rock would benefit from attracting younger population, to sustain and attract the younger population, newer rental buildings with such unique designs would offer attractive housing solutions for the younger population who are yet unable to afford to purchase properties.

White Rock's beautiful demographics could be supplemented by par rental buildings attracting great dynamics to the community.

Thanks

Parastoo Shirazi

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From: [Teresa Leung](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir Street, Whitebirch Apartments
Date: Monday, December 16, 2019 10:47:51 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Athena (City of White Rock),

I am in support of the new rental development, Whitebirch at 1485 Fir Street to address the current rental shortage in White Rock. There are many people who cannot purchase, who NEED to rent, therefore, this project will greatly help those individuals, since the current vacancy rate is almost zero at this time. Please do what you can to push this project forward.

I can be reached at 604-618-2128 should you have any questions.

Best wishes,

Teresa Leung

This communication, including attachments, is confidential, may be subject to legal privileges, and is intended for the sole use of the addressee. Any use, duplication, disclosure or dissemination of this communication, other than by the addressee, is prohibited. If you have received this communication in error, please notify the sender immediately and delete or destroy this communication and all copies. Thank you.

From: [Farid Kazemzadeh](#)
To: [Athena von Hausen](#)
Cc: [LEED AP](#)
Subject: Rental Building Proposed
Date: Tuesday, December 17, 2019 4:04:12 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Athena,

I wanted to take this time to show my support for the rental building proposed at 1485 Fir Street. Firstly, the building proposed fits well within the neighbourhood given its design. But even more important, the City has very little rental opportunities which make it very hard to find a suitable place to live. We need newer rental buildings for both younger families and the baby boomer generation.

I hope the City makes the right decision to approve this proposed development and more rental buildings in the future.

Best regards,
Farid K

From: [Yolande Levasseur](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: Proposed building project at 1485 Fir street White Rock
Date: Thursday, December 19, 2019 5:42:02 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern,

I am writing to you to indicate my support of a rental building project awaiting city approval. The address of this project is 1485 Fir Street in White Rock.

I'm a 69 year old woman and, although I'm in excellent health right now, I can see the writing on the wall when I will not have the energy or the ability to trek up four flights of steps. The design of this proposed rental property has all the amenities I would want in the near future and, furthermore, has a lot of style without being ostentatious. I believe it would fit very well in a neighbourhood that is progressive, offering sound housing accommodations in a classy-looking building.

Respectfully,
Yolande Levasseur

Sent from my iPad

From: [Derek Townsend](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir St white Rock Support
Date: Tuesday, December 17, 2019 11:35:53 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Athena,

I am email to express my support for the rental apartment development at 1485 Fir Street. I think new rental supply is key for our unaffordable market and any supply is great as we have had such a lack over the years.

The design is smart and well thought out and the only comment I would add is that it should be twice as big with twice as many units.

--

Derek Townsend
604.812.8312
dwtownsend@gmail.com

Public Feedback Summary Report



DEVELOPMENT APPLICATION FILE NO. 19-009
RE: 1485 FIR STREET



PREPARED BY BILLARD ARCHITECTURE INC.
JANUARY 8, 2020



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Overview

A Public Information Meeting was hosted by Billard Architecture at ThirdSpace Community Café, Unit #1 - 1381 George Street from 5:30 PM to 7:00 PM on Thursday, December 12, 2019 to discuss a development proposal application located at 1485 Fir Street, White Rock, BC. The purpose of this Public Information Meeting was to provide surrounding residents and business owners with an opportunity to provide their feedback on the proposal.

A Zoning Amendment and a Major Development Permit application have been submitted to allow for the construction of a six-storey multi-unit rental residential building over two levels of underground parking. The property is an existing rental building and the development would be subject to Council's Tenant Relocation Policy. The proposed use, height, and density is consistent with the Town Centre Transition Land Use Designation in the Official Community Plan.

Physical feedback forms and feedback letters sent by email were collected by the City of White Rock. Original feedback comments are presented as submitted, and have not been edited for spelling, grammar, or accuracy.

Total responses for 1485 Fir Street Feedback submitted:

Paper Feedback Form: **25** Feedback by Email: **11**

Responses:

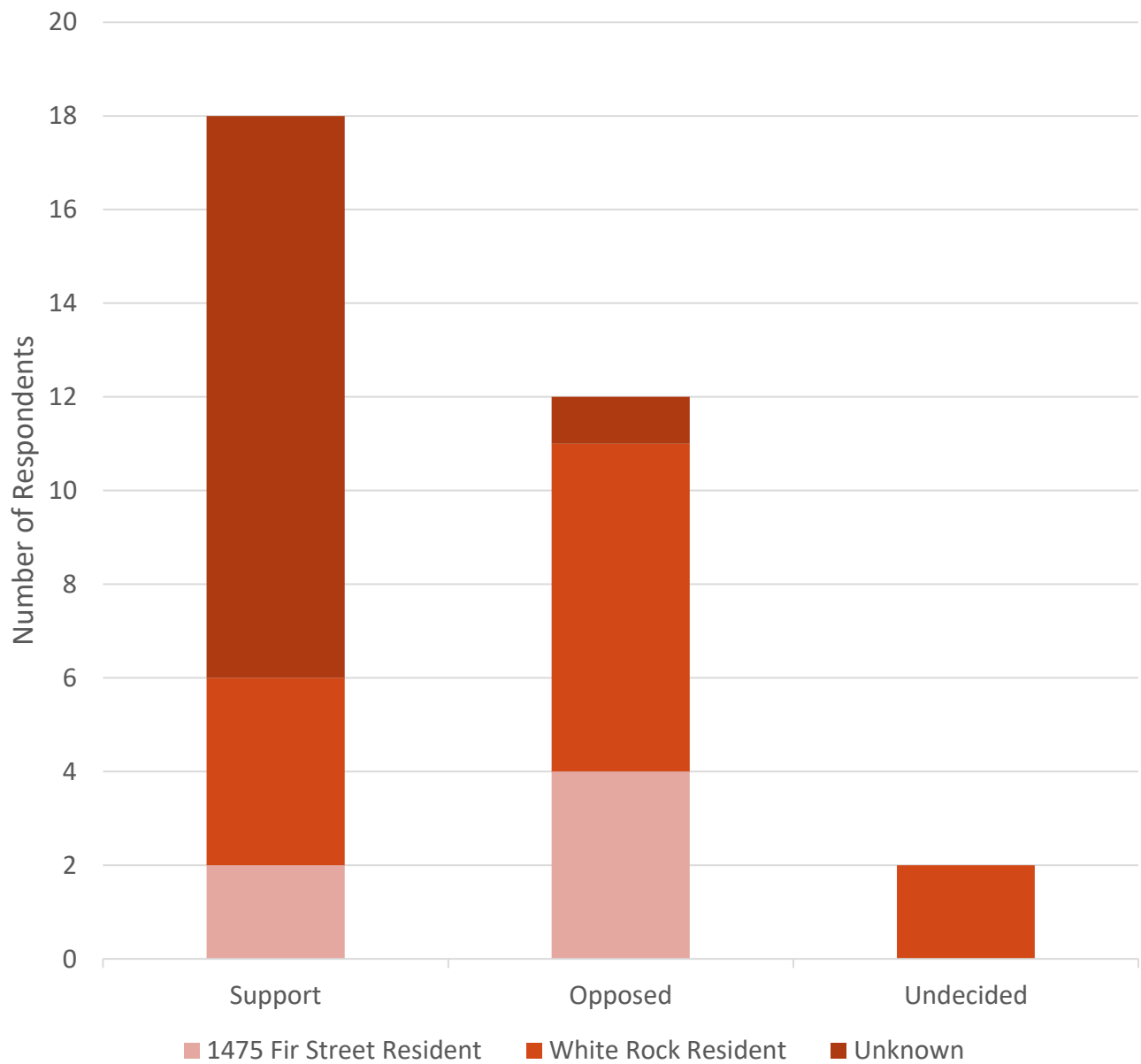
In favour of Proposal: **19**

Opposed to Proposal: **12** – multiple responses from two individuals counted as single response for each

Undecided of Proposal: **2**

Unrelated to Proposal: **2**

Feedback Data



Public Feedback Comments & Billard Architecture Inc. Responses

Support	Public Feedback Comments
1	The building/property owner has ambition to redevelop his property, it would be 'daffy' of me to oppose his wishes. He has assured me that all obligations placed on him by the province and the City of White Rock will be observed.
2	I highly recommend the building. In White Rock we have over 10% old age personnel who have difficulty climbing steps. This will be the first building in over 30 years to have elevators to help the elderly and help people have affordable housing finally.
3	This is exactly the kind of development this city needs. This is NOT a condo project - it is a RENTAL building. This is very much needed as all the current rental buildings are very old with no elevators, which is terrible for seniors as well as all others. I strongly support this project. It is very good for the future of White Rock.
4	I have lived at 1475 Fir Street for 9 years. I realize this building is aging and I am happy to move into a 500 sq ft, 1 bedroom. My current rent is \$1,029.00 Market value is my concern, \$1,200.00 is my max for rent as I am a senior on a budget. I am not proposed to a new building. East.
5	More rental housing is needed. Neighbourhood needs updating.
6	I support the project. I believe more rental housing is required in White Rock.
7	This project seems to be "right sized" with 80 suites, in a desirable location. The mix of suites offered will allow for a variety of family configurations.
8	I think higher density and corresponding more green spaces is a sound overall principle. I would like this project to be higher - for example an FAR of 4 to 5. If the lower mainland continues to have increased population (as anticipated) then housing towers are necessary to preserve parks and agricultural land.
9	We owned the White Birch - 1485 Fir Street and due to the high cost of running and maintaining the building had to sell. All tenants were told that it was as a development property and they all understood that. The costs - property taxes, insurance, heat, hot water, maintenance and repair require a higher density for it to be remotely viable. White Rock needs new rental buildings.

Support	Public Feedback Comments
10	<p>This community desperately needs new rental supply. The current supply is outdated and does not support the community. This proposal is modest in size and will help support the lack of supply. We need to remember this building will be here for 50+ years which will help support the population growth. Please approve! We need it!</p>
11	<p>Hello Athena (City of White Rock),</p> <p>I am in support of the new rental development, Whitebirch at 1485 Fir Street to address the current rental shortage in White Rock. There are many people who cannot purchase, who NEED to rent, therefore, this project will greatly help those individuals, since the current vacancy rate is almost zero at this time. Please do what you can to push this project forward.</p> <p>I can be reached at (redacted) should you have any questions.</p> <p>Best wishes, (redacted)</p>
12	<p>Dear Avon Hausen,</p> <p>Regrettably I was not able to attend the public information meeting for 1485 Fir St project in White Rock, yet I would like to express my support for the below reasons: Most rental buildings in White Rock are older developments. The current building is 60 years old, it is of great timing to have the old building replaced with a new rental building.</p> <p>White Rock would benefit from attracting younger population, to sustain and attract the younger population, newer rental buildings with such unique designs would offer attractive housing solutions for the younger population who are yet unable to afford to purchase properties.</p> <p>White Rock's beautiful demographics could be supplemented by par rental buildings attracting great dynamics to the community.</p> <p>Thanks</p>

Support	Public Feedback Comments
13	<p>Dear Ms. von Hausen,</p> <p>I am writing in support of the above-referenced Rental Development project that is being proposed by C2C Construction. I understand that you will accept email communications from those who were unable to attend the Public Meeting that was held on the project last Thursday.</p> <p>As someone who visits White Rock frequently, I understand that the City of White Rock has close to zero rental vacancy. Therefore, I am in support of any development which promotes rental accommodation for those who may wish to live in your beautiful municipality, but cannot afford to buy there. To that end, it seems very obvious that any developer that wishes to build rental accommodation in White Rock should be supported by the municipality.</p> <p>I understand that the proposed project at 1485 Fir Street will be replacing an almost 60 year-old building into a brand new 80-unit fully rental property. I also understand that most of the current rental buildings in White Rock are old and poorly maintained, with no elevator, ramp or other amenities which are needed by the community's senior citizens. Given the location of the site to White Rock's town centre, it escapes me as to what barriers the municipality sees in approving this project. In the circumstances, I respectfully ask that the city support this fully rental development to address the current rental shortage in White Rock.</p> <p>I'm copying the project architect, Mr. Robert Billard, so that he is aware of the support that this member of the public wishes to lend this worthy rental development project.</p> <p>Yours truly, (redacted)</p>
14	<p>Hello Athena,</p> <p>I am email to express my support for the rental apartment development at 1485 Fir Street. I think new rental supply is key for our unaffordable market and any supply is great as we have had such a lack over the years.</p> <p>The design is smart and well thought out and the only comment I would add is that it should be twice as big with twice as many units.</p>

Support	Public Feedback Comments
15	<p>Hi Athena,</p> <p>I wanted to take this time to show my support for the rental building proposed at 1485 Fir Street. Firstly, the building proposed fits well within the neighbourhood given its design. But even more important, the City has very little rental opportunities which make it very hard to find a suitable place to live. We need newer rental buildings for both younger families and the baby boomer generation.</p> <p>I hope the City makes the right decision to approve this proposed development and more rental buildings in the future.</p> <p>Best regards, (redacted)</p>
16	<p>To whom it may concern,</p> <p>I am writing to you to indicate my support of a rental building project awaiting city approval. The address of this project is 1485 Fir Street in White Rock.</p> <p>I'm a 69 year old woman and, although I'm in excellent health right now, I can see the writing on the wall when I will not have the energy or the ability to trek up four flights of steps. The design of this proposed rental property has all the amenities I would want in the near future and, furthermore, has a lot of style without being ostentatious. I believe it would fit very well in a neighbourhood that is progressive, offering sound housing accommodations in a classy-looking building.</p> <p>Respectfully, (redacted)</p>
17	<p>Athena,</p> <p>I was unable to attend the public information meeting held on December 12, 2019. This is to advise that I support the project planned for 1485 Fir Street, White Rock, B.C.</p>
18	<p>Dear City of White Rock,</p> <p>I write to express my support of this project. I was not able to attend the information meeting with council due to other commitments but thought I would send in my two cents, for what it is worth. White Rock is a beautiful area and my mother, retired, very much would like to live there. She has a decent pension but does not have the asset base that would allow her to buy. As a result she rents and likely always will. That does not limit her ability to contribute to the local economy of coffee shops and local artists. We have looked for a place for her and have only found either dated rental housing or basement suites. I was excited to see a proposal that might well meet her long term needs. I do hope that City Council will see the wisdom in encouraging modern new builds for middle income people who are not able or interested to buy homes but would still prove to be valuable and desirable members of your community.</p>

Support	Public Feedback Comments
19	<p>Athena,</p> <p>Thank you for listening at the information meeting December 12th. As I outlined to you White Rock will soon be facing significant problems due to the ageing infrastructure of virtually all the rental buildings that date primarily from the 60's.</p> <p>They are expensive to run and maintain - lacking modern energy efficient building envelopes, windows, pipes and wiring that are expensive to replace and also to maintain. The small number of apartments relative to the size of the lots they are built on make the property taxes and other fixed costs, heat, water, gas, recycling, garbage quite exorbitant even before the high cost of maintenance is added.</p> <p>Just maintaining heat in suites in winter with 60 year old equipment(despite new boilers etc)had become a major, very time consuming, challenge - the great majority of plumbing contractors are not even familiar with these issues - even if OEM parts are available - which frequently they are not meaning that it is necessary to substitute with after market parts. These areas of concern will grow exponentially with ageing.</p> <p>There are many significant advantages to dedicated rental buildings in comparison to condo's where tenants are at the mercy of individual owners and tend to be shorter term in most case.</p> <p>I was concerned about some of the misconceptions and misunderstandings that many of the tenants from the building had. From money laundering and shady accounts to a total lack of trust and understanding of the commitments required by the developer in order for him to attempt to redevelop the site was misinformed and rather scary. We explained to all tenants in the building that we could no longer manage to deal with the maintenance ourselves and it would have been too expensive to hire a management company with the already high overhead costs and this would have been reflected in much lower maintenance levels.</p> <p>I do not want to just ramble here so please contact me if you, Carl, or anyone else have any questions. My husband and I feel that a dedicated rental building on such a convenient location would be a tremendous asset to the community and a huge benefit to tenants who would have the convenience of a safer modern building with all that new technology has to offer.</p> <p>Kind Regards, (redacted)</p>

Opposed	Public Feedback Comments & Billard Architecture Inc Response
1	There is not enough water if there is a fire to go around as of now
	Response: Fire suppression systems, including sprinklers, will be in effect in all units. Water pressure should be addressed by the City of White Rock Engineering.
2	I oppose the proposal, as it is directly inline with my sunsets and I think it will invade my privacy. No thank you to this project, at the proposed height.
	Response: The designed height meets all zoning restrictions and bylaws set out by the City of White Rock.
3	I understand the zoning will be changed to allow this building to go ahead. Will that effect all older buildings in a close proximity? If so, I am opposed. I live on Fir Street across from proposed building. I understood this meeting would allow questions. A waste of time.
	Response: All new developments and redevelopments are subject to the current zoning and bylaws set out by the City of White Rock. The meeting provided opportunity for individuals to ask questions of the Architect, City Planner, and Developer as all three were present at the PIM.
4	I am worried about so much more traffic in our area with two stories of underground parking and 6 stories. Had the building been 3-4 stories I probably would have supported the proposal. Having more rental places is important but White Rock has been burdened with construction for several years now. Presently it's not that livable.
	Response: The amount of parking is mandated by the City of White Rock based on the number of units within the building. Underground parking reduces the number of vehicles parked on the street and enhances safety and security with well lit areas.
5	Not enough green space (no trees.) Too high blocks sky and sun. Too much more traffic congestion. Ugly design, aggressive front that doesn't blend in with the neighbourhood. Unevironmental to waste a substantial building and send all that quality material to the dump. Why do the community rules keep changing? My home will be devalued as it is submerged into darkness!
	Response: The proposed development provides more trees than are on the existing site and also has been revised in order to not impact the neighbouring trees. The proposed development will be substantially more energy efficient and uses materials with a higher level of recycled content than the existing building. The net result of this development will be significantly more sustainable than the existing building. The process of demolition is mandated to require as much of the existing building as possible to be sorted and recycled.

Opposed	Public Feedback Comments & Billard Architecture Inc Response
6	Concerns about more traffic congestion in an area originally zoned residential. Concerns about adequate to serve a growing population in White Rock in light of this proposed development.
	Response: The existing property's zoning does not change, it will still remain zoned for multifamily residential.
7a	Total farce. Ridiculous.
	Response:
7b	We understood this 'Public' meeting was for us to have our say about the project. We are loosing our homes! I am frankly disgusted.
	Response: The Notice of PIM was very clear that it would be an open house and not a public hearing. The Architect, City Planner, and Developer were present for questions and a feedback form was made available for comments and feedback, allowing for members of the public to voice their opinions.
8	Mahdi is representative for what company. Why all the secrets. We should protect low rental housing not knock them down so some unknown company can build apt to make money. I think this is looking in to find out who these people are.
	Response:
9	I understand why new rental units are necessary. The problem is that the rent will be too high. Being on a fixed income I cannot afford it unless some of the units are subsidized. Thank you.
	Response:
10	I'm very concerned. I feel the building is too high. The design doesn't contribute to our town's homeless! I also feel it is too high! I hope it isn't built. I wish you'd move your ideas to a different city.
	Response: The designed height meets all zoning restrictions and bylaws set out by the City of White Rock.

Opposed	Public Feedback Comments & Billard Architecture Inc Response
11a	Not allowed to speak. Complete farce. We are not allowed to speak.
	<p>Response: The Notice of PIM was very clear that it would be an open house and not a public hearing. The Architect, City Planner, and Developer were present for questions and a feedback form was available for comments and feedback, allowing for members of the public to voice their opinions.</p>
11b	<p>Dear Ms. Von Hausen: My name is (redacted) and my husband and I attended the above meeting last evening. We mistakenly thought it was a meeting that we could speak to. Many of the residents of 1475 Fir Street did not attend because they intended I speak for them. Therefore I am attaching the letters and petition that was signed by all the residents last February. Your department may already have a copy. The Mayor and Council recieved a copy of this petition at that time. We are very opposed to this development. My comments to the planning department on behalf of tenants of 1475 Fir Street are below.</p> <p>My name is (redacted) and I am representing the residents of 1475 Fir Street, who signed a petition in February of this year to protest the eviction of 30 people from this building. It was presented to the White Rock City Council, along with a letter and forwarded to the planning department. We live in sound affordable housing. This building is not derelict and we respectfully ask that you do allow this unknown developer to evict us and tear down a perfectly sound building. I have researched the rental market in the White Rock South Surrey area for a year now and the average rents are much higher than those we are paying now. In fact they have increased considerably in November. The average 1 bedroom is now \$1,500 and for a 2 bedroom the average is \$2,200. Burnaby has recently passed a revised residents assistance policy, which asks the developer to top up rents for tenants that have been evicted and allow them back to the new building at the same cost of the rents that they are currently paying. I would suggest that the City of White Rock make the amendment to their policy. We are a close knit community who look after each other. Many are elderly tenants who have lived there for 20 years. Some tenants are struggling young people on minimum wages. Some cannot afford the so called market rents. Some of us have been subject to harassment and intimidation by the representative for our building, which has been reported to the Residential Tenancy Branch. It has been very traumatic and upsetting for the tenants. 17 new highrises have been slated for construction in White Rock. Those that have already been completed are struggling to fill them. Everyone is stressed by the uncertainty of where they will go. As you are aware there is no affordable housing in White Rock and I urge you to consider this and do not have us evicted from our homes. Thank you.</p>
	<p>Response:</p>

Opposed	Public Feedback Comments & Billard Architecture Inc Response
<p>11c</p>	<p>Sent: Sunday, May 19, 2019 10:45 AM Subject: Re: 1075/1085 Fir Street, White Rock, BC To His Worship the Mayor and White Rock City Councillors: We, the tenants of the above property are aware that the owner of 1062822 BC Ltd. has submitted an OCP Amendment, a Zoning Bylaw Amendment and a Major Development Permit Application for the above property, which is a 25 unit rental property, not 24 as mentioned in the application. The mailing address is 1475, not 1485 as stated in the application. I understand this is an error on the part of City Hall. We sent you a letter and a signed petition by the residents on February 11th 2019 stating our concerns and dismay at being evicted from our homes. In the information given to us on May 14th 2019, by Mahdi Heidari on behalf of 1062822 BC Ltd., we would like to point out the following mis-information: The building has 25 suites not 24. It has a state of the art heating system. All windows were replaced with double-glazed high quality windows. Blinds have been replaced. New carpets have been installed in all suites Light fixtures and electrical outlets have been replaced in all suites. WiFi is installed in the building for tenants use. Telus installed optic fibre throughout the building last year. This building is not derelict and has been well maintained by the previous owners over the years. Many of the tenants have lived here for over 20 years and are in their seventies and eighties. No one is on welfare. We are a very quiet and respectable community. There are no drugs or smoking in the building. There are 4 suites on the 3rd floor and they are occupied by young working adults. Rents are between \$800 and \$1150 per month. Since 1062822 BC Ltd. took over the building in November 2018, 1 tenant has died and 2 have moved into care homes. These suites have been re-rented at \$1,100 and \$1,300 per month. The new owners appear to be letting the building slide into disrepair. We are determined not to let this happen, so now we, the tenants, are now maintaining the building, cleaning the hallways, laundry room and cutting the grass at no cost to the owners. How can these owners morally do this to us? We are happy community that look out for each other. We are all stressed to the max with this hanging over our heads. Where will we go? As you know, market rents are astronomical and not affordable by this community. Please do not let these greedy developers, who are not familiar with the area, throw us out of our homes. We do not know who they are. We do not know if the money is coming from off-shore and we do not know if the profits will be sent offshore!! They are hiding behind a numbered company. We hope this information will help you in determining your consideration at the Land Use and Planning Committee. We invite you all to come and visit the building to see for yourselves what a great community we have here. We will be happy to show you around. With much respect and best regards, (redacted) (on behalf of the tenants of 1475 Fir Street, White Rock, BC) Tel: (redacted)</p>
	<p>Response:</p>

Opposed	Public Feedback Comments & Billard Architecture Inc Response
<p>11d</p>	<p>April 3, 2019 City of White Rock 15322 Buena Vista Avenue White Rock, BC V4B 1Y6 Attn: Carl Johannsen, Director of Planning and Development Dear Sir: Re: 1475 Fir Street, White Rock, BC Our building was sold to a Vancouver developer last fall. It is our understanding that the developer is planning to tear down the building. My husband and I are the caretakers of this rental building and we, along with the rest of the tenants, are concerned for the welfare of the tenants. This building is solid and well maintained. It is not a "slum". Most of the tenants are elderly, with low incomes. We are a close community that takes care of each other. We are well aware of your Policy No. 514 (Tenant Relocation). Why tear down a perfectly good building, for some greedy Vancouver developer? Other properties that are being developed within the community are on land that was not occupied by residents of White Rock and no one lost their homes. I enclose a letter that I have written to the Mayor and Council, signed by all the residents. Two councillors have responded. We are prepared to do whatever it takes to keep our homes. Best Regards, (redacted)</p>
	<p>Response:</p>
<p>12</p>	<p>How will this "market" complex serve the needs of the caring community of diverse income levels and comfortable homes. Cubicles/urban micro squished spaces at inflated prices. The builder needs to work with Fed/CMHC and provincial non-profits to enable inclusive better choices. Sterile brutalistic design is financially advantageous for builders profits yet add zip to community feeling.</p>
	<p>Response: The homes designed within this project are larger than most market units being designed today. The initial design presented in February was larger and had more units. The revised design provided in July of 2019 is smaller, has fewer, yet larger, units. The design has also been dramatically revised to place the playground and courtyard facing Fir Street, significantly step back the fifth and sixth storeys and to introduce colour and West Coast elements such as wood. The design meets the design guidelines of the City of White Rock.</p>

Undecided	Public Feedback Comments & Billard Architecture Inc Response
1	<p>1.This development doesn't represent a gift of 55 apartments, but the loss of 25 affordable ones, and I have a hard time supporting that.</p> <p>2. I would really like council to consider the impact of construction on the immediate neighbourhood, not only in terms of parking, but also noise and emissions (ie heavy equipment idling.) We have already put up with a lot uptown White Rock on the last couple of years. Please consider existing residents while welcoming new ones.</p>
	Response:
2	<p>If more rentals needed, if this site may be controversial. Some would be less sites equal sites, like 1544 Fir would be more suitable. Maybe with 8 storey, in the middle of complexes and only 21 owners to deal with, who can easily relocate with the price paid per unit. No one would complain. Owners are ready for offers. If interested, or another investor, contact (redacted)</p>
	Response:

Other	General Questions & Comments from Public
1	<p>This is an open house - gallery. Not a public meeting for discussion and questions. Thank you.</p>
	<p>Response: The Notice of PIM was very clear that it would be an open house and not a public hearing. The Architect, City Planner, and Developer were present for questions and a feedback from was available for comments and feedback, allowing for members of the public to voice their opinions.</p>
2	<p>I am a tenant at 1485 Fir st. in White Rock which has been bought by developers. I have lived here over 11 years and am 87 yrs. old because an development permit application has been made I have put my name in a senior residence with a 6-12 month waiting list. My question is if a space comes available am I still eligible for the Tenant Relocation package or is it only after the developers have City approval and permits are approved. Thanks (redacted)</p>
	Response:

Appendices

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• PIM Raw Feedback	36 - 77

NOTICE OF PUBLIC INFORMATION MEETING

Development Application File No. 19-009
RE: 1485 Fir Street

PLEASE TAKE NOTICE THAT a Public Information Meeting will be hosted by **Billard Architecture** at ThirdSpace Community Café, Unit #1 - 1381 George Street from **5:30 PM to 7:00 PM on Thursday, December 12, 2019** to discuss a development proposal application located at 1485 Fir Street (please see Location Map on the other side of this page).

A Zoning Amendment and a Major Development Permit application have been submitted to allow for the construction of a six-storey multi-unit rental residential building over two levels of underground parking. The property is an existing rental building and the development would be subject to Council's Tenant Relocation Policy. The proposed use, height, and density is consistent with the Town Centre Transition Land Use Designation in the Official Community Plan.



Proposal Statistics (approximate)	
Dwelling Units	80
Parking Spaces	112 spaces
Height	18.9 metres (6 storeys)
Density (Gross Floor Area)	2.8 FAR (5707 m ²)

Rendering provided by applicant looking southwest at the corner of Fir Street and Russell Avenue

The purpose of this Public Information Meeting is to provide surrounding residents and business owners with an opportunity to provide their feedback on the proposal. This feedback will be used to identify concerns, address issues, and make improvements to the proposal early on in the application process.

The meeting is an open house format where City staff will be in attendance to introduce and monitor the meeting and to report back to Council.

For more information, please contact the City of White Rock at 604-541-2159 or avonhausen@whiterockcity.ca.

Planning and Development Services
P: 604.541.2136 | F: 604.541.2153
City of White Rock
15322 Buena Vista Avenue, White Rock BC, Canada V4B 1Y6

WHITE ROCK
My City by the Sea!
www.whiterockcity.ca

Public Information Meeting – Direct Mail Out

Location Map of Proposal



PROPOSAL SITE



This proposal for 1485 Fir Street is located near the Town Centre of White Rock with access to a variety of shops and restaurants.

Features:

- Modern, Spacious Rental Apartments
- Amenity Patio on Main Floor
- Patio Courtyard
- Underground Parking

NEIGHBOURHOOD CONTEXT



HEIGHT TRANSITION



Figure 10 Conceptual Height Transitions in the Town Centre, Town Centre Transition, and Lower Town Centre Areas

7



MASSING ANALYSIS

Policy 8.1.2
 Density and Height – Concentrate the highest heights and densities adjacent to the Town Centre along the North Bluff Road.

Maximum allowable densities (FAR) are outlined in Figure 9 and policy 8.1.3 and guidelines for height conditions are illustrated in Figure 10.

Policy 8.1.3
 Rental Housing – Allow a 40% increase to maximum FARs where at least half of this additional floor area is dedicated to and secured as residential rental units.

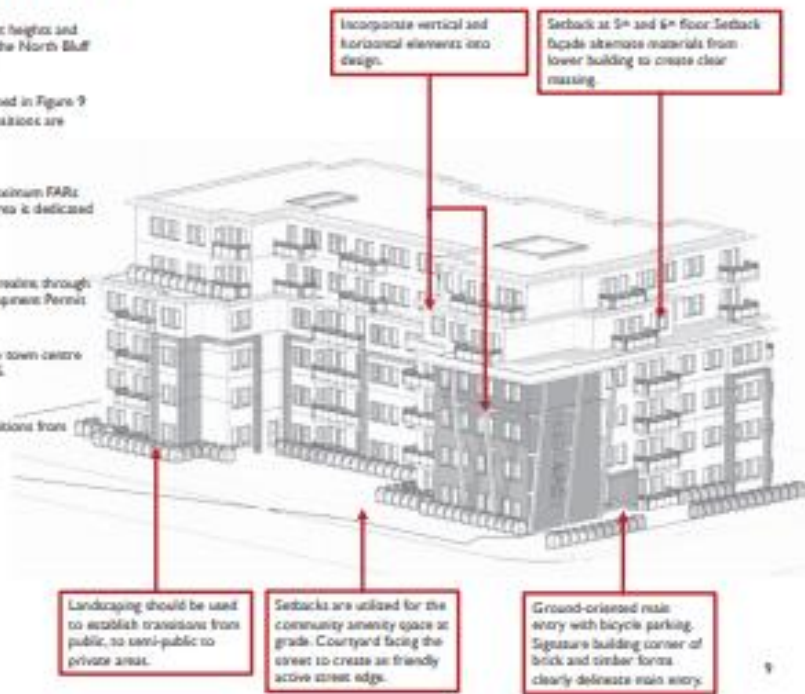
Policy 8.2.4
 Urban Design – Enhance the built and public realm through guidelines included in the Multi-Family Development Permit Area in Part D.

Focus on the establishment of a greenway the town centre and ceremonial Park as per Sections 1.3 and 1.5.

DPA Guideline 11.6.2 (d)
 Landscaping should be used to establish transitions from public, to semi-public to private areas.



Adjacent to Urban Neighbourhood areas at Thrift Avenue



NORTH ELEVATION

LEVEL NUMBER
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2.000
3.000
4.000
5.000
6.000
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EAST ELEVATION

FINISH SCHEDULE	
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11

SOUTH ELEVATION

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12

WEST ELEVATION



13

CONTEXT ELEVATION – FIR STREET



14

COLOUR ELEVATION & MATERIAL LEGEND



STAINED CEDAR COLUMNS



ALUMINUM



ALUMINUM GUARD RAILS WITH GLASS



STAINED CEDAR SHAKES (GREY)



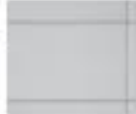
STAINED CEDAR TRELLIS



BRICK VENEER (BLACK)



STAINED CEDAR SHAKES



CEMENT BOARD PANEL (WHITE)



DESIGN RATIONALE

Zoning + Density

Existing Zoning: RM-2
Existing Buildings: Multi-Unit Residential
Existing Height: 3 Storey

Proposed Zoning: CD, BASED ON RM-4
Proposed Building: 80 Rental Apartments
Proposed Height: 6 Storeys

Location

80 modern rental homes near White Rock Town Centre with walkable access to retail and restaurants.

Within 800 m (10 minutes walking)

Community Services:

- White Rock Elementary School
- White Rock Community Centre
- Bryant Park
- White Rock Child Care Centre
- White Rock Farmers Market

Health Care:

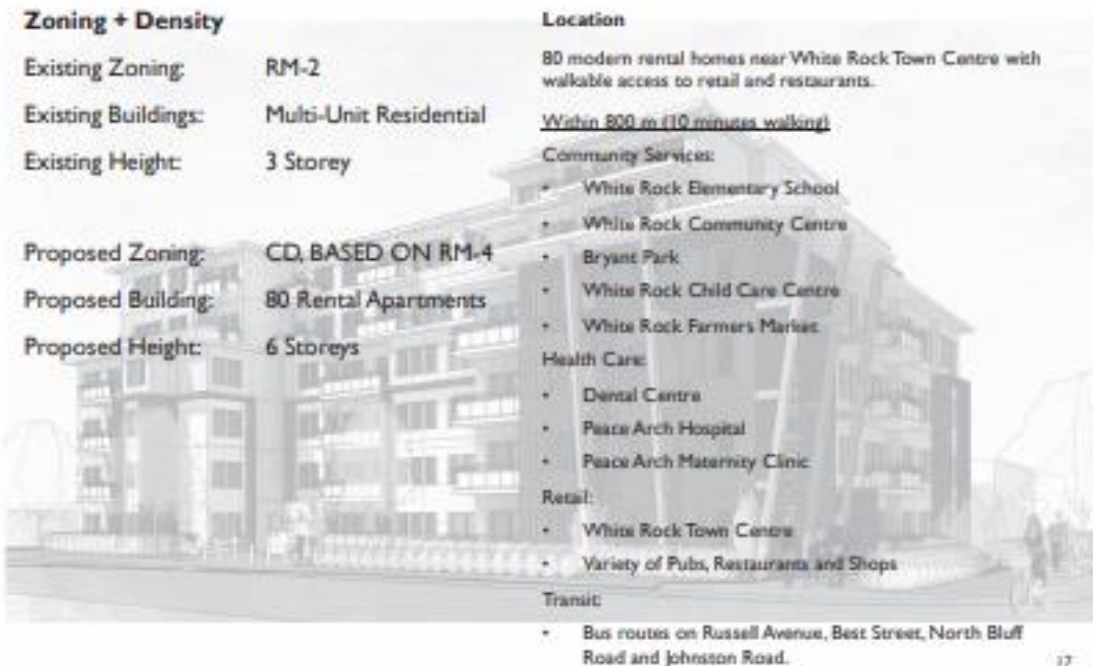
- Dental Centre
- Peace Arch Hospital
- Peace Arch Maternity Clinic

Retail:

- White Rock Town Centre
- Variety of Pubs, Restaurants and Shops

Transit:

- Bus routes on Russell Avenue, Best Street, North Bluff Road and Johnson Road.



DESIGN RATIONALE

Project Brief

80 modern rental apartments to replace 25 aging, non-accessible apartments.

Increase number of rental homes with long term livability and community feel provided by age-friendly amenity spaces.

6-storey wood construction.

Unit mix includes:

5 Studio	(6.25%)
42 Rental 1 Bed	(52.50%)
19 Rental 2 Bed	(23.75%)
4 Rental 2 Bed + Den	(5.00%)
10 Rental 3 Bed	(12.50%)

19 Adaptable Homes (23.75%) –

- Larger bathrooms
- Wider doorways
- Provision for grab-bars
- Mix of 1, 2 and 3-bedroom units

Landscape + Public Realm

Outdoor amenities will include tenant gardens and a children's play area in the courtyard.

Low-maintenance, hardy, local planting will create green space along pathways and existing sidewalks. Existing trees will be protected along the property line where possible to help create a park-like transition from public to residential realm.

Strategic lighting will enhance the development and create safe and welcoming spaces.

DESIGN RATIONALE

Traffic + Parking

Access to the underground parking is provided by the lane on the west side of the building. Access to the lane is available from Russell Avenue and Thrift Avenue.

Residential, visitor and accessible parking spaces are provided as well as a loading zone which can be accessed from the lane. In addition to the minimum parking requirements, this development includes ample bicycle storage and electric car charging stations.

Access to tenant garbage and recycling is located on-site in an enclosed area on the first floor of the underground parking.

Parking Breakdown:

- 112 Total Parking Spaces
 - 87 Residential
 - 24 Vehicle Charging
 - 2 Accessible
 - 1 Loading
 - 25 Visitor
 - 1 Accessible
 - 84 Long Term Bicycle Parking
 - 16 Short Term Bicycle Parking

Pedestrian / Bicycle Focus

Learn the Car Behind initiatives:

- Live updates on transit schedules at lobby
- Directional signage to nearest transit routes and local destinations at Lobby
- Secured bicycle storage with direct access to elevators and Lobby
- New tenant package to include:
 - Compass Pass
 - Transit info
 - Cycling info
 - Estimated walking times and distances
 - Car share info
 - Etc.

Community Development

The project intends to create a dynamic residential setting and easy access to community amenities. Providing contemporary design and thoughtful landscaping creates homes that residents can be proud to be part of.

The courtyard amenities are designed to bring residents together to connect and form a lasting community.

30

VIEW ANALYSIS

- 1) FIR STREET SOUTH
- 2) GEORGE STREET/
RUSSELL AVENUE
- 3) FIR STREET NORTH



- 4) RUSSELL AVENUE
(WEST)
- 5) FIR STREET/
RUSSELL AVENUE
- 6) RUSSELL AVENUE
(EAST)



- 7) AERIAL LOOKING EAST
(15152 RUSSELL AVENUE)
- 8) AERIAL LOOKING
NORTHWEST (1455 GEORGE
STREET)
- 9) AERIAL LOOKING SOUTH
(15280 NORTH BLUFF ROAD)



STATISTICS SUMMARY

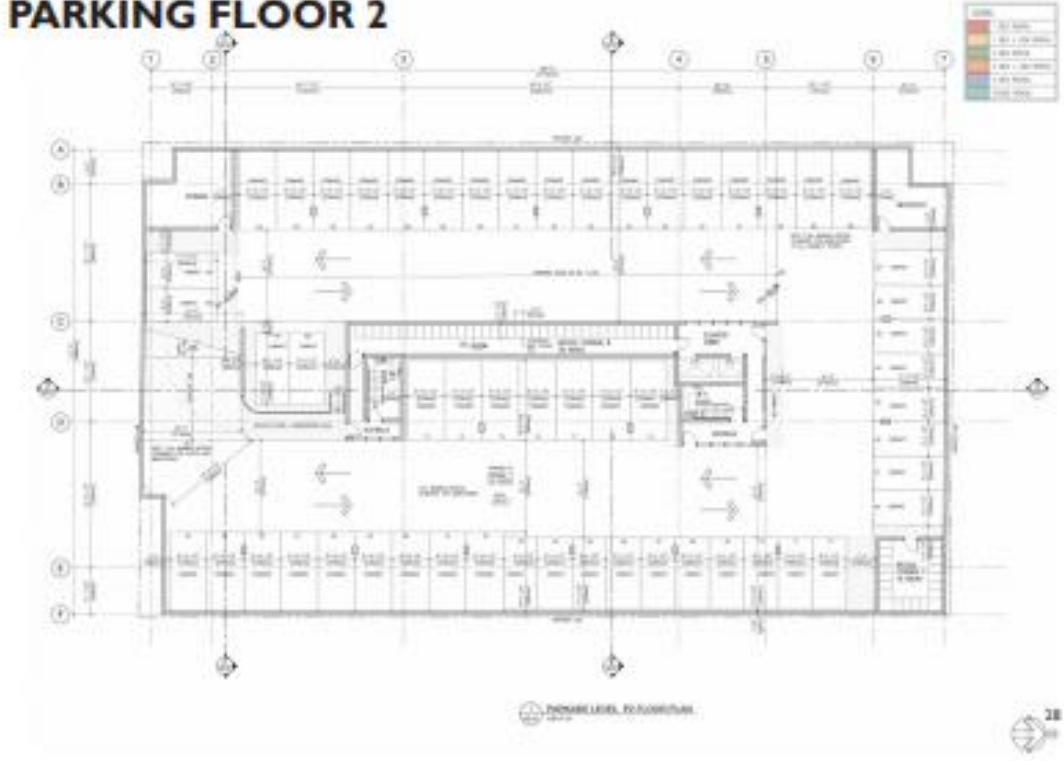
	Required / Allowed	Proposed
Lot Area		2036.15 SM
Lot Coverage	60%	49.98% (1017.69 SM)
FSR		2.80 (5706.73 SM)
Storeys		6
Setbacks	Front 9'-10" Rear 5'-11" Side (North) 9'-10" Side (South) 3'-11"	Front 11' - 4 ½" Rear 10' - 1 ½" Side (North) 16' - 6 ¾" Side (South) 17' - 2 ¾"
Building Height	N/A	62.0 FT
Number of Units		80
Parking	108	108

24

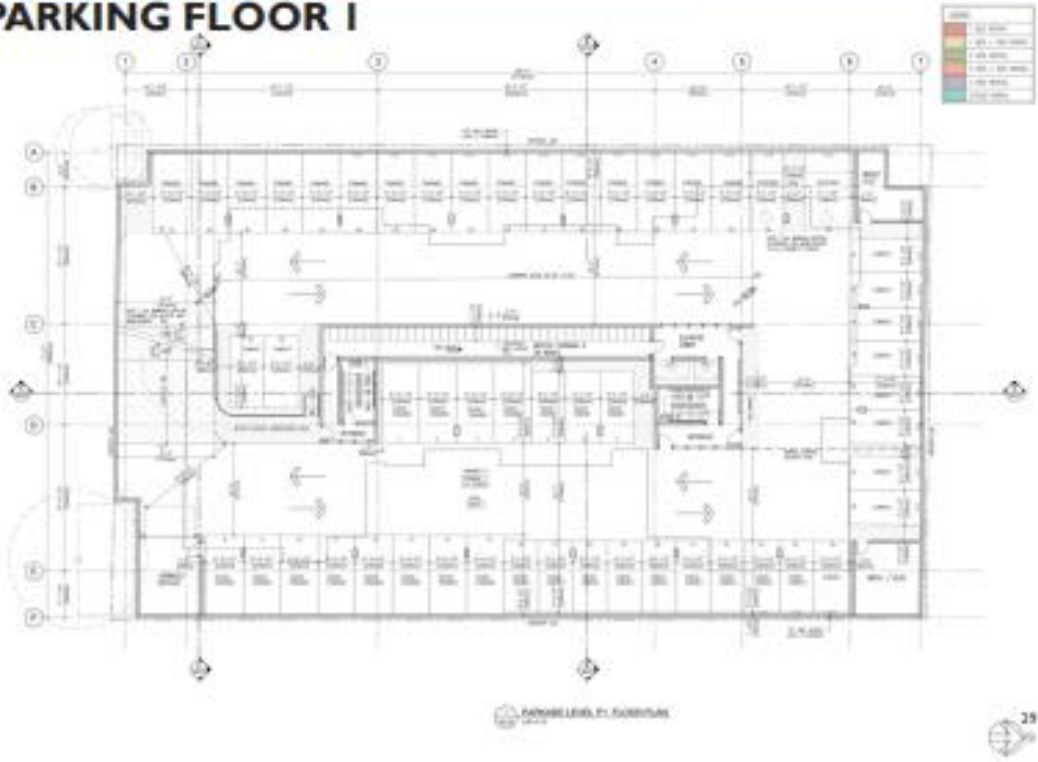
SITE



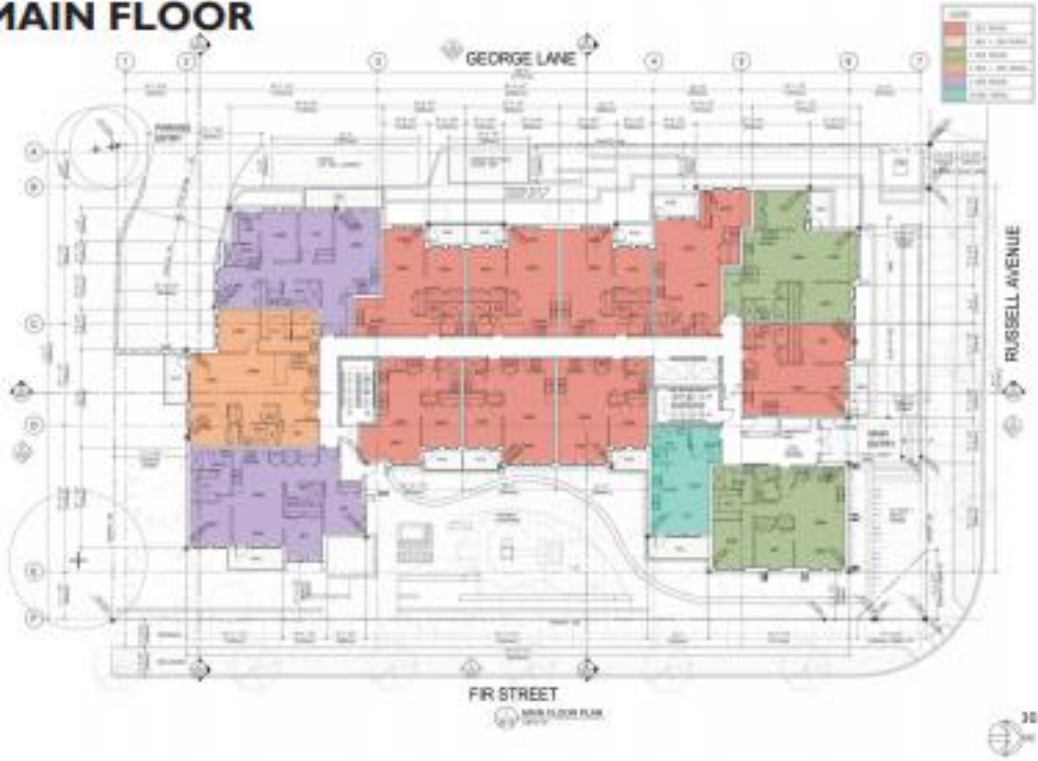
PARKING FLOOR 2



PARKING FLOOR I



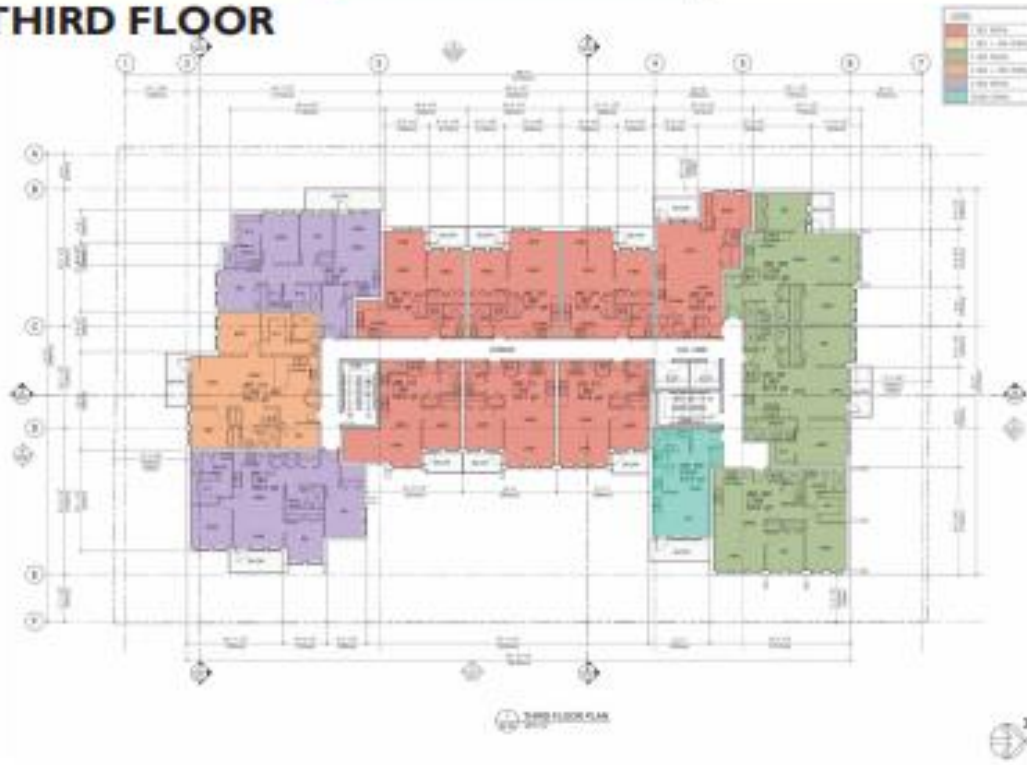
MAIN FLOOR



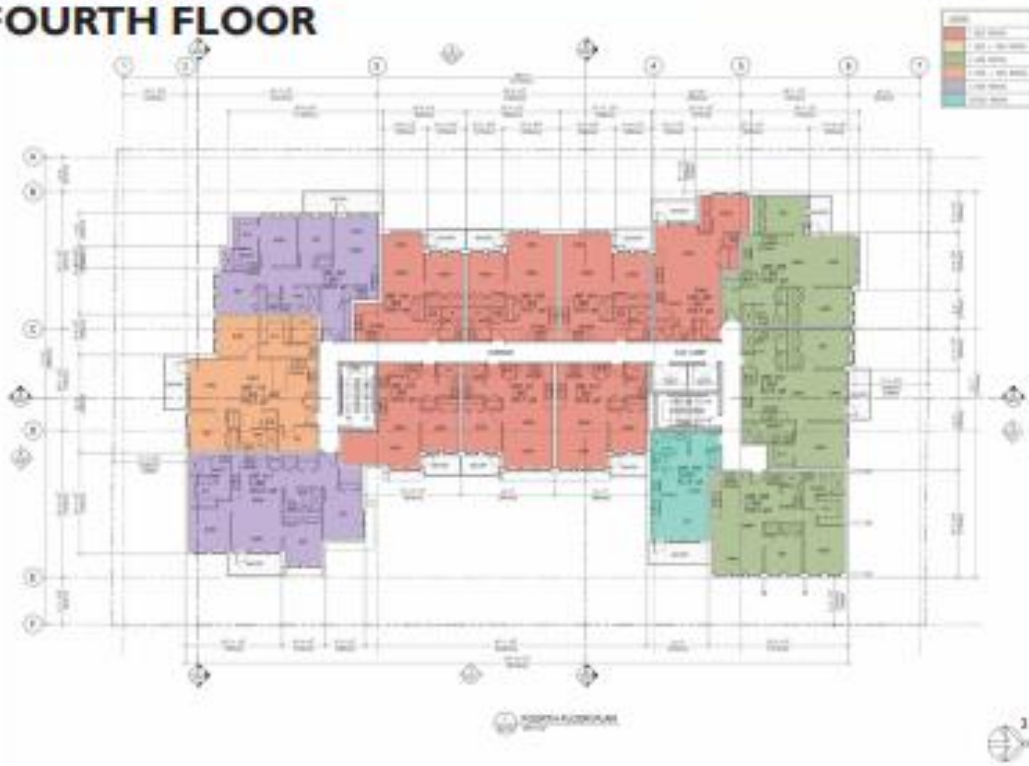
SECOND FLOOR



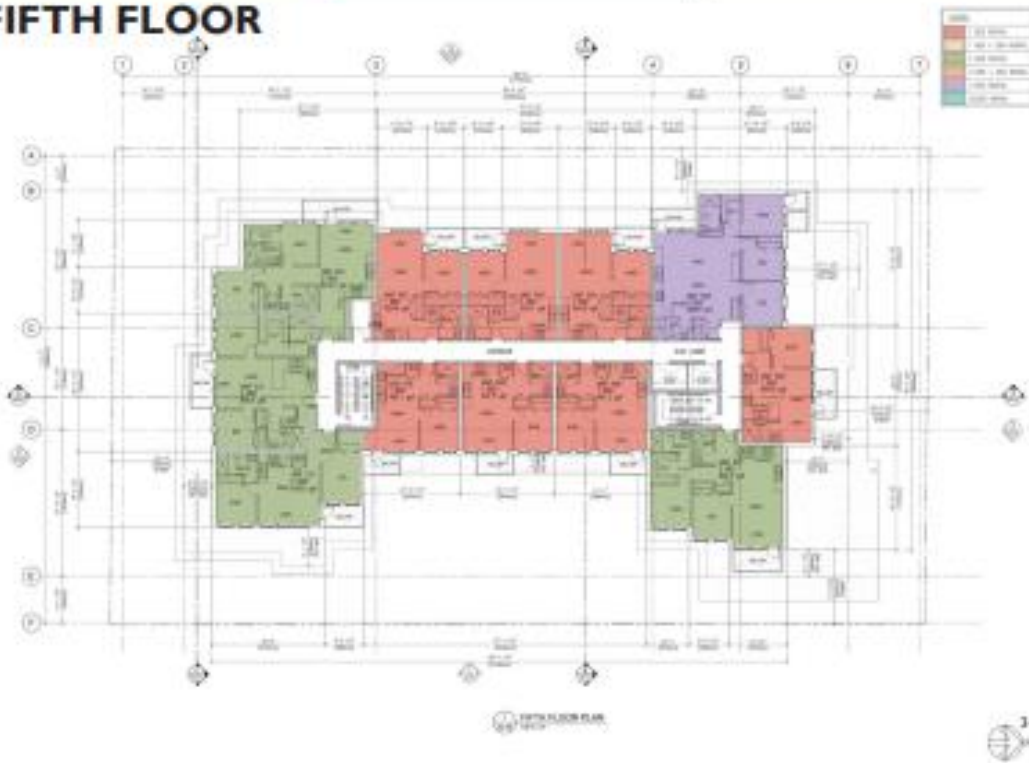
THIRD FLOOR



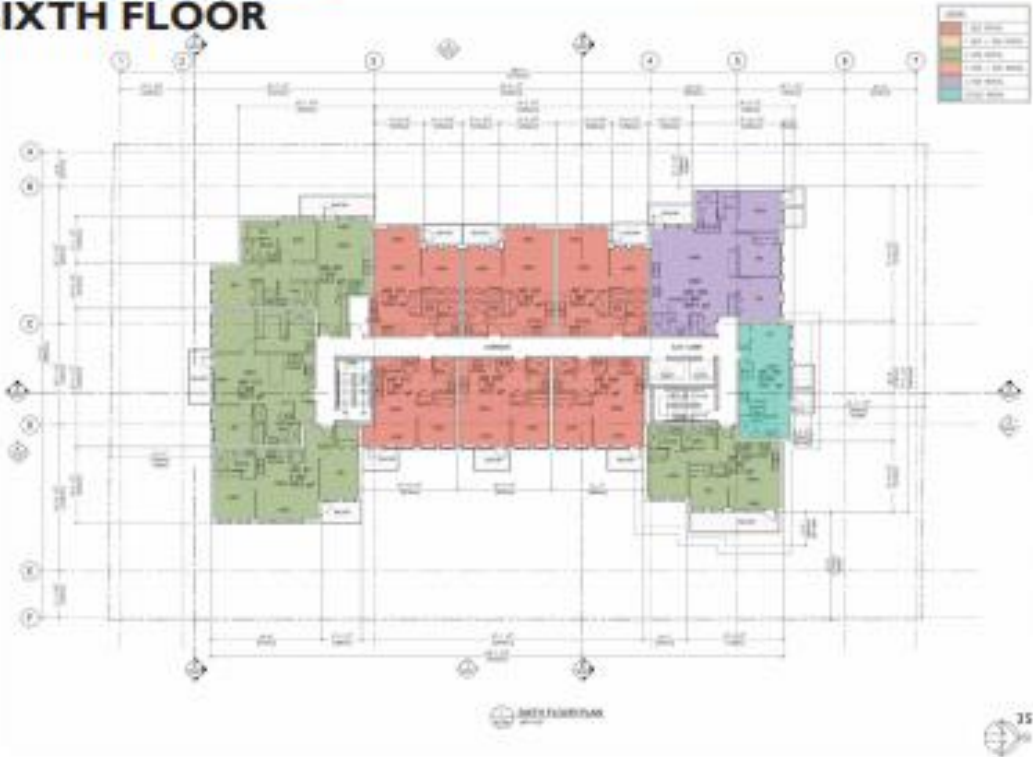
FOURTH FLOOR



FIFTH FLOOR



SIXTH FLOOR



BUILDING SECTION AA



SHADOW STUDY

DECEMBER 21

10:00 AM
12:00 PM
4:00 PM



JUNE 21

10:00 AM
12:00 PM
4:00 PM



MARCH 21

10:00 AM
12:00 PM
4:00 PM



Public
Information
Meeting –
Display Boards

1485 Fir Street White Rock Apartment

Issue for Development Permit



Sheet Number	Sheet Title
L-01	COVER SHEET
L-02	SITE PLAN
L-03	DETAILS
L-04	DETAILS

Contact Information

VDZ+A
 Project Landscape Architect
 2nd Langley Studio
 102-9181 Church Street
 Fort Langley, British Columbia, V1M 2R8

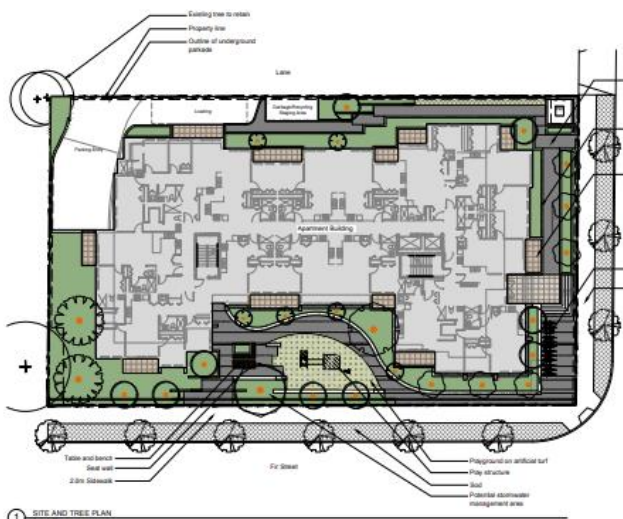
Mount Pleasant Studio
 100-300 Kingsway
 Vancouver, British Columbia, V5T 3J7

Contact Information

Blair Architecture
 Project Building, Architect
 4751- 825 Elm Avenue
 New Westchase, B.C. Canada,
 V3M 1A1
 604-271-0229
 info@blairarchitecture.ca
 www.blairarchitect.ca

Project	1485 Fir Street
Location	1485 Fir Street (Southwest), White Rock, BC
Client	Fir
Owner	Blair
Design	Blair
Scale	1:100
Date	2019-05-17
Notes	All details

PROJECT NO: DP2019-57
 SHEET NO: L-01
 COVER SHEET



TREE SCHEDULE

SYMBOL	COMMON / COMMON NAME	SIZE	DATE	ALL	REMARKS
[Symbol]	Iron palm tree / Chamaedorea Sealei	500	2/16/19	X	Existing planting, remove green, determine
[Symbol]	Amelanchier / Eastern Flowering Dogwood	500	2/16/19	X	Existing planting, remove green, determine tree, 1.5m (5')
[Symbol]	London Plane Tree / Platanus Occidentalis	500	2/16/19	X	Existing planting, remove green, determine tree, 1.5m (5')
[Symbol]	Single trunk / Matured Tree	500	2/16/19	X	Existing planting, remove green, determine tree, 1.5m (5')
[Symbol]	Single trunk / Young Tree / Street/Border/Plants	500	2/16/19	X	Existing planting, remove green, determine tree, 1.5m (5')
[Symbol]	Single trunk / Young Tree / Street/Border/Plants	500	2/16/19	X	Existing planting, remove green, determine tree, 1.5m (5')

HARDSCAPE MATERIALS

KEY	SYMBOL	DESCRIPTION
[Symbol]	[Symbol]	Coloured concrete
[Symbol]	[Symbol]	Retained planter wall
[Symbol]	[Symbol]	Hydrated glass

SOFTSCAPE MATERIALS

KEY	SYMBOL	DESCRIPTION
[Symbol]	[Symbol]	Coloured concrete
[Symbol]	[Symbol]	Grass
[Symbol]	[Symbol]	Gravel
[Symbol]	[Symbol]	Planting beds for landscape
[Symbol]	[Symbol]	Planting beds for landscape
[Symbol]	[Symbol]	Planting beds for landscape



Note:
 1. Materials are to be removed from site to property line fully at the applicant's expense.



Project	1485 Fir Street
Location	1485 Fir Street (Southwest), White Rock, BC
Client	Fir
Owner	Blair
Design	Blair
Scale	1:100
Date	2019-05-17
Notes	All details

PROJECT NO: DP2019-57
 SHEET NO: L-02
 SITE PLAN

Public Information Meeting – Display Boards

PUBLIC INFORMATION MEETING

1485 FIR STREET

REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009

DECEMBER 12, 2019

THIRDSPACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
1.	Shena	1475 - 204	
2.	Stephanie	14728 Upper Roper Ave	V4B 2C9
3.	blwy kator	101 - 1475 FIR ST.	
4.	Michelle McCallum	1544 fir -	
5.	FAT PERARA	5020N. BUFFRD	V4B 5A4
6.	Ron Reid	1467 Merchiz	V4B 4C9
7.	GEORGE WARTTIG	1475 FIR ST	V4B 4B2
8.	ALLIANTIA	1475 FIR ST.	✓
9.	HELENE TH WARTTIG	1477 FIR ST	✓
10.	Celine C hudson	1475 FIR ST	✓
11.	CHUCK BRYANT	1475 Fir ST	V4B 4D5
12.	Edgar Davics	1475 FIA ST	V4B 4B5
13.	Doreen Porowski	1448 FIR ST	V4B 4B4
14.	Reg Nash	1481 Markish ST-	V4B 4C4
15.	Tim Quivon	1475 Fir St.	V4B 4B5
16.	M.R. Becker	1475 Fir. #105	"

PUBLIC INFORMATION MEETING

1485 FIR STREET

REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009

DECEMBER 12, 2019

THIRDSPACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
17.	ISABEL STEWARD	1448 FIR ST	V4B 4B4
18.	EMILIE BELL	1497 MERKLIN ST	V4B 4C4
19.	EMILIE BELL	1493 Myrtle St	
20.	Bill Brennan	157 Scott	
21.	Liz Doucette	#1544 Fir Street	V4B 4B7
22.	Ray Doucette	1544 Fir Street	V4B 4B7
23.	Anthony Manning	WR City Hall	
24.	ANDREW ROSE		
25.	And Chelida	Delta	V4M 2K7
26.	FAN MACLEOD	White Rock	V4B 4B4
27.	Bob Dandron	Vancouver	V6J 2A6
28.	NELSON de AMANAL	Richmond	V7A 1G7
29.	KEVIN GAUTHAIE	1571 GERRARD	V4B 4A4
30.			
31.			
32.			

PUBLIC INFORMATION MEETING

1485 FIR SREET

REZONING, MAJOR DEVELOPMENT PERMIT, FILE NO. 19-009

DECEMBER 12, 2019

THIRDSPACE COMMUNITY CAFÉ 1381 GEORGE ST, UNIT #1 WHITE ROCK, BC V4B 2L1

	NAME (PLEASE PRINT)	ADDRESS	POSTAL CODE
33.	Barton Jessup	302-1544 Fir St.	V4B4B7
34.	SAM IRAND	14835 MARINE DR.	V4B1C1
35.	MATI BALI	14933 Beachview Ave	V4B1P2
36.	SHERRY SOOLE	1491 MERKIN ST.	V4B9C4
37.			
38.			
39.			
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42.			
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47.			
48.			

From: [Elizabeth Brearley](#)
To: [Athena von Hausen](#)
Subject: Public Information Meeting December 12, 2019
Date: Friday, December 13, 2019 11:12:34 AM
Attachments: [Public Meeting Dec-12-2019.odt](#)
[IMG_20191213_0001.pdf](#)
[Letter to Johanssen Apr-3-2019.odt](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Von Hausen:

My name is Elizabeth Warttig and my husband and I attended the above meeting last evening. We mistakenly thought that it was a meeting that we could speak to. Many of the residents of 1475 Fir Street did not attend because they intended that I speak for them. Therefore I am attaching the letters and petition that was signed by all the residents last February. Your department may already have a copy. The Mayor and Council received a copy of this petition and letter also in February.

I did not see you at the meeting, or I would have given you the letters and the petition at that time. We are very opposed to this development. My comments to the planning department on behalf of the tenants of 1475 Fir Street, are below.

Regards, Elizabeth

My name is Elizabeth Warttig and I am representing the residents of 1475 Fir Street, who signed a petition in February of this year to protest the eviction of 30 people from this building. It was presented to the White Rock City council, along with a letter and forwarded to the planning department.

We live in sound affordable housing. This building is not derelict and we respectfully ask that you do allow this unknown developer to evict us and tear down a perfectly sound building. I have researched the rental market in the White Rock South Surrey area for a year now and the average rents are much higher than those we are paying now. In fact they have increased considerably in November. The average for 1 bedroom is now \$1,500 and for a 2 bedroom the average is \$2,200. Burnaby has recently passed a revised residents assistance policy, which asks the developer to top up rents for tenants that

have been evicted and allow them back to the new building at the same cost of the rents that they are currently paying. I would suggest that the City of White Rock make that amendment to their policy.

We are a close knit community who look after each other. Many are elderly tenants who have lived there for 20 years. Some tenants are struggling young people on minimum wages. We cannot afford the so called market rents. Some of us have been subject to harassment and intimidation by the representative for our building, which has been reported to the Residential Tenancy Branch. It has been very traumatic and upsetting for the tenants. 17 new highrises have been slated for construction in White Rock. Those that have already been completed are struggling to fill them.

Everyone is stressed by the uncertainty of where they will go. As you are aware there is no affordable housing in White Rock and I urge you to consider this and do not have us evicted from our homes.

Thank you.

From: [Carl Isaak](#)
To: [Athena von Hausen](#)
Subject: FW: 1075/1085 Fir Street, White Rock, BC
Date: Wednesday, May 22, 2019 2:24:00 PM

Ms. Brearley has emailed Carl J on several occasions and this email is the most recent and detailed correspondence from her regarding her concerns with redevelopment of the 1485 Fir Street (building addressed as 1475 Fir Street) property where she lives.

From: Elizabeth Brearley <elizabethbrearley@hotmail.com>
Sent: Sunday, May 19, 2019 10:45 AM
To: Darryl Walker <DWalker@whiterockcity.ca>; David Chesney <DChesney@whiterockcity.ca>; Helen Fathers <HFathers@whiterockcity.ca>; Erika Johanson <EJohanson@whiterockcity.ca>; Scott Kristjanson <SKristjanson@whiterockcity.ca>; Anthony Manning <AManning@whiterockcity.ca>; Christopher Trevelyan <CTrevelyan@whiterockcity.ca>; Carl Johannsen <CJohannsen@whiterockcity.ca>; Carl Isaak <CIsaak@whiterockcity.ca>
Subject: Re: 1075/1085 Fir Street, White Rock, BC

To His Worship the Mayor and White Rock City Councillors:

We, the tenants of the above property are aware that the owner of 1062822 BC Ltd. has submitted an OCP Amendment, a Zoning Bylaw Amendment and a Major Development Permit Application for the above property, which is a 25 unit rental property, not 24 as mentioned in the application. The mailing address is 1475, not 1485 as stated in the application. I understand this is an error on the part of City Hall.

We sent you a letter and a signed petition by the residents on February 11th 2019 stating our concerns and dismay at being evicted from our homes.

In the information given to us on May 14th 2019, by Mahdi Heidari on behalf of 1062822 BC Ltd., we would like to point out the following mis-information:

- The building has 25 suites not 24
- It has a state of the art heating system
- All windows were replaced with double-glazed high quality windows
- Blinds have been replaced
- New carpets have been installed in all suites
- Light fixtures and electrical outlets have been replaced in all suites

- WiFi is installed in the building for tenants use
- Telus installed optic fibre throughout the building last year

This building is not derelict and has been well maintained by the previous owners over the years.

Many of the tenants have lived here for over 20 years and are in their seventies and eighties. No one is on welfare. We are a very quiet and respectable community. There are no drugs or smoking in the building. There are 4 suites on the 3rd floor and they are occupied by young working adults. Rents are between \$800 and \$1150 per month. Since 1062822 BC Ltd. took over the building in November 2018, 1 tenant has died and 2 have moved into care homes. These suites have been re-rented at \$1,100 and \$1,300 per month. The new owners appear to be letting the building slide into disrepair. We are determined not to let this happen, so now we, the tenants, are now maintaining the building, cleaning the hallways, laundry room and cutting the grass at no cost to the owners. How can these owners morally do this to us? We are happy community that look out for each other. We are all stressed to the max with this hanging over our heads. Where will we go? As you know, market rents are astronomical and not affordable by this community.

Please do not let these greedy developers, who are not familiar with the area, throw us out of our homes. We do not know who they are. We do not know if the money is coming from off-shore and we do not know if the profits will be sent offshore!! They are hiding behind a numbered company.

We hope this information will help you in determining your consideration at the Land Use and Planning Committee.

We invite you all to come and visit the building to see for yourselves what a great community we have here. We will be happy to show you around.

With much respect and best regards,

Elizabeth Brearley-Warttig (on behalf of the tenants of 1475 Fir Street, White Rock, BC)

Tel: 778-294-0647

April 3, 2019

City of White Rock
15322 Buena Vista Avenue
White Rock, BC
V4B 1Y6

Attn: Carl Johannsen,
Director of Planning and Development

Dear Sir:

Re: 1475 Fir Street, White Rock, BC

Our building was sold to a Vancouver developer last fall. It is our understanding that the developer is planning to tear down the building. My husband and I are the caretakers of this rental building and we, along with the rest of the tenants, are concerned for the welfare of the tenants. This building is solid and well maintained. It is not a "slum". Most of the tenants are elderly, with low incomes. We are a close community that takes care of each other.

We are well aware of your Policy No. 514 (Tenant Relocation).

Why tear down a perfectly good building, for some greedy Vancouver developer?

Other properties that are being developed within the community are on land that was not occupied by residents of White Rock and no one lost their homes.

I enclose a letter that I have written to the Mayor and Council, signed by all the residents. Two councillors have responded.

We are prepared to do whatever it takes to keep our homes.

Best Regards,

Elizabeth Warttig - Suite 104 Tel: 778-294-0647 elizabethbrearley@hotmail.com

Residents of 1475 Fir Street, White Rock, BC V4B 4B5			
Suite No.	Name	Signature	Telephone
101	Pauline Paton	<i>Pauline Paton (70)</i>	604-990-0320
102	Lillian King (88)	<i>Lillian King</i>	
103	Peggy Best	<i>Peggy Best</i>	604-379-7997
104	George Warttig (68)	<i>George Warttig</i>	778 968 1947
104	Elizabeth Warttig	<i>E. Warttig (72)</i>	778-294-0647
105	Maria Tajaro (71)	<i>Mariano Tajaro</i>	
106	Mike Becker	<i>M. Becker</i>	604.791.2537
107	Sadie Hadley	<i>Sadie Hadley (87)</i>	604-536-8559
108	Judy Belanger	<i>J. Belanger</i>	604-536-3342
109	Dan Jarvis	<i>Dan Jarvis</i>	
109	Sharon Jones	<i>Sharon Jones</i>	
201	Tony Brugger	<i>T. Brugger</i>	604-531-8490
201	Mrs. Brugger	<i>M. Brugger</i>	"
202	Charles Bryant	<i>C. Bryant</i>	604-535-4634
203	Julia Racher	<i>J. Racher</i>	604-733-3349
204	Elena Udot (65)	<i>E. Udot</i>	604 778-5458024
205	Chris McRae	<i>Chris McRae</i>	778-835-9699
206	Cindy Olynyk	<i>Cindy Olynyk</i>	604-536-9880
207	Leona Burnell	ALZHEIMER	
208	Sharon Wallace		
209	Jordon Hart Cameron	<i>Jordon Cameron</i>	604-836-7332
210	B. Intile		
211	Celine Chidlow	<i>C. Chidlow (85)</i>	604-536-4749
212	Jillian Andrews	<i>Jillian Andrews</i>	604-536-0880
301	Maria Simpson	<i>Maria Simpson</i>	778-552-3926
302	Ken Romaniuk	<i>Ken Romaniuk</i>	604-626-7976
303	Neil McEathron (86)	<i>N.E. McEathron</i>	778 232 5976
304	Don Mooney	<i>Don Mooney</i>	604 771-1796

From: [Sadie Hadley](#)
To: [Planning](#)
Subject: tenant
Date: Tuesday, May 21, 2019 3:31:59 PM

I am a tenant at 1485 Fir st. in White Rock which has been bought by developers. I have lived here over 11 years and am 87 yrs. old because an development permit application has been made I have put my name in a senior residence with a 6-12 month waiting list. My question is if a space comes available am I still eligible for the Tenant Relocation package or is it only after the developers have City approval and permits are approved. Thanks Sadie

From: [Maret Erickson](#)
To: [Athena von Hausen](#)
Subject: 1485 Fir Street, White Rock, B.C.
Date: Friday, December 13, 2019 1:46:43 PM

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Athena

I was unable to attend the public information meeting held on December 12, 2019. This is to advise that I support the project planned for 1485 Fir Street, White Rock, B.C.

Maret Erickson

From: [Barbara Holm](#)
To: [Athena von Hausen](#)
Subject: Development Project 1485 Fir Street
Date: Sunday, December 15, 2019 3:14:09 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Athena,

Thank you for listening at the information meeting December 12th.

As I outlined to you White Rock will soon be facing significant problems due to the ageing infrastructure of virtually all the rental buildings that date primarily from the 60's. They are expensive to run and maintain - lacking modern energy efficient building envelopes, windows, pipes and wiring that are expensive to replace and also to maintain. The small number of apartments relative to the size of the lots they are built on make the property taxes and other fixed costs, heat, water, gas, recycling, garbage quite exorbitant even before the high cost of maintenance is added. Just maintaining heat in suites in winter with 60 year old equipment (despite new boilers etc) had become a major, very time consuming, challenge - the great majority of plumbing contractors are not even familiar with these issues - even if OEM parts are available - which frequently they are not meaning that it is necessary to substitute with after market parts. These areas of concern will grow exponentially with ageing.

There are many significant advantages to dedicated rental buildings in comparison to condo's where tenants are at the mercy of individual owners and tend to be shorter term in most cases.

I was concerned about some of the misconceptions and misunderstandings that many of the tenants from the building had. From money laundering and shady accounts to a total lack of trust and understanding of the commitments required by the developer in order for him to attempt to redevelop the site was misinformed and rather scary. We explained to all tenants in the building that we could no longer manage to deal with the maintenance ourselves and it would have been too expensive to hire a management company with the already high overhead costs and this would have been reflected in much lower maintenance levels.

I do not want to just ramble here so please contact me if you, Carl, or anyone else have any questions.

My husband and I feel that a dedicated rental building on such a convenient location would be a tremendous asset to the community and a huge benefit to tenants who would have the convenience of a safer modern building with all that new technology has to offer.

Kind Regards,
Barbara Holm
604 535 3585

From: [Mahmoud Mahmoud](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca; [MobileMe](#)
Subject: Support for Proposed Rental Development Project: 1485 Fir St, White Rock
Date: Monday, December 16, 2019 5:55:23 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Attention: Athena von Hausen

Dear Ms. von Hausen,

I am writing in support of the above-referenced Rental Development project that is being proposed by C2C Construction. I understand that you will accept email communications from those who were unable to attend the Public Meeting that was held on the project last Thursday.

As someone who visits White Rock frequently, I understand that the City of White Rock has close to zero rental vacancy. Therefore, I am in support of any development which promotes rental accommodation for those who may wish to live in your beautiful municipality, but cannot afford to buy there. To that end, it seems very obvious that any developer that wishes to build rental accommodation in White Rock should be supported by the municipality.

I understand that the proposed project at 1485 Fir Street will be replacing an almost 60 year-old building into a brand new 80-unit fully rental property. I also understand that most of the current rental buildings in White Rock are old and poorly maintained, with no elevator, ramp or other amenities which are needed by the community's senior citizens. Given the location of the site to White Rock's town centre, it escapes me as to what barriers the municipality sees in approving this project. In the circumstances, I respectfully ask that the city support this fully rental development to address the current rental shortage in White Rock.

I'm copying the project architect, Mr. Robert Billard, so that he is aware of the support that this member of the public wishes to lend this worthy rental development project.

Yours truly,

Mahmoud Mahmoud, PhD, FEC
mahmoudmahmoud@icloud.com

From: [Michelle Guy](#)
To: [Athena von Hausen](#)
Subject: 1485 Fir Street
Date: Monday, December 16, 2019 8:29:05 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear City of White Rock,

I write to express my support of this project. I was not able to attend the information meeting with council due to other commitments but thought I would send in my two cents, for what it is worth.

White Rock is a beautiful area and my mother, retired, very much would like to live there. She has a decent pension but does not have the asset base that would allow her to buy. As a result she rents and likely always will. That does not limit her ability to contribute to the local economy of coffee shops and local artists.

We have looked for a place for her and have only found either dated rental housing or basement suites. I was excited to see a proposal that might well meet her long term needs.

I do hope that City Council will see the wisdom in encouraging modern new builds for middle income people who are not able or interested to buy homes but would still prove to be valuable and desirable members of your community.

Michelle

From: [Parastoo Shirazi](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir St white Rock
Date: Monday, December 16, 2019 6:15:53 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Avon Hausen,

Regrettably I was not able to attend the public information meeting for 1485 Fir St project in White Rock, yet I would like to express my support for the below reasons:

Most rental buildings in White Rock are older developments. The current building is 60 years old, it is of great timing to have the old building replaced with a new rental building.

White Rock would benefit from attracting younger population, to sustain and attract the younger population, newer rental buildings with such unique designs would offer attractive housing solutions for the younger population who are yet unable to afford to purchase properties.

White Rock's beautiful demographics could be supplemented by par rental buildings attracting great dynamics to the community.

Thanks

Parastoo Shirazi

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From: [Teresa Leung](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir Street, Whitebirch Apartments
Date: Monday, December 16, 2019 10:47:51 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Athena (City of White Rock),

I am in support of the new rental development, Whitebirch at 1485 Fir Street to address the current rental shortage in White Rock. There are many people who cannot purchase, who NEED to rent, therefore, this project will greatly help those individuals, since the current vacancy rate is almost zero at this time. Please do what you can to push this project forward.

I can be reached at 604-618-2128 should you have any questions.

Best wishes,

Teresa Leung

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From: [Farid Kazemzadeh](#)
To: [Athena von Hausen](#)
Cc: [LEED AP](#)
Subject: Rental Building Proposed
Date: Tuesday, December 17, 2019 4:04:12 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Athena,

I wanted to take this time to show my support for the rental building proposed at 1485 Fir Street. Firstly, the building proposed fits well within the neighbourhood given its design. But even more important, the City has very little rental opportunities which make it very hard to find a suitable place to live. We need newer rental buildings for both younger families and the baby boomer generation.

I hope the City makes the right decision to approve this proposed development and more rental buildings in the future.

Best regards,
Farid K

From: [Yolande Levasseur](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: Proposed building project at 1485 Fir street White Rock
Date: Thursday, December 19, 2019 5:42:02 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern,

I am writing to you to indicate my support of a rental building project awaiting city approval. The address of this project is 1485 Fir Street in White Rock.

I'm a 69 year old woman and, although I'm in excellent health right now, I can see the writing on the wall when I will not have the energy or the ability to trek up four flights of steps. The design of this proposed rental property has all the amenities I would want in the near future and, furthermore, has a lot of style without being ostentatious. I believe it would fit very well in a neighbourhood that is progressive, offering sound housing accommodations in a classy-looking building.

Respectfully,
Yolande Levasseur

Sent from my iPad

From: [Derek Townsend](#)
To: [Athena von Hausen](#)
Cc: robert@billardarchitecture.ca
Subject: 1485 Fir St white Rock Support
Date: Tuesday, December 17, 2019 11:35:53 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Athena,

I am email to express my support for the rental apartment development at 1485 Fir Street. I think new rental supply is key for our unaffordable market and any supply is great as we have had such a lack over the years.

The design is smart and well thought out and the only comment I would add is that it should be twice as big with twice as many units.

--

Derek Townsend
604.812.8312
dwtownsend@gmail.com

PUBLIC INFORMATION MEETING FEEDBACK FORM

Rezoning & Major Development Permit Proposal

Application No. 19-009 – 1485 Fir Street

5:30 PM to 7:00 PM, December 12, 2019

Please note that your completed feedback form will be disclosed to the public and presented to Mayor and Council as part of the information package attached to this development proposal application. Any personal information or commentary you provide on this document will form part of the public record.

Please provide your name and address below: (optional)

Name:

G. WARTTIG

Address:

1475 FIR ST.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

TOTAL FARCE .

RIDICULOUS .

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

PUBLIC INFORMATION MEETING FEEDBACK FORM

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Please provide your name and address below: (optional)

Name:

Moti Bahi

Address:

14933 Beuchemin Ave. White Rock

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I highly recommend the building. In White Rock we have over 60% of all age personnel who have difficulty climbing steps. This will be the first building in over 30 years to have elevators to help the elderly and help people have affordable housing facility.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

ELIZABETH WARTTIC

Address:

1475 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

NOT ALLOWED TO SPEAK

COMPLETE FARCE

WE ARE NOT ALLOWED TO SPEAK

Thank you for your participation. If you have any questions, please contact the following:

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Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

M. R. Becker

Address:

#105 - 1475 Fir St.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

The building/property owner has an ambition to redevelop his property; it would be 'daffy' of me to oppose his wishes. He has assured me that all obligations placed on him by the province & the City of White Rock will be observed. M. Becker

Thank you for your participation. If you have any questions, please contact the following:

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Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

ELAINE BELL

Address:

1497 MERKLIN

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I AM WORRIED ABOUT SO MUCH MORE TRAFFIC IN OUR AREA WITH TWO STORIES OF UNDERGROUND PARKING + 6 STORIES HAD THE BUILDING BEEN 3-4 STORIES, I PROBABLY WOULD HAVE SUPPORTED THE PROPOSAL.

HAVING MORE RENTAL PLACES IS IMPORTANT BUT WHITE ROCK ~~IS~~ HAS BEEN BURDENED WITH CONSTRUCTION SITES FOR SEVERAL YEARS NOW. PRESENTLY IT'S NOT THAT LIVEABLE

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name: DORSEY POROTOWSKI

Address: #313-1448 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

I UNDERSTAND ZONINGS WILL BE CHANGED TO ALLOW THIS BUILDING TO GO AHEAD. WILL THAT EFFECT ALL OLDER BUILDINGS IN A CLOSE PROXIMITY?? IF SO I AM OPPOSED. I ~~HE~~ LIVES ON FIR ST ACROSS FROM PROPOSED BUILDING I UNDERSTOOD THIS MEETING WOULD ALLOW QUESTIONS. A WASTE OF TIME

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Reg Nash

Address:

1481 Merlin St. White Rock V4B 4C4

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I oppose the proposal, as it is directly inline with my sunsets and I think it will invade my privacy.

No thank you to this project, at the proposed height.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: LILLIAN KING
Address: 102-1475 FIR ST

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

Mahdi is REPRESENTATIVE FOR what Company why ALL THE SECRETS WE SHOULD PROTECT LOW RENTAL HOUSING NOT KNOCK THEM DOWN SO SOME UNKNOWN CO. CAN BUILD APT TO MAKE MONEY. I THINK THIS LOOKING IN TO FIND OUT WHO THESE PEOPLE ARE

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: SHERRY SOOLE

Address: 1491 MERKLIN ST.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- NOT ENOUGH GREEN SPACE (NO TREES)
- TOO HIGH BLOCKS SKY & SUN!!!
- TOO MUCH MORE TRAFFIC CONGESTION
- UGLY DESIGN - AGGRESSIVE FRONT THAT DOESN'T BLEND WITH THE NEIGHBOURHOOD.
- UNENVIRONMENTAL TO WASTE A SUBSTANTIAL BUILDING - & SEND ALL THE ^{QUALITY} MATERIAL TO THE JOM
- WHY DO THE RULES COMMUNITY PLAN KEEP CHANGING?

Thank you for your participation. If you have any questions, please contact the following:

- MY HOME WILL BE DEVALUED AS IT IS SUBMERGED INTO DARKNESS!

I want to contact the CITY ...

Athena von Hausen
Planner, City of White Rock
Tel: (604)-541-2159
Email: avonhausen@whiterockcity.ca

I want to contact the APPLICANT ...

Robert Billard
Billard Architecture Inc.
Tel: (604)-619-0529
Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: CHUCK BRYANT

Address: 202-1475 Fir ST

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

I UNDERSTAND WITH NEW RESIDENTIAL UNITS ARE NECESSARY. THE PROBLEM ~~IS~~ IS THAT THE RENT WILL BE TOO HIGH. BEING ON A FIXED INCOME I CAN'T AFFORD IT UNLESS SOME OF THE UNITS ARE SUBSIDIZED

THANK-YOU.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

McCallum Michelle ~

Address:

1544 Fir

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am **UNDECIDED** on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

if more rentals needed, if this site may be controversial, some would be less = sites like 1544 Fir would be more suitable - Maybe with an 8 Storey, in the middle of complexes and only 21 owners to deal with, who can relocate easily with the price paid per unit - No one would complain - Owners are ready for offers. if interested, or another investor, contact FIONA at

Thank you for your participation. If you have any questions, please contact the following: 604 831-0099

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Address:

M. Rode
1493 Merklein
W.R.

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

I'm very concerned.
I feel the Building is too high.
The design doesn't contribute to our
Town's Homeness!
I also feel it is too high!
I hope it isn't Built-

I wish you'd move your ideas
to a different City -

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: ISABEL STEURER

Address: 304-1448 Fir St. 604 789 8600

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- ① This development doesn't represent a gain of 55 apartments, but the loss of 25 affordable ones, and I have a hard time supporting that.
- ② I would really like council to consider the impact of construction on the immediate neighbourhood, not only in terms of parking, but also noise & emissions (i.e. heavy equipment idling). We have already put up with a lot up town White Rock in the last couple of years. Please consider existing residents while welcoming new ones.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Liz Doucette

Address:

#106-1544 Fir Street
White Rock, BC V4B 4B7

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

- There is no enough water if there is a fire to go around as if now.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

① concerns about more traffic congestion in area originally zoned residential

② concerns about adequate to serve a growing population in WRock in light of this proposed development

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

IAN MIDDLETON

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

THIS COMMUNITY DESPERATELY NEEDS
NEW RENTAL SUPPLY. THE CURRENT
SUPPLY IS OUT DATED AND DOES NOT
SUPPORT THE COMMUNITY.

THIS PROPOSAL IS MODEST IN SIZE AND
WILL HELP SUPPORT THE LACK OF SUPPLY
WE NEED TO REMEMBER THIS BUILDING
WILL BE HERE FOR 50+ YEARS WHICH WILL
HELP SUPPORT THE POPULATION GROWTH.

PLEASE APPROVE! WE NEED IT!

Thank you for your participation. If you have any questions, please contact the following:

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PUBLIC INFORMATION MEETING FEEDBACK FORM

Rezoning & Major Development Permit Proposal

Application No. 19-009 – 1485 Fir Street

5:30 PM to 7:00 PM, December 12, 2019

Please note that your completed feedback form will be disclosed to the public and presented to Mayor and Council as part of the information package attached to this development proposal application. Any personal information or commentary you provide on this document will form part of the public record.

Please provide your name and address below: (optional)

Name:

Barbara + Bjorn Holm

Address:

14728 Upper Reper Avenue.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

We owned the White Birch - 1485 Fir Street and due to the high cost of running and maintaining the building had to sell. All tenants were told that it was sold as a development property & they all understood that. The costs - property taxes, insurance, heat, hot water, maintenance & repair require a higher density for it be to remotely viable. White Rock needs new

Thank you for your participation. If you have any questions, please contact the following:

rental buildings

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: Barton Jessup

Address: 302-1544 Fir St.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I think higher density and corresponding more green space is a sound overall principle. I would like this project to be higher. For example an FAR of 4 to 5. If the lower mainland continues to have increased population (as anticipated), then housing towers are necessary to preserve parks and agricultural land.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name: Harold Middleton

Address: 1022 Pacific Pl Delta BC

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

The project seems to be "right sized" with 80 suites, in a desirable location.

The mix of suites offered will allow for a variety of family configurations.

Thank you for your participation. If you have any questions, please contact the following:

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Please provide your name and address below: (optional)

Name:

Address:

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I support the project. I believe more rental housing is required in White Rock.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

IAN MAC LENNAN

Address:

13986 BLACKBURN AVE, White Rock

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

- MORE RENTAL HOUSING NEEDED.

- NEIGHBOURHOOD NEEDS UPDATING

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
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Please provide your name and address below: (optional)

Name:

PAT PETRALA

Address:

15020 N. BLUFF

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

How will this "Market" complex serve the needs of the CARLE Community of diverse income levels & comfortable homes. Cubicals/Urban micro squished spaces at inflated prices. The builder needs to work with Fed/CMHC + Provincial + NonProfits to enable inclusive better choices. Sterile brutalists design is financially advantageous for builders profits yet and Zip to

Thank you for your participation. If you have any questions, please contact the following:

Community
Feedback

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Pauline Paton

Address:

101-1475 FIR ST. WHITE ROCK, B.C.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

I have lived at 1475 FIR ST.
FOR 9 YEARS.

I realize this building is aging
& I am happy to move into a
500 sq. ft. 1 bedroom. My current
rent is 1029.⁰⁰. Market value
is my concern. \$1200.⁰⁰ is my
max for rent as I am a senior
on a budget. I am not proposed
to a new building. East.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name: GEORGE WARTTIG.

Address: 1475 FIR ST.

What is your position on the development proposal application?

(Please circle your preferred response)

~~I SUPPORT the proposal.~~

I am ~~UNDECIDED~~ on the proposal.

I ~~OPPOSE~~ the proposal.

I OPPOSE THE PROPOSAL

Please provide your comments in the box below:

WE UNDERSTOOD THIS 'PUBLIC' MEETING WAS FOR US TO HAVE OUR SAY ABOUT THE PROJECT. WE ARE LOOSING OUR HOMES! I AM FRANKLY DISGUSTED .

Thank you for your participation. If you have any questions, please contact the following:

G. Wantha

I want to contact the CITY ... <input checked="" type="checkbox"/>	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

SARAD DHAND

Address:

14835 MARINE DR.

What is your position on the development proposal application?

(Please circle your preferred response)

I SUPPORT the proposal.

I am UNDECIDED on the proposal.

I OPPOSE the proposal.

Please provide your comments in the box below:

This is exactly the kind of development this city needs.

This is NOT a condo project - it is a RENTAL building. This is very much needed as all the current rental buildings are very old with no elevators, which is terrible for seniors as well as all others. I strongly support this project. It is very good for the future of White Rock.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

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Please provide your name and address below: (optional)

Name:

Ines Quiroga

Address:

207 - 1475 - Fir St. White Rock

What is your position on the development proposal application?

(Please circle your preferred response)

I **SUPPORT** the proposal.

I am **UNDECIDED** on the proposal.

I **OPPOSE** the proposal.

Please provide your comments in the box below:

This is an Open House - Gallery
Not a public meeting for discussion
& questions.

Thank you.

Thank you for your participation. If you have any questions, please contact the following:

I want to contact the CITY ...	I want to contact the APPLICANT ...
Athena von Hausen Planner, City of White Rock Tel: (604)-541-2159 Email: avonhausen@whiterockcity.ca	Robert Billard Billard Architecture Inc. Tel: (604)-619-0529 Email: robert@billardarchitecture.ca

Thank you



ROCHELE POTTER, PROJECT COORDINATOR
ROCHELE@BILLARDARCHITECTURE.CA

APPENDIX G
DPA Guidelines Response Table

(Attached Separately)

The objectives of the **Multi-Family Development Permit Area** are to:

- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy, conserve water, and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock

Please provide a summary of how your proposal achieves the objectives and policies of the Multi-Family DPA below:

NOTE 1: All 'Applicant Response' sections must be filled out by the applicant.

NOTE 2: If your proposal cannot adequately address one of the below-listed DPA guidelines, provide a rationale (and alternative resolution) above, and in the applicable response section.

Section 22.6.1 - Buildings

Multi-Family DPA Guideline 22.6.1 (a)

Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design, with height transitions as outlined in Figure 9 in applicable areas. Vary heights, rooflines, and massing to minimize impacts to views and solar exposure enjoyed by adjacent buildings and open spaces.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (b)

Set buildings back from the property line at least 3 metres to provide enough space for gardens and shade trees in the front yard. Include a further step back above the fourth floor and consider an additional step back above the seventh floor. Tower portions of all buildings should be slim and be set back a minimum of 6 metres from the edge of the podium level to minimize view impacts and shading and to facilitate a minimum tower separation of 30 metres.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (c)

Create visual interest and comfort for pedestrians along all elevations with architectural details. Incorporate windows, doors, bay windows, porches, setbacks, and vary colours, massing, and materials. Townhouse developments are encouraged to provide for individuality from site to site and unit to unit, and to vary the front set-back between units. Non-street facing elevations shall be treated with the same architectural details as the street facing elevations.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.1 (d)

Ensure the main entrances of residential apartment buildings are level with the sidewalk to create a barrier free environment for aging in place. Townhouses may have elevated patios and entrances. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances. Residential units on the ground floor should be ground-oriented.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.1 (e)

Address all street edges on properties fronting multiple streets or public walkways. Orient buildings toward intersections or design independent frontages along both intersecting streets, and incorporate windows, doorways, landscaping, and architectural detailing along all street frontages and walkways.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (f)

Provide articulation to break up building mass and to establish a rhythm along the street front in commercial areas. Ground-level commercial spaces should reflect traditional patterns of diverse, small-scale retail with storefronts of approximately ten metres wide. Include no more than six contiguous units fronting a given street without incorporating architectural elements.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (g)

Provide shared outdoor amenity spaces for residents in mixed-use and residential buildings. Shared roof decks with gardens are encouraged where appropriate. Incorporate dining and seating areas with outdoor cooking facilities, play areas for children, areas for air-drying laundry, communal vegetable gardens, and appropriate landscaping.

Provide each residential unit with a private outdoor space where possible. Incorporating green-roofs to manage stormwater, reduce urban heat island effect, and contribute to biodiversity is encouraged.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (h)

Follow passive solar design principles and orient and site buildings to maximize views to the waterfront. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high-angle sun in summer. Alternatively, provide operable shading devices or window overhangs to control summer solar gain. Maximize passive ventilation and passive cooling through building orientation.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (i)

Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (j)

Integrate commercial signage with the building and/or landscaping. Signage shall have a pedestrian scale and be coordinated throughout each development and compatible with signage on adjacent properties to establish a unified and attractive commercial area. The use of natural materials and projecting signs is encouraged.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (k)	
Blocks of side-by-side townhouses are limited to a maximum of eight contiguous units. Lot consolidation to allow for street-fronting townhouse developments are encouraged.	
Applicant Response	

Section 22.6.2 – Public Realm and Landscape

Multi-Family DPA Guideline 22.6.2 (a)

Improve the public realm with widened sidewalks (minimum 1.8 metres). Plant street trees and design curb let-downs to accommodate wheelchairs and scooters.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.2 (b)

Provide consistency with street trees, plant materials, street furniture, and other aspects of the public realm to create cohesive streetscapes. Incorporate public art in both the public and private realm that is reflective of the local landscape and heritage.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.2 (c)

Site buildings to create through-block walking connections. These will create opportunities for a variety of pedestrian-oriented activities and a finer-grained street grid. Special attention should be paid to establishing a linear park connection between the Town Centre and Centennial Park. Enhance these public spaces with public art and opportunities for programmed uses.

Applicant Response

Multi-Family DPA Guideline 22.6.2 (d)

Use light coloured reflective paving materials such as white asphalt or concrete for paths, driveways, and parking areas to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, roads, and parking areas are landscaped. Use landscaping to establish transitions from public, to semi-public, to private areas.

Applicant Response

Multi-Family DPA Guideline 22.6.2 (e)

Increase the quantity, density, and diversity of trees planted. Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Projects should be designed to allow for the retention of large, mature, healthy trees, and landscape design should employ CPTED safety principles.

Applicant Response

Multi-Family DPA Guideline 22.6.2 (f)

Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.

Applicant Response

Multi-Family DPA Guideline 22.6.1 (g)

Incorporate Low Impact Development Techniques for stormwater management, where appropriate and in accordance with the City’s ISWMP. This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.

Applicant Response

Multi-Family DPA Guideline 22.6.2 (h)

Provide sufficient on-site illumination for pedestrian/vehicle safety and good exposure for retail uses. Light facades and highlight building entrances, and avoid “light spill” onto adjacent properties. The use of lighting systems that are powered by renewable energy, such as solar-power, are encouraged.

Applicant Response

Section 22.6.3 – Parking and Functional Elements

Multi-Family DPA Guideline 22.6.3 (a)

Locate parkade entrances at the rear or side of buildings where possible and separate from pedestrian entrances. Vehicular access from North Bluff Road will only be considered when alternative access is not available. If a parkade entrance faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of the parkade entrance is encouraged. Access ramps must be designed with appropriate sight lines and incorporate security features.

Applicant Response

Multi-Family DPA Guideline 22.6.3 (b)

Use a single internal vehicular access for townhouse developments where possible, with a shared parkade or individual garages. Provide landscaped areas between garages in townhouse developments that have multiple direct vehicular accesses from the street.

Applicant Response

Multi-Family DPA Guideline 22.6.3 (c)

Provide all off-street parking below grade or enclosed within a building, with the exception of some visitor parking spaces and short-term commercial parking spaces. Bicycle and scooter parking shall be provided for residents within parkades, with temporary bicycle parking available near building entrances. Ensure buildings are accessible from parkades for those with mobility impairments.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.3 (d)

Provide sufficient space for garbage, recycling, and composting within parkades. These areas are to be located so that they are convenient for users and accessible for waste/recycling/ compost collection and removal. Loading areas must also be incorporated within buildings wherever possible.

**Applicant
Response**

Multi-Family DPA Guideline 22.6.3 (e)	
Locate mechanical equipment to minimize exposure to the street and nearby buildings. Screening of rooftop mechanical equipment must be integrated into the overall architectural form of the building, and be designed to dampen noise where required.	
Applicant Response	

APPENDIX H
ADP Minutes July 21, 2020

(Attached Separately)

MEETING MINUTES

PRESENT:	K. Hammersley, Chairperson P. Byer J. Muego N. Waissbluth R. Dhall P. Rust
ABSENT:	None
NON-VOTING MEMBERS:	S. Greysen, BIA Representative
GUESTS:	R. Gill (Owner) (14947 Buena Vista Avenue) D. Funk, Su Casa Design (Designer) (14947 Buena Vista Avenue) N. Pullman, CitiWest (Applicant) (14947 Buena Vista Avenue) M. Heidari (Owner) (1485 Fir Street) R. Billard, Billard Architecture (Architect) (1485 Fir Street) R. Potter, Billard Architecture (Architect) (1485 Fir Street) S. Heller, VDZ (Landscape Architect) (1485 Fir Street)
STAFF:	G. Newman, Manager of Planning A. von Hausen, Planner

1. CALL TO ORDER

The meeting was called to order at 3:30pm.

2. MOTION TO HOLD ADVISORY DESIGN PANEL MEETING VIA ELECTRONIC MEANS

It was MOVED and SECONDED

THAT the Advisory Design Panel hold meetings as digital meetings using Microsoft Teams recognizing the COVID-19 global pandemic and efforts to support physical distancing while maintaining open government and the advancement of business.

CARRIED

3. ADOPTION OF AGENDA

It was MOVED and SECONDED

THAT the Advisory Design Panel adopts the July 21, 2020 agenda as circulated.

CARRIED

4. ADOPTION OF MINUTES

It was MOVED and SECONDED

THAT the Advisory Design Panel adopts the minutes from the July 7, 2019 meeting as amended.

CARRIED

5. SUBMISSION TO THE ADVISORY DESIGN PANEL

At the beginning of this section of the agenda, Athena von Hausen, Planner, provided an overview of the policy and regulatory framework applicable to the two applications under review by the ADP. The following subsections outlined the minutes of the meeting as they relate to each of the two applications.

5.1. Application 1: 14947 Buena Vista Avenue

A. Von Hausen provided overview of zoning, OCP and DP Guidelines.

D. Funk (Su Casa) presented the design background for the project.

P. Byer asked about the setbacks and impacts on view from decks on the neighbouring property to the east, and whether that neighbour received notice of the Public Information Meeting (PIM); A. von Hausen confirmed that the neighbours did receive notice and that outside of the height variance the building satisfies the requirements of the zoning bylaw.

P. Byer asked whether the homes were accessible. The designer noted that they have elevators off Blackwood Lane to address accessibility. P. Byer asked if the patios could be made permeable / light coloured. Designer – yes, we can do as much grass as client would like to do (e.g., permeable paver, lawn, etc.). Mr. Byer noted concern with tree removals & need for replacement trees, which he understands to include at least one per property as per city requirements.

J. Muego asked whether the building would be sprinklered. The designer provided that the building would be sprinklered. J. Muego – counting four storeys per BC Building Code offered caution regarding Code Requirements. J. Muego asked what is the cut in the grades (along sides) to accommodate window wells; building is 4 feet from the property line. The design will require significant retaining walls, important to identify that construction along the east property line would be extreme in terms of retaining walls being 10 feet high. Applicant acknowledged. J. Muego noted that the rendering does not show how the rooftop deck may be programed with patio furniture or how people may use the space, which could further encumber views.

P. Rust – likes design, illustrates angle of containment well, would be good to allow a little higher to be able to get an SUV in the garage – the bunker as presented is quite problematic as a space. The designer confirmed the intended use of the space is for storage. P. Rust noted that the design would need a railing on top of the retaining wall for safety of neighbour – materials on exterior of both houses – nice in and of themselves but may be a bit too much disharmony – are the forms enough to distinguish one property from the other? Perhaps better to harmonize the materials with form being more the distinguishing factor.

R. Dhall – height of bunker being 16 – 17 feet – is there an intent to create a space with a mezzanine and other space – noted concerns about steep slope – can the driveway be sloped downward to lower the height of the garage? – Applicant – looked at this earlier (with use of a trench drain) but noted that City Engineering would not consider this. R. Dhall raised a question of planters – lots at various levels – what type? Built in or portable? More information should be provided on how the plants will

be planted in the planter areas. Applicant – would defer to landscape architect – would design to be waterproof and meet the direction provided by the Landscape Architect. R. Dhall noted that the Applicant should explore ways to avoid the need for height variance. Noted neighbours were concerned about slope and effects on property with the slope cuts.

P. Byer recognized from the City’s preliminary comments that the City Arborist noted concerns with landscaping plan and potential ability to accommodate planting without causing structural issues and have sufficient soil to allow trees to reach maturity. G. Newman clarified that a Tree Management Plan will be required and the City Arborist will review for compliance with the Tree Management Bylaw. P. Rust acknowledged that many people seeking to remove trees that become an obstruction to their view. P. Byer also noted that many trees are coming down and wanted to confirm the one per lot requirement. G. Newman confirmed that the Tree Management Bylaw requires a minimum of one replacement tree within the lot when removals are proposed through a permit.

N. Waissbluth asked that the applicant look at the overhangs. Larger overhangs would benefit upper floors to decrease amount of heat retention, provide weather protection. Vertical slats should have more weight in the renderings. Walls along the sidewalk (originally 3 – 4 feet) now the walls are much higher (as proposed). N. Waissbluth noted that “recent developments” shown do not have as significant retaining walls along the sidewalk. Would like to see them stepped up (staggered/tiered retaining wall) – staircases that lead up to the house from the lower end are quite narrow – not very user-friendly, should widen by even a few inches.

P. Byer – concern with the height precedent– looking for a solution that does not require a variance – does like the designs – perhaps remove the mudroom by pushing the building down the slope to satisfy the height requirement of the zoning bylaw. Is there another solution to height variance—this should be explored.

J. Muego – pushing up and down – want to push back to clients – wants versus needs – views – perhaps rooftop deck shouldn’t be accessible (occupied) or should be smaller with garage pushed further back – guardrails staying within angle of containment would pull activities towards the garage – depth of the bunker too much for the site (logistics and costs) – leveling of Lot 1 (front yard) is creating a 6 foot high retaining wall – how is this going to affect the westerly neighbours enjoyment of their lot – would look to tier it back south-to-north and west-to-east – patio – capture interesting views through screening – slats blocking windows to frame the view – educate clients on tools available to designers to give them the best performance.

S. Greyson – planting massive trees where massive trees were – can the City not plant trees elsewhere? Noted concern with the bunker if used as living space as it would not have sprinklers / egress.

R. Dhall – landscape plan – more variety in the planting – mostly all HB – reasonably large patches of plantings here – looking at the front side (south) there is more variety – but more sought (more colour) – would like more details about planting systems – details of planters, how they’re supported (structurally) – represent paving systems (materials) in the landscape plan (surfaces) – label properly.

K. Hammersley – summary of issues regarding landscaping, tree planting, solar exposure (passive solar gain), issues of the bunker, height variance.

Designer (D. Funk) – noted that Engineering is not supportive of the variance to driveway slope.

P. Byer – owners / designers to re-consider the height variance sought.

J. Muego – owner may wish to look at alternatives (reduction to the rear yard setback) – Applicant – could look at moving the home down towards Buena Vista Avenue – reducing square footage of the home while maintaining the 15% slope.

P. Rust – garage built with hydraulics set into the bunker to raise and lower the garage enough so that it would not encroach on the height limit and allow for SUV’s to be stored.

Following the receipt of final comments the Chair asked for a motion

It was MOVED and SECONDED

THAT the Advisory Design Panel recommends that the application for the development proposal at 14947 Buena Vista Avenue be **referred to Council** once the applicant has had an opportunity to consider the comments pertaining to:

- 1) Providing a broader mix of plantings and surface treatments (e.g., patios) as shown in the Landscape Plan, and that plantings are satisfactory to the City Arborist;
- 2) Implementing a tiered southern retaining wall so that the structure does not overwhelm the pedestrian realm along the sidewalk of Buena Vista Avenue;
- 3) Efforts to mitigate solar gain (e.g., overhangs, eyebrows, etc.) and passive cooling options along the south facing elevation of the dwellings;
- 4) The intended use and function of the “bunker” and the compliance of the space with the applicable provisions of the Zoning Bylaw; and
- 5) The requested height variance and efforts to alter the design such that a variance is no longer required; in the event that the applicant proceeds with the variance, that staff identify to Council the efforts taken by the applicant to address this constraint.

CARRIED

5.2. Application 2: 1485 Fir Street

A. von Hausen began the review of the second application with an overview of the OCP, Zoning and DP Area.

R. Billard (Project Architect) – walked through the application – the various iterations of the design, efforts to address comments from City staff, mitigate traffic and access issues, program and locate amenity spaces, and step back the massing of the building. Mr. Billard also walked through materials as proposed in the design, the context of development within two blocks of the subject property, the composition of units (by # bedrooms), and efforts to support bicycle and transit use.

S. Heller (Landscape Architect) – parkade notched to accommodate the retention of trees, street trees will be replaced along Fir Street (depending on what happens with overhead power lines); overview of access to building, treatment of spaces to delineate public and private spaces, surface material treatment.

J. Muego – excited to see the front entrance (6 steps w/ accessible ramp) – not shown on the renderings. R. Billard – pointed to the access (ramp) versus stairs shared along Russell Avenue. R. Dhall requested to see Main Floor plan – requested confirmation of parkade access – wanted confirmation of the planters to be used and whether or not they are acceptable to the City. S. Heller confirmed that they have done similar plantings on other projects in the City. R. Dhall requested confirmation of whether fencing would be used to enclose the parkette. R. Billard noted they want to keep the space open so it reads as part of the community.

P. Byer – parking entrance - unclear whether there was adequate clearance into the parkade. R. Billard clarified that there would be sufficient clearance. P. Byer asked if there was a vehicle drop off within the boulevard near the access. P. Byer asked if there was accommodation to expand electrical charging to more spots in the future. R. Billard noted that White Rock does not require anything. G. Newman clarified that the City requires 1/10 spaces an energized outlet (level 2) and an additional 1/10 spaces to have a rough-in for EV charging.

P. Byer asked how recycling / garbage was being managed and whether it would be carried outside the building for collection. R. Billard confirmed that a hauling company would be retained for collection. P. Byer noted that a community garden / tenant garden appear to be planned for the roof as illustrated in the DP Guidelines Matrix. G. Newman clarified that the matrix was submitted with the first submission and that subsequent design has not been captured in a revised matrix. P. Byer asked about rooftop stormwater retention. R. Billard noted this is a costly component of the design and that a cistern may be used. S. Heller added reference to some of the features for stormwater management incorporated into the landscape design. P. Byer noted that there are only 2 handicapped (accessible) parking spaces for residents and encouraged more handicapped spaces to be offered.

S. Greyson identified a potential conflict between trees and overhead wires. A. von Hausen confirmed that wires would need to be underground as a City engineering requirement.

K. Hammersley – impressed with the proposal based on previous review

R. Dhall – good treatment of spaces along the street – encompassed most design elements – inconsistency in drawings showing access to parkade in other location (reference to the design matrix). Crime Prevention Through Environmental Design (CPTED) principles to be applied along edges – east elevation – use of a lot of fenestration (windows). R. Dhall noted that it would be good to consolidate the number of openings and windows alongside the red accents. 60% of units are one bedroom or less (studio). R. Dhall would like to see higher proportion of two bedroom units. R. Billard provided that at this time there is not an opportunity to change the mix of units (lending constraints). R. Dhall would like to see a little bit more design development of the entry feature with the inclusion of the feature within the heavy timber frame currently at the corner of Russell and Fir.

J. Muego – commended the Landscape Architect in looking at the ground plane – streetscape elements are good – design elements good (stepping down levels five and six). J. Muego notes that the upper levels need something more to distinguish them and cut down on the massing (colour / material treatment). Muego reiterated R. Dhall's comment regarding the repetition of the window patterns – would like to see some consolidation – corner buttress quite heavy / strong considering the roof they are holding as well as the base being quite ambiguous, more design development should be considered.

P. Rust – structure on the corner didn't quite capture the entrance – would like to see one more bay to capture the entrance – look at use of panels along the fifth and sixth storey – an original rendering indicated a panel system of cladding which would be more appropriate than shingles proposed at this height – use of 9 foot ceilings (why not 8 foot). R. Billard provided that higher ceilings are currently preferred by tenants. P. Rust - would like to see a galley kitchen. R. Billard noted that galley kitchens are less desired by target market.

P. Byer – bullet points for final

- Stormwater management plan must go to the Engineering Department – efforts to minimize the amount of stormwater going to the storm system;

- Rooftop to be designed to mitigate solar gain;
- Electrical charging – sufficient rough-in for future expansion
- Two accessible parking spots not sufficient – more should be made accessible

N. Waissbluth – main comment pertains to the landscaping in the amenity / courtyard space – concern that the play space may not be used – needs to be more purposefully activated without the use of “pre-builts”. R. Billard – wanting to re-evaluate the design of the space to ensure it is more accessible and more purposefully used as intended.

Following the receipt of final comments, the Chair asked for a motion.

It was MOVED and SECONDED

THAT the Advisory Design Panel recommends that the application for the development proposal at 1485 Fir Street be referred to Council once the applicant has had an opportunity to consider the comments pertaining to:

- 1) Stormwater management plan must go to the Engineering Department – efforts to minimize the amount of stormwater going to the storm system;
- 2) Rooftop to be designed to reduce solar gain;
- 3) Efforts to increase the number of electrical charging stations
- 4) Efforts to increase the number of accessible parking spaces
- 5) Design of the children’s play space – naturalization of the space

CARRIED

6. CONCLUSION OF THE MEETING

There being no further business, the Chairperson declared the meeting concluded at 6:40 pm.

Karen Hammersley
Chairperson, Advisory Design Panel

Greg Newman
ADP, Committee Secretary

APPENDIX I
CTS Traffic Study dated November 25, 2019

(Attached Separately)

Date: 25 November 2019
Our File No: 7025-01

BY EMAIL

Mahdi Heidari
1062822 BC Ltd.
1005-583 Beach Crescent
Vancouver, BC
V6Z 3E6

Dear Mr. Heidari,

**Re: 1485 Fir Street – Whitebirch Apartments Traffic Impact Assessment
White Rock, BC**

Creative Transportation Solutions Ltd. (CTS) is pleased to submit this DRAFT report summarising our work on the above study. CTS was retained by 1062822 BC Ltd. on March 21st, 2019 to undertake a traffic impact study for a proposed rental apartment development in the City of White Rock. The primary objectives of this study are as follows:

1. To undertake a traffic impact assessment of the development site; and,
2. To document the results in a report suitable for submission to the City of White Rock.

This report documents our analyses and findings.

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1.0 BACKGROUND

1.1 The Site

1062822 BC Ltd is proposing to build a rental apartment development at 1485 Fir Street in the City of White Rock.

The proposed development consists of 80 rental apartment dwelling units with two levels of underground parking.

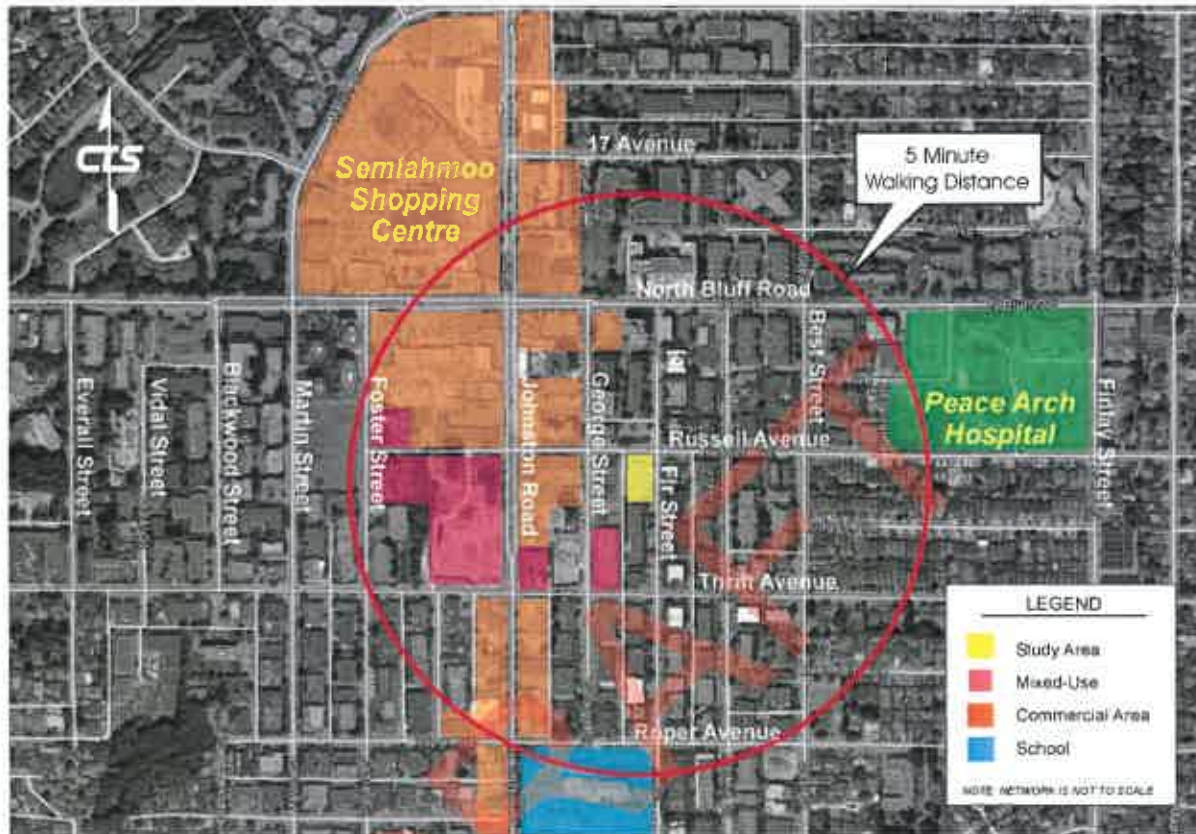
The property is currently occupied by a 3 level multifamily building with 21 residential units.

Vehicular access to the proposed development will be from George Lane and the main pedestrian access will be on the north face of the building, facing Russell Avenue.

The proposed development is to be constructed as per the architectural drawings included as **APPENDIX A**. The site context is illustrated in **FIGURE 1**.

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**FIGURE 1
SITE CONTEXT**



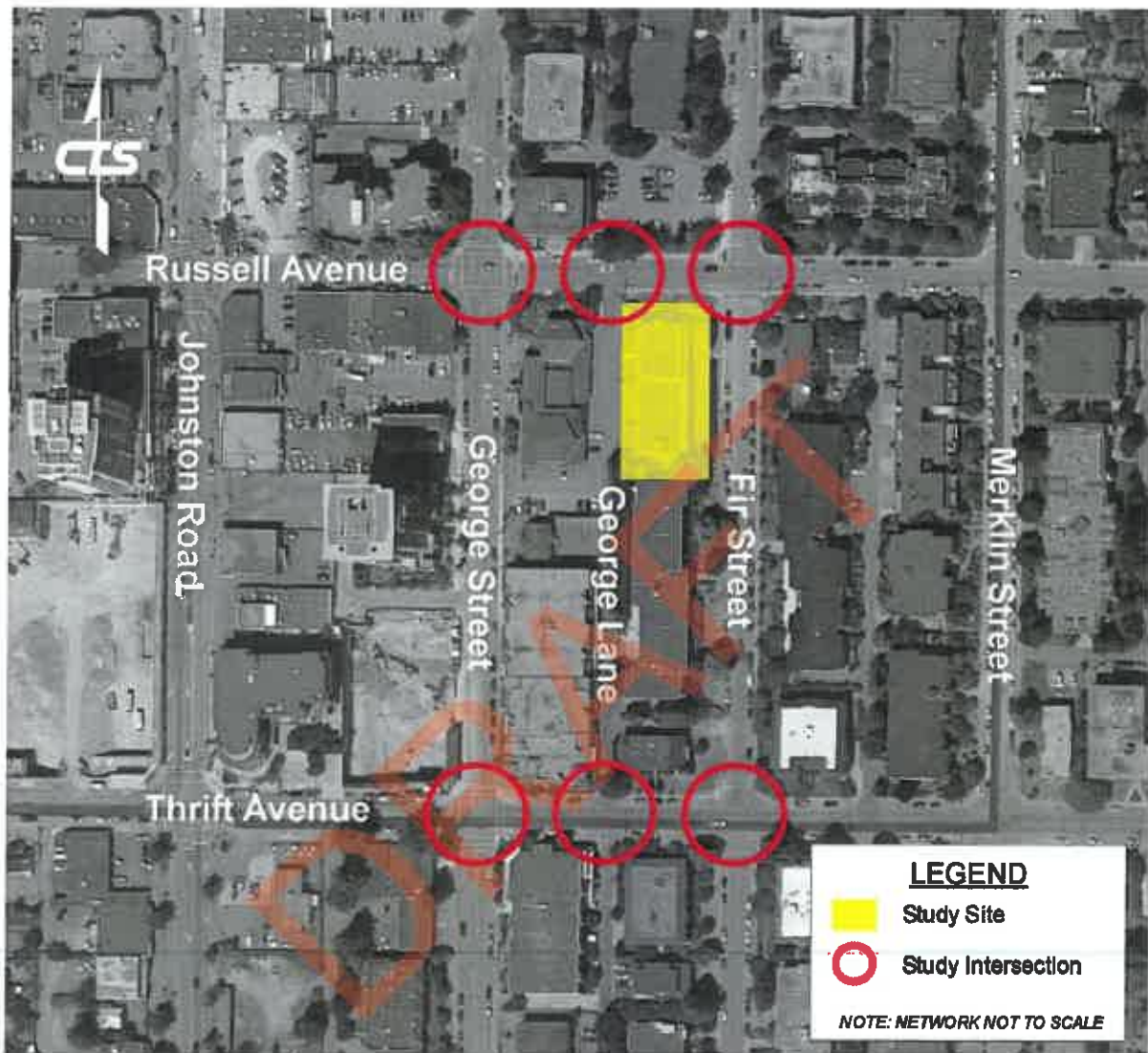
The 400 metre radius illustrated in **FIGURE 1** above represents a five minute walk from the site.

As illustrated in **FIGURE 1**, the site has excellent walking access to significant commercial and retail areas.

The following attractions and destinations are all approximately a five (5) to fifteen (15) minute walk from the study site:

- Semiahmoo Shopping Centre
- Peace Arch Hospital
- Commercial / retail developments all along Johnston Road
- White Rock Centre transit exchange
- Earl Marriott Secondary School
- Peach Arch Elementary
- Kent Street Activity Centre

**FIGURE 2
STUDY AREA AND INTERSECTIONS**



1.2 Site Visit/Road Network

A site visit was conducted in order to document current conditions. The following were the key observations from the site visit:

Fir Street

- North-South local road
- Two undivided lanes, one in each direction.
- East side of the road:
 - Concrete curb and gutter
 - Sidewalk
- West side of the road:
 - Concrete curb, gutter, and sidewalk for first 40 m south of Russell Avenue
 - Gravel shoulder, with some sections of a wooden curb on the rest of the block between Russell Avenue and Thrift Avenue
- Street lighting
- 50 km/h speed limit
- Parking permitted on both sides of the road

Russell Avenue

- East-West collector road
- Two lanes, one in each direction
- Parking permitted on the north side of the road between George Street and Fir Street
- Concrete curb and gutter
- Sidewalk on both sides of the road
- Street lighting
- Bus stops
- 50 km/h speed limit

George Street

- North-South local road
- Two lanes, one in each direction
- Parking permitted on both sides of the road
- Concrete curb and gutter along both sides of the road
- Sidewalk on both sides of the road
- Street lighting
- 50 km/h speed limit

Thrift Avenue

- East-West local road
- Two lanes, one in each direction
- Parking permitted on both sides of the road
- Concrete curb and gutter along both sides of the road
- Sidewalk on both sides of the road

- Street lighting
- 50 km/h speed limit

George Lane

- North-South laneway
- Off-street parking accesses and parking spaces on either side
- Only wide enough for one vehicle to travel at a time

George Lane at Thrift Avenue

During the site visit, it was observed that the sightlines for southbound vehicles exiting the laneway are deficient when looking west. The eastern sight lines are sufficient as drivers are able to look around the tree on the northeast corner as show in **FIGURE 3** below.

FIGURE 3
GEORGE LANE AT THRIFT AVE – SOUTHBOUND



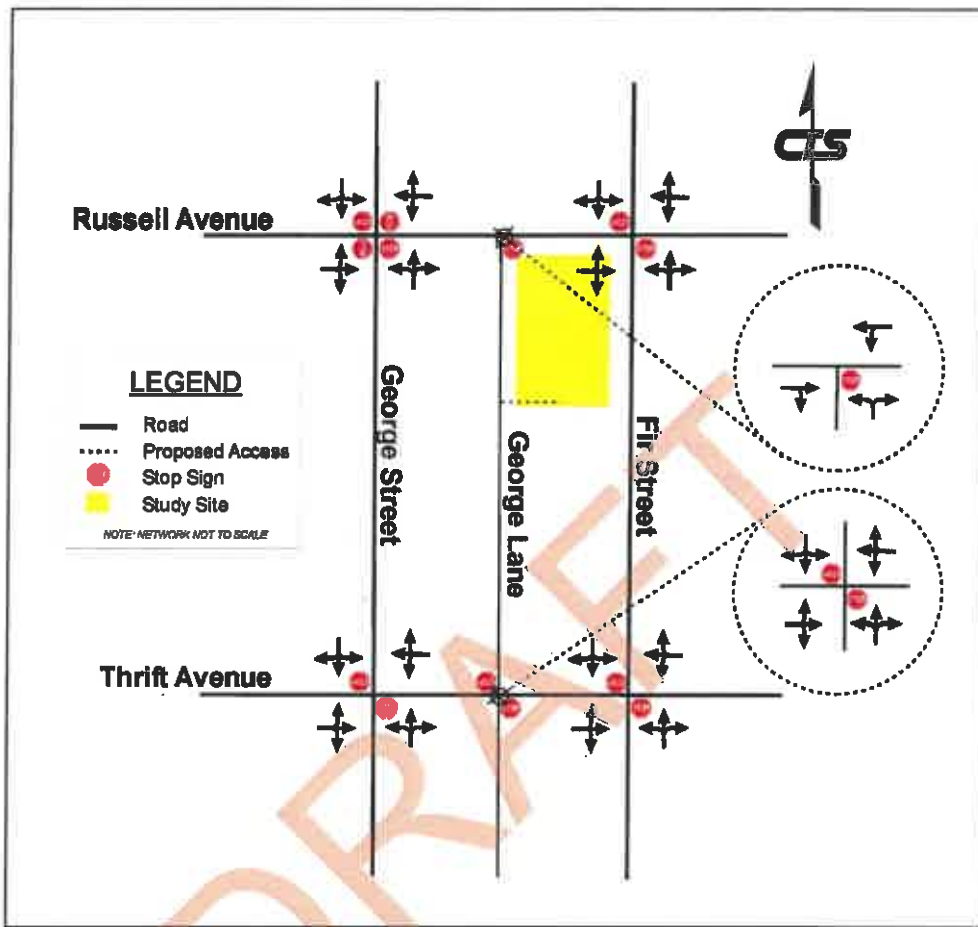
FIGURE 4
GEORGE LANE AT THRIFT AVENUE – SOUTHBOUND LOOKING WEST



As shown in **FIGURE 4** above, the western sightlines are deficient as the foliage and planter obstructs driver sightlines when stopped behind the stop bar. Sightlines for vehicular traffic are improved if drivers pull forward into the parking lane. However, sightlines for pedestrians crossing the laneway from the west side remain deficient. This poses a safety concern as pedestrians, especially those using mobility aids and / or children, are blocked from the driver's view by the planter and foliage. It is recommended that the City of White Rock consider installing yellow tactile paving blocks on the let-downs of either side of the crossing and a warning sign stating "Watch for Pedestrians" for southbound traffic at this intersection.

The laning configuration and current traffic control for the study intersections are illustrated in **FIGURE 5**.

**FIGURE 5
LANING CONFIGURATION**



1.3 Alternative Modes of Travel

Transit Network

The study area is in a transit oriented neighbourhood. The White Rock Centre transit exchange is only a six (6) minute walk from the site. 152nd Street is part of Translink's Frequent Transit Network. The site is serviced by the following bus routes:

- Route #321 Surrey Central Station / Newton Exchange / White Rock Centre / White Rock South – bus stops are serviced every 15 minutes all week
- Route #345 King George Station / White Rock Centre – bus stops are serviced every 30 minutes Monday to Friday
- Route #351 Bridgeport Station / Crescent Beach – bus stops are serviced every 15 minutes or less during weekday peak periods. Frequency decreases during off peak periods during the rest of the week.
- Route #354 Bridgeport Station / White Rock South – bus stops are serviced during weekday morning and afternoon peak periods every 15 to 30 minutes.
- Route #361 Ocean Park / White Rock Centre – bus stops are serviced every 30 minutes during the weekday and every hour during the weekend.
- Route #362 Seaside / White Rock Centre – bus stops are serviced every 30 minutes throughout the week.
- Route #363 Peace Arch Hospital / Southpoint – bus stops are serviced every 30 minutes throughout the week.
- Route #375 King George Station / White Rock South – bus stops are serviced every 30 minutes for most of the day during the week. Frequency decreases to every hour later in the evening and during the off peak periods on the weekend.
- Route #531 White Rock Centre / Willowbrook – bus stops are serviced every 30 minutes throughout the week.

Bicycle Network

Within the adjacent road network, Thrift Avenue is a shared lane bike route. Martin Street, Best Street, and Finlay Street are also shared lane bike routes as illustrated in **FIGURE 6** below.

Pedestrian Network

The study area is well connected with sidewalks. All roads within the study area have a sidewalk on at least one side.

**FIGURE 6
ALTERNATIVE MODES OF TRAVEL WITHIN 400 METRES**



1.4 Scope of Work

CTS selected the weekday morning and afternoon peak hours as the analysis design hours for this study as it represents the peak traffic time for the adjacent road network and the peak traffic times for the proposed development.

The following scenarios were used in this traffic impact assessment:

1. 2019 existing base traffic (from traffic surveys)
2. 2022 future base traffic
3. 2027 future base traffic
4. 2022 future base traffic + proposed development traffic
5. 2027 future base traffic + proposed development traffic
6. 2045 estimated link traffic volumes

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2.0 BASE TRAFFIC VOLUMES

2.1 Existing Base Traffic Volumes

2019 Base Traffic Volumes

CTS conducted turning movement counts on Wednesday April 3rd, 2019 from 07:00 to 09:00, 11:00 to 13:00, and 15:00 to 18:00 to document the typical weekday peak hour traffic volumes for the following intersections:

- George Street at Russell Avenue
- George Lane at Russell Avenue
- Fir Street at Russell Avenue
- Fir Street at Thrift Avenue
- George Lane at Thrift Avenue
- George Street at Thrift Avenue

The traffic count data was summarized and reviewed to ensure data integrity and validity. The summarized traffic data sheets are included in **APPENDIX C**.

From the collected data, the weekday morning and afternoon peak hours were chosen as the dominant design hours and was observed to occur from 08:00 to 09:00 and from 15:00 to 16:00.

The 2019 base weekday morning and afternoon peak hour volumes are illustrated in **FIGURE 7** and **FIGURE 8**.

FIGURE 7
2019 WEEKDAY MORNING PEAK HOUR BASE TRAFFIC VOLUMES

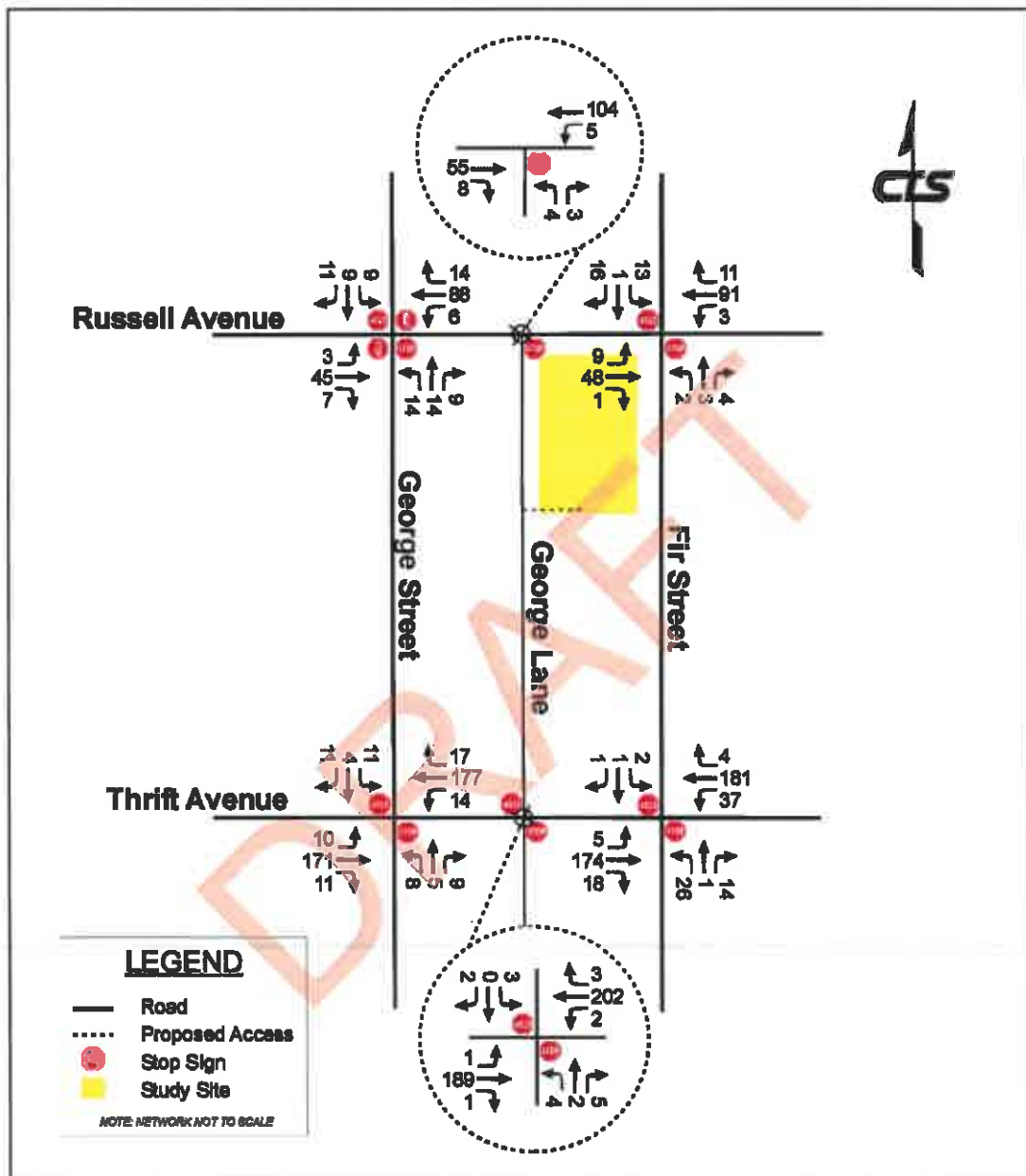
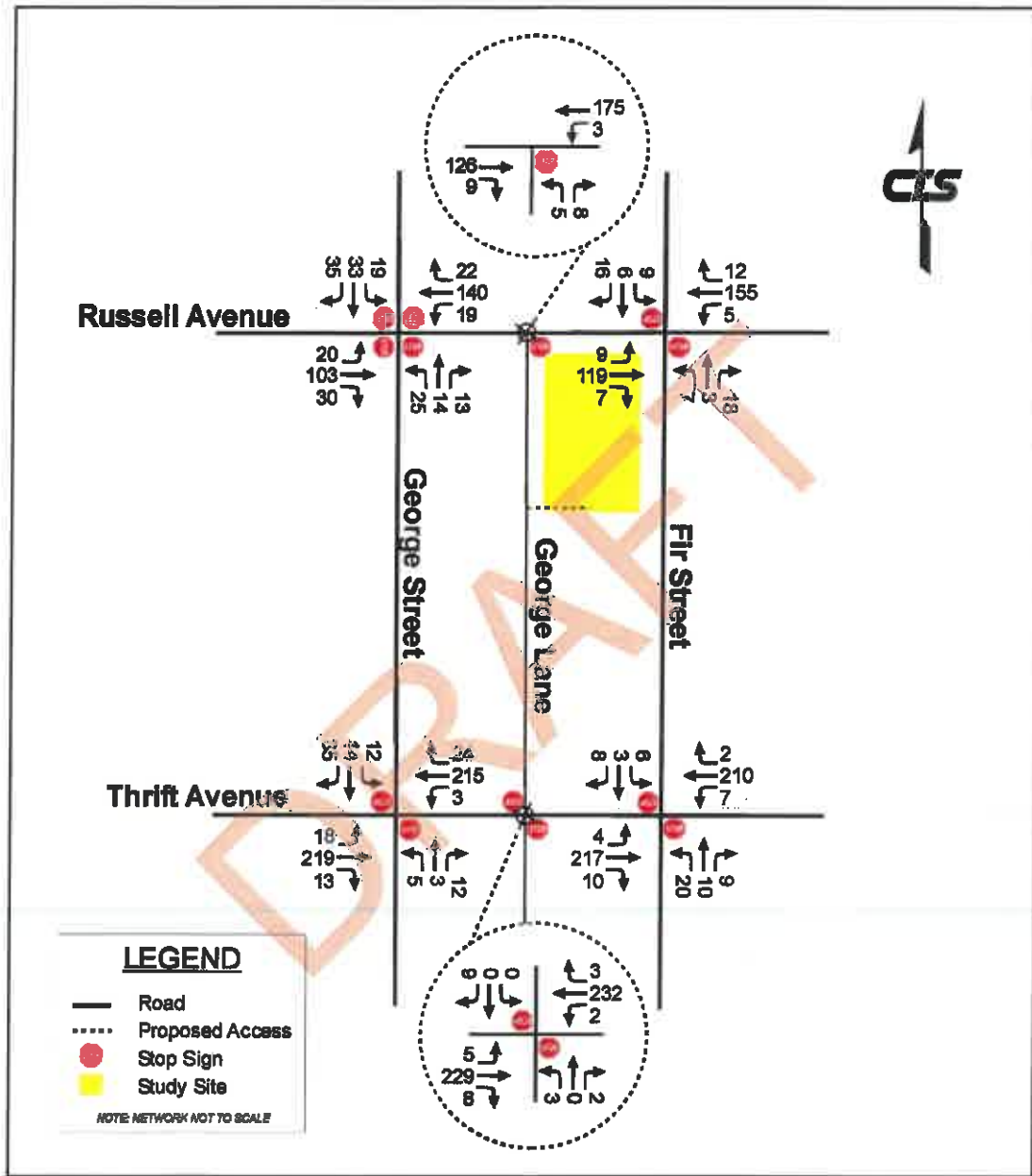


FIGURE 8
2019 WEEKDAY AFTERNOON PEAK HOUR BASE TRAFFIC VOLUMES



2.2 Future Base Traffic Volumes

The 2019 base volumes were factored up by a traffic volume growth rate of 2.0% to the 2022 and 2027 horizon years.

2022 Future Base Traffic Volumes

The expected build-out year for the proposed development is 2022. The 2019 base traffic volumes were factored up by an approved traffic volume growth rate of 2.0% per annum (simple-straight line) within the study network to represent the base 2022 traffic volumes.

FIGURE 9 and **FIGURE 10** illustrates the future base weekday morning and afternoon peak hour vehicle volumes for the year 2022.

2027 Future Base Traffic Volumes

2027 is five (5) years after the anticipated year of full buildout for the proposed development. The 2019 base traffic volumes were factored up by a traffic volume growth rate of 2.0% per annum (simple straight line) to represent the base 2027 volumes.

FIGURE 11 and **FIGURE 12** illustrates the future base weekday morning and afternoon peak hour vehicle volumes for the year 2027.

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FIGURE 9
2022 WEEKDAY MORNING PEAK HOUR BASE TRAFFIC VOLUMES

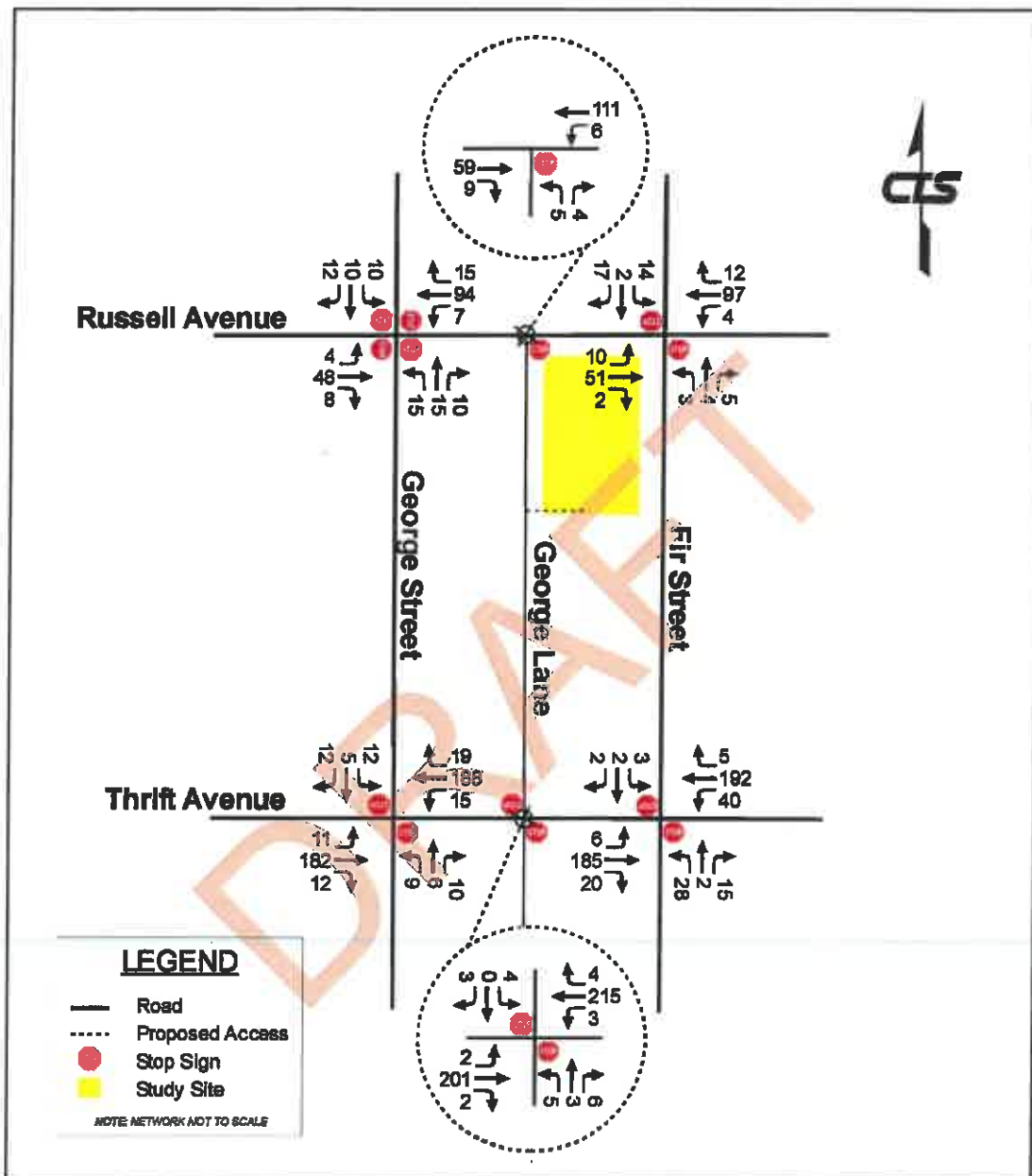


FIGURE 10
2022 WEEKDAY AFTERNOON PEAK HOUR BASE TRAFFIC VOLUMES

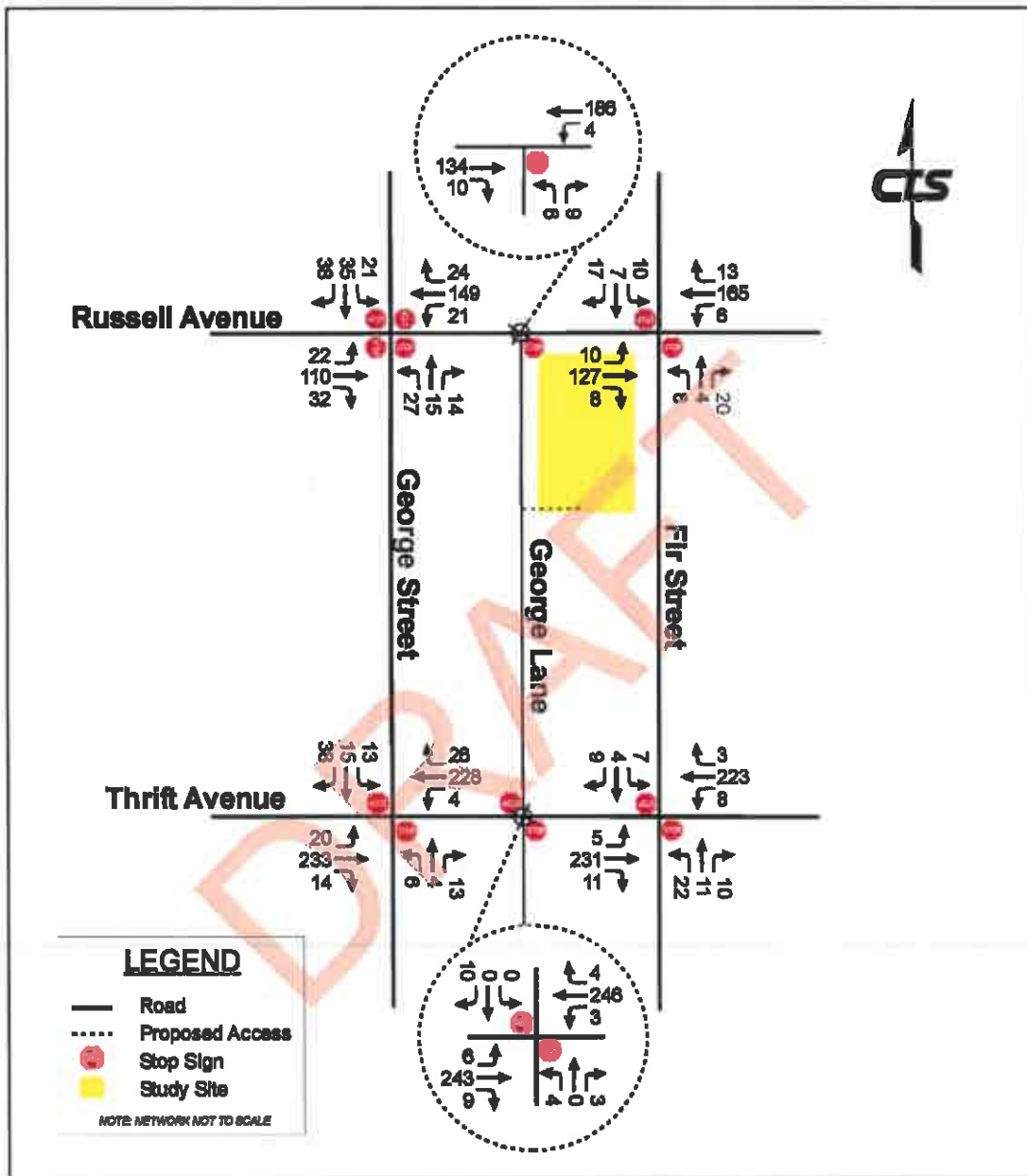


FIGURE 11
2027 WEEKDAY MORNING PEAK HOUR BASE TRAFFIC VOLUMES

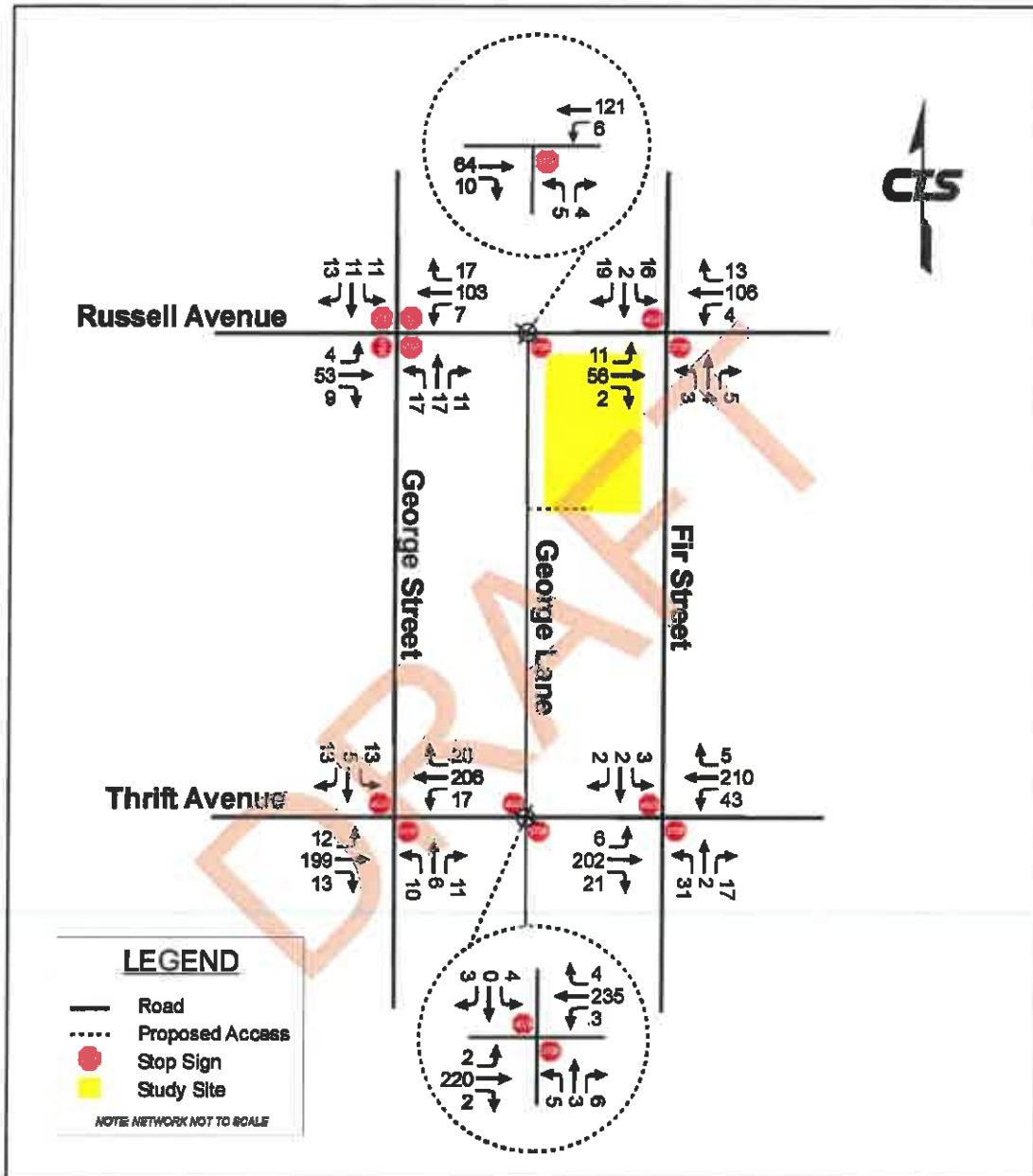
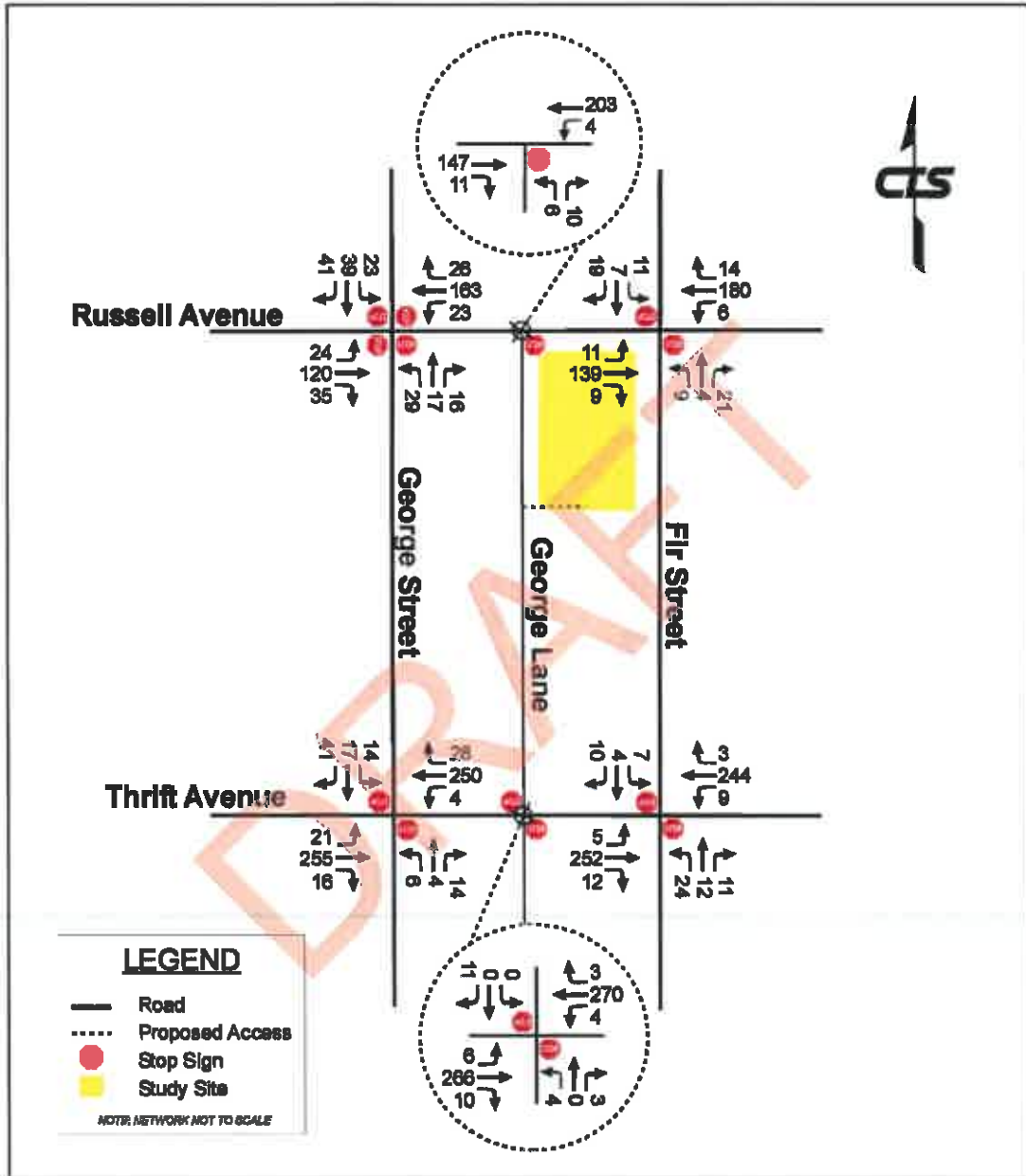


FIGURE 12
2027 WEEKDAY AFTERNOON PEAK HOUR BASE TRAFFIC VOLUMES



3.0 SITE TRAFFIC VOLUMES

3.1 Trip Generation

The published vehicle trip rates from *the Institute of Transportation Engineers (ITE) 10th Edition* were used to forecast the site generated traffic volumes. An apartment building with 21 rental dwelling units currently occupies the property. The proposed apartment development includes 80 rental dwelling units.

TABLE 1 summarizes the estimated site generated traffic for the existing apartment building as well as the forecast site generated traffic from the proposed development.

**TABLE 1
SUMMARY OF SITE GENERATED TRAFFIC**

Land Use	Peak Hour	Trip Generation Variable	Scope of Development	Vehicle Trip Generation Rate	Trip Rate Source	Directional Split		Peak Hour Volumes (vph)		
						% in	% out	in	out	total
Proposed Apartment Building - Multifamily Housing (Mid-Rise)	Weekday Morning	Dwelling Units	80	0.36	ITE 10th Edition - Code 221	26%	74%	8	21	29
	Weekday Afternoon			0.44		61%	39%	22	14	36
<i>Proposed Site Traffic</i>			Weekday Morning Peak Hour			26%	74%	8	21	29
			Weekday Afternoon Peak Hour			61%	39%	22	14	36
Existing Apartment Building - Multifamily Housing (Mid-Rise)	Weekday Morning	Dwelling Units	21	0.36	ITE 10th Edition - Code 221	26%	74%	2	6	8
	Weekday Afternoon			0.44		61%	39%	6	4	10
<i>Existing Site Traffic</i>			Weekday Morning Peak Hour			26%	74%	2	6	8
			Weekday Afternoon Peak Hour			60%	40%	6	4	10
<i>Net Site Traffic</i>			Weekday Morning Peak Hour			29%	71%	6	15	21
			Weekday Afternoon Peak Hour			62%	38%	16	10	26

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have three or more levels (floors). The Vehicle Trip Generation Rate was selected using the General Urban/Suburban setting.

From **TABLE 1**, the proposed development is forecasted to generate a total of 29 vehicle trips (8 inbound, 21 outbound) during the weekday morning peak hour and 36 vehicle trips (22 inbound, 14 outbound) during the weekday afternoon peak hour.

Subtracting the estimated site traffic generated by the existing apartment building, the net increase in site traffic from the proposed development is forecasted to be 21 vehicle trips (6 inbound, 15 outbound) during the weekday morning peak hour and 26 vehicle trips (16 inbound, 10 outbound) during the weekday afternoon peak hour. This is the equivalent of

one vehicle movement every 2.9 minutes during the weekday morning peak hour and one vehicle movement every 2.3 minutes during the weekday afternoon peak hour.

Note: The traffic analysis for the draft report was conducted under a previous site plan that proposed 84 rental dwelling units. As the number of dwelling units has only gone down by four (4) units, the traffic analysis in Section 3.2 and 5.0 was not redone as the analysis conducted simply represents a more conservative analysis.

3.2 Trip Distribution

The trip distribution parameters for distributing site generated vehicle trips to / from the site were developed from existing traffic patterns entering and exiting the study area. The traffic volume assignment is summarized in TABLE 2.

**TABLE 2
TRIP DISTRIBUTION VEHICLE VOLUMES
FOR SITE GENERATED TRAFFIC**

FROM/TO	WEEKDAY AM PEAK HOUR		WEEKDAY PM PEAK HOUR	
	INBOUND	OUTBOUND	INBOUND	OUTBOUND
George St (North)	0	1	2	1
Fir St (North)	0	1	1	0
Russell Ave (East)	1	2	4	2
Thrift Ave (East)	4	6	5	3
Fir St (South)	0	2	1	0
George Ln (South)	0	0	0	0
George St (South)	0	1	0	0
Thrift Ave (West)	2	6	6	5
Russell Ave (West)	1	4	4	3
TOTAL	8	23	23	14
	31		37	

The weekday morning and afternoon peak hour site generated traffic volumes of the proposed development for the build-out year of 2022 are illustrated in FIGURE 13 and FIGURE 14.

**FIGURE 13
WEEKDAY MORNING PEAK HOUR SITE TRAFFIC VOLUMES**

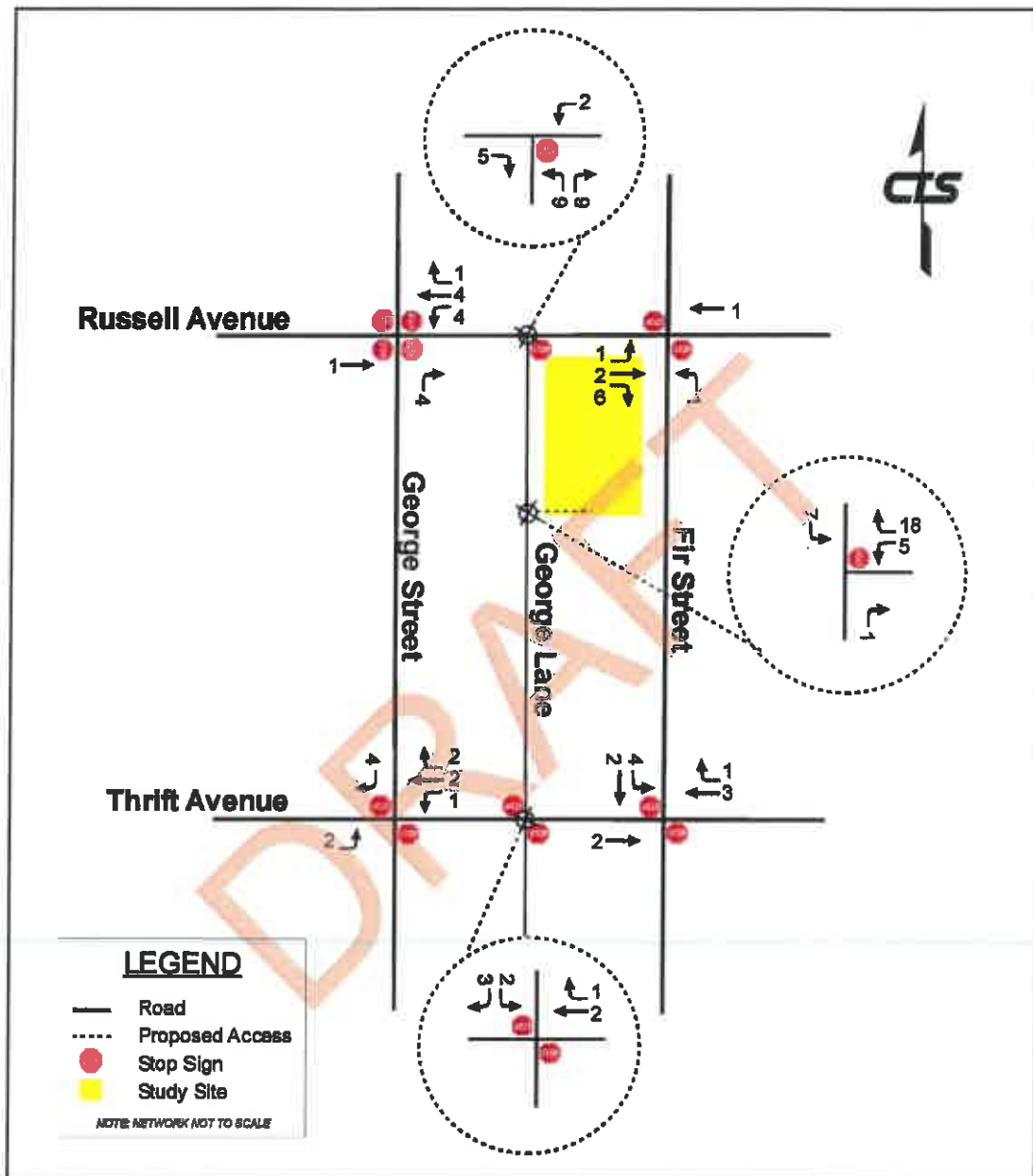
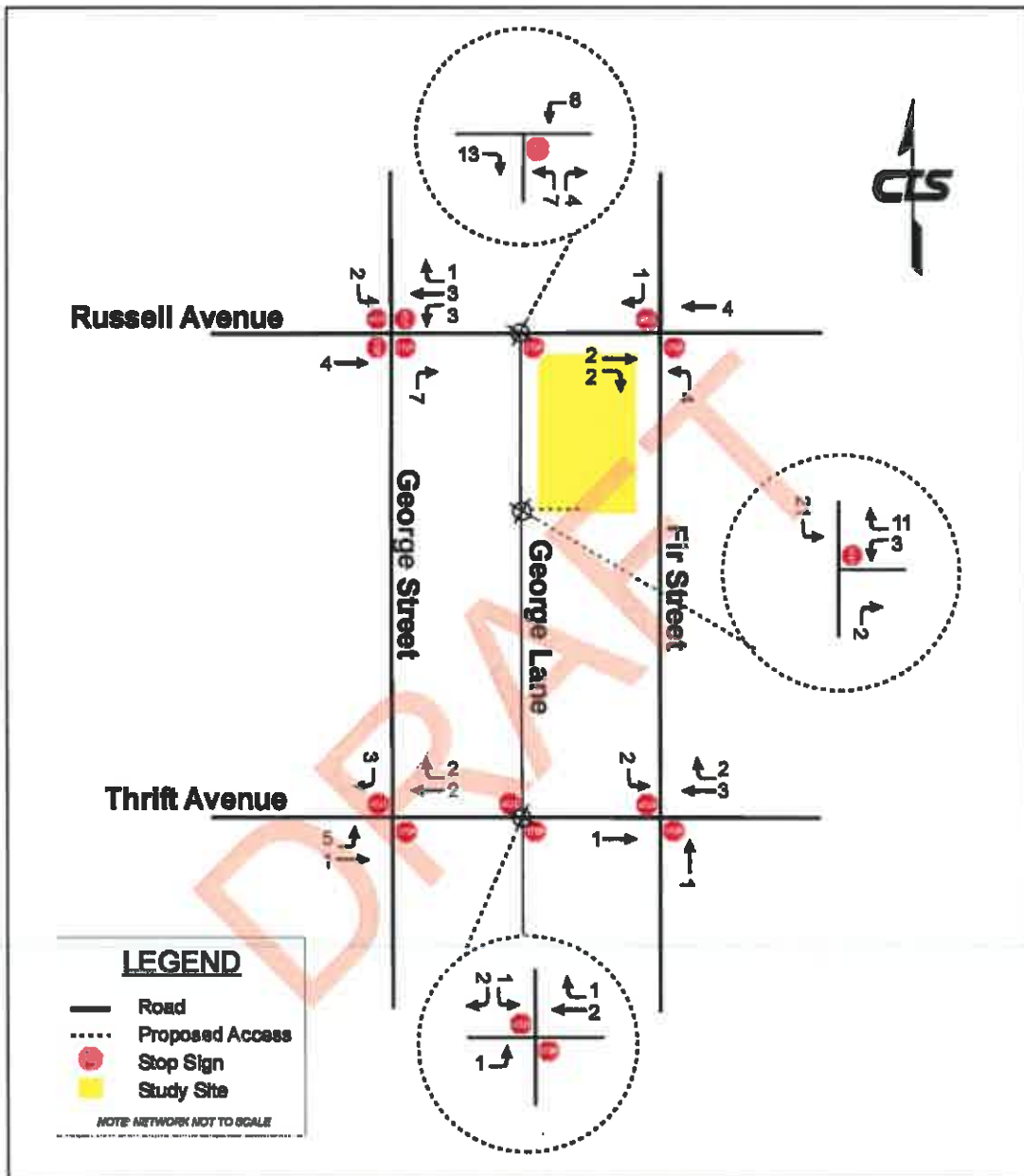


FIGURE 14
WEEKDAY AFTERNOON PEAK HOUR SITE TRAFFIC VOLUMES



4.0 BASE + SITE TRAFFIC VOLUMES

4.1 2022 Future Base + Site Traffic Volumes

The proposed development is anticipated to be fully built-out and occupied by the year 2022. The 2022 future base plus proposed development traffic volumes were calculated by first factoring up the 2019 base traffic volumes up by the approved growth rate of 2.0% per annum (simple-straight line) to the year 2022. The estimated traffic generated by the existing apartment building on the property were then subtracted from the 2022 base. Lastly, the forecast traffic generated by the proposed development were added to the 2020 base traffic volumes.

FIGURE 15 illustrates the total projected traffic for the 2022 weekday morning peak hour consisting of the future base (minus existing apartment building traffic) plus the proposed development site generated traffic.

FIGURE 16 illustrates the total projected traffic for the 2022 weekday afternoon peak hour consisting of the future base (minus existing apartment building traffic) plus the proposed development site generated traffic.

4.2 2027 Future Base + Site Traffic Volumes

The proposed development is anticipated to have been fully built-out and occupied for five years by the year 2027. The 2027 future base plus proposed development traffic volumes were calculated by first factoring up the 2019 base traffic volumes up by the approved growth rate of 2.0% per annum (simple-straight line) to the year 2027. The estimated traffic generated by the existing apartment building on the property were then subtracted from the 2027 base. Lastly, the forecast traffic generated by the proposed development were added to the 2020 base traffic volumes.

FIGURE 17 illustrates the total projected traffic for the 2027 weekday morning peak hour consisting of the future base (minus existing apartment building traffic) plus the proposed development site generated traffic.

FIGURE 18 illustrates the total projected traffic for the 2027 weekday afternoon peak hour consisting of the future base (minus existing apartment building traffic) plus the proposed development site generated traffic.

FIGURE 15
2022 WEEKDAY MORNING PEAK HOUR BASE + SITE TRAFFIC VOLUMES

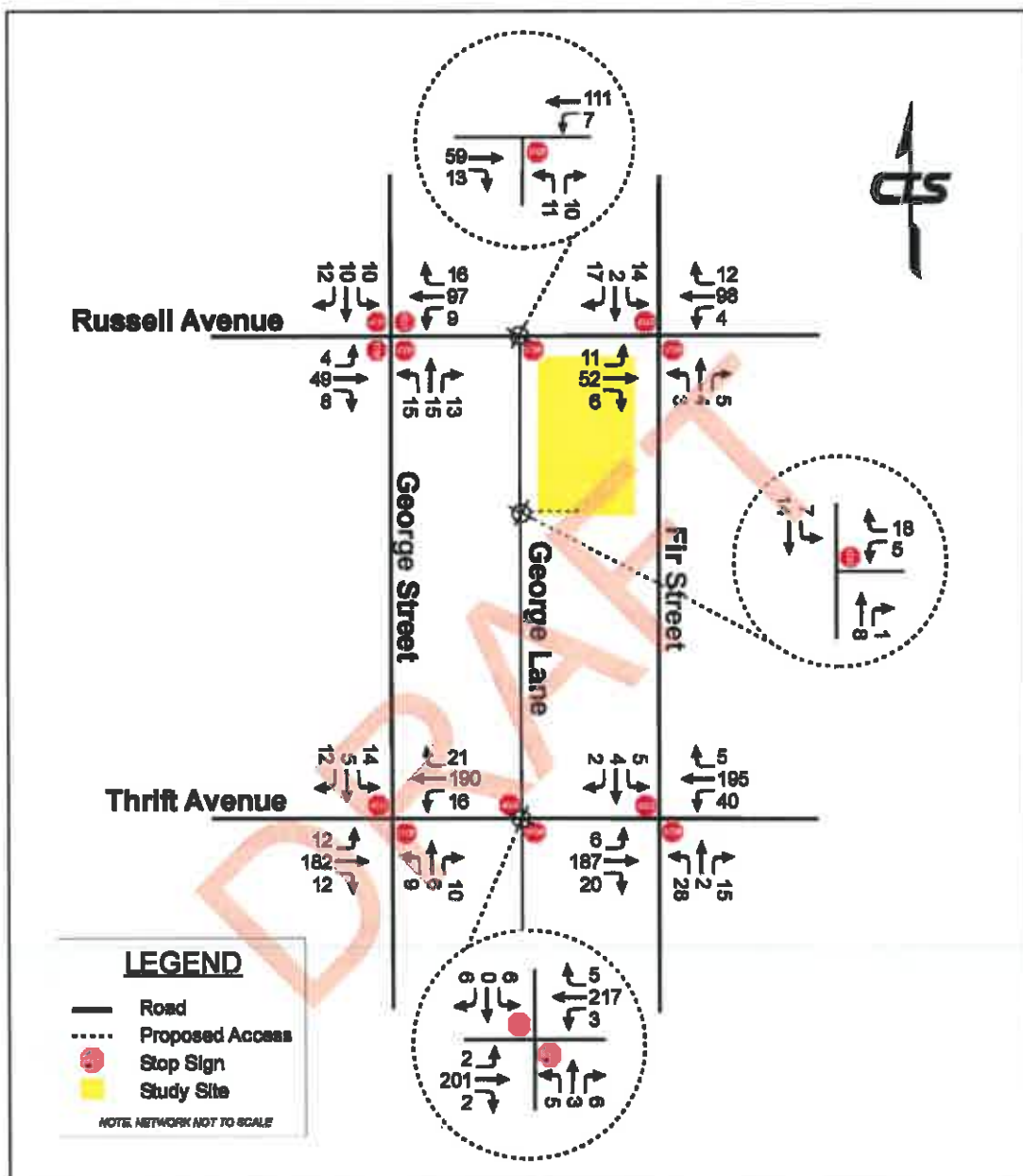


FIGURE 16
2022 WEEKDAY AFTERNOON PEAK HOUR BASE + SITE TRAFFIC VOLUMES

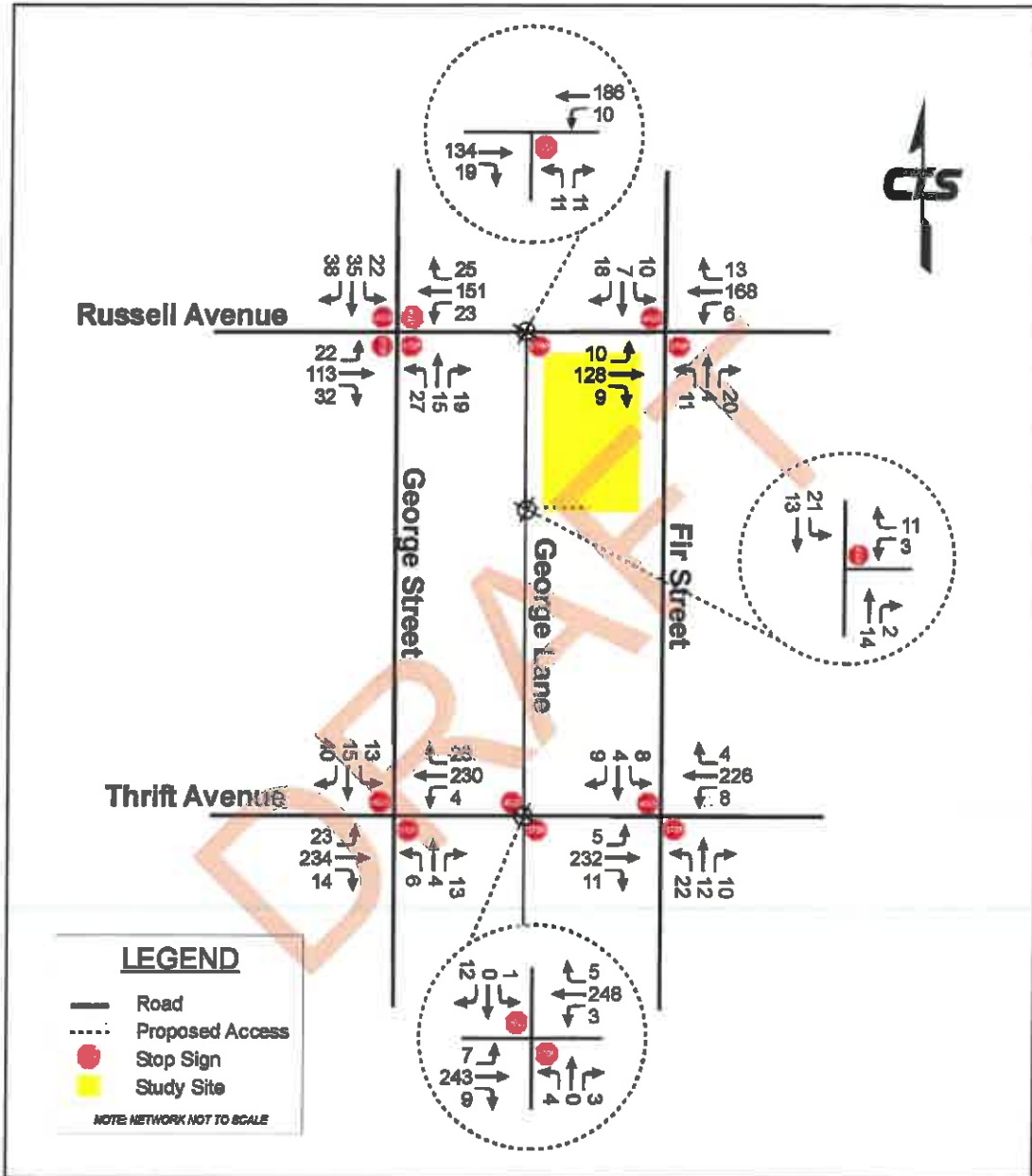


FIGURE 17
2027 WEEKDAY MORNING PEAK HOUR BASE + SITE TRAFFIC VOLUMES

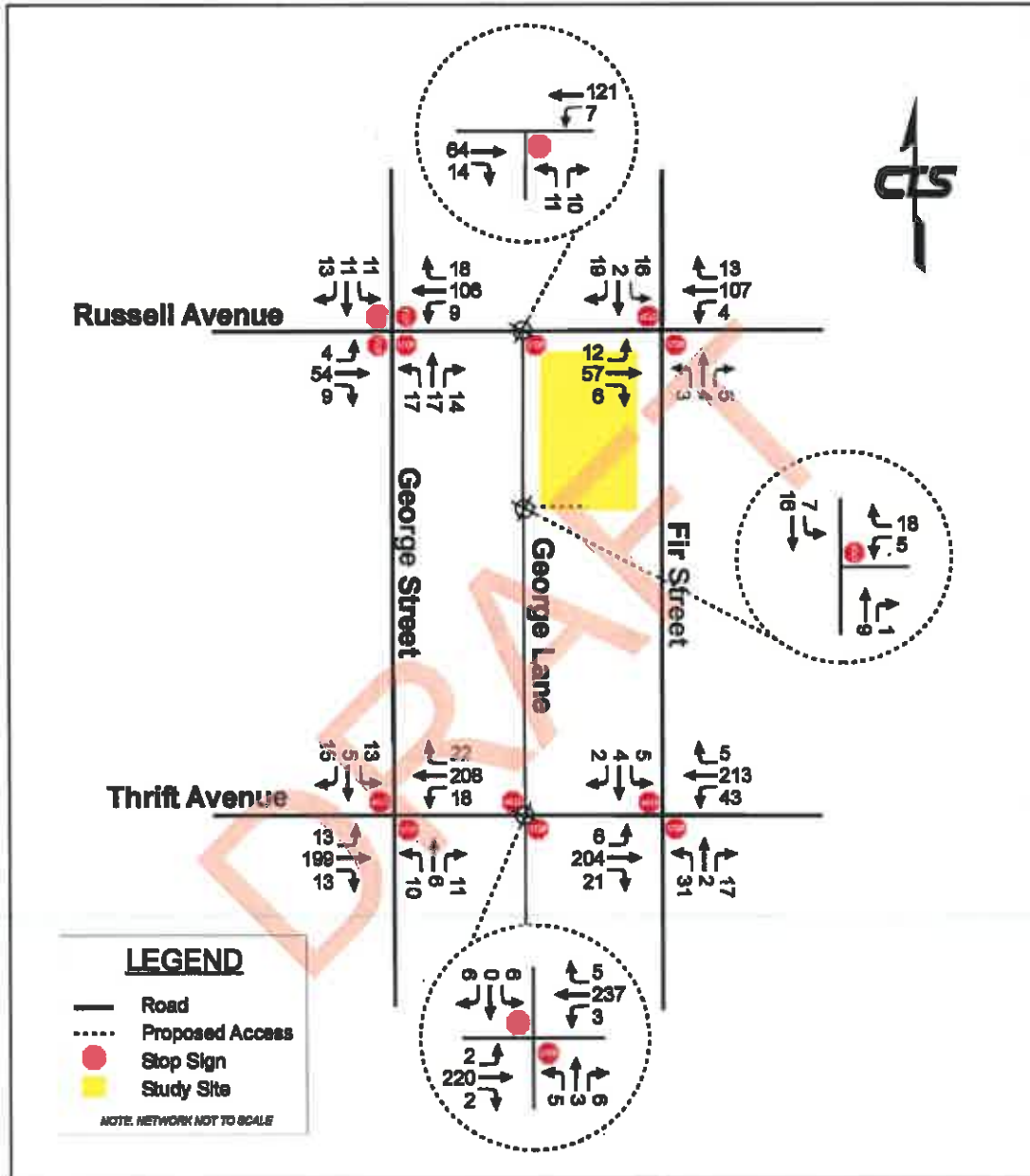
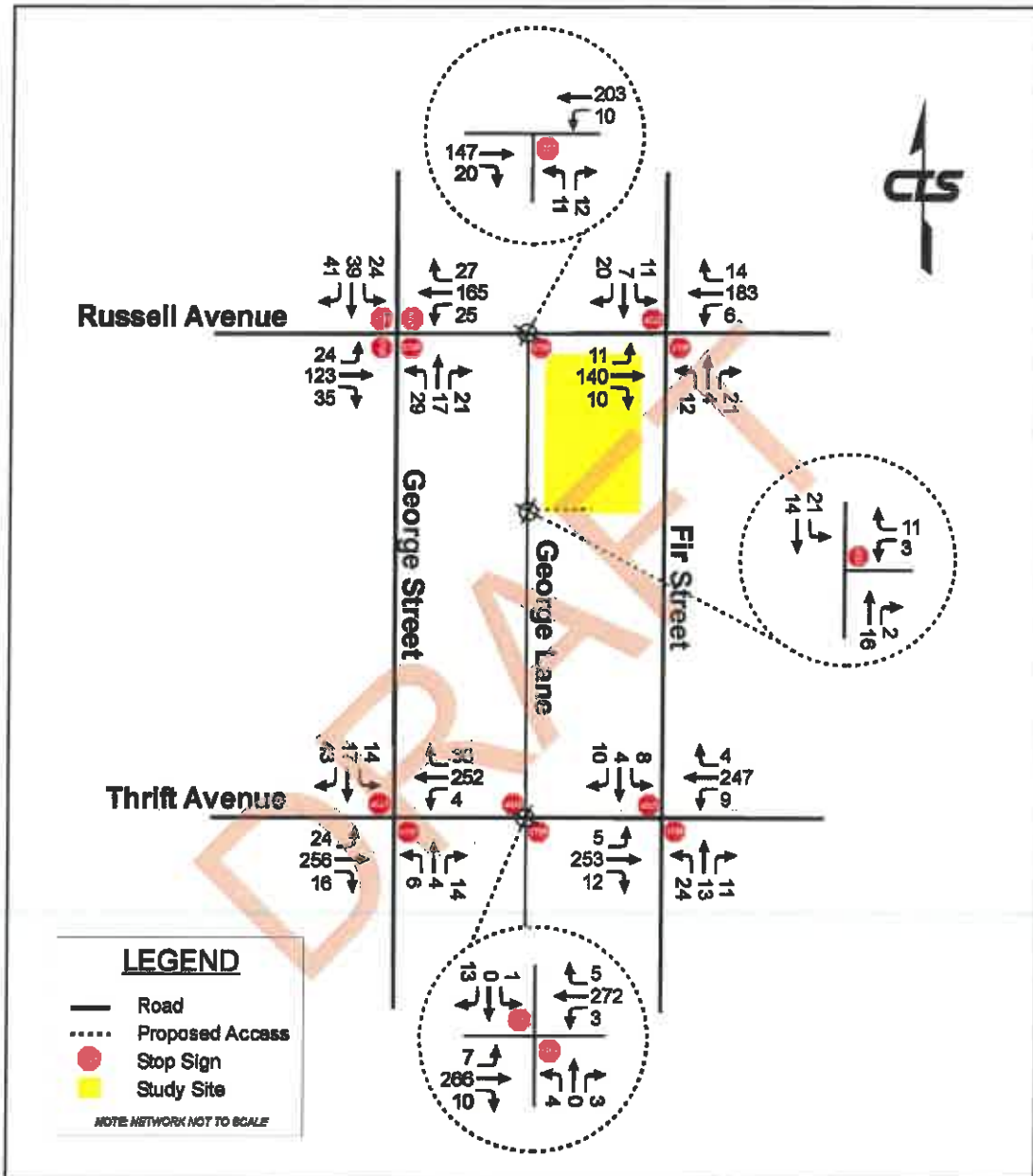


FIGURE 18
2027 WEEKDAY AFTERNOON PEAK HOUR BASE + SITE TRAFFIC VOLUMES



5.0 INTERSECTION CAPACITY ANALYSIS

5.1 Capacity Analysis

Capacity analysis was performed at each of the locations in order to determine the intersection levels of service (LOS) that is provided to motorists. The LOS for intersections and movements is defined in terms of delay (seconds per vehicle), which is a measure of driver discomfort and frustration, fuel consumption and lost travel time.

An intersection or movement LOS can range from "A" (Excellent) to "F" (Fail). See **TABLE 3**. A LOS of "F" (Fail) indicates that an intersection or movement is failing because the intersection or movement is over capacity and delays are considered excessive. A LOS of "D" during the critical peak hours is considered acceptable by many public agencies in large urban areas for overall intersection operation and a LOS of "E" or better is considered acceptable for left turn movements as it recognizes that the intersections normally perform much better the remaining 90% of the day.

**TABLE 3
LEVEL OF SERVICE DESCRIPTIONS**

Level of Service	Description
A	Excellent
B	Good
C	Fair
D	Poor
E	Very Poor
F	Fail

Highway Capacity Software (HCS 7.6) was used for the analysis of the unsignalized intersections.

The following assumptions were made with respect to the intersection capacity analysis:

- *Saturation flow rate* = 1,900 passenger cars/hour of green time/lane (pcphgpl)
- *Peak hour factor* (PHF) = 0.83 (weekday morning peak hour) and 0.87 (weekday afternoon peak hour) were the average factors observed from the surveyed intersections.
- Heavy vehicle percentage for roads = 2%

Saturation flow rate is the equivalent hourly rate at which previously queued vehicles can traverse an intersection approach under prevailing conditions, assuming that the green signal is available at all times and no lost times are experienced. It is a base rate to which adjustment factors are applied.

Peak Hour Factor is a measure of traffic demand fluctuation within the analysis hour. The closer the number is to 1.00, the less fluctuation during the hour.

TABLE 4 to TABLE 10 summarizes and compares the main performance parameters of the intersection capacity analysis for the unsignalized intersections.

For unsignalized intersections, the delay time in seconds for each lane group is summarized. Delay is additional travel time experienced by a driver, passenger, bicyclist, or pedestrian beyond that required to travel at the desired speed.

Wherever necessary, attempts at improvements have been made to maintain intersection and approach movement level of service standards for each of the post-development scenarios. The capacity analysis worksheets with level of services for each individual movement are included in **APPENDIX D**.

DRAFT

**TABLE 4
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
GEORGE STREET AT RUSSELL AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right		
George Street (N/S) at Russell Avenue (EW)	Weekday Morning Peak Hour	2019 Base	Volumes	3	45	7	6	88	14	14	14	9	9	9	11	A	Okay.
			Delay	7.8			7.8			7.8			7.4				
			95% Queue (veh)	0.2			0.5			0.2			0.1				
		2022 Base	Volumes	4	48	8	7	94	15	15	15	10	10	10	12	A	Okay.
			Delay	7.8			7.9			7.8			7.5				
			95% Queue (veh)	0.3			0.8			0.2			0.1				
		2027 Base	Volumes	4	53	9	7	103	17	17	17	11	11	11	13	A	Okay.
			Delay	7.7			8.1			7.7			7.6				
			95% Queue (veh)	0.3			0.8			0.2			0.2				
		2022 Base + Site	Volumes	4	49	8	9	97	16	15	15	13	10	10	12	A	Okay.
			Delay	7.6			8.0			7.6			7.5				
			95% Queue (veh)	0.3			0.8			0.2			0.1				
	2027 Base + Site	Volumes	4	54	9	9	108	18	17	17	14	11	11	13	A	Okay.	
		Delay	7.7			8.1			7.7			7.6					
		95% Queue (veh)	0.3			0.7			0.2			0.2					
	Weekday Afternoon Peak Hour	2019 Base	Volumes	20	108	30	19	140	22	25	14	13	19	33	38	A	Okay.
			Delay	8.7			8.9			8.3			8.4				
			95% Queue (veh)	0.8			1.0			0.3			0.4				
		2022 Base	Volumes	22	110	32	21	149	24	27	15	14	21	35	38	A	Okay.
			Delay	9.0			9.1			8.5			8.6				
			95% Queue (veh)	0.9			1.2			0.3			0.5				
		2027 Base	Volumes	24	120	36	23	163	28	29	17	16	23	39	41	A	Okay.
			Delay	9.3			9.7			8.7			8.8				
			95% Queue (veh)	1.1			1.3			0.3			0.6				
2022 Base + Site		Volumes	22	113	33	23	151	25	27	15	19	22	35	38	A	Okay.	
		Delay	9.0			9.4			8.6			8.7					
		95% Queue (veh)	1.0			1.2			0.3			0.5					
2027 Base + Site	Volumes	24	123	35	25	165	27	29	17	21	24	39	41	A	Okay.		
	Delay	9.4			9.8			8.7			8.9						
	95% Queue (veh)	1.1			1.4			0.4			0.6						

Delay = Average Delay (seconds/vehicle)
 Intersection approaching capacity (LOS 'D' or 'E'); or medium approach delays (25sec to <50sec)
 Intersection equals or exceeds capacity (LOS 'F'); or high approach delays (>= 50sec)

From TABLE 4, the following observations can be made:

George Street at Russell Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 5
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
GEORGE LANE AT RUSSELL AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHEBOUND			SOUTHBOUND			LOS	NOTES
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right		
George Lane (N/S) at Russell Avenue (EW)	Weekday Morning Peak Hour	2019 Base	Volumes	55	8	5	104		4		3					A	Okay.
			Delay	0.0			7.4			9.3							
			95% Queue (veh)	0.0			0.0			0.0							
		2022 Base	Volumes	59	9	8	111		5		4					A	Okay.
			Delay	0.0			7.4			8.4							
			95% Queue (veh)	0.0			0.0			0.0							
		2027 Base	Volumes	64	10	8	121		5		4					A	Okay.
			Delay	0.0			7.4			8.4							
			95% Queue (veh)	0.0			0.0			0.0							
		2022 Base + Site	Volumes	59	13	7	111		11		10					A	Okay.
			Delay	0.0			7.4			9.4							
			95% Queue (veh)	0.0			0.0			0.1							
	2027 Base + Site	Volumes	64	14	7	121		11		10					A	Okay.	
		Delay	0.0			7.4			9.5								
		95% Queue (veh)	0.0			0.0			0.1								
	Weekday Afternoon Peak Hour	2019 Base	Volumes	128	9	3	176		5		8				A	Okay.	
			Delay	0.0			7.7			10.1							
			95% Queue (veh)	0.0			0.0			0.1							
		2022 Base	Volumes	134	10	4	186		6		9				A	Okay.	
			Delay	0.0			7.7			10.3							
			95% Queue (veh)	0.0			0.0			0.1							
		2027 Base	Volumes	147	11	4	203		6		10				A	Okay.	
			Delay	0.0			7.7			10.4							
			95% Queue (veh)	0.0			0.0			0.1							
2022 Base + Site		Volumes	134	19	10	186		11		11				A	Okay.		
		Delay	0.0			7.7			10.7								
		95% Queue (veh)	0.0			0.0			0.1								
2027 Base + Site	Volumes	147	20	10	203		11		12				A	Okay.			
	Delay	0.0			7.8			10.8									
	95% Queue (veh)	0.0			0.0			0.1									

Delay = Average Delay (seconds/vehicle)
 Intersection approaching capacity (LOS D or E); or medium approach delay (25sec to <50sec)
 Intersection equals or exceeds capacity (LOS F); or high approach delay (>= 50sec)

From **TABLE 5**, the following observations can be made:

George Lane at Russell Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 6
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
FIR STREET AT RUSSELL AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right		
Fir Street (NB) at Russell Avenue (EW)	Weekday Morning Peak Hour	2019 Base	Volumes	9	48	1	3	93	11	2	3	4	13	1	16	A	Okay.
			Delay	7.6			7.4			9.7			9.8				
			95% Queue (veh)	0.0			0.0			0.0			0.1				
		2022 Base	Volumes	10	51	2	4	97	12	3	4	5	14	2	17	A	Okay.
			Delay	7.6			7.4			9.9			10.0				
			95% Queue (veh)	0.0			0.0			0.1			0.2				
		2027 Base	Volumes	11	66	2	4	108	13	3	4	5	16	2	19	A	Okay.
			Delay	7.6			7.4			10.0			10.1				
			95% Queue (veh)	0.0			0.0			0.1			0.2				
		2022 Base + Site	Volumes	11	62	6	4	98	12	3	4	5	14	2	17	A	Okay.
			Delay	7.6			7.4			9.9			10.0				
			95% Queue (veh)	0.0			0.0			0.1			0.2				
	2027 Base + Site	Volumes	12	67	6	4	107	13	3	4	5	16	2	19	A	Okay.	
		Delay	7.6			7.4			10.0			10.2					
		95% Queue (veh)	0.0			0.0			0.1			0.2					
	Weekday Afternoon Peak Hour	2019 Base	Volumes	9	119	7	5	155	12	7	3	18	9	6	16	A	Okay.
			Delay	7.8			7.7			10.7			11.4				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
		2022 Base	Volumes	10	127	8	6	165	13	8	4	20	10	7	17	A	Okay.
			Delay	7.9			7.7			11.0			11.7				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
		2027 Base	Volumes	11	139	9	6	180	14	9	4	21	11	7	19	A	Okay.
			Delay	7.9			7.7			11.3			12.0				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
2022 Base + Site		Volumes	10	128	9	6	168	13	11	4	20	10	7	18	A	Okay.	
		Delay	7.9			7.7			11.3			11.7					
		95% Queue (veh)	0.0			0.0			0.2			0.2					
2027 Base + Site	Volumes	11	140	10	6	189	14	12	4	21	11	7	20	A	Okay.		
	Delay	7.9			7.7			11.8			12.0						
	95% Queue (veh)	0.0			0.0			0.2			0.3						

Delay = Average Delay (seconds/veh)
 Intersection approaching capacity (LOS D' or E'); or medium approach delays (25sec to <30sec)
 Intersection equals or exceeds capacity (LOS F); or high approach delays (>= 50sec)

From **TABLE 6**, the following observations can be made:

Fir Street at Russell Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 7
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
FIR STREET AT THRIFT AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES				
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right						
Fir Street (N/S) at Thrift Avenue (E/W)	Weekday Morning Peak Hour	2019 Base	Volumes	5	174	18	37	181	4	26	11	14	2	1	1	A	Okay.				
			Delay	7.8			7.8			13.1			13.2								
			95% Queue (veh)	0.0			0.1			0.3			0.0								
		2022 Base	Volumes	6	185	20	40	182	5	28	2	15	3	2	2			A	Okay.		
			Delay	7.9			7.9			13.8			13.6								
			95% Queue (veh)	0.0			0.1			0.4			0.1								
		2027 Base	Volumes	6	202	21	43	210	5	31	2	17	3	2	2					A	Okay.
			Delay	7.9			8.0			14.6			14.3								
			95% Queue (veh)	0.0			0.1			0.5			0.1								
		2022 Base + Site	Volumes	6	187	20	40	195	5	28	2	15	5	4	2	A	Okay.				
			Delay	7.9			7.9			13.9			14.4								
			95% Queue (veh)	0.0			0.1			0.4			0.1								
	2027 Base + Site	Volumes	6	204	21	43	213	5	31	2	17	5	4	2	A			Okay.			
		Delay	7.9			8.0			14.8			15.1									
		95% Queue (veh)	0.0			0.1			0.5			0.1									
	Weekday Afternoon Peak Hour	2019 Base	Volumes	4	217	10	7	210	2	20	10	9	6	3					8	A	Okay.
			Delay	7.9			7.8			13.6			12.2								
			95% Queue (veh)	0.0			0.0			0.3			0.1								
		2022 Base	Volumes	5	231	11	8	223	3	22	11	10	7	4		9	A		Okay.		
			Delay	7.9			7.9			14.2			12.7								
			95% Queue (veh)	0.0			0.0			0.4			0.1								
		2027 Base	Volumes	5	252	12	8	244	3	24	12	11	7	4	10	A		Okay.			
			Delay	7.9			8.0			15.1			13.1								
			95% Queue (veh)	0.0			0.0			0.5			0.2								
2022 Base + Site		Volumes	5	232	11	8	228	4	22	12	10	8	4	9	A					Okay.	
		Delay	7.9			7.9			14.3			12.9									
		95% Queue (veh)	0.0			0.0			0.4			0.2									
2027 Base + Site	Volumes	5	253	12	9	247	4	24	13	11	8	4	10	A			Okay.				
	Delay	8.0			8.0			16.2			13.4										
	95% Queue (veh)	0.0			0.0			0.5			0.2										

Delay = Average Delay (seconds/vehicle)
 Intersection approaching capacity (LOS D or E); or medium approach delays (25sec to <80sec)
 Intersection equals or exceeds capacity (LOS F); or high approach delays (>= 80sec)

From TABLE 7, the following observations can be made:

Fir Street at Thrift Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 8
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
GEORGE LANE AT THRIFT AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right		
George Lane (NB) at Thrift Avenue (EW)	Weekday Morning Peak Hour	2019 Base	Volumes	1	188	1	2	202	0	4	2	8	3	0	2	A	Okay.
			Delay	7.8			7.7			11.4			11.8				
			95% Queue (veh)	0.0			0.0			0.1			0.0				
		2022 Base	Volumes	2	201	2	3	215	4	5	3	6	4	0	3	A	Okay.
			Delay	7.9			7.8			11.9			12.1				
			95% Queue (veh)	0.0			0.0			0.1			0.0				
		2027 Base	Volumes	2	220	2	3	235	4	5	3	6	4	0	3	A	Okay.
			Delay	8.0			7.8			12.3			12.6				
			95% Queue (veh)	0.0			0.0			0.1			0.1				
		2022 Base + Site	Volumes	2	201	2	3	217	5	5	3	6	4	0	3	A	Okay.
			Delay	7.9			7.8			11.9			11.9				
			95% Queue (veh)	0.0			0.0			0.1			0.1				
	2027 Base + Site	Volumes	2	220	2	3	237	5	5	3	6	4	0	3	A	Okay.	
		Delay	8.0			7.8			12.4			12.3					
		95% Queue (veh)	0.0			0.0			0.1			0.1					
	Weekday Afternoon Peak Hour	2019 Base	Volumes	5	229	8	2	232	3	3	0	2	0	0	8	A	Okay.
			Delay	8.0			7.9			12.5			10.2				
			95% Queue (veh)	0.0			0.0			0.0			0.0				
		2022 Base	Volumes	6	248	8	3	248	4	4	0	3	0	0	10	A	Okay.
			Delay	8.1			8.0			12.9			10.4				
			95% Queue (veh)	0.0			0.0			0.1			0.1				
		2027 Base	Volumes	6	268	10	3	270	4	4	0	3	0	0	11	A	Okay.
			Delay	8.1			8.1			13.5			10.8				
			95% Queue (veh)	0.0			0.0			0.1			0.1				
2022 Base + Site		Volumes	7	243	8	3	248	5	4	0	3	1	0	12	A	Okay.	
		Delay	8.1			8.0			12.9			10.8					
		95% Queue (veh)	0.0			0.0			0.1			0.1					
2027 Base + Site	Volumes	7	268	10	3	272	5	4	0	3	1	0	13	A	Okay.		
	Delay	8.1			8.1			13.5			11.0						
	95% Queue (veh)	0.0			0.0			0.1			0.1						

Delay = Average Delay (seconds/vehicle)
 Intersection approaching capacity (LOS D' or E'); or medium approach delays (25sec to <50sec)
 Intersection equals or exceeds capacity (LOS F); or high approach delays (>= 60sec)

From TABLE 8, the following observations can be made:

George Lane at Thrift Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 9
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
GEORGE STREET AT THRIFT AVENUE**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right		
George Street (N/S) at Thrift Avenue (EW)	Weekday Morning Peak Hour	2019 Base	Volumes	10	171	11	14	177	17	8	5	9	11	4	11	A	Okay.
			Delay	7.8			7.7			12.1			12.4				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
		2022 Base	Volumes	11	182	12	15	188	19	9	6	10	12	5	12	A	Okay.
			Delay	7.9			7.8			12.5			12.9				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
		2027 Base	Volumes	12	199	13	17	206	20	10	6	11	13	5	13	A	Okay.
			Delay	7.9			7.8			13.1			13.5				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
		2022 Base + Site	Volumes	12	182	12	16	190	21	9	6	10	12	5	14	A	Okay.
			Delay	7.9			7.8			12.6			12.8				
			95% Queue (veh)	0.0			0.0			0.2			0.2				
	2027 Base + Site	Volumes	13	199	13	18	208	22	10	6	11	13	5	15	A	Okay.	
		Delay	7.9			7.8			13.2			13.4					
		95% Queue (veh)	0.0			0.1			0.2			0.3					
	Weekday Afternoon Peak Hour	2019 Base	Volumes	18	219	13	3	215	24	5	3	12	12	14	36	A	Okay.
			Delay	8.0			7.9			12.6			13.5				
			95% Queue (veh)	0.1			0.0			0.1			0.6				
		2022 Base	Volumes	20	233	14	4	226	26	6	4	13	13	15	36	A	Okay.
			Delay	8.1			7.9			13.3			14.0				
			95% Queue (veh)	0.1			0.0			0.2			0.6				
		2027 Base	Volumes	21	255	16	4	250	28	6	4	14	14	17	41	A	Okay.
			Delay	8.2			8.0			13.8			15.0				
			95% Queue (veh)	0.1			0.0			0.2			0.7				
2022 Base + Site		Volumes	23	254	14	4	230	28	6	4	13	13	15	40	A	Okay.	
		Delay	8.1			7.9			13.4			14.1					
		95% Queue (veh)	0.1			0.0			0.2			0.6					
2027 Base + Site	Volumes	24	256	16	4	252	30	6	4	14	14	17	43	A	Okay.		
	Delay	8.2			8.0			14.0			15.1						
	95% Queue (veh)	0.1			0.0			0.2			0.7						

Delay = Average Delay (seconds/veh/htg)
 Intersection approaching capacity (LOS D or E); or medium approach delays (25sec to <50sec)
 Intersection equals or exceeds capacity (LOS F); or high approach delays (>= 50sec)

From **TABLE 9**, the following observations can be made:

George Thrift at Thrift Avenue:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

**TABLE 10
CAPACITY ANALYSIS FOR UNSIGNALIZED INTERSECTION
GEORGE LANE AT SITE ACCESS**

INTERSECTION	TIME OF DAY	SCENARIO	PERFORMANCE MEASURE	EASTBOUND			WESTBOUND			NORTHBOUND			SOUTHBOUND			LOS	NOTES			
				Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right					
George Lane (NS) at Site Access (EW)	2022 Base + Site	Volumes				5		18			0	1	7	14	A	Okay.				
			Delay					8.6				0.0		7.3						
				95% Queue (veh)					0.1				0.0				0.0			
		2027 Base + Site	Volumes				5		18			8	1	7			16	A	Okay.	
				Delay					8.6				0.0				7.3			
					95% Queue (veh)					0.1				0.0						0.0
	2022 Base + Site	Volumes				3		11			14	2	21	13	A	Okay.				
			Delay					8.6				0.0		7.3						
				95% Queue (veh)					0.0				0.0				0.0			
		2027 Base + Site	Volumes				3		11			16	2	21			14	A	Okay.	
				Delay					8.6				0.0				7.3			
					95% Queue (veh)					0.0				0.0						0.0

Delay = Average Delay (seconds/vehicle)
 Intersection approaching capacity (LOS 'D' or 'E'); or medium approach delays (25sec to <30sec)
 Intersection equals or exceeds capacity (LOS 'F'); or high approach delays (>= 30sec)

From **TABLE 10**, the following observations can be made:

George Lane at Site Access:

- During the weekday morning peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.
- During the weekday afternoon peak hour:
 - The intersection is forecast to continue to operate at LOS A (Excellent) for all horizon years and scenarios.

Capacity Analysis Conclusion:

The capacity analyses showed that all of the study intersections are anticipated to continue to operate at level of service A (Excellent) in all of the analyzed horizon years with the proposed development traffic included. No geometric changes are required in order to accommodate the traffic volume growth anticipated within this study.

6.0 2045 LINK VOLUMES

The City of White Rock requested that estimates for the 2045 traffic volumes within the study network be made in order to provide a point of reference for the City of White Rock 2045 OCP.

As this scenario is 26 years into the future, it is difficult to accurately forecast vehicle volumes in the context of intersection analysis. Therefore, peak hour road link volumes were determined to provide an estimated magnitude of vehicle volumes.

FIGURE 19 and **FIGURE 20** show the estimated 2-way link volumes for the morning and afternoon peak hour based on an approved growth rate of 2.0% per annum (simple-straight line) factored up from the 2019 turning movement counts with the proposed site traffic included.

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FIGURE 19
2045 WEEKDAY MORNING PEAK HOUR LINK VOLUMES

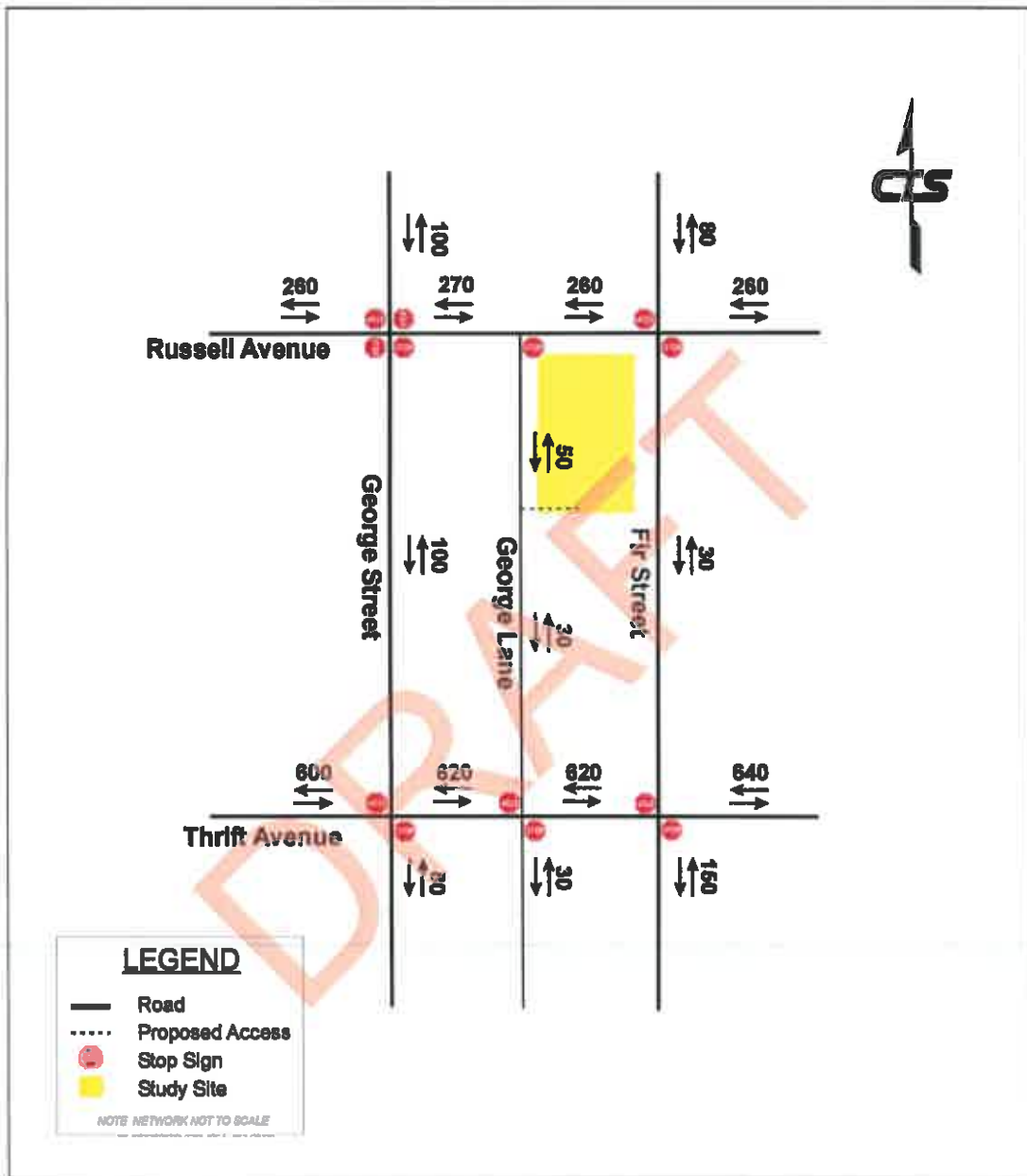
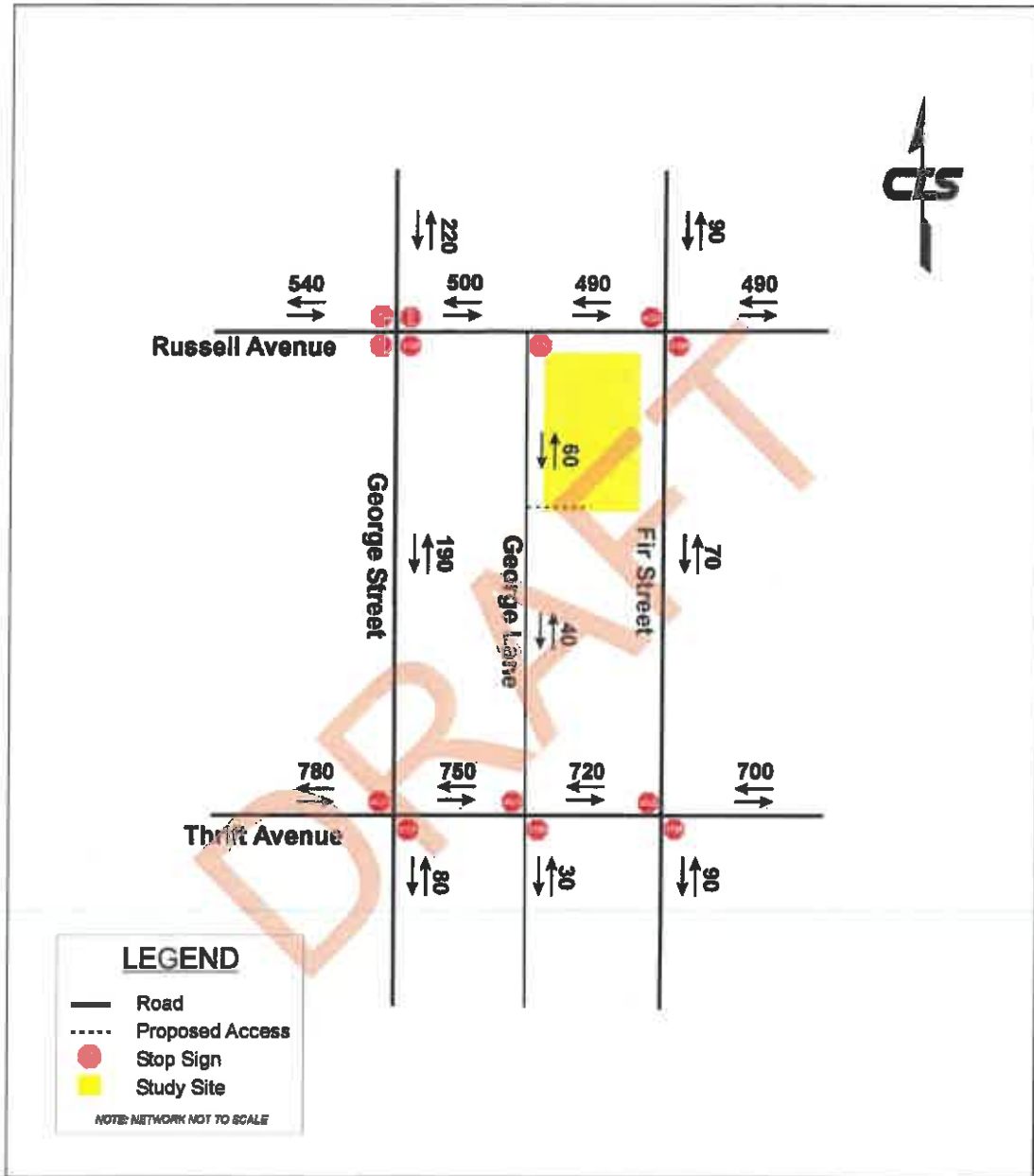


FIGURE 20
2045 WEEKDAY AFTERNOON PEAK HOUR LINK VOLUMES



7.0 PARKING REVIEW

7.1 Parking Requirements

7.1.1 Vehicle Parking Requirements

The required parking spaces for the proposed development are summarized in **TABLE 11** with reference to the *City of White Rock Zoning Bylaw Section 4: General Provisions & Regulations*.

**TABLE 11
BYLAW VEHICLE PARKING SPACE REQUIREMENTS**

Land Use Bylaw Classification	Parking Space Type	Required Parking Rate	# of Units	Parking Stalls Required
Table 4.14.1 - Apartment	Residential Spaces	1.2 per Dwelling Unit	80	96
Table 4.14.1 - Apartment	Visitor Spaces	0.3 per Dwelling Unit	80	24
Total Required Parking Spaces				120

As shown in **TABLE 11** above, the total number of required parking spaces for the proposed development is 120 parking spaces – 96 parking spaces for residents and 24 parking spaces for visitors. The proposed development is providing a total of 108 parking spaces – 84 parking spaces for residents and 24 parking spaces for visitors, resulting in a variance of 12 parking spaces, or 10%.

With reference to the *City of White Rock Zoning Bylaw Section 4.14.9*, 40% of the total parking provided may be small car spaces, resulting in an allowance of 43 small car spaces within the proposed development. The proposed development is providing a total of 33 small car parking spaces.

With reference to the *City of White Rock Zoning Bylaw Section 4.14.6*, two (2) handicapped / accessible parking spaces are required of the proposed development. The proposed development is providing a total of three (3) handicapped / accessible parking spaces – two (2) parking spaces for residents and one (1) parking space for visitors.

With reference to the *City of White Rock Zoning Bylaw Section 4.17.1*, "a minimum of 1 of every 10 off-street parking spaces shall feature an energized outlet capable of providing Level 2 charging...[and] an additional 1 of every 10 off-street parking spaces shall feature roughed-in electric vehicle charging infrastructure". The proposed development will be exceeding the bylaw requirement in its provision of 24 electric vehicle charging stations.

7.1.2 Bicycle Parking Requirements

The required bicycle parking spaces for the proposed development are summarized in **TABLE 12** with reference to the *City of White Rock Zoning Bylaw Section 4: General Provisions & Regulations*.

**TABLE 12
BYLAW BICYCLE PARKING SPACE REQUIREMENTS**

Land Use Bylaw Classification	Bicycle Space Type	Required Bicycle Space Rate	# of Units	Bicycle Space Required
Table 4.16.3 - Apartment	Class I	1 per Dwelling Unit	80	80
Table 4.16.3 - Apartment	Class II	0.2 per Dwelling Unit	80	16
Total Required Parking Spaces				96

As shown in **TABLE 12** above, the total number of required bicycle parking spaces for the proposed development is 96 bicycle parking spaces – 80 Class I parking spaces and 16 Class II parking spaces. The proposed development is exceeding the bylaw requirement in its provision of 90 Class I parking spaces and 16 Class II parking spaces.

7.1.3 Loading Space Requirements

With reference to the *City of White Rock Zoning Bylaw Section 4.15.2 and 4.15.3*, one (1) off-street loading space is required. The propose development meets the bylaw requirement in its provision of one (1) loading space accessed via George Lane as per the architectural drawings attached in **APPENDIX A**.

7.2 Parking Variance

To consider the 10% parking variance, the proposed land uses, existing nearby amenities and infrastructure, and opportunities for alternative modes of travel were considered.

7.2.1 City of White Rock Policy

With reference to the City of White Rock 2045 OCP, Objective 11.2 is “to support rental housing and a range of non-market housing options and needs along the housing spectrum”. This proposed rental apartment development aligns with the City’s OCP objectives and a parking relaxation should be considered given Policy 11.2.1 g) recommends “reviewing parking requirements to determine the extent to which they can be relaxed for non-market and rental housing within walking distance of frequent transit service and / or commercial areas.

The propose rental apartment development is only a six (6) minute walk to White Rock Centre, which connects to Translink’s Frequent Transit Network.

7.2.2 Adjacent Land Uses and Amenities

As previously noted in **Section 1.0**, the site is conveniently located near amenities and public transit. The following attractions and destinations are all approximately a five (5) to fifteen (15) minute walk from the study site:

- Semiahmoo Shopping Centre
- Peace Arch Hospital
- Commercial / retail developments all along Johnston Road
- White Rock Centre transit exchange
- Earl Marriott Secondary School
- Peach Arch Elementary
- Kent Street Activity Centre

7.2.2 Sustainable Transportation

The study area has good connectivity to transit, as well as cycling and pedestrian infrastructure as noted previously in **Section 1.3**. The White Rock Centre transit exchange is only a six (6) minute walk from the site, resulting in a total of nine (9) bus routes servicing the study network. Within the adjacent road network, Thrift Avenue is a shared lane bike route. Martin Street, Best Street, and Finlay Street are also shared lane bike routes. The study area is well connected with sidewalks. All roads within the study area have a sidewalk on at least one side.

7.2.3 ITE Peak Parking Demand

In order to consider the peak parking demand of the proposed development, the Institute of Transportation Engineers (ITE) Parking Generation Manual 5th Edition is referenced.

The parking generation manual contains observed data for common land uses, along with an average peak parking demand based on variables such as gross floor area, number of dwelling units, or number of bedrooms.

Land Use Code 221 – Multi-family Housing (Mid-Rise), provides data that represents multi-family developments, that include apartments, townhouses, and condominiums located within the same building, and are between three and ten levels (floor) of residence. The peak period of parking demand occurs between 22:00 to 05:00.

For the parking demand analysis, CTS only considered data from the general urban/sub-urban scenario. General urban/sub-urban areas are associated with almost homogenous vehicle centred access. Although the proposed development is located in an area with good alternative transportation infrastructure, this setting is applied as it describes the City of White Rock as a whole.

With reference to the ITE Parking Generation Manual 5th Edition, the average parking demand of the proposed development is summarized in **TABLE 13** below.

**TABLE 13
ITE PEAK PARKING DEMAND**

Land Use Description	Land Use Code	Period	Average Rate as per ITE Parking Generation Se	Scope	Unit Of Measurement	Average Parking Demand
Multi Family (Mid-Rise)	222	Weekday	1.31 Per Dwelling Unit	80	Dwelling Units	104.8
Total Required Parking Spaces						104.8

As illustrated in **TABLE 13** above, if the average peak parking demand rate is applied to the proposed development, the average peak parking demand is forecasted to be approximately 105 parked vehicles. The proposed development is providing 108 parking spaces, exceeding the ITE peak parking demand by three (3) parking spaces.

7.2.4 Transportation Demand Management Initiatives

7.2.4.1 Pedestrian Facilities and Initiatives

Walking is the primary mode of transportation for nearly everyone whether linking with cycling, transit or vehicle modes. People will generally walk for up to fifteen minutes or within a distance of 400 to 800 meters (five to ten minute walk) to connect with another mode or access local amenities.

To encourage transportation by walking, the applicant has committed to providing the following initiatives:

- Directional signage within the lobby or at the main entrance for the nearest bus stops and estimated walking times to popular destinations

7.2.4.2 Bicycle Facilities and Initiatives

People will generally travel by bicycle up to five kilometers to their place of work, for recreation, or personal reasons.

To encourage transportation by bicycling, the applicant has committed to providing the following initiatives:

- Residents will receive a welcome package containing transit and cycling information.
- A bicycle repair station with including a tire pump will be provided within the parkade.
- Extra hooks for helmets to be locked to will be included in the bicycle lockers for convenience
- Many of the Class I bicycle parking spaces are located on P1 and P2 with direct access to the elevators. The rest of the Class I bicycle parking spaces on P2 have convenient access to the elevators.
- Residents will receive a \$30 gift card towards a local bicycle shop.
- All of the sixteen (16) Class II bicycle parking spaces are located at street level next to the main entrance.
- Gently sloped ramps leading up to the main entrance for cyclist comfort.

7.2.4.3 Transit Facilities and Initiatives

To encourage the use of public transit, the applicant has committed to providing the following initiatives:

- Directional signage within the lobby or at the main entrance for the nearest bus stops and estimated walking times to popular destinations.
- Residents will receive a pre-loaded compass card (e.g. \$100).
- Residents will receive a welcome package containing transit and cycling information.
- A live screen within the lobby displaying current bus route schedules

7.2.4.4 Vehicle Facilities and Initiatives

To encourage more sustainable transportation, the applicant has committed to providing the following initiatives:

- Twenty-four (24) electric charging stations on P2 of the proposed development.

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8.0 ON-SITE CIRCULATION

Loading Bay:

Swept path analysis was conducted on the medium-size loading bay off George Lane using MSUTAC (medium single-unit truck) as the design vehicle. An MSUTAC is able to parallel park in a single manoeuvre and exit without any conflicts. See **FIGURE 21** for the swept path analysis.

Parkade Access:

FIGURE 22 illustrates the swept path analysis for two PTACs (TAC passenger vehicles) passing each other while entering and exiting the parkade access simultaneously. Please note that the length of a PTAC (TAC passenger vehicle) is 5.6 metres, which is the length of a crew cab short bed pick-up truck. As illustrated in **FIGURE 22**, while the space is tight for two crew cab short bed pick-up trucks passing each other, there are no conflicts.

While the swept path analysis in **FIGURE 22** showed that two crew cab short bed pick-up trucks are able to narrowly pass each other without conflicts, in reality, most passenger vehicles are smaller and will be able to pass each other more easily. Swept path analysis was conducted on the same manoeuvre using two Honda Accords to illustrate typical passenger cars passing each other on the parkade access.

FIGURE 23 illustrates the swept path analysis for two Honda Accords passing each other while entering and exiting the parkade access simultaneously. The length of the Honda Accord used is 4.93 metres, which is more of a typical passenger vehicle. As illustrated in **FIGURE 23**, two small cars are able to pass each other without conflicts.

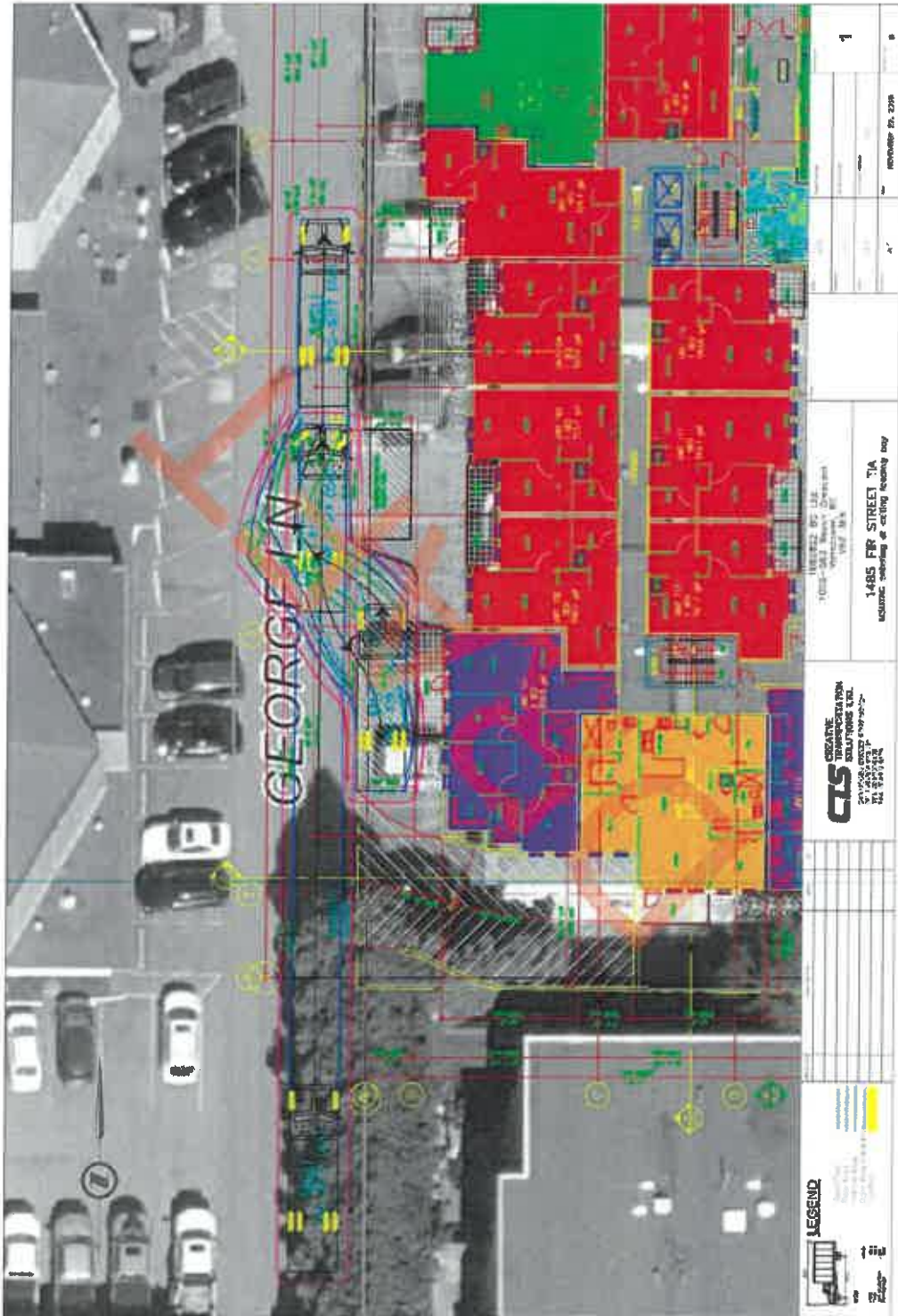
South Drive Aisle Corners:

FIGURE 24 illustrates the swept path analysis for two PTACs (TAC passenger vehicles) passing each other on the southeast corner from which connects the parkade access ramp to P1 and on the southwest corner which connects P1 to P2. Please note that the length of a PTAC (TAC passenger vehicle) is 5.6 metres, which is the length of a crew cab short bed pick-up truck. As illustrated in **FIGURE 24**, while the space is tight for two crew cab short bed pick-up trucks passing each other, there are no conflicts.

While the swept path analysis in **FIGURE 24** showed that two crew cab short bed pick-up trucks are able to narrowly pass each other without conflicts, in reality, most passenger vehicles are smaller and will be able to pass each other more easily. Swept path analysis was conducted on the same manoeuvre using two Honda Accords to illustrate typical passenger cars passing each other on the on the south drive aisle corners.

FIGURE 25 illustrates the swept path analysis for two Honda Accords (small cars) passing each other on the southeast corner from which connects the parkade access ramp to P1 and on the southwest corner which connects P1 to P2. The length of the Honda Accord used is 4.93 metres, which is the length of a typical passenger vehicle. As illustrated in **FIGURE 25**, two small cars are able to pass each other without conflicts.

**FIGURE 21
MSUTAC SWEEP PATH ANALYSIS ON LOADING BAY**



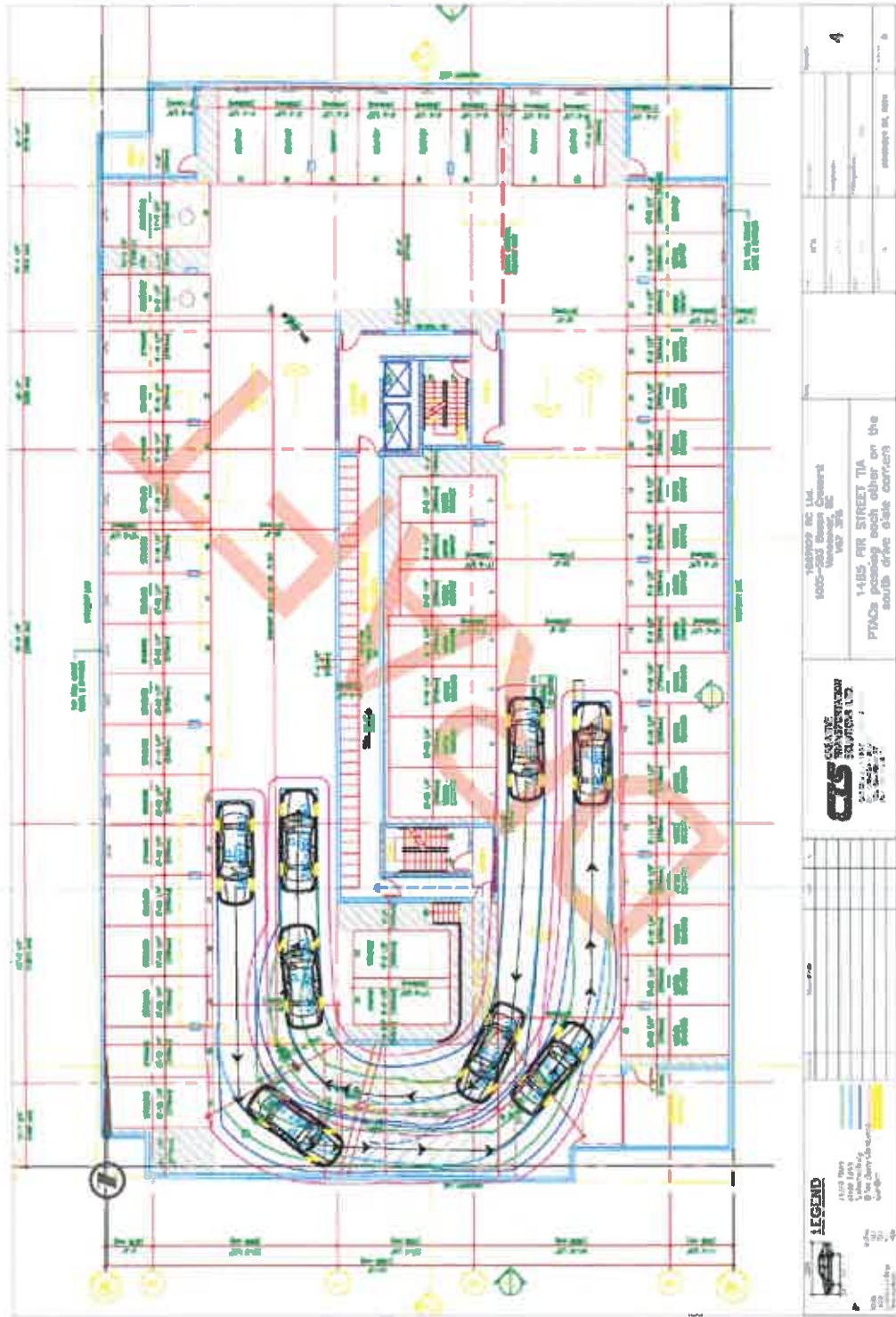
**FIGURE 22
PTAC SWEEP PATH ANALYSIS ON 2-WAY PARKADE ACCESS**



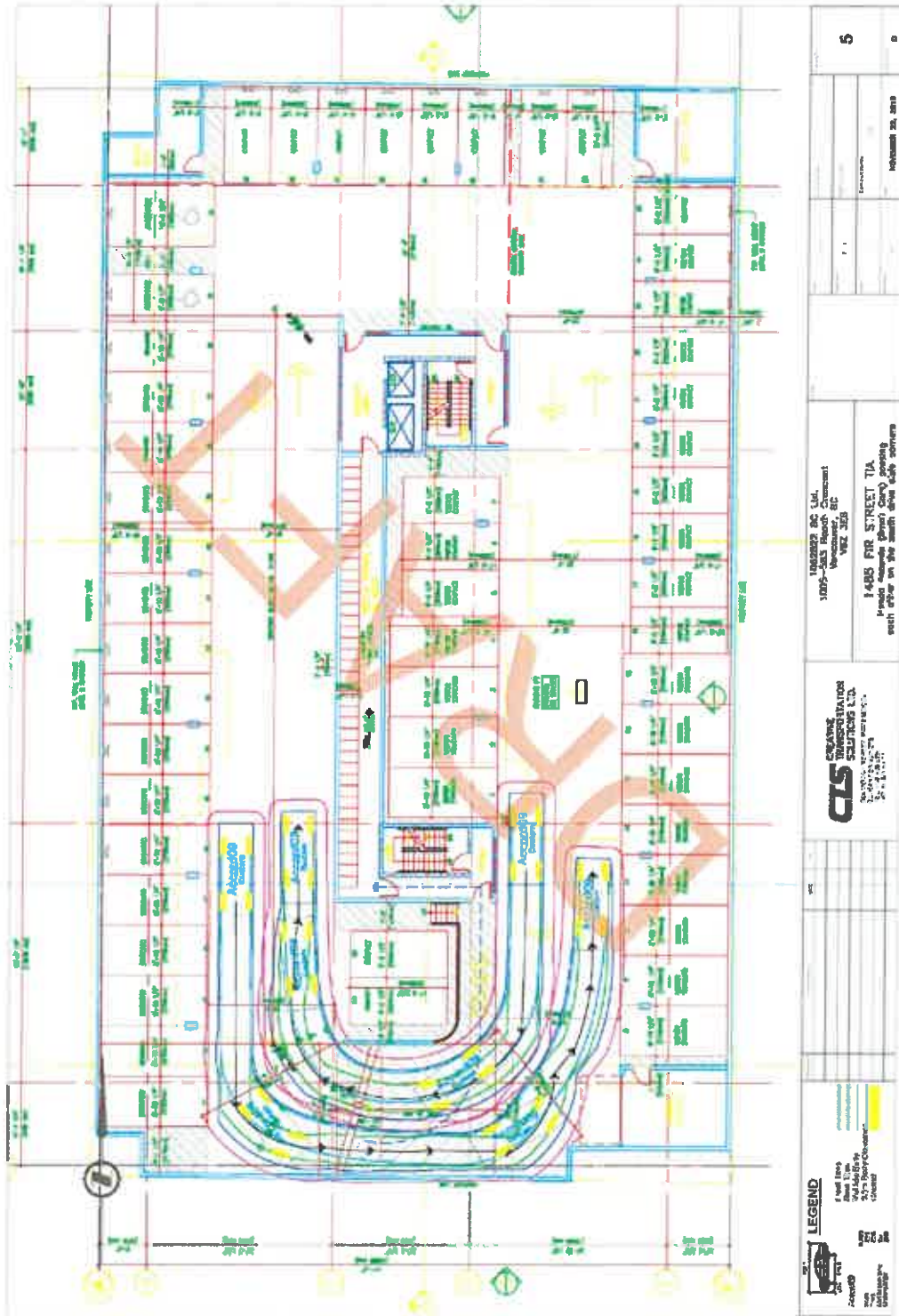
FIGURE 23
HONDA ACCORD SWEEP PATH ANALYSIS ON 2-WAY PARKADE ACCESS



FIGURE 24
PTAC SWEEP PATH ANALYSIS ON 2-WAY SOUTH DRIVE AISLE CORNERS



**FIGURE 25
HONDA ACCORD SWEEP PATH ANALYSIS ON 2-WAY SOUTH DRIVE AISLE CORNERS**



9.0 CONCLUSIONS & RECOMMENDATIONS

9.1 Conclusions

- 1) 1062822 BC Ltd is proposing to build a rental apartment development consisting of 80 dwelling units at 1485 Fir Street in the City of White Rock.
- 2) The study site is situated in a transit oriented neighbourhood with convenient access to commercial and retail developments:
 - There are nine (9) bus routes servicing the study area, with the White Rock Centre transit exchange only a six (6) minute walk from the site.
 - Within the adjacent road network, Thrift Avenue is a shared lane bike route. Martin Street, Best Street, and Finlay Street are also shared lane bike routes.
 - All roads within the study area have a sidewalk on at least one side.
- 3) The southwestern sightlines at George Lane at Thrift Avenue for pedestrians crossing from the northwest to northeast corner are deficient. A "Watch for Pedestrians" warning sign and tactile paving on the northwestern and northeastern corners is recommended.
- 4) CTS conducted turning movement counts on Wednesday April 3rd, 2019 from 07:00 to 09:00, 11:00 to 13:00, and 15:00 to 18:00 to document the typical weekday peak hour traffic volumes for the study area.
- 5) The weekday morning and afternoon peak hours were chosen as the dominant design hours and was observed to occur from 08:00 to 09:00 and from 15:00 to 16:00.
- 6) 2022 is anticipated to be the year of full buildout for the proposed development. The 2019 base traffic volumes were factored up by a traffic volume growth rate of 2.0% per annum (simple straight line) to represent the future base 2022, 2027 and estimated 2045 volumes.
- 7) The proposed development is forecasted to generate a total of 29 vehicle trips (8 inbound, 21 outbound) during the weekday morning peak hour and 36 vehicle trips (22 inbound, 14 outbound) during the weekday afternoon peak hour.
- 8) Subtracting the estimated site traffic generated by the existing apartment building, the net increase in site traffic from the propose development is forecasted to be 21 vehicle trips (6 inbound, 15 outbound) during the weekday morning peak hour and 26 vehicle trips (16 inbound, 10 outbound) during the weekday afternoon peak hour.
- 9) The intersection capacity analysis noted that during the weekday morning and afternoon peak hours, all intersections are forecasted to operate at LOS A (Excellent) for all horizon years and scenarios.

- 10) The total number of required parking spaces for the proposed development is 120 parking spaces – 96 parking spaces for residents and 24 parking spaces for visitors. The proposed development is providing a total of 108 parking spaces – 84 parking spaces for residents and 24 parking spaces for visitors, resulting in a variance of 12 parking spaces, or 10%.
- 11) The City of White Rock 2045 OCP objective and policies support rental housing developments and consideration of parking requirement relaxations for such developments within walking distance of frequent transit service and / or commercial areas. The proposed development satisfies these conditions.
- 12) The ITE Parking Generation Manual 5th Edition data yield's a peak parking demand of 105 parking spaces for the proposed development which is 15 parking spaces less than the Bylaw requirement and three (3) parking spaces below what is being provided by the proposed development.
- 13) The applicant is providing the following transportation demand management initiatives:
 - Residents will receive a welcome package containing transit and cycling information.
 - A bicycle repair station with including a tire pump will be provided within the parkade.
 - Extra hooks for helmets to be locked to will be included in the bicycle lockers for convenience
 - Many of the Class I bicycle parking spaces are located on P1 and P2 with direct access to the elevators. The rest of the Class I bicycle parking spaces on P2 have convenient access to the elevators.
 - Residents will receive a \$30 gift card towards a local bicycle shop.
 - All of the seventeen (17) Class II bicycle parking spaces are located at street level next to the main entrance.
 - Gently sloped ramps leading up to the main entrance for cyclist comfort.
 - Directional signage within the lobby or at the main entrance for the nearest bus stops and estimated walking times to popular destinations
 - Residents will receive a pre-loaded compass card (e.g. \$100).
 - A live screen within the lobby displaying current bus route schedules
 - Twenty-four (24) electric charging stations on P2 of the proposed development.

9.2 Recommendations

Based on this transportation impact assessment study, CTS recommends the following:

- 1) The applicant work with the City of White Rock to ensure any improvements to the fronting sidewalks align with the City of White Rock's Strategic Transportation Plan.
- 2) The City of White Rock grant the applicant's 10% or 12 vehicle parking stall variance request for the proposed development based on the supporting information provided in this report.
- 3) The City of White Rock consider installing tactile paving on the northwestern and northeastern corners of George Lane at Thrift Avenue.
- 4) The City of White Rock consider installing a "Watch for Pedestrians" warning sign for southbound traffic at George Lane at Thrift Avenue.

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We would like to take this opportunity to thank you for this unique project and we look forward to working with you again in the future. Please call the undersigned should you have any questions or comments.

Yours truly,

CREATIVE TRANSPORTATION SOLUTIONS LTD.

Reviewed by:

Prepared by:

Gary Vileg, P.Eng.
Engineering Group Manager

Jacqueline Lee, EIT
Junior Traffic Engineer

Attachment

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Appendix A
Architectural Drawing

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ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
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NO.	DESCRIPTION
1	EXISTING BUILDING
2	PROPOSED BUILDING
3	PROPOSED ENTRY
4	PROPOSED DRIVEWAY
5	PROPOSED DRIVEWAY
6	PROPOSED DRIVEWAY
7	PROPOSED DRIVEWAY
8	PROPOSED DRIVEWAY
9	PROPOSED DRIVEWAY
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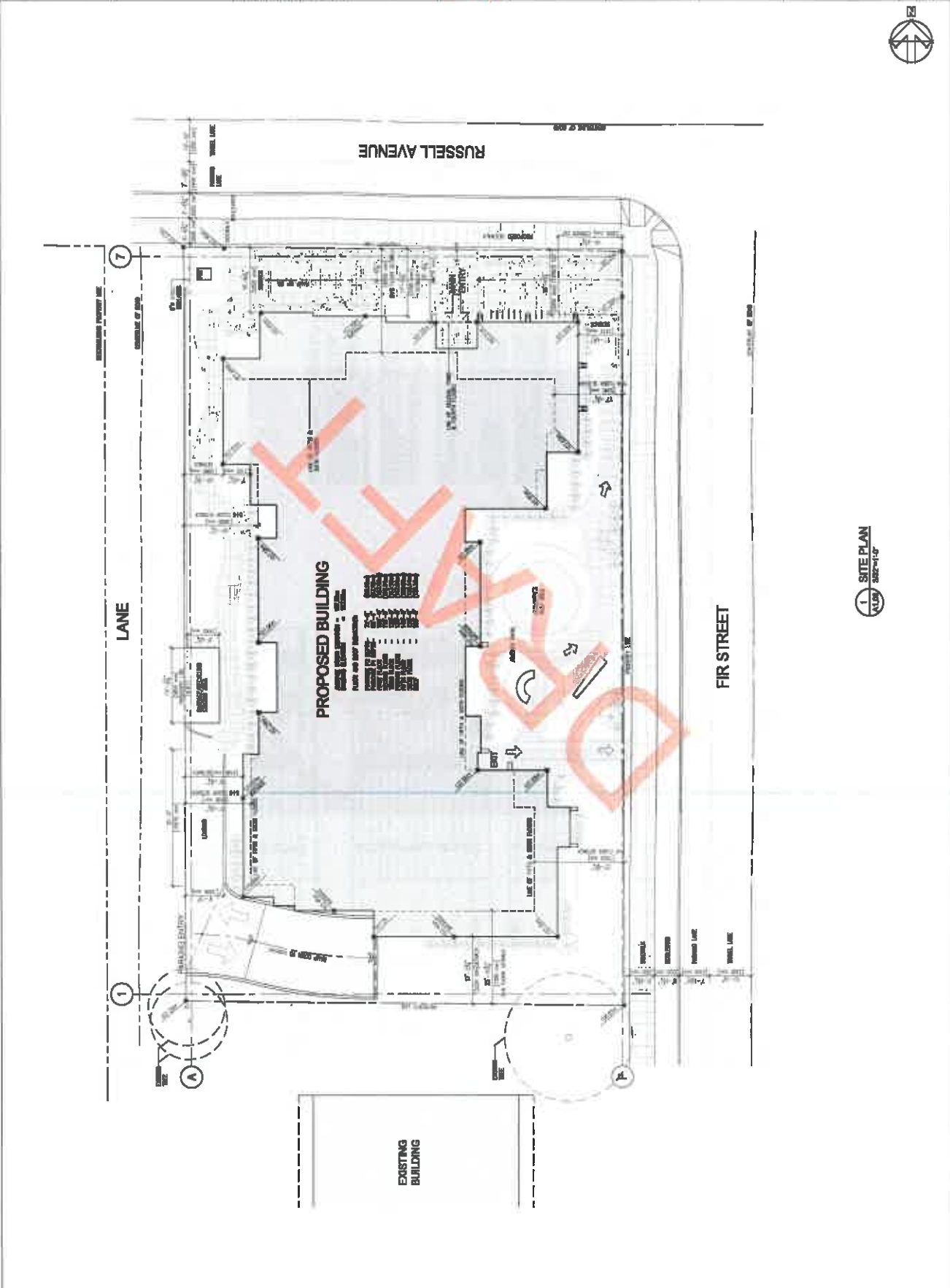
SITE PLAN

SCALE: 1/8" = 1'-0"

A1.00

DATE: 08/14/14

FILE: 140104



**Appendix B
Translink Timetables**

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321 Surrey Central Station/Newton Exchange/ White Rock Centre/White Rock South



October 2017

For route and schedule information, visit www.translink.ca

CMBC Service Planning



Surrey Central Stn
-see Inset

Newton Exchange
Bay 2: 321 Surrey Central Station
Bay 6: 321 White Rock Centre

South Surrey Park and Ride
Bay 2: 321 White Rock Centre/
White Rock South
Bay 4: 321 Surrey Central Station

White Rock Centre
-see inset

King George Hwy @ 11th Ave (Pacific Inn)
321 Surrey Central Station

Surrey Central Station

North Surrey Recreation Centre

Bay 10: 321 White Rock Centre

Central City Mall

University Drive

Old Yale

King George

King George Blvd

Note: Some early AM trips depart from Bay 9 to Scott Road and to New Westminster Station.

White Rock Centre

Bay 7: 321 Surrey Central Station

152nd St

North Bluff Rd

Thrift Ave

Bay 4: 321 White Rock South

LEGEND

- Regular Routing
- - - Limited Service Routing
- Expo Line
- T Route Terminus/Scheduled Stop
- S SkyTrain / Canada Line / West Coast Express Station

321 Surrey Central Station

From King George Blvd at 1100 Block via King George, 16 Ave, 152 St (White Rock Centre); or from North Bluff at Foster via North Bluff, 152 St (White Rock Centre); then via 152 St, King George, South Surrey Park & Ride, King George, 68 Ave, 138 St, 72 Ave, Newton Exchange, 72 Ave, King George (King George Station), 102 Ave, City Pkwy to Surrey Central Station.

King George at 1100 Block #55375	16th Ave at King George #55378	White Rock Centre Bay 7 (E)	Newton Exchange Bay 2	Surrey Central Station (E)	King George at 1100 Block #55375	16th Ave at King George #55378	White Rock Centre Bay 7 (E)	Newton Exchange Bay 2	Surrey Central Station (E)
MONDAY TO FRIDAY					MONDAY TO FRIDAY				
-	-	-	4.43	4.59	..... CONTINUED				
-	-	-	4.58	5.14	-	-	5.46	6.16	6.34
-	-	-	5.13	5.29	-	-	5.58	6.28	6.46
-	-	-	5.28	5.46	-	-	6.10	6.40	6.58
-	5.14	5.19	5.43	5.59	-	-	6.22	6.52	7.10
-	-	5.28	5.53	6.15	-	-	6.34	7.04	7.22
-	-	5.46	6.11	6.30	-	-	6.53	7.21	7.37
-	-	6.02	6.27	6.46	-	-	7.12	7.40	7.56
6.07	6.09	6.14	6.39	7.01	-	-	7.24	7.52	8.08
6.25	6.27	6.32	6.57	7.19	-	-	7.41	8.08	8.24
6.40	6.42	6.48	7.13	7.35	-	-	7.56	8.23	8.39
6.55	6.57	7.03	7.28	7.50	-	-	8.10	8.37	8.53
-	-	7.20	7.47	8.06	-	-	8.22	8.47	9.05
-	-	7.35	8.02	8.21	-	-	8.39	9.04	9.22
7.44	7.46	7.52	8.18	8.37	-	-	8.56	9.23	9.39
-	-	8.00	8.29	8.48	9.27	9.29	9.33	9.56	10.14
-	-	8.09	8.38	8.59	-	-	9.57	10.24	10.40
-	-	8.30	8.59	9.18	10.31	10.33	10.37	11.01	11.19
-	-	8.45	9.14	9.33	-	-	10.57	11.21	11.39
-	-	9.00	9.29	9.48	11.21	11.22	11.30	11.46	12.01
-	-	9.10	9.39	10.03	-	-	11.56	12.19	12.32
-	-	9.30	9.59	10.18	12.29	12.30	12.34	12.52	1.04
-	-	9.42	10.12	10.33	-	-	-	1.27	1.37
-	-	9.57	10.27	10.48	-	-	-	1.57	2.07
-	-	10.12	10.42	11.03	SATURDAY				
-	-	10.27	10.57	11.18	-	-	-	5.47	6.02
-	-	10.41	11.12	11.33	-	-	5.43	6.06	6.24
-	-	10.56	11.27	11.48	-	-	6.03	6.26	6.44
-	-	11.11	11.42	12.03	-	-	6.23	6.46	7.04
-	-	11.26	11.57	12.18	6.33	6.35	6.47	7.10	7.28
-	-	11.42	12.13	12.34	-	-	7.03	7.28	7.46
-	-	11.58	12.29	12.50	7.08	7.10	7.22	7.45	8.03
-	-	12.14	12.45	1.06	-	-	7.32	8.00	8.18
-	-	12.29	1.00	1.21	7.38	7.40	7.52	8.15	8.33
-	-	12.42	1.15	1.36	-	-	8.02	8.30	8.48
-	-	12.58	1.31	1.52	8.08	8.10	8.21	8.45	9.03
-	-	1.13	1.46	2.07	-	-	8.32	9.00	9.20
-	-	1.28	2.01	2.22	-	-	8.47	9.15	9.35
-	-	1.46	2.19	2.40	-	-	9.00	9.28	9.48
-	-	2.01	2.34	2.55	-	-	9.15	9.45	10.08
-	-	2.14	2.47	3.08	-	-	9.34	10.00	10.23
-	-	2.29	3.02	3.23	-	-	9.40	10.12	10.37
-	-	2.42	3.17	3.38	-	-	9.55	10.27	10.52
-	-	2.57	3.32	3.53	-	-	10.10	10.42	11.07
-	-	3.13	3.48	4.09	-	-	10.28	10.58	11.23
-	-	3.29	4.04	4.25	-	-	10.43	11.15	11.40
-	-	3.40	4.13	4.41	-	-	10.56	11.28	11.53
-	-	3.55	4.28	4.56	-	-	11.13	11.45	12.10
-	-	4.10	4.45	5.03	-	-	11.28	12.00	12.25
-	-	4.22	4.57	5.15	-	-	11.43	12.15	12.40
-	-	4.34	5.09	5.27	-	-	11.58	12.30	12.55
-	-	4.46	5.21	5.39	-	-	12.13	12.45	1.10
-	-	4.58	5.33	5.51	-	-	12.28	1.00	1.25
-	-	5.10	5.45	6.03	-	-	12.43	1.15	1.40
-	-	5.22	5.53	6.11	-	-	12.58	1.30	1.55
-	-	5.34	6.07	6.25	CONTINUED				

321 Surrey Central Station

White Rock South to White Rock Centre, Newton Exchange and Surrey Central Station.

(Refer to Monday to Friday for routing)

King George at 1100 Block #55375	16th Ave at King George #5537	White Rock Centre Bay 7 (E)	Newton Exchange Bay 2	Surrey Central Station (E)	King George at 1100 Block #55375	16th Ave at King George #55378	White Rock Centre Bay 7 (E)	Newton Exchange Bay 2	Surrey Central Station (E)
SATURDAY					SUNDAY & HOLIDAYS				
..... CONTINUED					..... CONTINUED				
-	-	1.13	1.45	2.10	-	-	10.04	10.30	10.49
-	-	1.28	2.00	2.25	-	-	10.16	10.45	11.04
-	-	1.43	2.15	2.40	-	-	10.31	11.00	11.19
-	-	1.58	2.30	2.55	-	-	10.46	11.15	11.34
-	-	2.13	2.45	3.10	-	-	11.01	11.30	11.49
-	-	2.25	2.57	3.22	-	-	11.16	11.45	12.04
-	-	2.41	3.13	3.39	-	-	11.31	12.00	12.19
-	-	2.56	3.28	3.54	-	-	11.44	12.15	12.34
-	-	3.13	3.43	4.05	-	-	11.59	12.30	12.49
-	-	3.28	3.58	4.20	-	-	12.12	12.43	1.04
-	-	3.43	4.13	4.35	-	-	12.27	12.58	1.19
-	-	3.58	4.30	4.52	-	-	12.42	1.13	1.34
-	-	4.13	4.45	5.07	-	-	12.57	1.28	1.49
-	-	4.28	4.59	5.21	-	-	1.12	1.43	2.04
-	-	4.43	5.13	5.35	-	-	1.27	1.58	2.19
-	-	4.59	5.29	5.51	-	-	1.43	2.14	2.35
-	-	5.13	5.43	6.05	-	-	1.58	2.29	2.50
-	-	5.28	5.58	6.20	-	-	2.13	2.44	3.05
-	-	5.43	6.13	6.35	-	-	2.28	2.59	3.20
-	-	5.56	6.25	6.47	-	-	2.43	3.14	3.35
-	-	6.13	6.40	7.02	-	-	2.58	3.29	3.50
-	-	6.31	6.58	7.20	-	-	3.14	3.45	4.05
-	-	6.46	7.19	7.35	-	-	3.29	4.00	4.20
-	-	7.01	7.28	7.50	-	-	3.44	4.15	4.35
-	-	7.16	7.43	8.05	-	-	3.59	4.30	4.50
-	-	7.31	7.58	8.20	-	-	4.14	4.45	5.05
-	-	7.46	8.13	8.35	-	-	4.29	5.00	5.20
-	-	8.01	8.28	8.50	-	-	4.44	5.15	5.35
8.21	8.23	8.28	8.53	9.10	-	-	4.59	5.30	5.50
-	-	8.38	9.03	9.20	-	-	5.14	5.45	6.05
-	-	8.55	9.20	9.37	-	-	5.29	6.00	6.20
9.22	9.24	9.29	9.54	10.11	-	-	5.44	6.15	6.35
-	-	9.55	10.20	10.37	-	-	5.59	6.30	6.50
10.22	10.24	10.29	10.53	11.10	-	-	6.14	6.45	7.05
-	-	10.55	11.18	11.31	-	-	6.32	7.03	7.23
11.20	11.22	11.27	11.47	12.00	-	-	6.46	7.17	7.37
-	-	11.54	12.17	12.30	-	-	7.01	7.32	7.52
12.21	12.23	12.28	12.48	1.01	-	-	7.18	7.47	8.07
-	-	-	1.24	1.37	-	-	7.33	8.02	8.22
-	-	-	1.53	2.06	-	-	7.48	8.17	8.37
SUNDAY & HOLIDAYS					8.21	8.23	8.28	8.50	9.07
-	-	-	6.09	6.23	-	-	8.37	9.05	9.22
-	-	6.12	6.30	6.49*	-	-	8.52	9.20	9.37
-	-	6.21	6.47	7.03	9.21	9.23	9.28	9.50	10.07
-	-	6.41	7.07	7.23	-	-	9.52	10.20	10.37
-	-	7.01	7.27	7.43	10.22	10.24	10.29	10.50	11.07
-	-	7.21	7.47	8.03	-	-	10.57	11.24	11.37
-	-	7.36	8.02	8.18	11.21	11.23	11.28	11.49	12.02
-	-	7.51	8.17	8.33	-	-	11.54	12.19	12.32
-	-	8.06	8.32	8.48	-	-	12.24	12.49	1.02
-	-	8.23	8.49	9.05	-	-	1.23	1.35	
-	-	8.36	9.02	9.20					
8.50	8.52	8.58	9.20	9.36					
-	-	9.04	9.30	9.48	* Trip leaves from Surrey Central Station - Bay 9 to Scott Road Station. Leaves Scott Road Station - Bay 7 at 6:58am to New Westminster Station, arriving there at 7:07am.				
-	-	9.19	9.45	10.03					
-	-	9.35	10.01	10.19					
-	-	9.49	10.15	10.34					
CONTINUED									

321 Newton Exchange/White Rock Centre/ White Rock South

From Surrey Central Station via City Pkwy, 102 Ave, University Dr, Old Yale Rd, King George Blvd (King George Station), 72 Ave, Newton Exchange, 72 Ave, 138 St, 68 Ave, King George, South Surrey Park & Ride, King George, 152 St (White Rock Centre), Johnston, Thrift, Oxford, North Bluff to 152 St, to White Rock Centre. Some trips from Johnston/North Bluff instead via North Bluff, King George, 8 Ave roundabout, King George to 1100 Block.

Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)					Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)					Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)					
MONDAY TO FRIDAY					MONDAY TO FRIDAY					SATURDAY					
5.08	5.22	-	5.46	5.58 CONTINUED	-	5.23	5.45	-	-	-	5.23	5.45	-	-
5.23	5.37	-	6.02	6.14	4.00	4.25	5.02	-	-	5.58	6.10	6.36	-	6.13	6.20
5.38	5.52	-	-	-	4.12	4.37	5.14	-	-	6.16	6.28	-	6.51	7.00	
5.53	6.07	-	6.35	6.47	4.24	4.49	5.26	-	-	6.38	6.50	7.18	-	-	
6.18	6.33	-	7.01	7.13	4.36	5.01	5.38	-	-	6.58	7.10	-	7.33	7.42	
6.32	6.47	7.19	-	-	4.48	5.13	5.50	-	-	7.13	7.25	7.53	-	-	
6.44	6.59	-	7.27	7.39	5.00	5.25	6.02	-	-	7.28	7.41	-	8.07	8.15	
6.59	7.14	7.44+	-	-	5.12	5.37	6.14	-	-	7.43	7.56	8.24	-	-	
7.15	7.34	8.05+	-	-	5.24	5.49	6.22+	-	-	7.58	8.11	-	8.37	8.45	
7.30	7.49	8.20+	-	-	5.36	5.59	6.33	-	-	8.13	8.26	8.54	-	-	
7.45	8.04	8.35+	-	-	5.48	6.11	6.45	-	-	8.28	8.42	-	9.08	9.16	
8.00	8.20	8.55	-	-	6.00	6.23	6.57	-	-	8.44	8.58	9.28	-	-	
8.15	8.35	9.09+	-	-	6.15	6.38	7.12	-	-	8.59	9.13	9.43	-	-	
8.30	8.50	9.23	-	-	6.32	6.52	7.24	-	-	9.14	9.28	9.58	-	-	
8.45	9.04	9.34+	-	-	6.45	7.07	7.39	-	-	9.29	9.43	10.13	-	-	
9.00	9.19	9.52	-	-	7.00	7.20	7.50	-	-	9.44	9.58	10.28	-	-	
9.15	9.34	10.04+	-	-	7.15	7.35	8.05	-	-	9.59	10.13	10.43	-	-	
9.30	9.49	10.22	-	-	7.30	7.49	8.19	-	-	10.14	10.28	10.58	-	-	
9.45	10.04	10.34+	-	-	7.45	8.04	8.34	-	-	10.29	10.43	11.13	-	-	
10.00	10.20	10.53	-	-	8.00	8.19	8.49	-	-	10.44	10.59	11.31	-	-	
10.15	10.35	11.07+	-	-	8.15	8.34	9.04	-	-	10.59	11.14	11.46	-	-	
10.30	10.49	11.24	-	-	8.30	8.49	-	9.14	9.22	11.14	11.31	12.03	-	-	
10.46	11.05	11.37+	-	-	8.45	9.03	9.32	-	-	11.29	11.46	12.18	-	-	
11.00	11.19	11.55	-	-	9.10	9.26	9.55	-	-	11.44	12.02	12.34	-	-	
11.15	11.34	12.07+	-	-	9.24	9.41	-	-	-	11.59	12.17	12.49	-	-	
11.30	11.52	12.28	-	-	9.38	9.54	-	10.19	10.27	12.14	12.32	1.04	-	-	
11.45	12.07	12.41+	-	-	9.54	10.10	10.39	-	-	12.29	12.47	1.19	-	-	
12.00	12.22	12.58	-	-	10.29	10.45	-	11.10	11.18	12.44	1.02	1.34	-	-	
12.15	12.37	1.11+	-	-	11.00	11.16	11.43	-	-	12.58	1.16	1.48	-	-	
12.30	12.52	1.28	-	-	11.32	11.47	-	12.12	12.20	1.14	1.32	2.06	-	-	
12.45	1.07	1.41+	-	-	12.03	12.19	-	-	-	1.29	1.47	2.21	-	-	
1.00	1.22	1.58	-	-	12.35	12.49	-	1.12	1.20	1.44	2.04	2.37	-	-	
1.15	1.37	2.13	-	-	1.09	1.24	-	-	-	1.59	2.19	2.52	-	-	
1.30	1.52	2.29	-	-	1.39	1.53	2.20	-	-	2.14	2.34	3.08	-	-	
1.45	2.07	2.44	-	-	2.09	2.24	-	-	-	2.29	2.49	3.23	-	-	
2.00	2.23	3.00	-	-						2.44	3.04	3.38	-	-	
2.15	2.38	3.15	-	-						2.59	3.19	3.53	-	-	
2.30	2.54	3.32	-	-						3.14	3.34	4.07	-	-	
2.45	3.09	3.47	-	-						3.29	3.49	4.22	-	-	
3.00	3.24	4.02	-	-						3.44	4.04	4.37	-	-	
3.12	3.36	4.14	-	-						3.59	4.19	4.52	-	-	
3.24	3.48	4.26	-	-						4.14	4.34	5.07	-	-	
3.36	4.00	4.38	-	-						4.29	4.49	5.22	-	-	
3.48	4.12	4.50	-	-						CONTINUED					

321 Newton Exchange/White Rock Centre/ White Rock South

Surrey Central Station to Newton Exchange, White Rock Centre and White Rock South.

(Refer to Monday to Friday for routing)

Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)	Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)	Surrey Central Station Bay 10 Newton Exchange Bay 6 White Rock Centre White Rock Centre Bay 4 King George at 1100 Block (E)
SATURDAY		SUNDAY & HOLIDAYS
..... CONTINUED		..... CONTINUED
4.44	5.04 5.37 - -	5.43 5.56 6.23 - -
4.59	5.19 5.52 - -	6.13 6.26 6.53 - -
5.14	5.34 6.07 - -	6.33 6.46 7.13 - -
5.29	5.49 6.22 - -	6.53 7.06 7.33 - -
5.44	6.04 6.37 - -	x7.08 7.21 7.46 - -
5.59	6.16 6.46 - -	7.21 7.34 8.03 - -
6.14	6.31 7.01 - -	x7.32 7.45 8.10 - -
6.29	6.46 7.16 - -	7.42 7.55 8.24 - -
6.44	7.00 7.30 - -	7.56 8.09 - 8.32 8.41
7.02	7.18 7.48 - -	8.12 8.26 8.56 - -
7.17	7.33 8.03 - -	8.27 8.41 9.11 - -
7.32	7.45 - 8.10 8.19	8.40 8.53 - 9.16 9.25
7.47	8.03 8.33 - -	8.57 9.11 9.41 - -
8.02	8.18 8.48 - -	9.12 9.26 9.56 - -
8.17	8.33 9.03 - -	9.27 9.41 10.11 - -
8.32	8.45 - 9.10 9.19	9.42 9.57 10.29 - -
8.47	9.00 9.30 - -	9.57 10.12 10.44 - -
9.02	9.15 9.45 - -	10.12 10.27 10.59 - -
9.17	9.32 - - -	10.27 10.42 11.14 - -
9.32	9.45 - 10.10 10.19	10.42 10.57 11.29 - -
9.47	10.02 - - -	10.57 11.12 11.44 - -
10.02	10.15 10.45 - -	11.12 11.28 12.02 - -
10.31	10.44 - 11.09 11.18	11.27 11.43 12.17 - -
11.01	11.14 11.44 - -	11.42 11.58 12.32 - -
11.33	11.46 - 12.11 12.20	11.57 12.13 12.48 - -
12.04	12.19 - - -	12.12 12.29 1.05 - -
12.38	12.48 - 1.13 1.20	12.27 12.44 1.20 - -
1.07	1.22 - - -	12.42 1.00 1.36 - -
1.40	1.52 2.17 - -	12.57 1.15 1.51 - -
2.08	2.23 - - -	1.12 1.31 2.07 - -
		1.27 1.46 2.22 - -
		1.42 2.01 2.37 - -
		1.57 2.16 2.52 - -
		2.12 2.31 3.07 - -
		2.27 2.46 3.22 - -
		2.42 3.02 3.38 - -
		2.57 3.17 3.53 - -
		3.12 3.32 4.08 - -
		3.27 3.46 4.20 - -
		3.42 4.01 4.35 - -
		3.57 4.16 4.50 - -
		4.12 4.31 5.05 - -
		4.27 4.46 5.20 - -
		CONTINUED

x Trips start from New Westminster Station - Bay 4 at 6.53 & 7.17am., then Non-stop to Scott Road Station. Leaves Scott Road Station - Bay 7 at 7.00 & 7.24am to Surrey Central Station, Page 21 of 524, 104 Ave, University Dr to Surrey Central Stn.

345 King George Station / White Rock Centre

January 2019



For route and schedule information, visit www.translink.ca



CMBC Service Planning

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**King George Station
Bay 2: 345 White Rock Centre**

**White Rock Centre
Bay 7: 345 King George Station**

LEGEND

- Regular Routing
- Expo Line
- Route Terminus/Scheduled Stop
- SkyTrain / Canada Line / West Coast Express Station

345 White Rock Centre 345 King George Station

From King George Station via King George Blvd, 100 Ave, Whalley Blvd, Fraser Hwy, 152 St (White Rock Centre), Johnston, Thrift, Oxford, North Bluff to Foster

From North Bluff at Foster via North Bluff, 152 St (White Rock Centre), Fraser Hwy, King George Blvd to King George Station.

King George Station Bay 2 152 St at Hwy 10 #56068 White Rock Centre (E)	King George Station Bay 2 152 St at Hwy 10 #56068 White Rock Centre (E)	White Rock Centre Bay 7 152 st at Hwy 10 #56045 King George Station (E)	White Rock Centre Bay 7 152 st at Hwy 10 #56045 King George Station (E)
MONDAY TO FRIDAY		MONDAY TO FRIDAY	
5.58 6.18 6.36	.... CONTINUED	5.04 5.20 5.44	.... CONTINUED
6.28 6.48 7.06	3.28 3.58 4.19	5.34 5.50 6.14	1.33 1.54 2.19
6.58 7.21 7.44	4.00 4.30 4.51	6.04 6.23 6.49	2.03 2.26 2.50
7.28 7.51 8.14	4.30 5.04 5.23	6.24 6.43 7.09	2.32 2.56 3.23
7.58 8.23 8.42	5.00 5.34 5.53	6.44 7.03 7.32	3.02 3.29 3.58
8.28 8.53 9.10	5.30 5.59 6.19	7.04 7.23 7.54	3.32 3.59 4.28
8.59 9.26 9.41	5.59 6.28 6.48	7.24 7.43 8.14	4.02 4.29 4.58
9.28 9.52 10.09	6.29 6.56 7.15	7.44 8.03 8.34	4.32 4.59 5.28
9.58 10.20 10.38	6.59 7.26 7.45	8.03 8.23 8.54	5.02 5.26 5.52
10.28 10.50 11.08	7.28 7.52 8.07	8.33 8.52 9.16	5.32 5.56 6.22
10.58 11.20 11.38	7.55 8.19 8.34	9.03 9.22 9.46	6.02 6.24 6.50
11.28 11.50 12.08	8.26 8.48 9.02	9.33 9.52 10.16	6.35 6.55 7.16
11.58 12.20 12.38	8.56 9.18 9.32	10.03 10.22 10.46	7.03 7.23 7.44
12.28 12.50 1.08		10.33 10.52 11.16	7.35 7.55 8.15
12.59 1.21 1.39		11.03 11.22 11.48	8.05 8.25 8.45
1.29 1.54 2.12		11.33 11.52 12.18	8.35 8.55 9.15
1.59 2.24 2.42		12.03 12.24 12.49	
2.29 2.56 3.18		12.33 12.54 1.19	
2.59 3.26 3.48		1.03 1.24 1.49	
CONTINUED		CONTINUED	

351 Bridgeport Station/ Crescent Beach



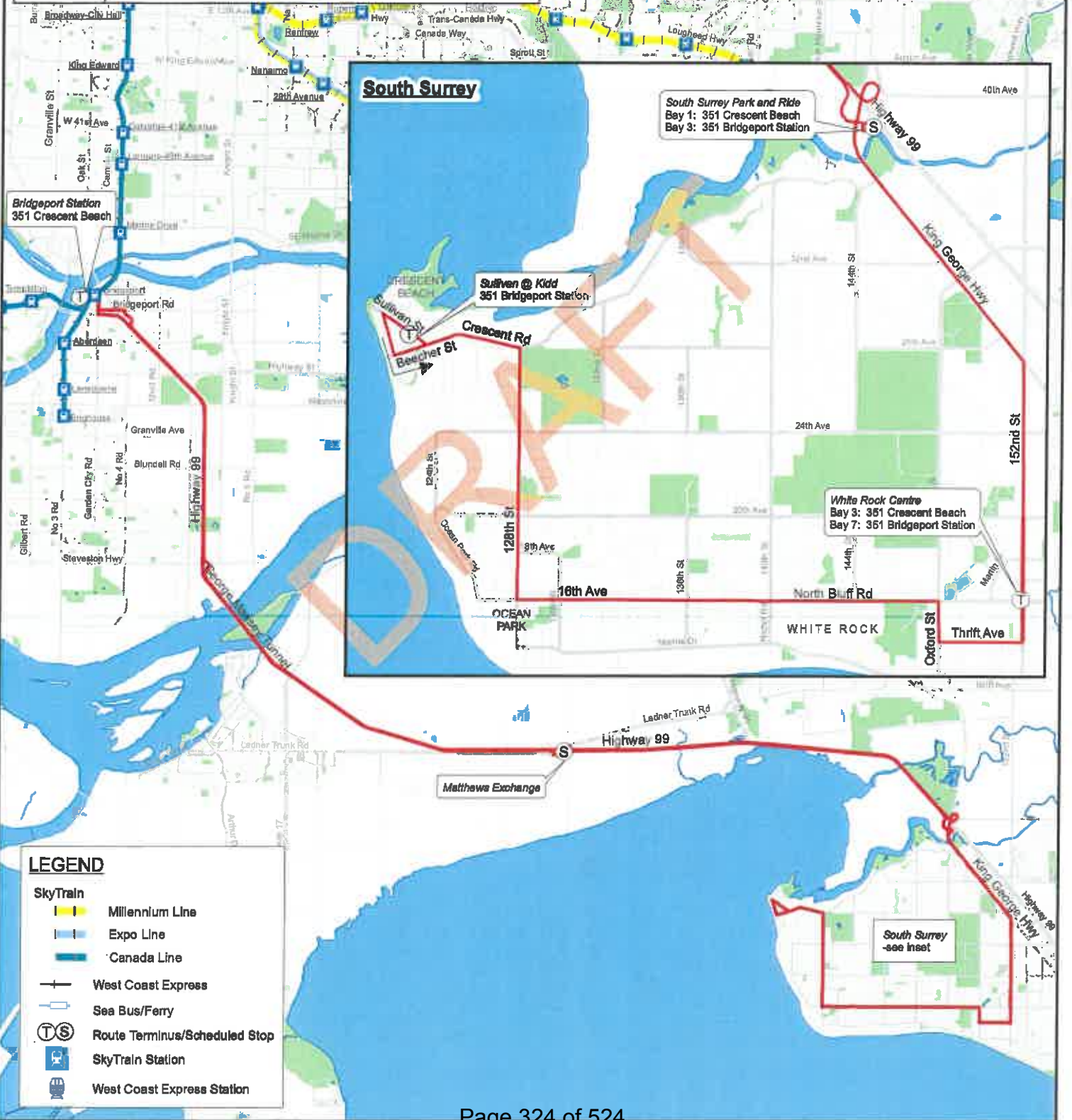
September 2009



For route and schedule information, visit www.translink.ca



CMBC Service Planning



LEGEND

- SkyTrain**
 - Millennium Line
 - Expo Line
 - Canada Line
- West Coast Express
- Sea Bus/Ferry
- Route Terminus/Scheduled Stop
- SkyTrain Station
- West Coast Express Station

351 Bridgeport Station

From Sullivan at Kidd via Sullivan, McBride, Beecher, Crescent, 128 St, North Bluff, Oxford, Thrift, Johnston, 152 St, King George Blvd, South Surrey Park & Ride, King George Blvd, Hwy 99, Matthews Exchange (except before 8:30 AM weekdays, when the 351 stops at Matthews only to unload upon request), Hwy 99, Massey Exchange, Hwy 99, Bridgeport Rd, Great Canadian Way to Bridgeport Station.

Sullivan at Kidd #56082	128 St at Crescent Rd (E) #56088	Oxford at Russell (E) #56110	White Rock Centre Bay 5	South Surrey Park & Ride Bay 3	Bridgeport Station (E)	Sullivan at Kidd #56082	128 St at Crescent Rd (E) #56088	Oxford at Russell (E) #56110	White Rock Centre Bay 5	South Surrey Park & Ride Bay 3	Bridgeport Station (E)
MONDAY TO FRIDAY						MONDAY TO FRIDAY					
-	4.17	4.28	4.31	4.40	5.05	CONTINUED					
-	-	4.46	4.50	5.02	5.28	3.34	3.38	3.52	3.56	4.10	4.43
-	4.53	5.05	5.08	5.20	5.46	3.44	3.48	4.02	4.06	4.20	4.53
-	-	5.24	5.28	5.40	6.08	3.55	3.59	4.13	4.17	4.30	5.03
-	5.35	5.47	5.50	6.02	6.30	4.05	4.09	4.23	4.27	4.40	5.13
-	5.55	6.07	6.10	6.22	6.52	4.16	4.19	4.33	4.37	4.50	5.23
-	6.05	6.17	6.20	6.32	7.03	4.26	4.29	4.43	4.47	5.00	5.33
6.12	6.15	6.27	6.30	6.42	7.13	4.38	4.41	4.55	4.59	5.12	5.45
-	6.25	6.37	6.40	6.52	7.23	4.50	4.53	5.07	5.11	5.24	5.57
6.27	6.30	6.42	6.45	6.57	7.29	5.04	5.07	5.20	5.24	5.36	6.08
-	-	6.48	6.52	7.04	7.38	5.15	5.18	5.31	5.35	5.47	6.17
-	6.43	6.55	6.59	7.11	7.45	5.28	5.31	5.44	5.48	6.00	6.30
-	6.50	7.02	7.06	7.18	7.52	5.43	5.46	5.59	6.03	6.15	6.45
-	-	7.09	7.13	7.25	7.59	5.60	5.63	6.15	6.19	6.30	6.59
7.02	7.05	7.17	7.20	7.32	8.06	6.15	6.18	6.30	6.34	6.45	7.14
7.09	7.12	7.24	7.27	7.39	8.13	6.31	6.34	6.46	6.49	7.00	7.28
-	7.19	7.31	7.34	7.46	8.20	6.46	6.49	7.01	7.04	7.15	7.43
7.23	7.26	7.38	7.41	7.53	8.27	7.02	7.05	7.17	7.20	7.30	7.57
-	7.31	7.44	7.48	8.00	8.32	7.17	7.20	7.32	7.35	7.45	8.12
7.38	7.41	7.54	7.58	8.10	8.42	7.32	7.35	7.47	7.50	8.00	8.27
7.48	7.51	8.04	8.08	8.20	8.54	7.52	7.55	8.07	8.10	8.20	8.47
-	-	8.12	8.18	8.30	9.04	8.12	8.15	8.27	8.30	8.40	9.07
8.09	8.12	8.26	8.30	8.42	9.16	8.33	8.36	8.48	8.51	9.00	9.27
8.21	8.24	8.38	8.42	8.54	9.28	9.03	9.06	9.18	9.21	9.30	9.57
8.33	8.36	8.50	8.54	9.08	9.38	9.33	9.36	9.48	9.51	10.00	10.27
8.47	8.50	9.04	9.08	9.20	9.52	10.03	10.06	10.18	10.21	10.30	10.56
9.02	9.05	9.19	9.23	9.35	10.07	10.33	10.36	10.48	10.51	11.00	11.26
9.17	9.20	9.34	9.38	9.50	10.22	11.03	11.06	11.18	11.21	11.30	11.56
9.30	9.34	9.48	9.52	10.05	10.35	12.05	12.07	12.18	12.21	12.30	12.56
9.45	9.49	10.03	10.07	10.20	10.50	SATURDAY					
9.58	10.02	10.17	10.21	10.35	11.04	-	4.17	4.28	4.31	4.39	5.05
10.13	10.17	10.32	10.36	10.50	11.19	-	5.17	5.28	5.31	5.39	6.05
-	-	10.45	10.51	11.05	11.34	5.58	6.01	6.13	6.16	6.25	6.52
10.43	10.47	11.02	11.06	11.20	11.49	-	-	6.28	6.33	6.43	7.10
-	-	11.15	11.21	11.35	12.04	6.30	6.33	6.45	6.48	6.58	7.25
11.13	11.17	11.32	11.36	11.50	12.19	-	-	6.58	7.03	7.13	7.40
-	-	11.45	11.51	12.05	12.34	7.00	7.03	7.15	7.18	7.28	7.55
11.43	11.47	12.02	12.06	12.20	12.49	-	-	7.28	7.33	7.43	8.10
-	-	12.15	12.21	12.35	1.04	7.32	7.35	7.47	7.51	8.01	8.29
12.13	12.17	12.32	12.36	12.50	1.19	-	-	8.01	8.06	8.16	8.44
-	-	12.45	12.51	1.05	1.32	8.02	8.05	8.17	8.21	8.31	8.59
12.43	12.47	1.02	1.06	1.20	1.47	-	-	8.31	8.36	8.46	9.14
-	-	1.15	1.21	1.35	2.02	8.31	8.34	8.46	8.50	9.01	9.29
1.13	1.17	1.32	1.36	1.50	2.17	-	-	9.03	9.08	9.19	9.47
-	-	1.45	1.51	2.05	2.33	9.02	9.05	9.18	9.22	9.34	10.02
1.43	1.47	2.02	2.06	2.20	2.48	-	-	9.32	9.37	9.49	10.17
-	-	2.15	2.21	2.35	3.05	9.31	9.34	9.48	9.52	10.04	10.32
2.13	2.17	2.32	2.36	2.50	3.20	-	-	10.02	10.07	10.19	10.47
-	-	2.45	2.51	3.05	3.38	10.01	10.04	10.18	10.22	10.34	11.04
2.43	2.47	3.02	3.06	3.20	3.54	-	-	10.32	10.37	10.49	11.19
2.53	2.57	3.12	3.16	3.30	4.04	10.30	10.33	10.47	10.51	11.04	11.34
3.03	3.07	3.22	3.26	3.40	4.14	-	-	11.01	11.06	11.19	11.49
3.14	3.18	3.32	3.36	3.50	4.24	11.00	11.03	11.17	11.21	11.34	12.04
3.24	3.28	3.42	3.46	4.00	4.33	CONTINUED					

351 Bridgeport Station

Crescent Beach to White Rock Centre and Bridgeport Station.
(Refer to Monday to Friday for routing)

Sullivan at Kidd #56082 128 St at Crescent Rd (E) #56088	Oxford at Russell (E) #56110	White Rock Centre Bay 5	South Surrey Park & Ride Bay 3	Bridgeport Station (E)	Sullivan at Kidd #56082 128 St at Crescent Rd (E) #56088	Oxford at Russell (E) #56110	White Rock Centre Bay 5	South Surrey Park & Ride Bay 3	Bridgeport Station (E)		
SATURDAY					SUNDAY & HOLIDAYS						
.... CONTINUED					.... CONTINUED						
-	-	11.31	11.36	11.49	12.19	-	-	8.25	8.30	8.40	9.08
11.30	11.33	11.47	11.51	12.04	12.34	8.26	8.29	8.41	8.45	8.55	9.23
-	-	12.01	12.06	12.19	12.49	-	-	8.54	8.59	9.10	9.38
11.58	12.02	12.17	12.21	12.34	1.04	8.55	8.58	9.10	9.14	9.25	9.53
-	-	12.31	12.36	12.49	1.19	-	-	9.24	9.29	9.41	10.09
12.31	12.35	12.50	12.54	1.07	1.37	9.24	9.27	9.40	9.44	9.56	10.24
-	-	1.04	1.09	1.22	1.52	-	-	9.53	9.58	10.10	10.38
1.01	1.05	1.20	1.24	1.37	2.07	9.52	9.55	10.09	10.13	10.25	10.53
-	-	1.34	1.39	1.52	2.22	-	-	10.23	10.28	10.40	11.08
1.31	1.35	1.50	1.54	2.07	2.37	10.22	10.25	10.39	10.43	10.55	11.23
-	-	2.04	2.09	2.22	2.52	-	-	10.53	10.58	11.11	11.41
2.01	2.05	2.20	2.24	2.37	3.07	10.52	10.55	11.09	11.13	11.26	11.56
-	-	2.34	2.39	2.52	3.22	-	-	11.23	11.28	11.41	12.11
2.33	2.36	2.50	2.54	3.07	3.37	11.25	11.28	11.42	11.46	11.59	12.29
-	-	3.04	3.09	3.22	3.52	-	-	11.59	12.04	12.17	12.47
3.03	3.06	3.20	3.24	3.37	4.07	11.59	12.03	12.18	12.22	12.35	1.05
-	-	3.34	3.39	3.52	4.22	-	-	12.32	12.37	12.50	1.20
3.33	3.36	3.50	3.54	4.07	4.35	12.29	12.33	12.48	12.52	1.05	1.35
-	-	4.04	4.09	4.22	4.50	-	-	1.02	1.07	1.20	1.50
4.04	4.07	4.21	4.25	4.37	5.05	12.59	1.03	1.18	1.22	1.35	2.03
-	-	4.35	4.40	4.52	5.20	-	-	1.32	1.37	1.50	2.18
4.34	4.37	4.51	4.55	5.07	5.35	1.30	1.34	1.49	1.53	2.06	2.34
-	-	5.05	5.10	5.22	5.50	-	-	2.03	2.08	2.21	2.49
5.04	5.07	5.21	5.25	5.37	6.05	2.00	2.04	2.19	2.23	2.36	3.04
-	-	5.35	5.40	5.52	6.20	-	-	2.31	2.38	2.51	3.19
5.35	5.38	5.51	5.55	6.07	6.35	2.30	2.34	2.49	2.53	3.06	3.34
-	-	6.05	6.10	6.22	6.50	-	-	3.01	3.08	3.21	3.49
6.07	6.10	6.22	6.26	6.37	7.05	3.00	3.04	3.19	3.23	3.36	4.04
-	-	6.36	6.41	6.52	7.20	-	-	3.31	3.38	3.51	4.19
6.38	6.41	6.53	6.57	7.07	7.34	3.30	3.34	3.49	3.53	4.06	4.34
-	-	7.07	7.12	7.22	7.49	-	-	4.01	4.08	4.21	4.49
7.08	7.11	7.23	7.27	7.37	8.04	4.01	4.05	4.20	4.24	4.38	5.04
-	-	7.37	7.42	7.52	8.19	-	-	4.32	4.39	4.51	5.19
7.38	7.41	7.53	7.57	8.07	8.34	4.31	4.35	4.50	4.54	5.06	5.34
-	-	8.07	8.12	8.22	8.49	-	-	5.02	5.09	5.21	5.49
8.09	8.12	8.24	8.27	8.37	9.04	5.01	5.05	5.20	5.24	5.36	6.04
-	-	8.37	8.42	8.52	9.19	-	-	5.32	5.39	5.51	6.19
8.40	8.43	8.54	8.57	9.07	9.34	5.32	5.36	5.50	5.54	6.06	6.34
-	-	9.07	9.12	9.22	9.49	-	-	6.02	6.09	6.21	6.49
9.10	9.13	9.24	9.27	9.37	10.04	6.04	6.07	6.21	6.25	6.36	7.04
9.30	9.33	9.44	9.47	9.57	10.24	-	-	6.33	6.40	6.51	7.19
10.00	10.03	10.14	10.17	10.27	10.53	6.35	6.38	6.52	6.56	7.06	7.33
10.30	10.33	10.44	10.47	10.57	11.23	-	-	7.06	7.11	7.21	7.48
11.00	11.03	11.14	11.17	11.27	11.53	7.05	7.08	7.22	7.26	7.36	8.03
11.45	11.48	11.59	12.02	12.11	12.37	-	-	7.36	7.41	7.51	8.18
SUNDAY & HOLIDAYS					SUNDAY & HOLIDAYS						
-	4.17	4.28	4.31	4.39	5.05	7.35	7.38	7.52	7.56	8.06	8.33
-	5.17	5.28	5.31	5.39	6.05	-	-	8.06	8.11	8.21	8.48
5.58	6.01	6.13	6.16	6.25	6.52	8.06	8.09	8.22	8.26	8.36	9.03
-	-	6.25	6.30	6.40	7.07	-	-	8.36	8.41	8.51	9.18
6.27	6.30	6.42	6.45	6.55	7.22	8.37	8.40	8.52	8.56	9.06	9.33
-	-	6.55	7.00	7.10	7.37	-	-	9.06	9.11	9.21	9.48
6.57	7.00	7.12	7.15	7.25	7.52	9.07	9.10	9.22	9.26	9.36	10.03
-	-	7.25	7.30	7.40	8.07	9.25	9.28	9.40	9.44	9.54	10.21
7.27	7.30	7.42	7.45	7.55	8.22	9.57	10.00	10.11	10.14	10.24	10.50
-	-	7.55	8.00	8.10	8.38	10.27	10.30	10.41	10.44	10.54	11.20
7.56	7.59	8.11	8.15	8.25	8.53	11.00	11.03	11.14	11.17	11.27	11.53
CONTINUED					CONTINUED						

351 Crescent Beach

From Bridgeport Station via Great Canadian Way, Sea Island Way, Hwy 99, Massey Exchange, Hwy 99, Matthews Exchange, Hwy 99, offramp, South Surrey Park & Ride, King George Blvd, 152 St; then some trips continue from 152 St via 16 Ave, Oxford to Vine; or most trips continue from 152 St via Johnston, Thrift, Oxford, North Bluff, 128 St, Crescent, Beecher, Sullivan to Kidd.

Bridgeport Station Bay 9	South Surrey Park & Ride (E) Bay 1	White Rock Centre Bay 1 (E)	Johnston at Russell #60246	Sullivan at Kidd (E)	Bridgeport Station Bay 9	South Surrey Park & Ride (E) Bay 1	White Rock Centre Bay 1 (E)	Johnston at Russell #60246	Sullivan at Kidd (E)
MONDAY TO FRIDAY					MONDAY TO FRIDAY				
5.15	5.42	-	5.49	6.06	CONTINUED				
5.30	6.01	-	6.08	6.23	6.00	6.37	-	6.46	7.05
5.45	6.20	6.28	-	-	6.12	6.49	-	6.58	7.17
6.00	6.31	-	6.38	6.53	6.18	6.54	-	-	-
6.15	6.47	-	6.54	7.11	6.24	7.01	-	7.10	7.29
6.30	7.02	-	7.09	7.26	6.36	7.11	-	7.19	7.38
6.45	7.17	-	7.24	7.41	6.42	7.16	-	-	-
7.00	7.38	7.47	-	-	6.48	7.23	-	7.31	7.50
7.15	7.48	-	7.55	8.12	7.00	7.34	-	7.42	8.00
7.30	8.03	-	8.10	8.27	7.15	7.49	-	7.57	8.15
7.45	8.19	-	8.27	8.46	7.30	8.04	-	8.12	8.30
8.00	8.34	-	8.42	9.01	7.45	8.19	-	8.27	8.45
8.15	8.49	-	8.57	9.16	8.00	8.33	-	8.41	8.59
8.30	9.04	-	9.12	9.31	8.15	8.48	-	8.56	9.14
8.45	9.19	-	9.27	9.46	8.30	9.03	-	9.11	9.29
9.00	9.34	-	9.42	10.01	8.45	9.18	-	9.26	9.44
9.15	9.52	10.00	-	-	9.00	9.33	-	9.41	9.59
9.30	10.03	-	10.11	10.30	9.20	9.53	-	10.01	10.19
9.45	10.20	10.27	-	-	9.40	10.13	-	10.21	10.39
10.00	10.34	-	10.42	11.00	10.00	10.33	-	10.41	10.59
10.15	10.51	10.58	-	-	10.30	11.03	-	11.11	11.29
10.30	11.04	-	11.12	11.32	11.02	11.34	-	11.41	11.59
10.45	11.21	11.29	-	-	12.02	12.33	-	12.40	12.57
11.00	11.34	-	11.42	12.02	1.00	1.31	-	1.38	1.55
11.15	11.51	11.59	-	-	1.59	2.30	-	2.37	2.54
11.30	12.03	-	12.11	12.31	SATURDAY				
11.45	12.20	12.28	-	-	5.20	5.48	-	5.54	6.09
12.00	12.34	-	12.42	1.02	5.45	6.13	6.19	-	-
12.15	12.50	12.58	-	-	6.00	6.29	-	6.36	6.51
12.30	1.04	-	1.12	1.32	6.15	6.44	6.50	-	-
12.45	1.22	1.31	-	-	6.30	6.59	-	7.06	7.21
1.00	1.35	-	1.43	2.05	6.45	7.16	7.22	-	-
1.15	1.52	2.01	-	-	7.00	7.29	-	7.36	7.51
1.30	2.05	-	2.13	2.35	7.15	7.46	7.52	-	-
1.45	2.20	2.28	-	-	7.30	7.59	-	8.06	8.21
2.00	2.37	-	2.45	3.07	7.45	8.16	8.22	-	-
2.15	2.52	-	3.00	3.22	8.00	8.29	-	8.36	8.51
2.30	3.07	-	3.15	3.37	8.15	8.46	8.52	-	-
2.45	3.22	-	3.30	3.52	8.30	9.00	-	9.07	9.24
3.00	3.39	-	3.48	4.09	8.42	9.13	9.19	-	-
3.10	3.49	-	3.58	4.19	8.55	9.25	-	9.32	9.49
3.20	3.59	-	4.08	4.29	9.10	9.41	9.48	-	-
3.30	4.09	-	4.18	4.39	9.25	9.56	-	10.03	10.20
3.40	4.19	-	4.28	4.49	9.40	10.12	10.19	-	-
3.50	4.30	-	4.39	5.00	9.55	10.26	-	10.33	10.50
4.00	4.42	-	4.51	5.12	10.10	10.43	10.50	-	-
4.10	4.52	-	5.01	5.22	10.25	10.57	-	11.04	11.21
4.20	5.02	-	5.11	5.32	10.40	11.13	11.20	-	-
4.30	5.12	-	5.21	5.42	10.55	11.27	-	11.35	11.54
4.40	5.22	-	5.31	5.52	11.10	11.44	11.51	-	-
4.50	5.32	-	5.41	6.01	11.25	11.58	-	12.06	12.26
5.00	5.41	-	5.50	6.09	11.40	12.15	12.22	-	-
5.12	5.53	-	6.02	6.21	11.55	12.28	-	12.36	12.56
5.24	6.04	-	6.13	6.32	12.10	12.47	12.54	-	-
5.36	6.15	-	6.24	6.43	12.25	1.00	-	1.08	1.28
5.48	6.27	-	6.36	6.55	CONTINUED ...				

351 Crescent Beach

Bridgeport Station to White Rock Centre and Crescent Beach.
(Refer to Monday to Friday for routing)

Bridgeport Station Bay 9	South Surrey Park & Ride (E) Bay 1	White Rock Centre Bay 1 (E)	Johnston at Russell #60246	Sullivan at Kidd (E)	Bridgeport Station Bay 9	South Surrey Park & Ride (E) Bay 1	White Rock Centre Bay 1 (E)	Johnston at Russell #60246	Sullivan at Kidd (E)
SATURDAY					SUNDAY & HOLIDAYS				
... CONTINUED					... CONTINUED				
12.40	1.17	1.25	-	-	8.51	9.21	-	9.28	9.45
12.55	1.30	-	1.38	1.58	9.06	9.38	9.45	-	-
1.10	1.47	1.55	-	-	9.21	9.52	-	9.59	10.16
1.25	2.01	-	2.09	2.29	9.36	10.08	10.15	-	-
1.40	2.17	2.24	-	-	9.51	10.22	-	10.29	10.46
1.55	2.31	-	2.39	2.59	10.06	10.38	10.45	-	-
2.10	2.47	2.54	-	-	10.21	10.53	-	11.01	11.18
2.25	3.01	-	3.09	3.29	10.36	11.08	11.15	-	-
2.40	3.15	3.22	-	-	10.51	11.24	-	11.32	11.52
2.55	3.29	-	3.37	3.57	11.06	11.42	11.50	-	-
3.10	3.45	3.52	-	-	11.21	11.56	-	12.04	12.24
3.25	3.59	-	4.07	4.27	11.36	12.12	12.20	-	-
3.40	4.15	4.22	-	-	11.51	12.26	-	12.34	12.54
3.55	4.29	-	4.37	4.57	12.06	12.42	12.50	-	-
4.10	4.44	4.51	-	-	12.21	12.56	-	1.04	1.26
4.25	4.58	-	5.06	5.27	12.36	1.14	1.22	-	-
4.40	5.14	5.21	-	-	12.51	1.28	-	1.34	1.56
4.55	5.28	-	5.36	5.57	1.06	1.44	1.52	-	-
5.10	5.44	5.51	-	-	1.21	1.56	-	2.04	2.26
5.25	5.58	-	6.06	6.25	1.36	2.14	2.22	-	-
5.40	6.14	6.21	-	-	1.51	2.26	-	2.34	2.56
5.55	6.28	-	6.36	6.55	2.06	2.44	2.52	-	-
6.12	6.46	6.53	-	-	2.21	2.56	-	3.04	3.26
6.30	7.03	-	7.11	7.30	2.36	3.14	3.22	-	-
6.45	7.18	7.25	-	-	2.51	3.26	-	3.34	3.56
7.00	7.33	-	7.41	7.59	3.06	3.42	3.49	-	-
7.15	7.48	7.55	-	-	3.21	3.57	-	4.06	4.28
7.30	8.03	-	8.10	8.28	3.36	4.12	4.19	-	-
7.45	8.18	8.25	-	-	3.51	4.27	-	4.36	4.58
8.00	8.33	-	8.40	8.58	4.06	4.41	4.48	-	-
8.15	8.48	8.55	-	-	4.21	4.56	-	5.05	5.26
8.30	9.03	-	9.10	9.27	4.36	5.11	5.18	-	-
8.45	9.18	-	9.25	9.42	4.51	5.26	-	5.35	5.56
9.00	9.32	-	9.39	9.56	5.06	5.41	5.48	-	-
9.20	9.52	-	9.59	10.16	5.21	5.56	-	6.05	6.24
9.40	10.12	-	10.19	10.36	5.36	6.12	6.20	-	-
10.00	10.32	-	10.39	10.56	5.51	6.26	-	6.34	6.53
10.30	11.02	-	11.09	11.26	6.07	6.43	6.51	-	-
11.00	11.32	-	11.39	11.56	6.23	6.58	-	7.06	7.25
12.00	12.31	-	12.38	12.55	6.39	7.15	7.22	-	-
12.45	1.16	-	1.23	1.40	6.55	7.29	-	7.37	7.55
1.45	2.16	-	2.23	2.40	7.10	7.46	7.53	-	-
SUNDAY & HOLIDAYS					SUNDAY & HOLIDAYS				
5.20	5.48	-	5.54	6.09	7.25	7.59	-	8.07	8.25
5.36	6.04	6.10	-	-	7.40	8.16	8.23	-	-
5.51	6.20	-	6.27	6.42	7.55	8.29	-	8.37	8.55
6.06	6.35	6.41	-	-	8.10	8.46	8.53	-	-
6.21	6.50	-	6.57	7.12	8.25	8.58	-	9.05	9.22
6.36	7.07	7.13	-	-	8.40	9.13	-	9.20	9.37
6.51	7.20	-	7.27	7.42	8.58	9.31	-	9.38	9.55
7.06	7.38	7.45	-	-	9.18	9.50	-	9.57	10.14
7.21	7.50	-	7.57	8.12	9.38	10.10	-	10.17	10.34
7.36	8.08	8.15	-	-	9.58	10.30	-	10.37	10.54
7.51	8.20	-	8.27	8.42	10.28	11.00	-	11.07	11.24
8.06	8.38	8.45	-	-	11.00	11.32	-	11.39	11.56
8.21	8.51	-	8.58	9.15	12.00	12.31	-	12.38	12.55
8.36	9.08	9.15	-	-	12.45	1.16	-	1.23	1.40
CONTINUED ...					1.45	2.16	-	2.23	2.40

Effective - December 31, 2018

STOPPING PROCEDURES - 351, 352 & 354

- To Bridgeport Station

Stops for pick-up and drop-off are made at all local stops up to and including South Surrey Park & Ride and Massey Exchange (Hwy 99 at Steveston Hwy)(Matthews Exchange for 351 only, after the AM peak). Note - request stop only for 352 & 354 at Massey Exchange.

- To White Rock

Starting at Massey Exchange (Hwy 99 at Steveston Hwy) stops are made for both pick-up and drop-off at all stops to the termini.

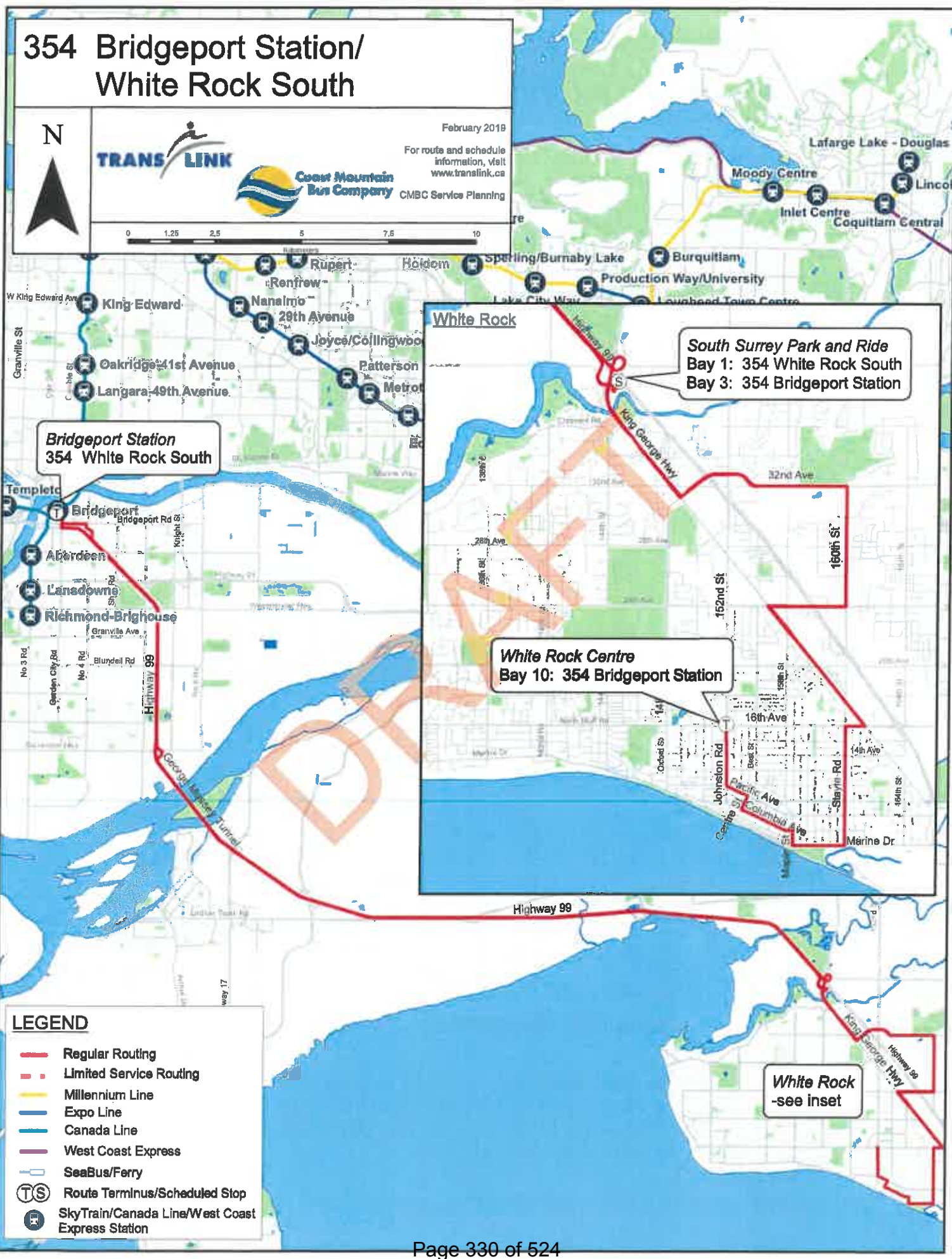
Note - Only the 351 stops at Matthews Exchange for pick up and drop-off.

354 Bridgeport Station/ White Rock South



February 2019
 For route and schedule information, visit www.translink.ca
 CMBC Service Planning





South Surrey Park and Ride
 Bay 1: 354 White Rock South
 Bay 3: 354 Bridgeport Station

White Rock Centre
 Bay 10: 354 Bridgeport Station

White Rock
 -see inset

Bridgeport Station
 354 White Rock South

LEGEND

- Regular Routing
- - - Limited Service Routing
- Millennium Line
- Expo Line
- Canada Line
- West Coast Express
- SeaBus/Ferry
- TS Route Terminus/Scheduled Stop
- E SkyTrain/Canada Line/West Coast Express Station

354 White Rock South/White Rock Centre/ Bridgeport Station

From White Rock Centre via North Bluff, Johnston, Pacific, Centre, Columbia, Maple, Marine, 160 St, 16 Ave, King George Blvd, 24 Ave, 160 St, 32 Ave, 32 Ave Diversion, King George Blvd South Surrey Park & Ride, King George Blvd, Hwy 99, Bridgeport Rd, Great Canadian Way to Bridgeport Station.

From Bridgeport Station via Great Canadian Way, Sea Island Way, Hwy 99, Massey Exchange, Hwy 99, offramp, South Surrey Park & Ride, King George Blvd, 32 Ave Diversion, 32 Ave, 160 St, 24 Ave, King George Blvd, 16 Ave, Stayte, Marine, Maple, Columbia, Centre, Pacific, Johnston, North Bluff to White White Rock Centre.

White Rock Centre Bay 2	160 St at 24 Ave (E) #56073	South Surrey Park & Ride Bay 3	Bridgeport Station (E)	Bridgeport Station Bay 10	South Surrey Park & Ride (E) Bay 1	24 Ave at 160 St (E) #61630	White Rock Centre (E)
MONDAY TO FRIDAY				MONDAY TO FRIDAY			
5.16	5.32	5.45	6.11	4.05	4.54	5.04	5.17
5.46	6.02	6.15	6.41	4.20	5.09	5.19	5.32
6.09	6.26	6.40	7.10	4.35	5.23	5.33	5.46
6.17	6.34	6.48	7.18	4.50	5.38	5.48	6.01
6.24	6.41	6.55	7.25	5.05	5.52	6.02	6.15
6.35	6.52	7.06	7.39	5.20	6.07	6.17	6.30
6.43	7.01	7.15	7.52	5.35	6.18	6.27	6.39
6.57	7.15	7.29	8.06	5.55	6.38	6.47	6.59
7.17	7.35	7.49	8.23				
7.35	7.53	8.07	8.41				

STOPPING PROCEDURES - 351, 352 & 354

- To Bridgeport Station

Stops for **pick-up and drop-off** are made at all local stops up to and including South Surrey Park & Ride and Massey Exchange (Hwy 99 at Steveston Hwy)(Matthews Exchange for 351 only, after the AM peak). **Note** - request stop only for 352 & 354 at Massey Exchange.

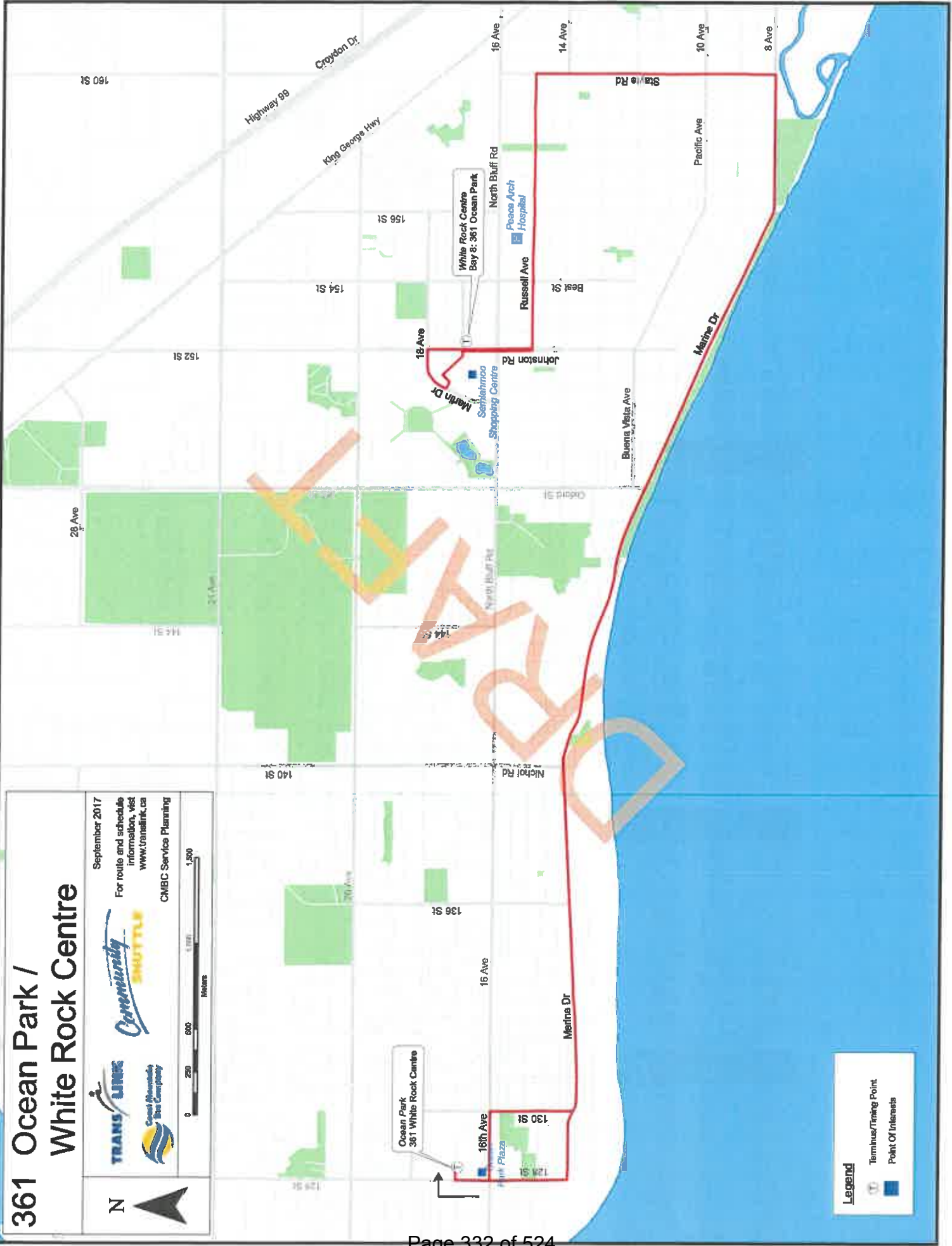
- To White Rock

Starting at Massey Exchange (Hwy 99 at Steveston Hwy) stops are made for both **pick-up and drop-off** at all stops to the termini.

Note - Only the 351 stops at Matthews Exchange for pick up and drop-off.

361 Ocean Park / White Rock Centre

September 2017
For route and schedule
information, visit
www.translink.ca
CMBC Service Planning



Legend

- Terminus/Trailing Point
- Point Of Interest

361 White Rock Centre / 361 Ocean Park

From 128 St at 16 Ave via 128 St, Marine, 160 St, Russell, Johnston, 152 St to White Rock Centre.

From White Rock Centre via 152 St, Martin, mall access road, 152 St, Johnston, Russell, Stayte, Marine, 130 St, 16 Ave, 128 St to 17 Ave.

128 St at 16 Ave #56220 Marine at Finlay (E) #56243 White Rock Centre (E)	128 St at 16 Ave #56220 Marine at Finlay (E) #56243 White Rock Centre (E)	White Rock Centre Bay 8 Marine at Maple (E) #56196 128 St at 17 th Ave (E)	White Rock Centre Bay 8 Marine at Maple (E) #56196 128 St at 17 th Ave (E)
MONDAY TO FRIDAY		MONDAY TO FRIDAY	
5.05 5.16 5.25	.... CONTINUED	5.30 5.42 5.56	.... CONTINUED
5.35 5.46 5.55	12.35 12.49 12.59	6.00 6.12 6.26	1.00 1.13 1.29
6.05 6.16 6.25	1.05 1.19 1.29	6.30 6.42 6.56	1.30 1.43 1.59
6.35 6.46 6.55	1.35 1.49 1.59	7.00 7.12 7.26	2.00 2.13 2.29
7.05 7.17 7.26	2.05 2.19 2.29	7.30 7.42 7.56	2.30 2.43 2.59
7.35 7.47 7.56	2.35 2.49 2.59	8.00 8.12 8.26	3.00 3.13 3.29
8.05 8.18 8.27	3.05 3.19 3.29	8.30 8.43 8.59	3.30 3.43 3.59
8.35 8.48 8.57	3.35 3.49 3.59	9.00 9.12 9.27	4.00 4.13 4.29
9.05 9.18 9.27	4.05 4.19 4.29	9.30 9.42 9.57	4.30 4.43 4.59
9.35 9.48 9.57	4.35 4.48 4.57	10.00 10.12 10.27	5.00 5.13 5.29
10.05 10.18 10.27	5.05 5.18 5.27	10.30 10.42 10.57	5.30 5.43 5.59
10.35 10.49 10.59	5.35 5.48 5.57	11.00 11.12 11.27	6.00 6.13 6.29
11.05 11.19 11.29	6.05 6.18 6.27	11.30 11.42 11.57	6.30 6.43 6.58
11.35 11.49 11.59	6.35 6.48 6.57	12.00 12.13 12.29	6.55 7.07 7.22
12.05 12.19 12.29	7.05 7.17 7.26	12.30 12.43 12.59	7.25 7.37 7.52
CONTINUED		CONTINUED	
SATURDAY		SATURDAY	
7.30 7.41 7.49	.... CONTINUED	8.00 8.10 8.25	.... CONTINUED
8.30 8.41 8.49	4.30 4.45 4.56	9.00 9.10 9.25	5.00 5.10 5.25
9.30 9.41 9.49	5.30 5.42 5.51	10.00 10.12 10.27	6.00 6.10 6.25
10.30 10.42 10.51	6.30 6.42 6.51	11.00 11.12 11.27	7.00 7.10 7.25
11.30 11.43 11.53	7.30 7.42 7.51	12.00 12.13 12.28	8.00 8.10 8.25
12.30 12.43 12.53	8.30 8.42 8.51	1.00 1.13 1.28	9.00 9.10 9.25
1.30 1.43 1.53		2.00 2.13 2.28	
2.30 2.43 2.53		3.00 3.13 3.28	
3.30 3.44 3.54		4.00 4.13 4.28	
CONTINUED		CONTINUED	
SUNDAY & HOLIDAYS		SUNDAY & HOLIDAYS	
8.29 8.40 8.49	.... CONTINUED	9.00 9.12 9.27	.... CONTINUED
9.29 9.42 9.51	3.29 3.46 3.58	10.00 10.12 10.27	4.00 4.14 4.31
10.29 10.42 10.51	4.29 4.44 4.55	11.00 11.13 11.29	5.00 5.14 5.30
11.29 11.43 11.54	5.29 5.43 5.54	12.00 12.14 12.31	6.00 6.13 6.29
12.29 12.43 12.54	6.33 6.47 6.58	1.00 1.14 1.32	
1.29 1.44 1.55		2.00 2.14 2.32	
2.29 2.45 2.56		3.00 3.15 3.33	
CONTINUED		CONTINUED	

362 Seaside / White Rock Centre



September 2017
For route and schedule
information, visit
www.translink.ca
CMBC Service Planning



Legend

- Terminus/Timing Point
- Point Of Interests



362 Seaside / 362 White Rock Centre

Loop service from White Rock Centre via 152 St, Martin, mall access road, 152 St, Johnston, Buena Vista, Oxford, Marine to Martin; returning via Marine, Maple, Columbia, Centre, Pacific, Johnston, 152 St to White Rock Centre.

White Rock Centre Bay 8	Marine at Martin (E) #56238	White Rock Centre (E)	White Rock Centre Bay 8	Marine at Martin (E) #56238	White Rock Centre (E)	White Rock Centre Bay 8	Marine at Martin (E) #56238	White Rock Centre (E)
MONDAY TO FRIDAY			SATURDAY			SUNDAY & HOLIDAYS		
9.30	9.42	9.52	7.59	8.10	8.22	8.31	8.41	8.51
10.00	10.12	10.22	8.29	8.40	8.52	9.01	9.11	9.21
10.30	10.42	10.52	8.59	9.10	9.22	9.31	9.41	9.51
11.00	11.12	11.24	9.29	9.40	9.52	10.01	10.11	10.21
11.30	11.42	11.54	9.58	10.10	10.22	10.30	10.41	10.51
12.00	12.12	12.24	10.28	10.40	10.52	11.00	11.11	11.21
12.30	12.42	12.54	10.58	11.10	11.22	11.29	11.41	11.52
1.00	1.12	1.24	11.28	11.40	11.52	11.59	12.11	12.22
1.30	1.42	1.54	11.58	12.10	12.23	12.29	12.41	12.53
2.00	2.12	2.24	12.28	12.40	12.53	12.57	1.11	1.23
2.30	2.42	2.54	12.58	1.10	1.23	1.27	1.41	1.54
3.00	3.12	3.24	1.28	1.40	1.53	1.57	2.11	2.24
3.30	3.42	3.54	1.58	2.10	2.24	2.29	2.41	2.54
4.00	4.12	4.22	2.28	2.40	2.54	2.59	3.11	3.24
4.30	4.42	4.52	2.58	3.10	3.24	3.29	3.41	3.53
5.00	5.12	5.22	3.28	3.40	3.54	3.59	4.11	4.23
5.30	5.42	5.52	3.58	4.10	4.24	4.29	4.41	4.52
6.00	6.12	6.22	4.28	4.40	4.54	4.59	5.11	5.22
6.30	6.42	6.52	4.59	5.10	5.22	5.30	5.41	5.52
7.00	7.12	7.22	5.29	5.40	5.52	6.00	6.11	6.22
7.30	7.42	7.52	5.59	6.10	6.22	6.30	6.41	6.52
8.00	8.12	8.22	6.29	6.40	6.52	7.00	7.11	7.22
8.30	8.42	8.52	7.00	7.10	7.21	7.30	7.41	7.52
9.00	9.12	9.22	7.30	7.40	7.51	8.00	8.11	8.22
9.30	9.42	9.52	8.00	8.10	8.21	8.30	8.41	8.52
10.10*	10.22*	10.32*	8.30	8.40	8.51	9.00	9.11	9.22
10.50*	11.02*	11.12*	9.00	9.10	9.21	9.30	9.41	9.52
11.30*	11.42*	11.52*	9.30	9.40	9.51			
			10.00	10.10	10.21			
			10.30	10.40	10.51			
			11.00	11.10	11.21			
			11.30	11.40	11.51			

363 Peace Arch Hospital/ Southpoint

September 2017



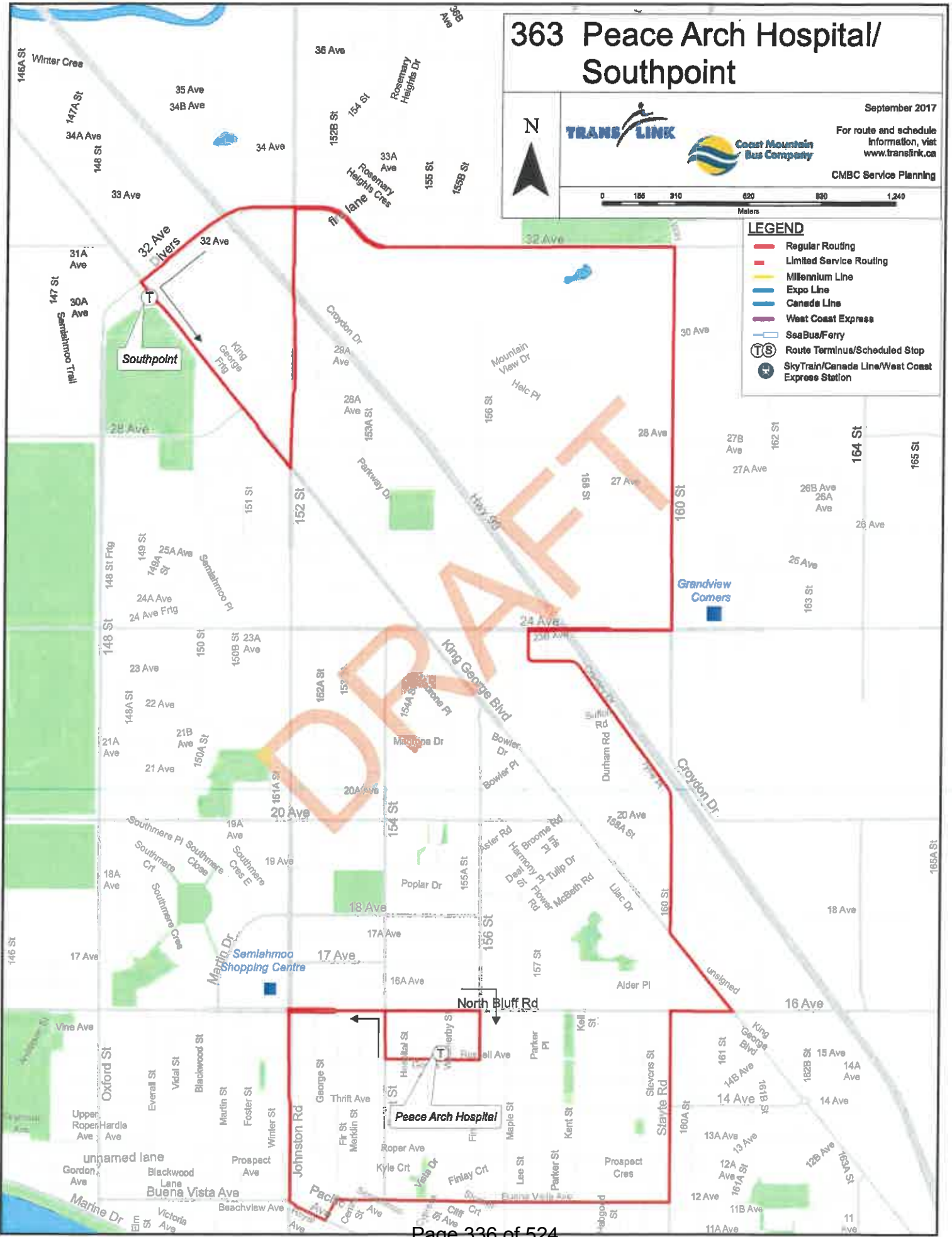
For route and schedule information, visit www.translink.ca

CMBC Service Planning



LEGEND

- Regular Routing
- - - Limited Service Routing
- Millennium Line
- Expo Line
- Canada Line
- West Coast Express
- SeaBus/Ferry
- T Route Terminus/Scheduled Stop
- T SkyTrain/Canada Line/West Coast Express Station



363 Southpoint

From Peace Arch Hospital via Russell, Best, 16 Ave, Johnston, Pacific, Fir, Buena Vista, 160 St, 16 Ave, King George Blvd, 160 St, Cranley, 157 St, 24 Ave, 160 St, 32 Ave, 32 Ave Diversion to King George Blvd.

Peace Arch Hospital #56258	160 St at 12 Ave (E) #56250	160 St at 24 Ave (E) #56073	King George Blvd at 32 Ave Div (E)	Peace Arch Hospital #56258	160 St at 12 Ave (E) #56250	160 St at 24 Ave (E) #56073	King George Blvd at 32 Ave Div (E)
MONDAY TO FRIDAY				SATURDAY			
5.30	5.38	5.48	5.59	.... CONTINUED			
6.00	6.08	6.18	6.29	5.00	5.09	5.19	5.31
6.30	6.38	6.48	6.59	5.30	5.38	5.48	6.00
7.00	7.08	7.18	7.29	6.00	6.08	6.18	6.30
7.30	7.38	7.48	7.59	6.30	6.38	6.48	7.00
8.00	8.08	8.18	8.29	7.00	7.08	7.18	7.30
8.30	8.38	8.48	9.00	7.30	7.38	7.48	8.00
9.00	9.08	9.18	9.30	8.00	8.08	8.18	8.30
9.30	9.38	9.48	10.00	8.30	8.38	8.48	8.57
10.00	10.08	10.18	10.30	9.00	9.08	9.16	9.27
10.30	10.38	10.48	11.00	9.30	9.38	9.46	9.57
11.00	11.08	11.18	11.30	10.00	10.08	10.16	10.27
11.30	11.38	11.48	12.00	10.30	10.38	10.46	10.57
12.00	12.08	12.18	12.30	SUNDAY & HOLIDAYS			
12.30	12.38	12.48	1.00	7.00	7.08	7.18	7.29
1.00	1.08	1.18	1.30	7.30	7.38	7.48	7.59
1.30	1.38	1.48	2.00	8.00	8.09	8.19	8.31
2.00	2.09	2.20	2.33	8.30	8.39	8.49	9.01
2.30	2.39	2.50	3.03	9.00	9.09	9.19	9.31
3.00	3.09	3.20	3.33	9.30	9.39	9.49	10.01
3.30	3.39	3.50	4.03	10.00	10.09	10.19	10.31
4.00	4.09	4.20	4.33	10.30	10.39	10.49	11.01
4.30	4.39	4.50	5.03	11.00	11.09	11.19	11.31
5.00	5.09	5.20	5.33	11.30	11.39	11.49	12.01
5.30	5.38	5.48	6.00	12.00	12.09	12.19	12.31
6.00	6.08	6.18	6.30	12.30	12.39	12.49	1.01
6.30	6.38	6.48	6.59	1.00	1.09	1.20	1.32
7.00	7.08	7.18	7.29	1.30	1.39	1.50	2.02
7.30	7.38	7.48	7.59	2.00	2.09	2.19	2.31
8.00	8.08	8.18	8.29	2.30	2.38	2.48	3.00
8.30	8.38	8.48	8.59	3.00	3.08	3.18	3.30
9.00	9.08	9.18	9.29	3.30	3.38	3.48	4.00
9.30	9.38	9.48	9.59	4.00	4.08	4.18	4.30
10.00	10.08	10.18	10.29	4.30	4.38	4.48	5.00
10.30	10.38	10.48	10.59	5.00	5.08	5.18	5.30
SATURDAY				5.30	5.38	5.48	6.00
7.00	7.08	7.18	7.29	6.00	6.08	6.18	6.30
7.30	7.38	7.48	7.59	6.30	6.38	6.48	7.00
8.00	8.09	8.19	8.31	7.00	7.08	7.18	7.30
8.30	8.39	8.49	9.01	7.30	7.38	7.48	8.00
9.00	9.09	9.19	9.31	8.00	8.08	8.18	8.30
9.30	9.39	9.50	10.02	8.30	8.38	8.48	9.00
10.00	10.09	10.20	10.32	9.00	9.08	9.17	9.28
10.30	10.39	10.50	11.02	9.30	9.38	9.47	9.58
11.00	11.09	11.20	11.32				
11.30	11.39	11.50	12.03				
12.00	12.09	12.20	12.33				
12.30	12.40	12.51	1.04				
1.00	1.09	1.20	1.32				
1.30	1.39	1.50	2.02				
2.00	2.09	2.20	2.32				
2.30	2.39	2.50	3.02				
3.00	3.09	3.20	3.32				
3.30	3.39	3.50	4.02				
4.00	4.09	4.19	4.31				
4.30	4.39	4.49	5.01				

363 Peace Arch Hospital

From King George Blvd at 32 Ave Diversion via King George Blvd, 152 St, 32 Ave Diversion, 32 Ave, 160 St, 24 Ave, 157 St, Cranley, 160 St, King George Blvd, 16 Ave, Stayte, Buena Vista, Fir, Pacific, Johnston, North Bluff, Finlay, Russell to Peace Arch Hospital.

King George Blvd at 32 Ave Div #55474	24 Ave at 160 St (E) #61630	Buena Vista at Stayte (E) #56267	Peace Arch Hospital (E)	King George Blvd at 32 Ave Div #55474	24 Ave at 160 St (E) #61630	Buena Vista at Stayte (E) #56267	Peace Arch Hospital (E)
MONDAY TO FRIDAY				SATURDAY			
5.59	6.11	6.19	6.28	.... CONTINUED			
6.29	6.41	6.50	7.00	5.40	5.52	6.01	6.10
6.59	7.11	7.20	7.30	6.10	6.22	6.31	6.40
7.29	7.41	7.50	8.00	6.40	6.52	7.01	7.10
7.59	8.11	8.20	8.30	7.05	7.17	7.26	7.35
8.29	8.41	8.49	8.58	7.30	7.40	7.48	7.56
9.00	9.12	9.20	9.29	8.00	8.10	8.18	8.26
9.30	9.42	9.50	9.59	8.30	8.40	8.48	8.56
10.00	10.12	10.21	10.30	8.57	9.07	9.15	9.23
10.30	10.42	10.51	11.00	9.27	9.37	9.45	9.53
11.00	11.12	11.21	11.30	9.57	10.07	10.15	10.23
11.30	11.42	11.51	12.00	10.27	10.37	10.45	10.53
12.00	12.12	12.21	12.30	10.57	11.07	11.15	11.23
12.30	12.42	12.51	1.00	SUNDAY & HOLIDAYS			
1.00	1.12	1.21	1.30	7.40	7.51	7.59	8.08
1.30	1.42	1.51	2.00	8.10	8.21	8.29	8.38
2.00	2.13	2.23	2.33	8.40	8.51	8.59	9.08
2.33	2.46	2.56	3.06	9.10	9.21	9.29	9.38
3.03	3.16	3.26	3.36	9.40	9.51	9.59	10.08
3.33	3.46	3.56	4.06	10.10	10.21	10.29	10.38
4.03	4.16	4.26	4.36	10.40	10.52	11.01	11.10
4.33	4.46	4.55	5.05	11.10	11.22	11.31	11.40
5.03	5.16	5.25	5.35	11.40	11.52	12.01	12.10
5.33	5.44	5.52	6.01	12.10	12.22	12.31	12.40
6.00	6.11	6.19	6.28	12.40	12.52	1.01	1.10
6.30	6.41	6.49	6.58	1.10	1.22	1.31	1.40
6.59	7.10	7.18	7.27	1.40	1.52	2.01	2.11
7.29	7.40	7.48	7.57	2.10	2.22	2.31	2.41
7.59	8.10	8.18	8.27	2.40	2.52	3.01	3.10
8.29	8.39	8.46	8.54	3.10	3.22	3.31	3.40
8.59	9.09	9.16	9.24	3.40	3.52	4.00	4.09
9.29	9.39	9.46	9.54	4.10	4.22	4.30	4.39
9.59	10.09	10.16	10.24	4.40	4.51	4.59	5.08
10.29	10.39	10.46	10.54	5.10	5.21	5.29	5.38
10.59	11.09	11.16	11.24	5.40	5.51	5.58	6.07
SATURDAY				6.10	6.21	6.28	6.37
7.40	7.53	8.02	8.12	6.40	6.50	6.58	7.06
8.10	8.22	8.31	8.40	7.05	7.15	7.23	7.31
8.40	8.52	9.01	9.10	7.30	7.40	7.47	7.55
9.10	9.22	9.31	9.40	8.00	8.10	8.17	8.25
9.40	9.52	10.01	10.11	8.30	8.40	8.47	8.55
10.10	10.22	10.31	10.41	9.00	9.10	9.17	9.25
10.40	10.52	11.01	11.11	9.28	9.38	9.45	9.53
11.10	11.22	11.31	11.41	9.58	10.08	10.15	10.23
11.40	11.52	12.01	12.11				
12.10	12.22	12.31	12.41				
12.40	12.52	1.01	1.11				
1.10	1.22	1.31	1.41				
1.40	1.52	2.01	2.11				
2.10	2.22	2.31	2.41				
2.40	2.52	3.01	3.11				
3.10	3.22	3.31	3.40				
3.40	3.52	4.01	4.10				
4.10	4.22	4.31	4.40				
4.40	4.52	5.01	5.10				
5.10	5.22	5.31	5.40				
CONTINUED							

375 King George Station / White Rock South



January 2019

For route and schedule information, visit www.translink.ca



CMBC Service Planning

N



Guildford Exchange
Bay 1: 375 White Rock South

White Rock Centre
Bay 5: 375 Guildford

King George Highway @ 8th Ave
375 Guildford

LEGEND

- Regular Routing
- Expo Line
- Route Terminus/Scheduled Stop
- SkyTrain / Canada Line / West Coast Express Station

375 White Rock South / 375 Guildford

From Guildford Exchange via 104 Ave, 152 St, North Bluff (White Rock Centre), North Bluff, King George, 8 Ave roundabout, King George to 1100 Block.

From King George Blvd at 1100 Block via King George, 16 Ave, 152 St (White Rock Centre), 152 St, 100 Ave, 150 St, 104 Ave to Guildford Exchange.

Guildford Exchange Bay 1 White Rock Centre Bay 4 King George at 1100 Block (E)			Guildford Exchange Bay 1 White Rock Centre Bay 4 King George at 1100 Block (E)			King George at 1100 Block #55375 White Rock Centre Bay 5 Guildford Exchange Hudson's Bay entrance (E) Guildford Exchange Bay 3 (E)			King George at 1100 Block #55375 White Rock Centre Bay 5 Guildford Exchange Hudson's Bay entrance (E) Guildford Exchange Bay 3 (E)				
MON TO FRI			SATURDAY			MON TO FRI			SATURDAY				
6.18	6.53	7.03	....	CONTINUED		6.13	6.19	-	6.57	....	CONTINUED		
6.48	7.25	7.38	3.20	4.02	4.11	6.43	6.49	7.25	-	3.49	3.56	-	4.41
7.18	7.58	8.11	3.50	4.32	4.41	7.18	7.24	-	8.02	4.19	4.26	-	5.11
7.45	8.29	8.44	4.20	5.02	5.11	7.47	7.54	8.33	-	4.49	4.56	-	5.41
8.15	8.59	9.14	4.50	5.32	5.41	8.17	8.24	-	9.07	5.19	5.26	-	6.07
8.45	9.23	9.36	5.20	6.02	6.11	8.49	8.56	-	9.37	5.49	5.56	-	6.37
9.18	9.58	10.11	5.50	6.32	6.41	9.19	9.26	10.05	-	6.19	6.26	-	7.04
9.48	10.28	10.41	6.20	6.58	7.07	9.47	9.54	-	10.35	6.49	6.56	-	7.34
10.18	10.59	11.12	6.50	7.28	7.37	10.17	10.24	11.03	-	7.49	7.56	-	8.34
10.48	11.29	11.42	7.50	8.28	8.37	10.47	10.54	11.33	-	8.49	8.56	-	9.31
11.18	11.59	12.12	8.50	9.28	9.37	11.17	11.24	12.03	-	SUN & HOL			
11.48	12.29	12.42	9.50	10.25	10.34	11.47	11.54	-	12.36	6.49	6.56	7.32	-
12.17	12.58	1.11	10.50	11.25	11.34	12.17	12.25	1.04	-	7.19	7.26	-	8.01
12.47	1.28	1.41	SUN & HOL			12.47	12.55	1.34	-	7.49	7.56	8.32	-
1.17	1.58	2.11	8.50	7.22	7.32	1.17	1.25	2.04	-	8.19	8.26	9.02	-
1.47	2.29	2.42	7.20	7.52	8.05	1.48	1.56	2.39	-	8.49	8.56	9.32	-
2.17	3.02	3.15	7.50	8.22	8.35	2.19	2.27	-	3.14	9.19	9.26	10.02	-
2.47	3.34	3.47	8.20	8.55	9.08	2.49	2.57	-	3.50	9.49	9.56	10.32	-
3.17	4.06	4.18	8.50	9.25	9.38	3.20	3.28	-	4.22	10.19	10.26	-	11.07
3.47	4.36	4.48	9.20	9.55	10.05	3.52	4.00	-	4.54	10.49	10.56	-	11.37
4.17	5.06	5.18	9.50	10.25	10.35	4.23	4.31	-	5.27	11.19	11.26	-	12.07
4.48	5.33	5.45	10.20	10.57	11.07	4.53	5.01	-	5.56	11.49	11.56	-	12.37
5.17	6.02	6.14	10.50	11.26	11.36	5.23	5.31	6.16	-	12.19	12.26	-	1.07
5.48	6.29	6.41	11.20	11.56	12.06	5.50	5.58	6.40	-	12.49	12.56	-	1.37
6.18	6.57	7.09	11.50	12.26	12.36	6.19	6.27	-	7.08	1.19	1.26	-	2.07
6.48	7.26	7.38	12.20	12.57	1.07	6.48	6.56	7.34	-	1.49	1.56	-	2.37
7.48	8.23	8.35	12.50	1.27	1.37	7.48	7.56	8.30	-	2.19	2.26	-	3.05
8.48	9.23	9.35	1.20	1.59	2.09	8.48	8.56	-	9.31	2.49	2.56	-	3.35
9.48	10.23	10.33	1.50	2.29	2.39	SATURDAY			3.49	3.56	-	4.37	
10.48	11.23	11.33	2.20	2.59	3.09	6.19	6.26	-	7.02	4.19	4.26	-	5.07
SATURDAY			2.50	3.29	3.39	6.49	6.56	-	7.32	4.49	4.56	-	5.37
6.20	6.53	7.04	3.20	4.01	4.11	7.19	7.26	-	8.03	5.19	5.26	-	6.07
6.50	7.23	7.34	3.50	4.30	4.40	7.49	7.56	-	8.33	5.49	5.56	-	6.37
7.20	7.53	8.04	4.20	5.00	5.10	8.19	8.26	-	9.04	6.19	6.26	-	7.04
7.50	8.26	8.37	4.50	5.27	5.37	8.49	8.56	-	9.34	6.49	6.56	-	7.34
8.20	8.59	9.10	5.20	5.57	6.07	9.19	9.26	-	10.06	7.49	7.56	-	8.32
8.50	9.29	9.40	5.50	6.27	6.37	9.49	9.56	-	10.36	8.49	8.56	-	9.32
9.20	9.59	10.10	6.20	6.57	7.07	10.19	10.26	-	11.09				
9.50	10.29	10.40	6.50	7.27	7.37	10.49	10.56	-	11.39				
10.20	10.59	11.10	7.50	8.27	8.37	11.19	11.26	-	12.09				
10.50	11.29	11.40	8.50	9.27	9.37	11.49	11.56	-	12.39				
11.20	12.01	12.12				12.19	12.26	-	1.10				
11.50	12.32	12.41				12.49	12.56	-	1.40				
12.20	1.02	1.11				1.19	1.26	-	2.11				
12.50	1.32	1.41				1.49	1.56	-	2.41				
1.20	2.02	2.11				2.19	2.26	-	3.11				
1.50	2.32	2.41				2.49	2.56	-	3.41				
2.20	3.02	3.11				3.19	3.26	-	4.11				
2.50	3.32	3.41											
CONTINUED													

531 White Rock Centre/ Willowbrook

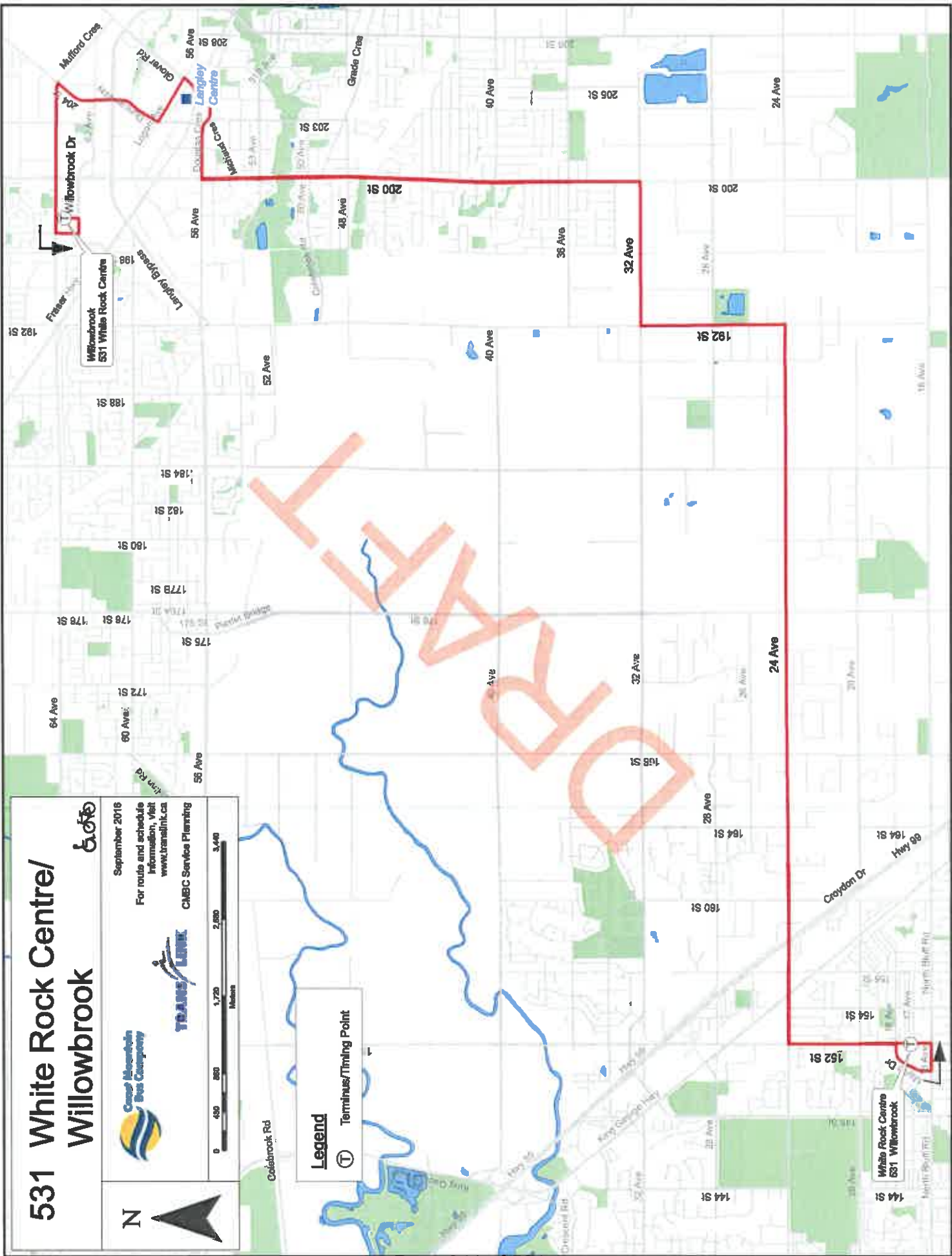


September 2016
For route and schedule
information, visit
www.translink.ca
CMBC Service Planning



Legend

- ① Terminus/Timing Point



531 White Rock Centre / 531 Willowbrook

From 198 St at 64 Ave via 198 St, 64 Ave, 204 St, 204 St overpass, Logan (Langley Centre), Glover, 204 St, Douglas, 56 Ave, 200 St, 32 Ave, 192 St, 24 Ave, 152 St, Martin, North Bluff, 152 St to White Rock Centre.

From White Rock Centre via 152 St, 24 Ave, 192 St, 32 Ave, 200 St, 56 Ave, Douglas, 204 St, Glover, Logan (Langley Centre), 204 St overpass, 204 St, 64 Ave, 197 St Willowbrook Dr, to 198 St.

198 St at 64 Ave #61668	Langley Centre Bay 4 (E)	White Rock Centre (E)	White Rock Centre Bay 9	Langley Centre Bay 7 (E)	198 St at 64 Ave (E)
MONDAY TO FRIDAY			MONDAY TO FRIDAY		
5.40	5.45	6.23	-	5.19*	-
6.10	6.15	6.58	-	5.49*	-
6.40	6.46	7.30	5.47	6.21	6.29
7.10	7.16	8.01	6.17	6.52	7.00
7.40	7.46	8.33	6.47	7.23	7.31
8.10	8.16	9.01	7.17	7.54	8.02
8.40	8.46	9.29	7.47	8.22	8.30
9.10	9.16	9.59	8.16	8.55	9.04
9.40	9.46	10.31	8.45	9.22	9.30
10.10	10.16	11.01	9.15	9.52	10.01
10.40	10.46	11.31	9.45	10.23	10.32
11.10	11.16	12.03	10.15	10.52	11.01
11.40	11.46	12.33	10.45	11.22	11.31
12.10	12.16	1.03	11.15	11.55	12.04
12.40	12.46	1.33	11.45	12.24	12.33
1.10	1.16	2.04	12.15	12.54	1.03
1.40	1.47	2.34	12.45	1.24	1.33
2.10	2.17	3.07	1.15	1.54	2.03
2.40	2.48	3.40	1.45	2.24	2.33
3.10	3.17	4.09	2.12	2.51	3.00
3.40	3.47	4.39	2.42	3.24	3.33
4.10	4.17	5.09	3.12	3.57	4.07
4.40	4.47	5.39	3.42	4.25	4.35
5.40	5.46	6.32	4.12	4.55	5.05
5.10	5.17	6.04	4.42	5.24	5.33
6.10	6.16	6.58	5.12	5.51	6.00
6.40	6.45	7.24	5.45	6.22	6.31
7.10	7.15	7.54	6.15	6.52	7.00
7.40	7.45	8.25	6.45	7.22	7.30
8.10	8.15	8.53	7.20	7.55	8.03
8.40	8.45	9.22	7.50	8.23	8.31
			8.15	8.47	8.55
			8.45	9.17	9.24
SATURDAY			SATURDAY		
5.50	5.55	6.34	5.50	6.26	6.34
6.20	6.25	7.05	6.20	6.56	7.04
6.50	6.55	7.35	6.50	7.26	7.34
7.20	7.25	8.05	7.20	7.56	8.04
7.50	7.55	8.35	7.50	8.26	8.34
8.20	8.26	9.09	8.20	8.57	9.06
8.50	8.56	9.39	8.50	9.27	9.36
9.20	9.26	10.12	9.20	10.00	10.09
9.50	9.56	10.42	9.50	10.30	10.39
10.20	10.26	11.12	10.20	11.00	11.09
10.50	10.56	11.42	10.50	11.30	11.39
11.20	11.26	12.12	11.20	12.01	12.10
11.50	11.56	12.42	11.50	12.31	12.40
12.20	12.27	1.15	12.20	1.01	1.10
12.50	12.57	1.45	12.50	1.31	1.40
1.20	1.27	2.15	1.20	2.01	2.10
1.50	1.57	2.45	1.50	2.31	2.40
2.20	2.27	3.15	2.20	3.01	3.10
2.50	2.57	3.45	2.50	3.31	3.40
3.20	3.27	4.15	3.20	4.01	4.10
3.50	3.57	4.45	3.50	4.31	4.40
4.20	4.27	5.15	4.20	5.01	5.10
CONTINUED			CONTINUED		

* This trip starts 15 minutes earlier from 204 St at 40 Ave, routing via 204 St, 42 Ave, 208 St, 40 Ave, 200 St then regular route to Langley Centre only.

531 White Rock Centre / 531 Willowbrook

198 St at 64 Ave to White Rock Centre.

White Rock Centre to 198 St at 64 Ave.

(Refer to Monday to Friday for routing)

198 St at 64 Ave #61668	Langley Centre Bay 4 (E)	White Rock Centre (E)	White Rock Centre Bay 9	Langley Centre Bay 7 (E)	198 St at 64 Ave (E)
SATURDAY			SATURDAY		
.... CONTINUED			.... CONTINUED		
4.50	4.56	5.41	4.50	5.29	5.38
5.20	5.26	6.11	5.20	5.57	6.05
5.50	5.56	6.41	5.50	6.27	6.35
6.20	6.26	7.11	6.20	6.57	7.05
6.50	6.56	7.41	6.50	7.27	7.35
7.20	7.26	8.08	7.20	7.57	8.05
7.50	7.56	8.38	7.50	8.24	8.32
8.20	8.26	9.08	8.20	8.54	9.02
8.50	8.56	9.36	8.50	9.24	9.32
SUNDAY & HOLIDAYS			SUNDAY & HOLIDAYS		
5.50	5.55	6.32	5.50	6.27	6.36
6.20	6.25	7.03	6.20	6.57	7.06
6.50	6.55	7.33	6.50	7.27	7.36
7.20	7.25	8.04	7.20	7.57	8.06
7.50	7.55	8.34	7.50	8.27	8.36
8.20	8.25	9.05	8.20	8.57	9.06
8.50	8.55	9.35	8.50	9.27	9.36
9.20	9.26	10.08	9.20	9.57	10.06
9.50	9.56	10.38	9.50	10.27	10.36
10.20	10.26	11.08	10.20	10.57	11.06
10.50	10.56	11.38	10.50	11.27	11.36
11.20	11.26	12.08	11.20	11.57	12.06
11.50	11.56	12.38	11.50	12.27	12.36
12.20	12.26	1.08	12.20	12.57	1.06
12.50	12.56	1.38	12.50	1.29	1.38
1.20	1.26	2.08	1.20	1.59	2.08
1.50	1.56	2.38	1.50	2.29	2.38
2.20	2.26	3.08	2.20	2.59	3.08
2.50	2.56	3.38	2.50	3.29	3.38
3.20	3.26	4.08	3.20	3.59	4.08
3.50	3.56	4.38	3.50	4.29	4.38
4.20	4.26	5.08	4.20	4.59	5.08
4.50	4.56	5.38	4.50	5.29	5.38
5.20	5.26	6.08	5.20	5.57	6.05
5.50	5.56	6.38	5.50	6.27	6.35
6.20	6.26	7.08	6.20	6.57	7.05
6.50	6.56	7.38	6.50	7.27	7.35
7.20	7.26	8.06	7.20	7.57	8.05
7.50	7.56	8.36	7.50	8.27	8.35
8.20	8.26	9.06	8.20	8.57	9.05
8.50	8.55	9.36	8.50	9.27	9.35

Appendix C
Traffic Count Data

DRAFT



Fir St & Russell Ave

Wednesday, April 03, 2019

Vehicle Classification Summary

Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

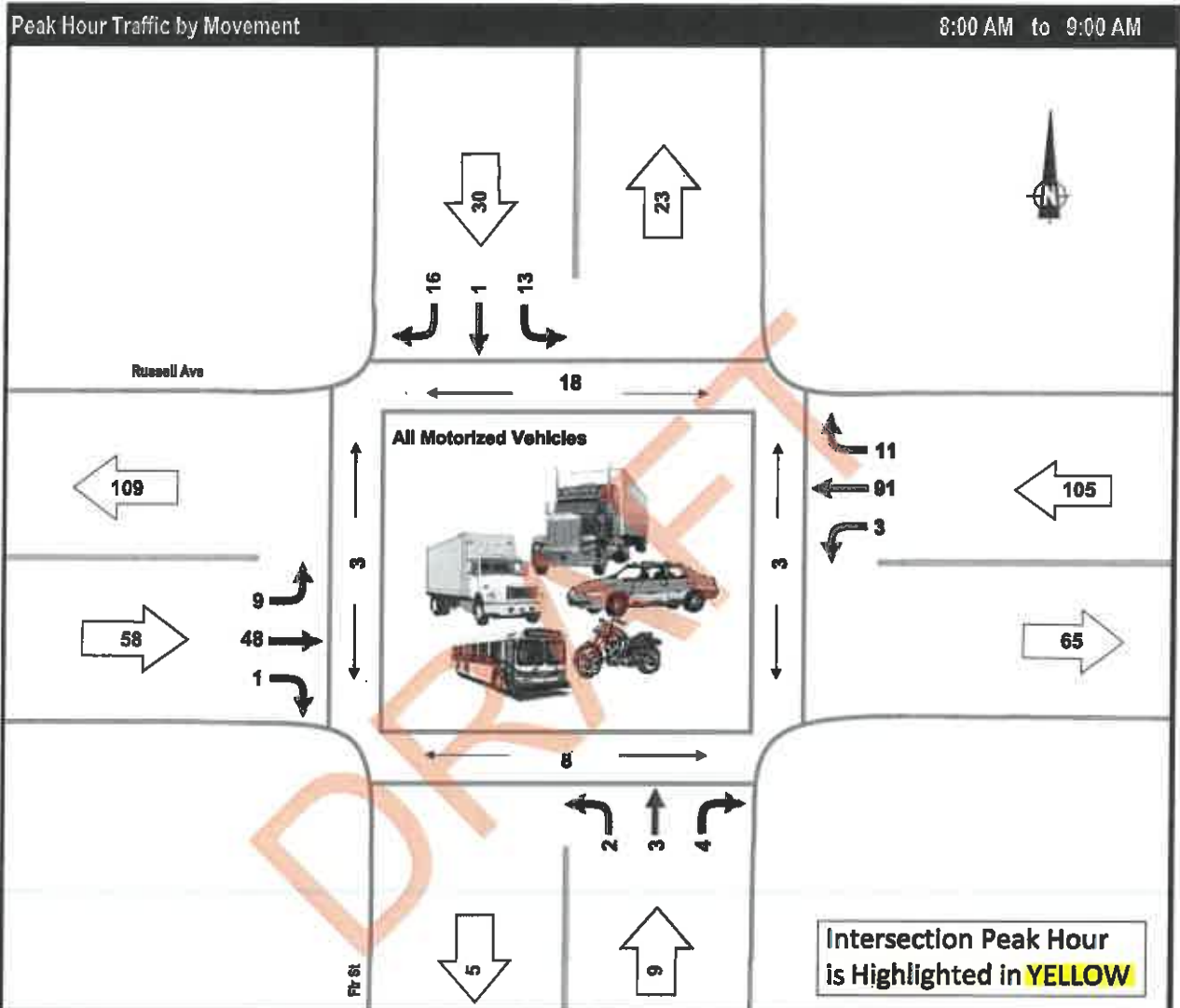
Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axes)			
Morning (07:00 - 09:00)	Volume	335	1			336
	%	99.7%	0.3%			100.0%
Midday (11:00 - 13:00)	Volume	665	3			671
	%	99.6%	0.4%			100.0%
Afternoon (15:00 - 18:00)	Volume	963	1			964
	%	99.9%	0.1%			100.0%
Total (7 Hours)	Volume	1,966	5			1,971
	%	99.7%	0.3%			100.0%

DRAFT



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

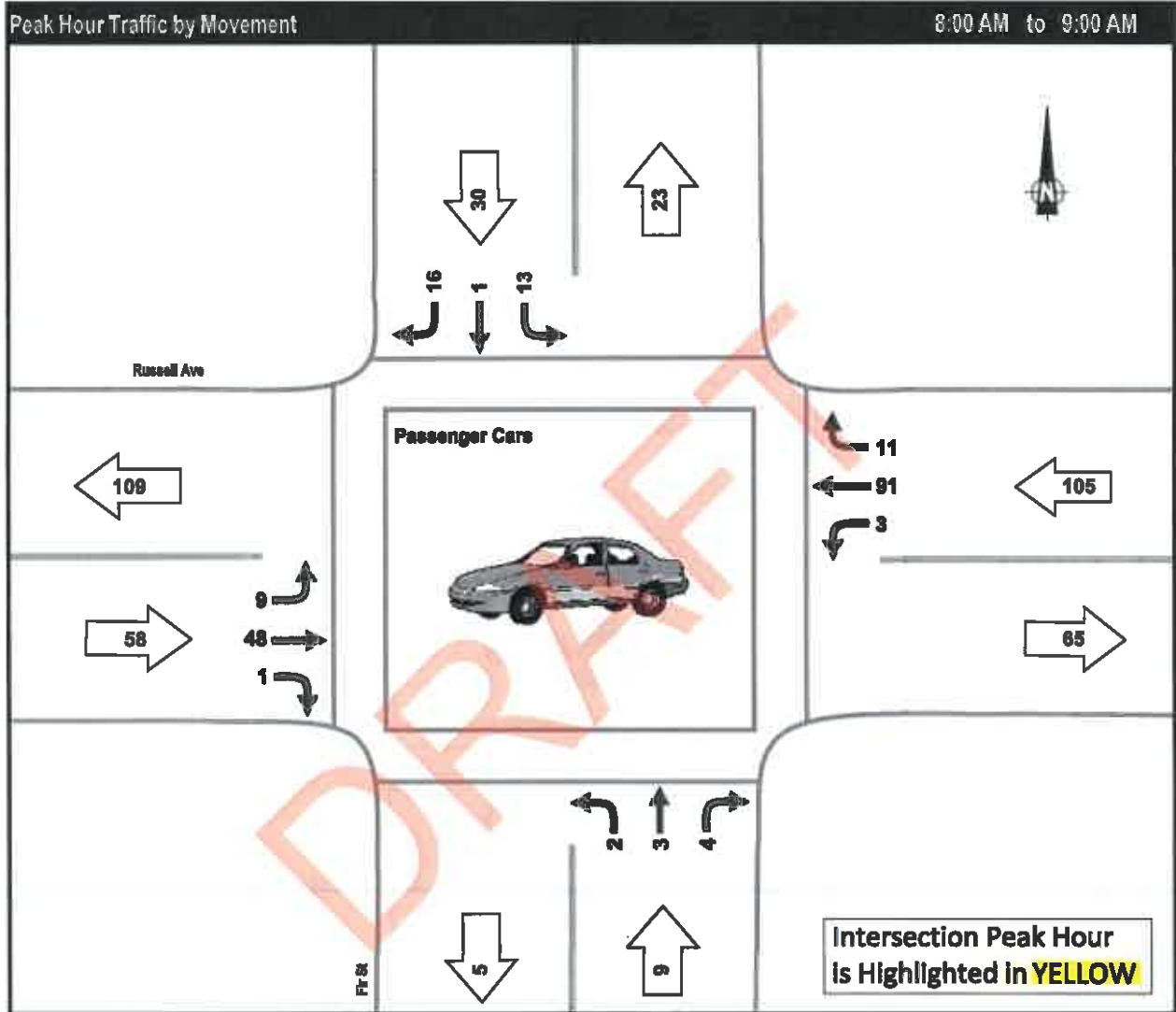
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	13	1	16	2	3	4	9	48	1	3	97	11	16	8	3	3	202
PHF	0.81	0.25	1.00	0.50	0.75	1.00	0.38	0.75	0.25	0.75	0.78	0.69	0.56	0.60	0.38	0.38	0.89
Peak 15 X 4	15	4	16	4	4	4	24	64	4	4	116	16	32	16	8	8	226
Average Hour	13	1	13	4	2	5	7	42	8	6	62	9	20	10	11	3	172
Survey Total	26	1	25	7	4	10	13	83	15	11	123	18	39	19	21	5	336
7:00	3	0	2	3	1	2	0	6	4	6	7	1	6	3	12	0	35
7:15	4	0	3	0	0	2	2	11	6	2	7	2	9	4	6	2	39
7:30	3	0	2	0	0	1	0	11	2	0	5	2	6	3	0	0	26
7:45	3	0	2	2	0	1	2	7	2	0	13	2	0	1	0	0	34
8:00	4	0	4	0	0	1	0	13	0	0	17	3	6	2	2	1	42
8:15	3	0	4	0	1	1	1	16	1	1	21	1	2	0	0	0	50
8:30	3	1	4	1	1	1	2	12	0	1	24	3	2	2	0	0	53
8:45	3	0	4	1	1	1	6	7	0	1	29	4	8	4	1	2	57

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

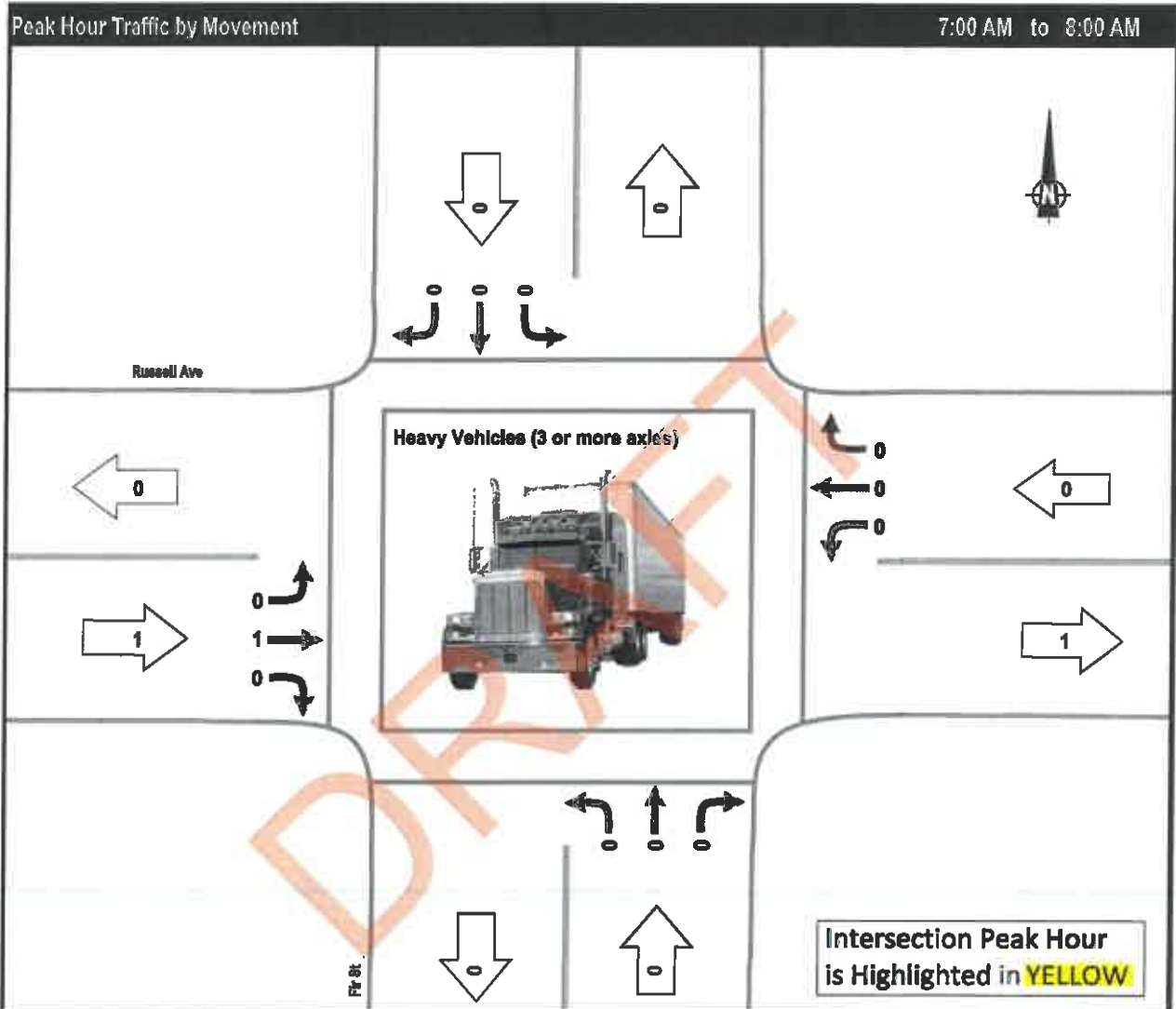
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	13	1	16	2	3	4	9	48	1	3	91	11					202
PHF	0.81	0.25	1.00	0.50	0.75	1.00	0.38	0.75	0.25	0.75	0.78	0.59					0.80
Peak 15 X 4	16	4	16	4	4	4	24	64	4	4	116	16					228
Average Hour	13	1	13	4	2	5	7	41	8	6	62	9					171
Survey Total	26	1	25	7	4	10	13	82	15	11	123	18					336
7:00	3	0	2	3	1	2	0	5	4	6	7	1					34
7:15	4	0	3	0	0	2	2	11	6	2	7	2					39
7:30	3	0	2	0	0	1	0	11	2	0	5	2					26
7:45	3	0	2	2	0	1	2	7	2	0	13	2					34
8:00	4	0	4	0	0	1	0	13	0	0	17	3					42
8:15	3	0	4	0	1	1	1	16	1	1	21	1					50
8:30	3	1	4	1	1	1	2	12	0	1	24	3					53
8:45	3	0	4	1	1	1	6	7	0	1	29	4					57

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Morning Peak Period

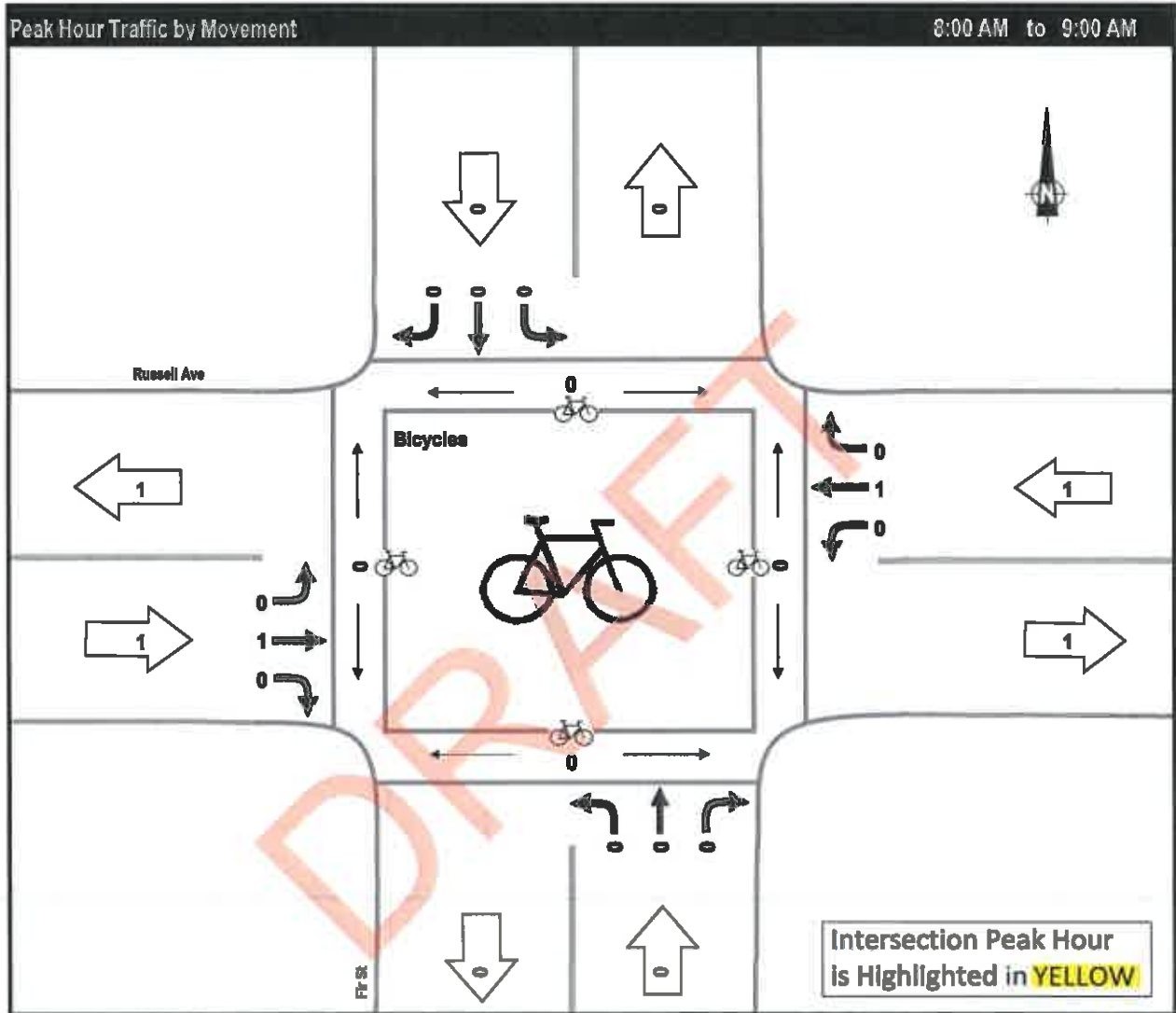


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	0	0					1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00					0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	0	0					4
Average Hour	0	0	0	0	0	0	0	1	0	0	0	0					1
Survey Total	0	0	0	0	0	0	0	1	0	0	0	0					1
7:00	0	0	0	0	0	0	0	1	0	0	0	0					1
7:15	0	0	0	0	0	0	0	0	0	0	0	0					0
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	0	0					0
8:30	0	0	0	0	0	0	0	0	0	0	0	0					0
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Morning Peak Period

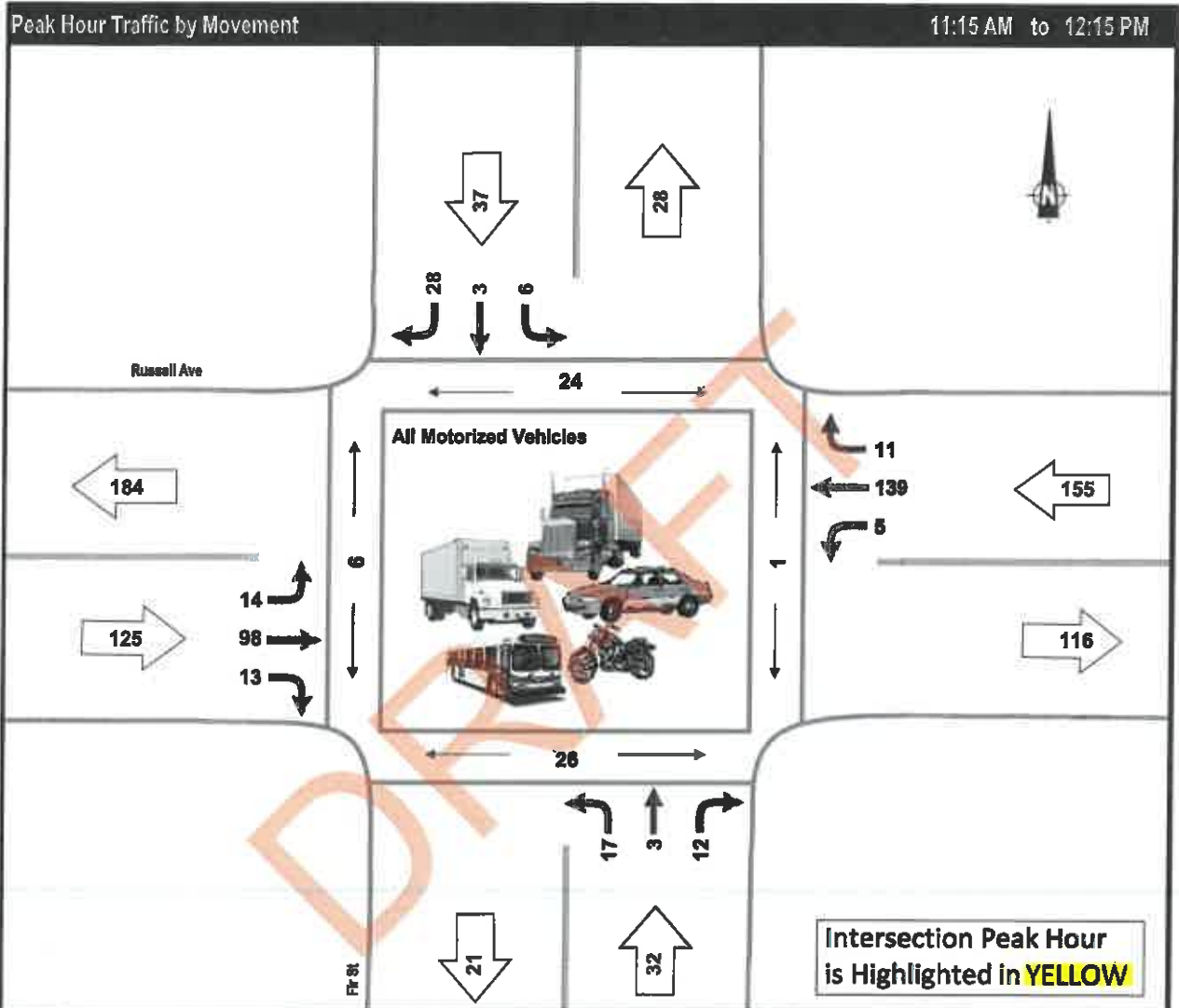
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

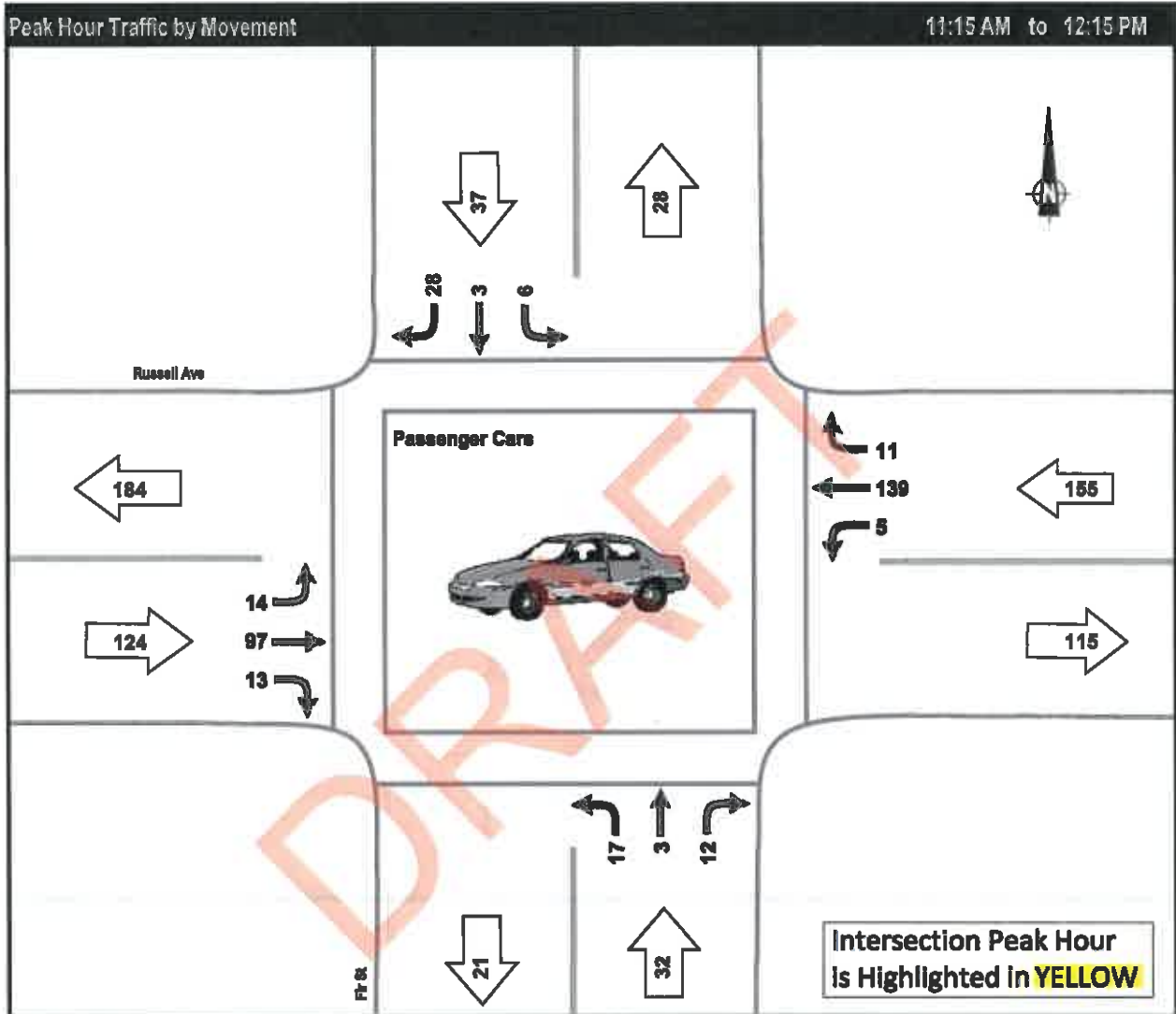
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	6	3	28	17	3	12	14	98	13	5	139	11	24	26	6	1	349
PH Factor	0.50	0.75	0.54	0.61	0.75	0.50	0.50	0.77	0.65	0.42	0.89	0.55	0.75	0.72	0.30	0.25	0.84
PHF	12	4	52	28	4	24	28	125	20	12	156	20	32	36	20	4	416
Average Hour	8	3	25	14	3	12	14	91	9	6	139	13	24	20	4	2	337
Survey Total	16	6	49	28	6	24	28	182	17	11	278	26	48	40	7	4	671
11:00	2	1	5	3	1	2	3	17	1	2	43	2	6	3	0	1	82
11:15	2	1	2	1	1	4	2	19	4	1	39	2	6	7	1	0	78
11:30	3	1	7	3	1	1	4	22	2	0	30	5	7	6	0	1	79
11:45	1	1	13	7	0	1	7	25	5	3	39	2	4	9	5	0	104
12:00	0	0	6	5	1	6	1	32	2	1	31	2	5	4	0	0	88
12:15	2	1	2	1	2	2	2	20	1	1	26	1	3	3	1	1	61
12:30	2	0	10	4	0	1	4	27	0	3	37	7	9	7	0	1	95
12:45	4	1	4	3	0	7	5	20	2	0	33	5	6	1	0	0	84

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

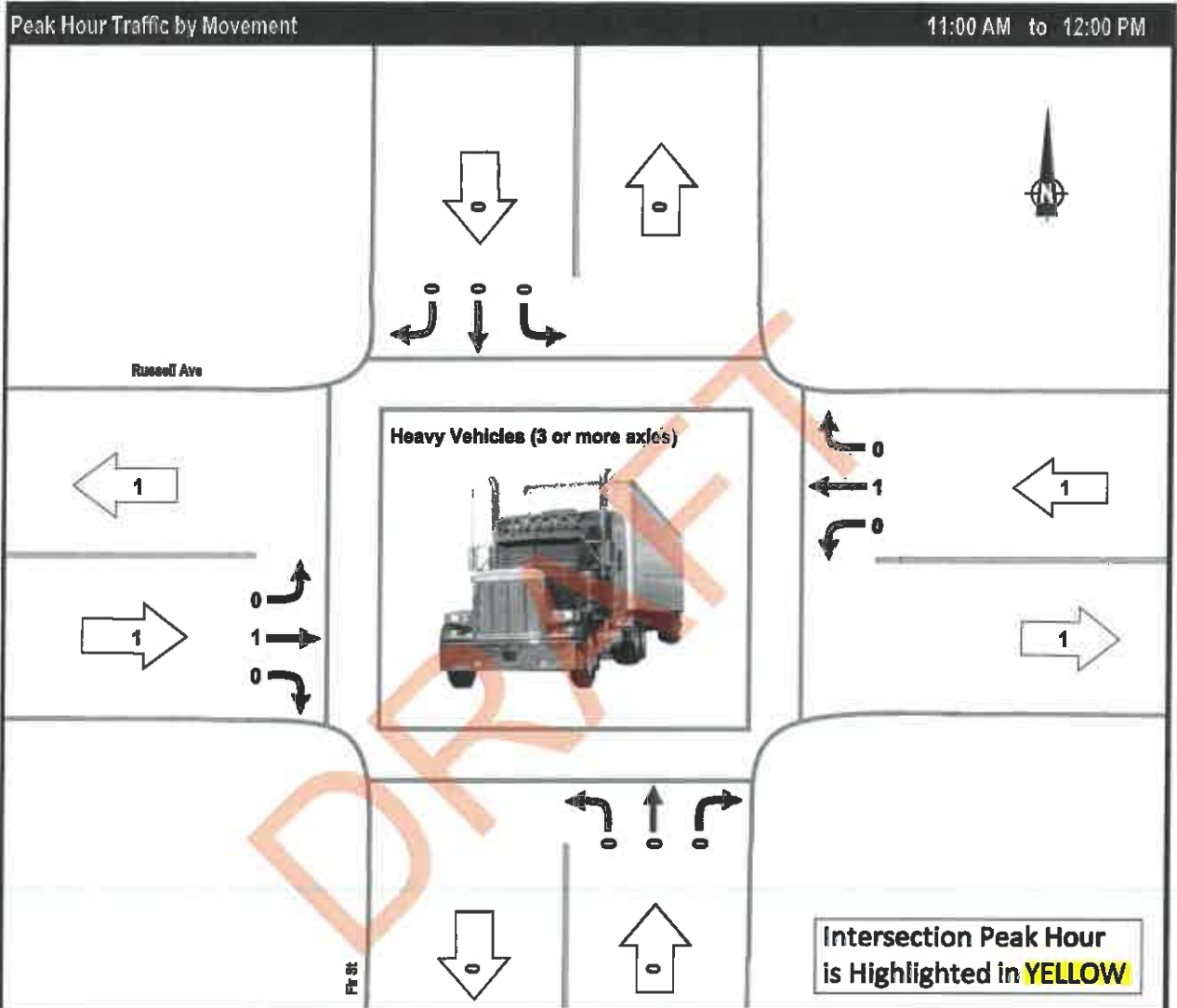
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	6	3	28	17	3	12	14	97	13	5	139	11					348
PHF	0.50	0.75	0.54	0.61	0.75	0.50	0.50	0.76	0.65	0.42	0.89	0.55					0.84
Peak 15 X 4	12	4	52	28	4	24	28	128	20	12	156	20					412
Average Hour	8	3	24	14	3	12	14	91	9	6	139	13					336
Survey Total	16	6	48	28	6	24	28	161	17	11	277	26					668
11:00	2	1	5	3	1	2	3	17	1	2	42	2					81
11:15	2	1	2	1	1	4	2	19	4	1	39	2					78
11:30	3	1	7	3	1	1	4	22	2	0	30	5					79
11:45	1	1	13	7	0	1	7	24	5	3	39	2					103
12:00	0	0	6	6	1	6	1	32	2	1	31	2					88
12:15	2	1	1	1	2	2	2	20	1	1	26	1					60
12:30	2	0	10	4	0	1	4	27	0	3	37	7					85
12:45	4	1	4	3	0	7	5	20	2	0	33	5					84

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

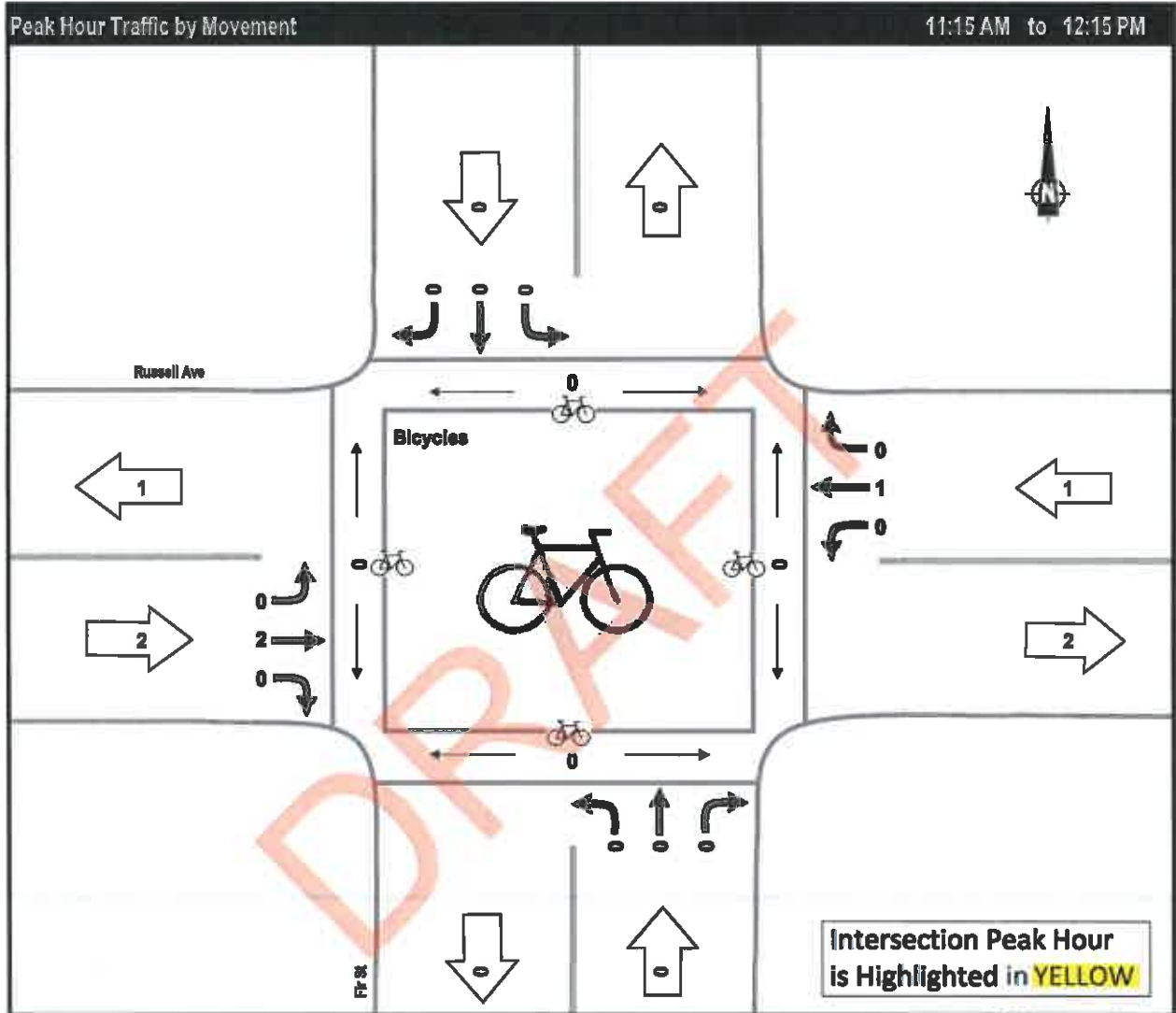
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
PHP	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					4
Average Hour	0	0	1	0	0	0	0	1	0	0	1	0					3
Survey Total	0	0	1	0	0	0	0	1	0	0	1	0					3
11:00	0	0	0	0	0	0	0	0	0	0	1	0					1
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0
11:30	0	0	0	0	0	0	0	0	0	0	0	0					0
11:45	0	0	0	0	0	0	0	1	0	0	0	0					1
12:00	0	0	0	0	0	0	0	0	0	0	0	0					0
12:15	0	0	1	0	0	0	0	0	0	0	0	0					1
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7026: 1486 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles
 Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals.

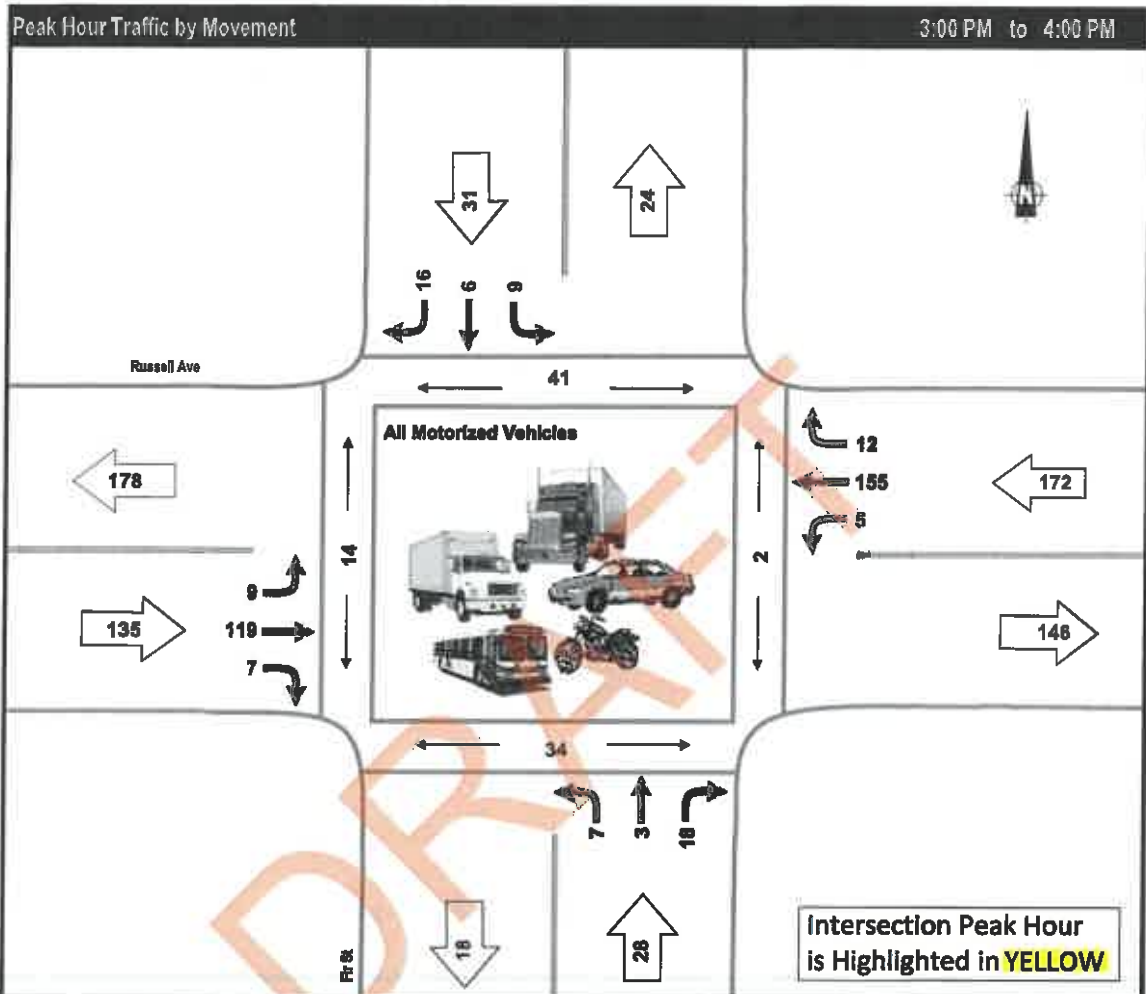
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	3
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.38
Peak 15 X 4	0	0	0	0	0	0	0	8	0	0	4	0	0	0	0	0	8
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Survey Total	0	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	3
11:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025; 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

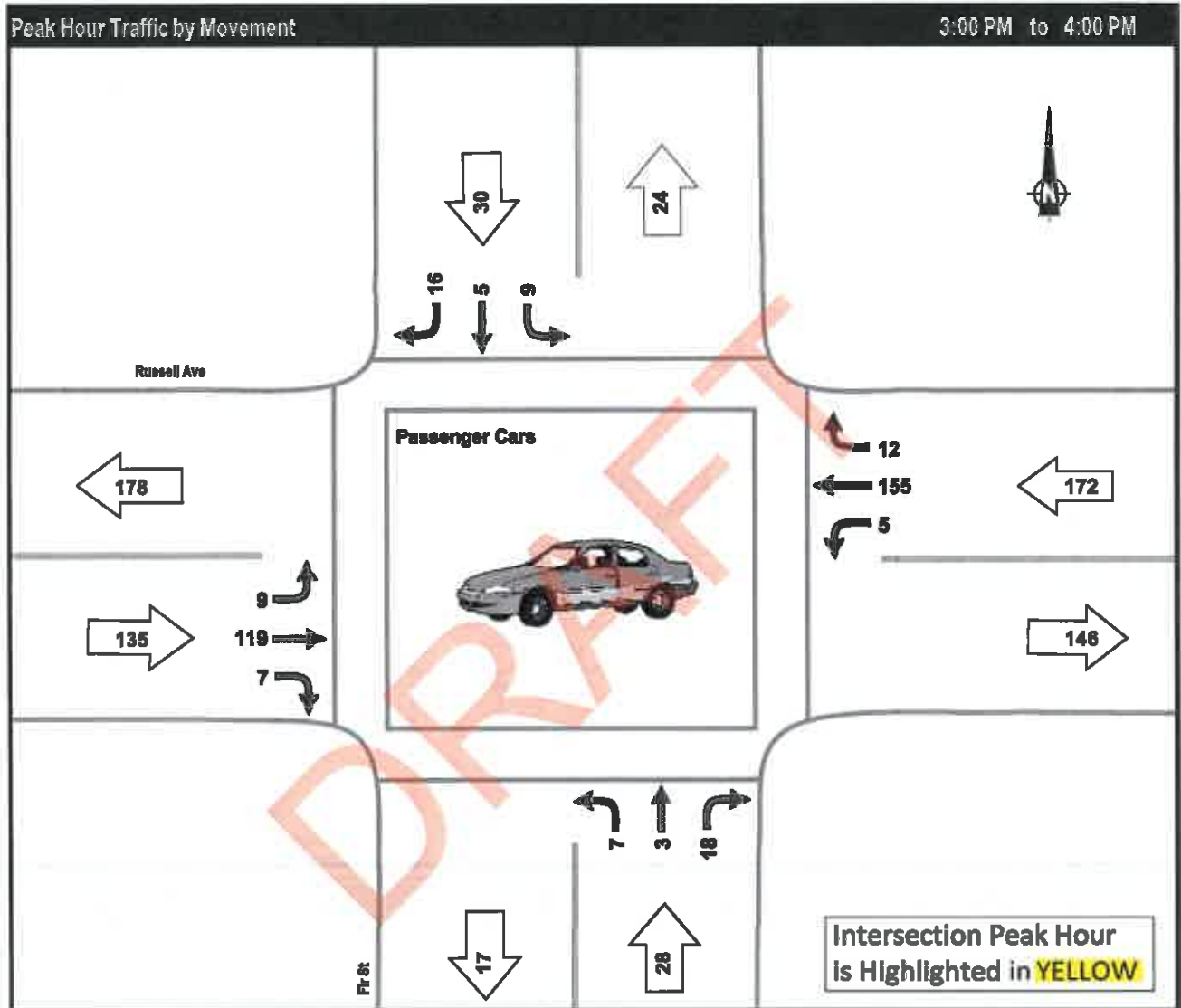
Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	9	6	16	7	3	18	9	119	7	5	155	12	41	34	14	2	366
PHF	0.56	0.75	0.67	0.44	0.75	0.50	0.58	0.78	0.58	0.63	0.82	0.50	0.64	0.71	0.70	0.50	0.91
Peak 15 X 4	16	8	24	16	4	36	16	152	12	8	188	24	64	48	20	4	404
Average Hour	12	5	16	8	3	11	7	106	8	4	130	12	29	26	8	5	323
Survey Total	37	14	47	26	8	33	22	317	23	11	391	35	88	78	25	16	964
16:00	2	2	6	1	1	3	2	31	2	5	47	4	7	12	4	1	101
15:15	2	1	3	4	1	3	0	21	3	1	47	1	18	6	5	1	87
15:30	1	1	4	2	0	9	3	38	1	2	24	1	8	7	4	0	66
15:45	4	2	3	0	1	3	4	29	1	2	37	6	10	9	1	0	92
16:00	5	1	4	5	2	5	1	29	2	0	37	3	5	7	8	3	94
16:15	4	1	4	1	1	2	2	22	0	0	21	4	6	9	0	2	62
16:30	1	1	2	4	1	0	1	27	1	3	32	4	8	3	0	0	77
16:45	2	1	2	1	0	4	1	24	3	2	41	1	9	4	2	0	82
17:00	5	1	6	2	0	3	2	23	2	1	25	2	1	1	0	1	72
17:15	7	0	3	1	0	0	2	22	2	0	28	3	6	8	0	3	68
17:30	2	3	2	1	0	0	1	26	2	0	27	4	6	8	1	1	68
17:45	2	0	8	4	1	1	3	25	4	0	25	2	7	4	0	4	75

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	9	5	16	7	3	18	9	119	7	5	155	12					365
PHF	0.56	0.63	0.67	0.44	0.75	0.50	0.56	0.78	0.58	0.63	0.82	0.50					0.91
Peak 15 X 4	16	8	24	16	4	36	16	152	12	8	188	24					400
Average Hour	12	4	16	9	3	11	7	106	8	4	130	12					322
Survey Total	37	13	47	28	8	33	22	317	23	11	391	35					963
15:00	2	1	6	1	1	3	2	31	2	0	47	4					100
16:15	2	1	3	4	1	3	0	21	3	1	47	1					87
15:30	1	1	4	2	0	9	3	38	1	2	24	1					86
15:45	4	2	3	0	1	3	4	29	1	2	37	6					92
18:00	5	1	4	5	2	6	1	29	2	0	37	3					94
18:15	4	1	4	1	1	2	2	22	0	0	21	4					62
18:30	1	1	2	4	1	0	1	27	1	3	32	4					77
18:45	2	1	2	1	0	4	1	24	3	2	41	1					82
17:00	5	1	6	2	0	3	2	23	2	1	25	2					72
17:15	7	0	3	1	0	0	2	22	2	0	28	3					68
17:30	2	3	2	1	0	0	1	26	2	0	27	4					68
17:45	2	0	8	4	1	1	3	25	4	0	25	2					75

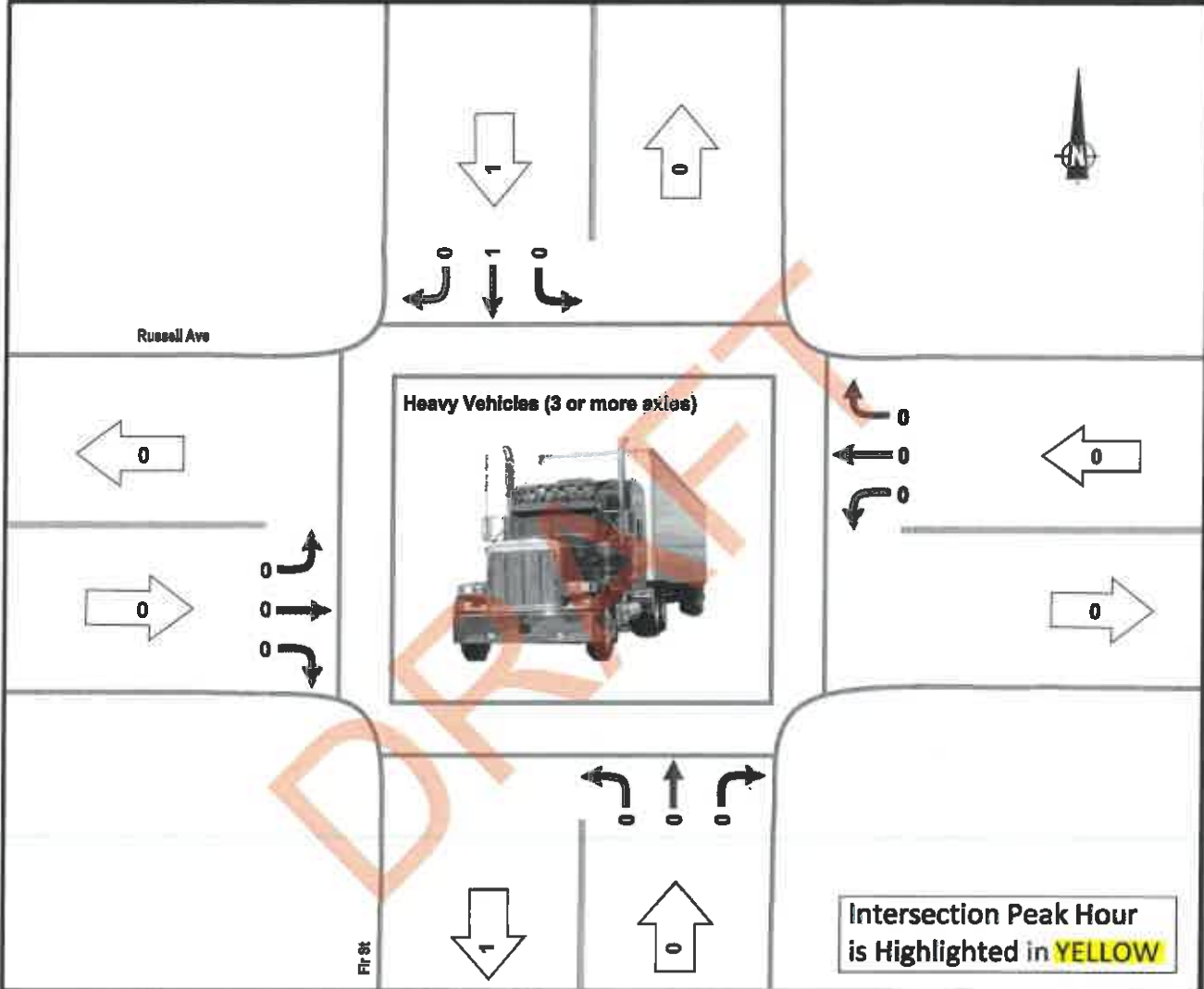


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Afternoon Peak Period

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM

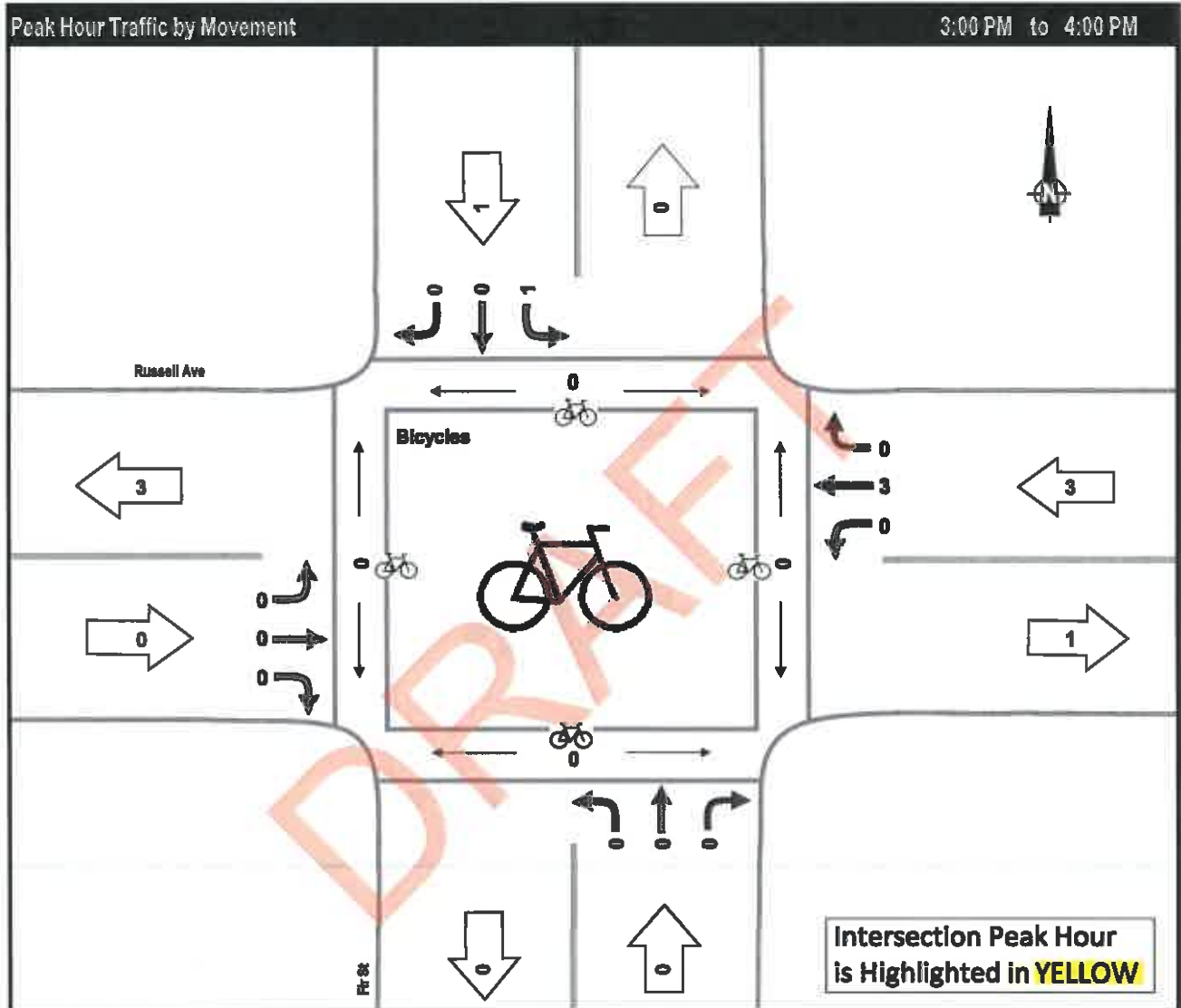


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	1	0	0	0	0	0	0	0	0	0	0					1
PHF	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					0.25
Peak 15 X 4	0	4	0	0	0	0	0	0	0	0	0	0					4
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	1	0	0	0	0	0	0	0	0	0	0					1
15:00	0	1	0	0	0	0	0	0	0	0	0	0					1
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	0	0					0
16:00	0	0	0	0	0	0	0	0	0	0	0	0					0
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals.



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	1	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	4
PHF	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.33
Peak 15 X 4	4	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	12
Average Hour	1	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	4
Survey Total	2	0	0	0	0	1	1	2	0	0	5	0	0	0	0	0	11
15:00	1	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	3
16:15	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
18:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2
18:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
18:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
18:45	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2



Vehicle Classification Summary

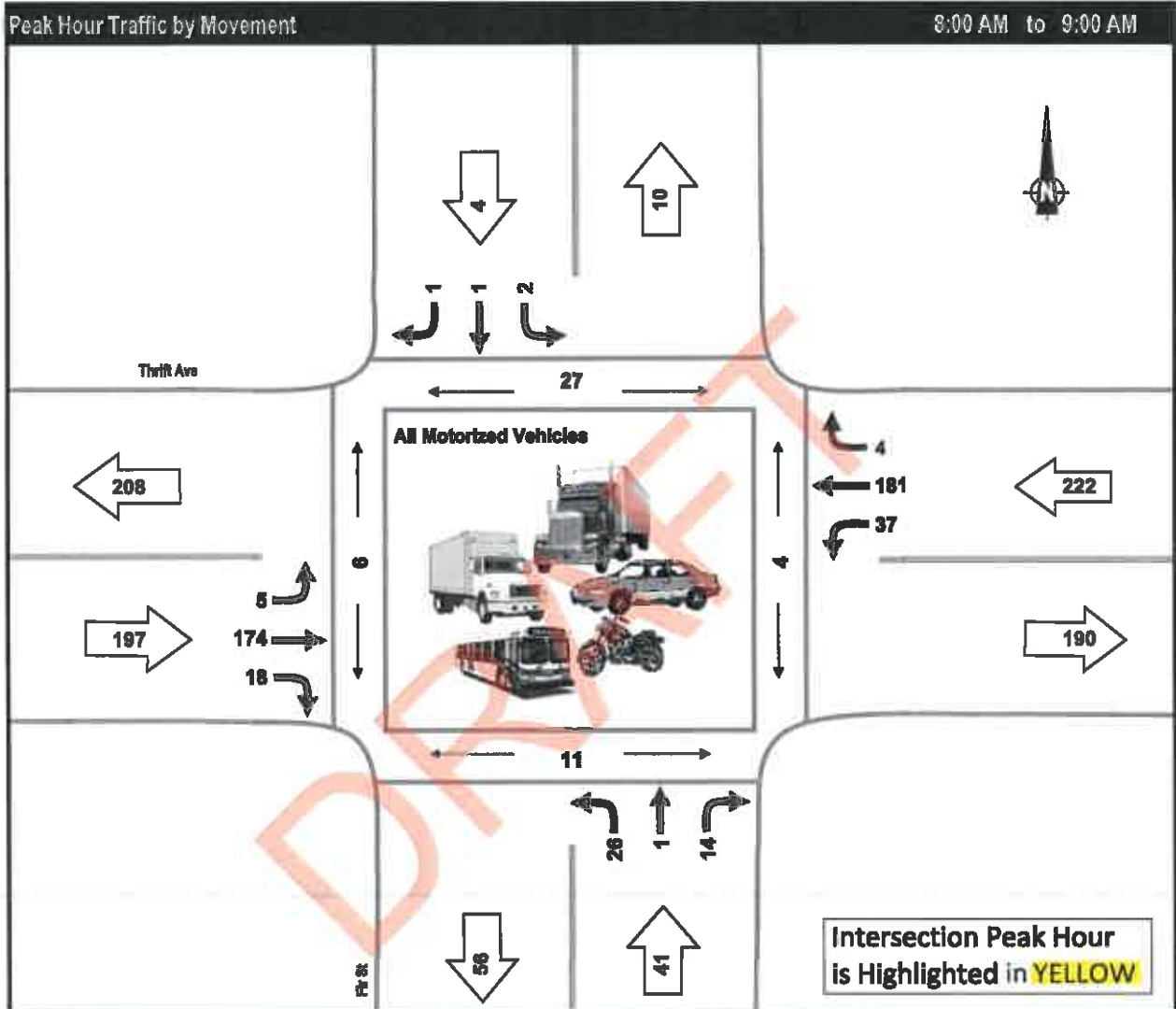
Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 09:00)	Volume	667	3			670
	%	99.6%	0.4%			100.0%
Midday (11:00 - 13:00)	Volume	874	2			876
	%	99.8%	0.2%			100.0%
Afternoon (15:00 - 18:00)	Volume	1,493	5			1,498
	%	99.7%	0.3%			100.0%
Total (7 Hours)	Volume	3,034	10			3,044
	%	99.7%	0.3%			100.0%

DRAFT

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

Morning Peak Period

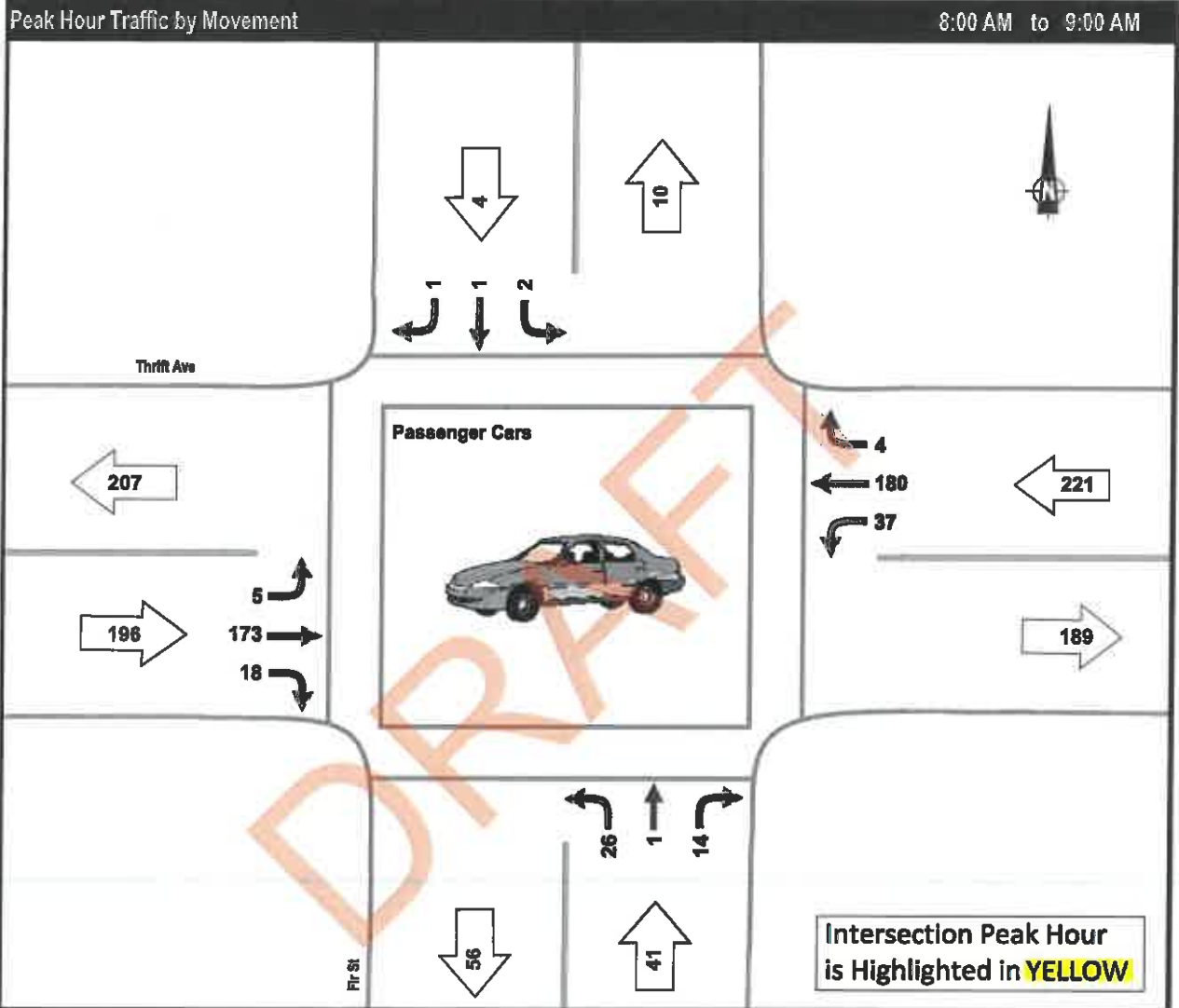


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	2	1	1	20	1	14	5	174	18	37	181	4	27	11	5	4	484
PHF	0.50	0.25	0.25	0.60	0.25	0.58	0.63	0.84	0.64	0.49	0.81	0.50	0.52	0.55	0.75	0.50	0.81
Peak 15 X 4	4	4	4	52	4	24	8	208	28	76	224	8	52	20	8	8	572
Average Hour	3	7	4	15	4	9	8	124	12	21	131	3	22	7	9	4	339
Survey Total	6	14	7	29	7	18	11	247	23	42	281	6	44	14	18	7	670
7:00	2	5	2	0	3	1	3	15	1	2	17	2	4	0	1	2	53
7:15	1	5	2	2	0	1	2	15	2	1	19	0	7	1	7	1	50
7:30	0	2	1	1	1	2	0	22	1	1	14	0	5	0	4	0	45
7:45	0	1	1	0	2	0	1	21	1	1	30	0	1	2	0	0	58
8:00	0	0	0	3	0	3	0	39	2	5	39	1	3	1	2	0	92
8:15	1	1	0	5	0	3	1	48	7	19	56	2	13	2	2	1	143
8:30	1	0	1	13	0	8	2	52	5	12	38	1	6	5	2	2	131
8:45	0	0	0	5	1	2	2	35	4	1	48	0	5	3	0	1	98



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

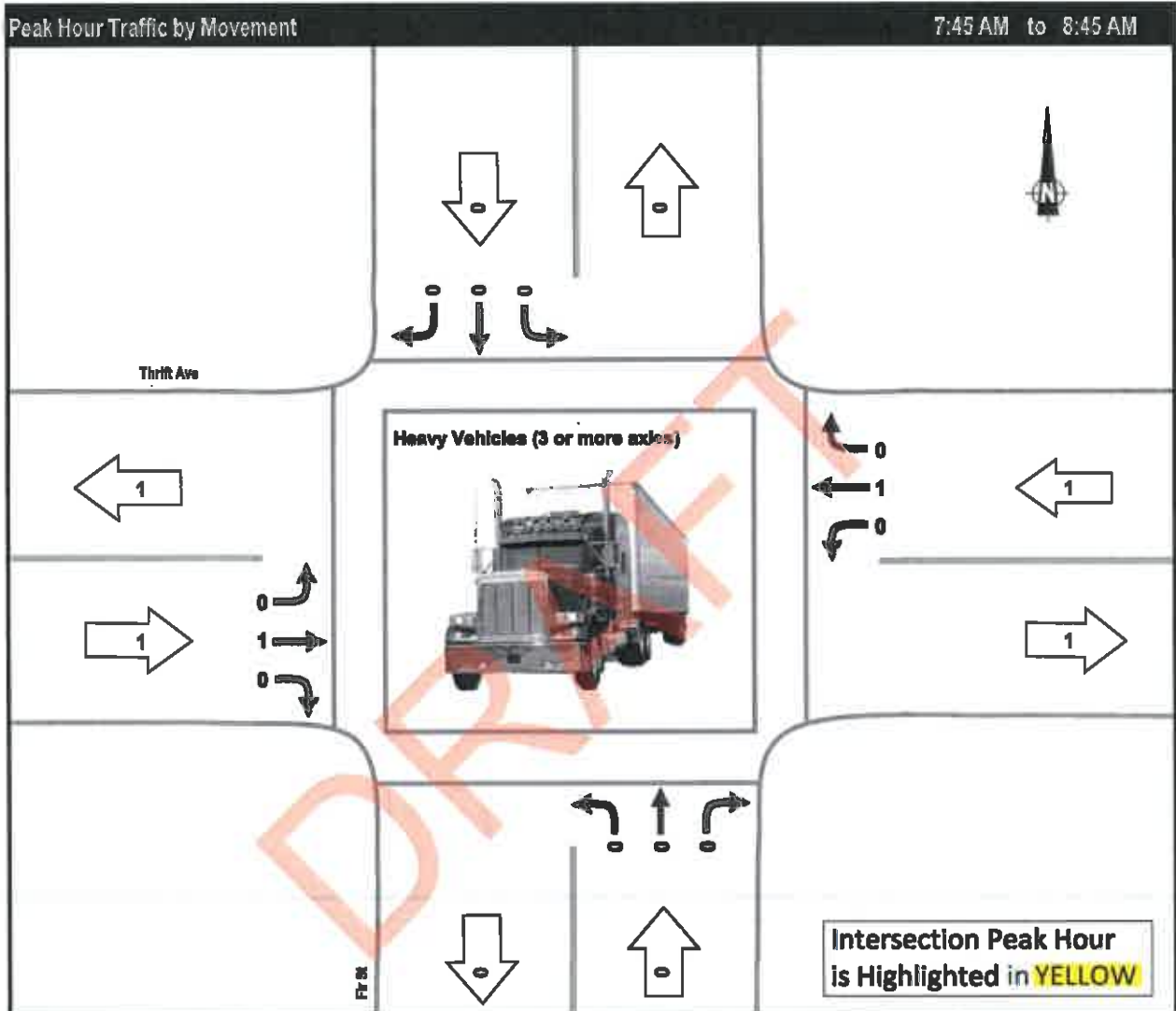
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	2	1	1	26	1	14	5	173	15	37	180	4					462
PHF	0.50	0.25	0.25	0.50	0.25	0.58	0.63	0.85	0.64	0.49	0.82	0.50					0.81
Peak 15 X 4	4	4	4	52	4	24	8	204	28	76	220	8					568
Average Hour	3	7	4	15	4	9	6	123	12	21	130	3					337
Survey Total	5	14	7	29	7	18	11	246	23	42	259	6					667
7:00	2	5	2	0	3	1	3	15	1	2	17	2					53
7:15	1	5	2	2	0	1	2	15	2	1	18	0					49
7:30	0	2	1	1	1	2	0	22	1	1	14	0					45
7:45	0	1	1	0	2	0	1	21	1	1	30	0					58
8:00	0	0	0	3	0	3	0	39	2	5	39	1					92
8:15	1	1	0	5	0	3	1	48	7	19	55	2					142
8:30	1	0	1	13	0	6	2	51	5	12	38	1					130
8:45	0	0	0	5	1	2	2	35	4	1	48	0					98

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Morning Peak Period



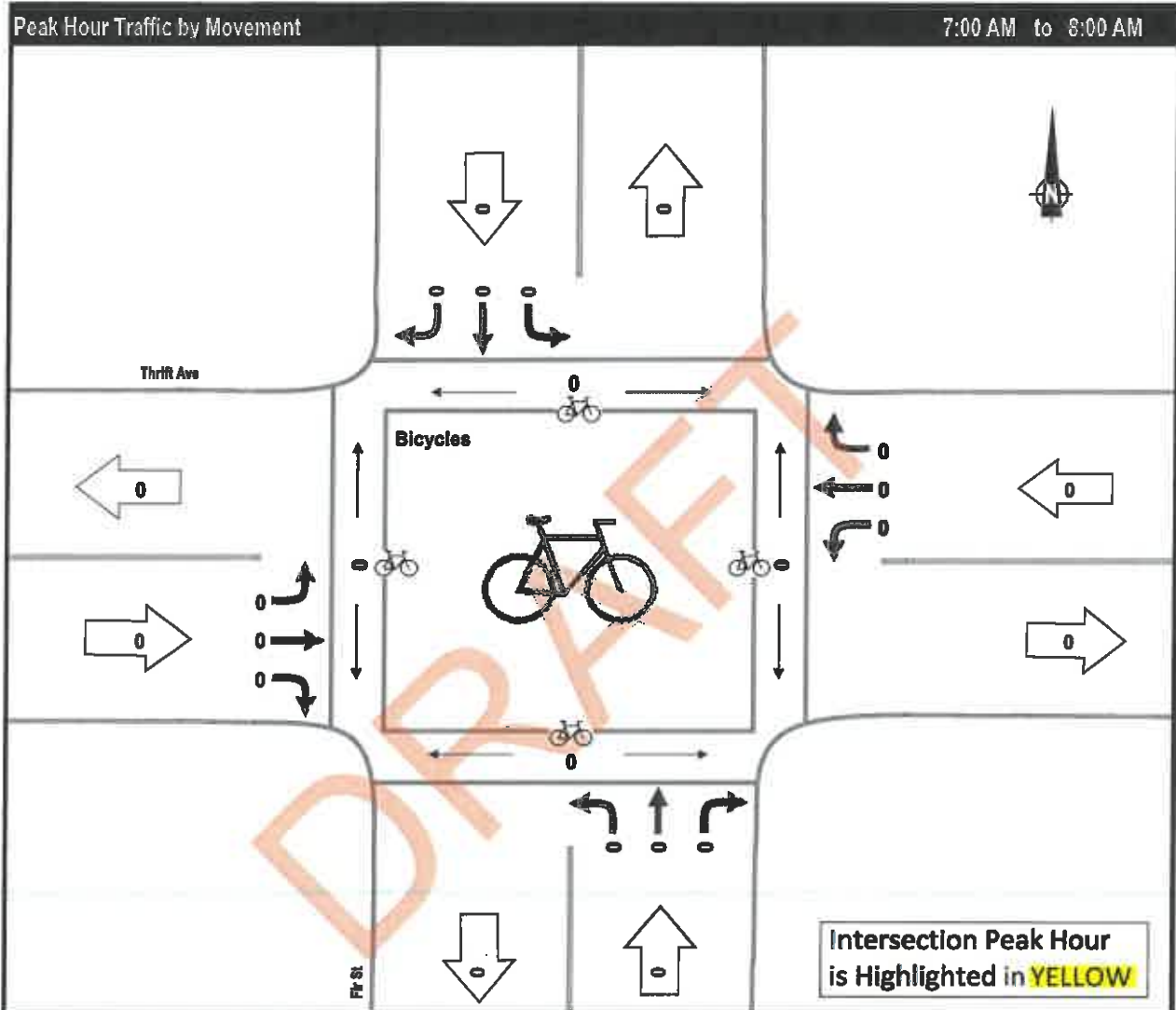
Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.55
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					4
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
Survey Total	0	0	0	0	0	0	0	1	0	0	2	0					3
7:00	0	0	0	0	0	0	0	0	0	0	0	0					0
7:15	0	0	0	0	0	0	0	0	0	0	1	0					1
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	1	0					1
8:30	0	0	0	0	0	0	0	1	0	0	0	0					1
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Morning Peak Period

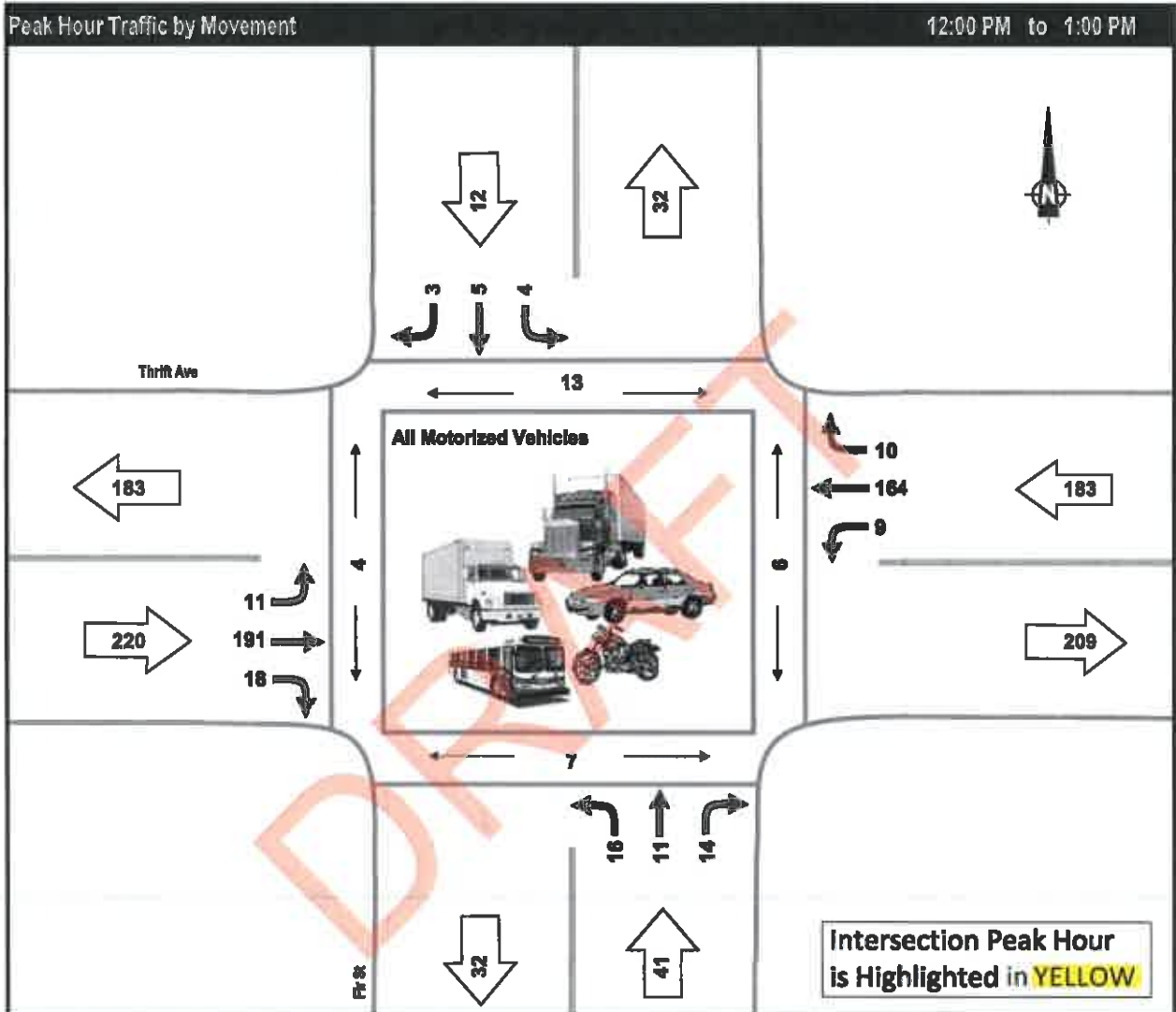
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

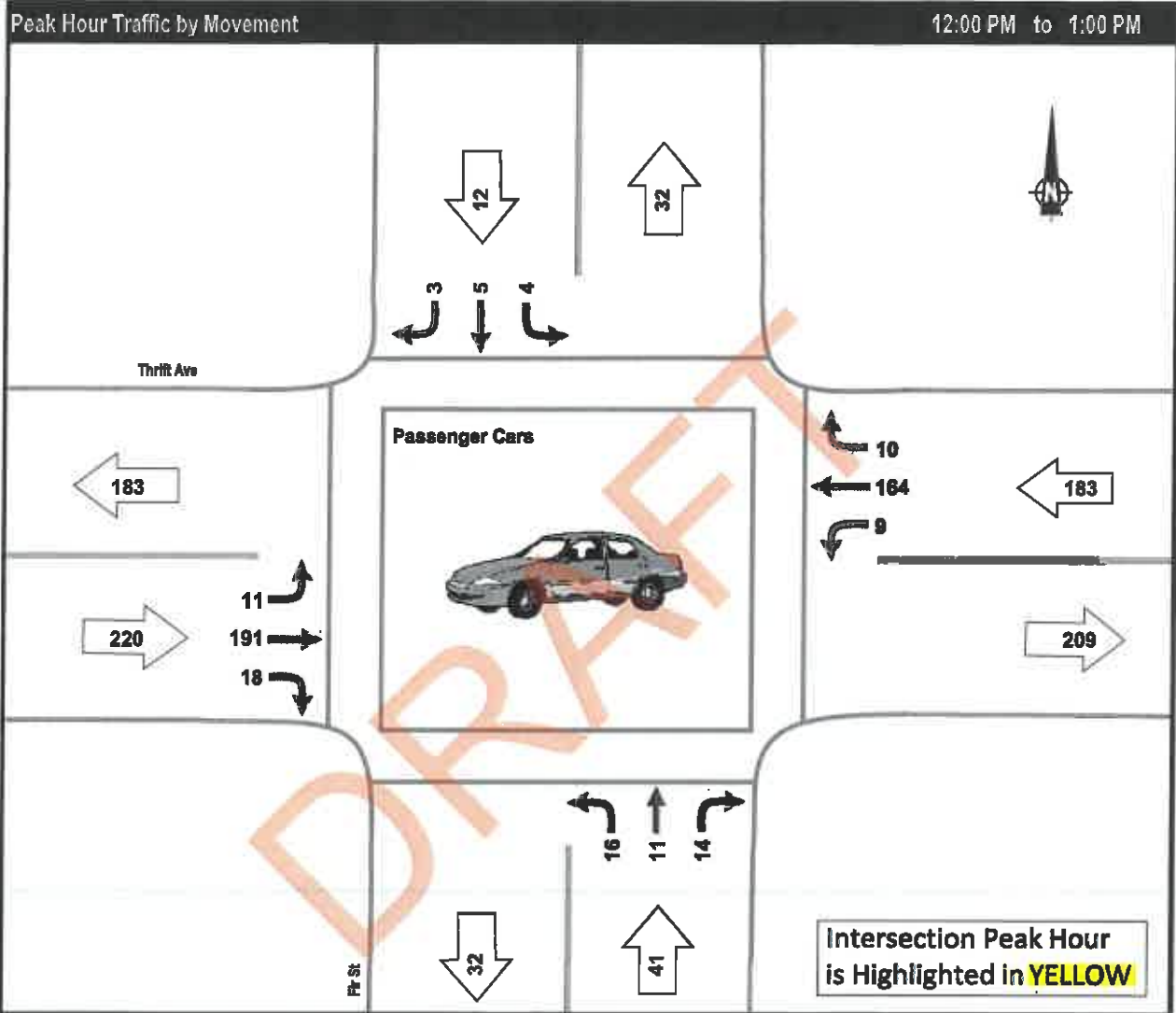
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	4	5	3	16	11	14	11	191	18	9	164	10	15	7	4	6	488
PH Factor	1.00	0.53	0.38	0.67	0.69	0.68	0.46	0.96	0.75	0.75	0.89	0.50	0.65	0.58	0.50	0.50	0.93
PHF	4	8	8	24	16	16	24	200	24	12	184	20	20	12	8	12	488
Average Hour	3	4	8	22	11	13	9	170	17	10	166	8	13	12	5	5	441
Survey Total	6	8	15	43	22	25	17	339	34	19	332	16	26	23	9	10	876
11:00	1	0	4	10	3	2	1	26	4	5	40	1	8	0	2	2	97
11:15	0	1	2	8	1	2	1	43	6	1	52	2	2	3	2	2	117
11:30	0	1	3	6	3	3	2	41	2	2	41	2	1	6	1	0	108
11:45	1	1	3	5	4	4	2	38	4	2	36	1	2	7	0	0	100
12:00	1	0	2	2	4	3	2	48	4	1	45	5	1	2	2	0	115
12:15	1	2	0	4	3	4	1	50	6	3	31	1	3	1	0	0	108
12:30	1	1	0	6	2	4	2	48	2	2	42	3	5	1	0	3	113
12:45	1	2	1	4	2	3	6	47	6	3	46	1	4	3	2	3	122

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

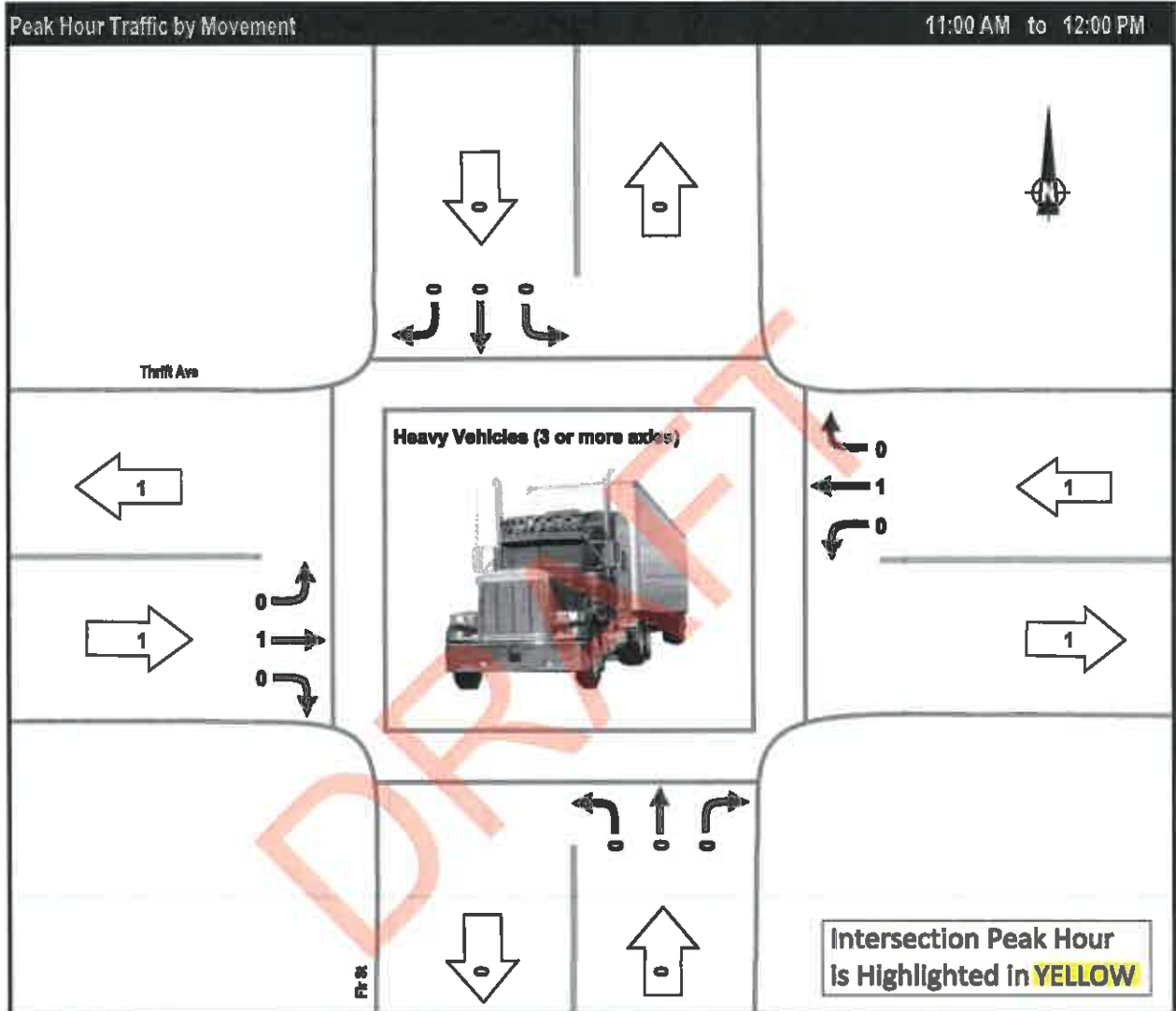
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	4	5	3	16	11	14	11	191	19	9	164	10					456
PHF	1.00	0.63	0.36	0.67	0.69	0.88	0.46	0.96	0.75	0.75	0.89	0.50					0.93
Peak 15 X 4	4	8	8	24	16	16	24	200	24	12	184	20					488
Average Hour	3	4	8	22	11	13	9	169	17	10	166	8					440
Survey Total	6	8	15	43	22	25	17	338	34	19	331	16					874
11:00	1	0	4	10	3	2	1	25	4	5	40	1					96
11:15	0	1	2	6	1	2	1	43	6	1	52	2					117
11:30	0	1	3	6	3	3	2	41	2	2	40	2					105
11:45	1	1	3	5	4	4	2	38	4	2	35	1					100
12:00	1	0	2	2	4	3	2	48	4	1	45	5					116
12:15	1	2	0	4	3	4	1	50	6	3	31	1					108
12:30	1	1	0	6	2	4	2	48	2	2	42	3					113
12:45	1	2	1	4	2	3	6	47	6	3	46	1					122

Project: #7026: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Midday Peak Period

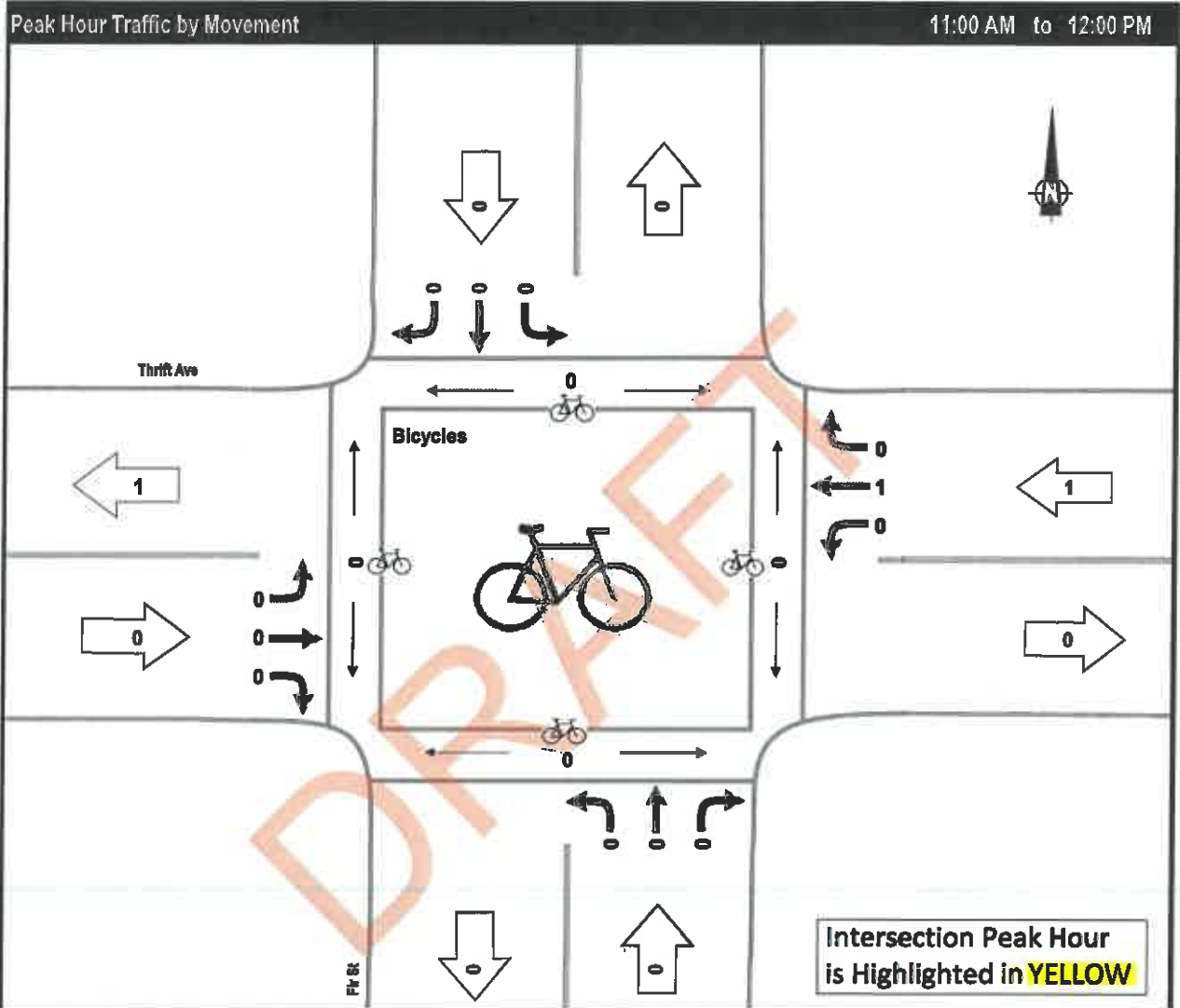


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS					Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E		
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2	
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					(0.50)	
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					4	
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0					2	
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0					2	
11:00	0	0	0	0	0	0	0	1	0	0	0	0					1	
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0	
11:30	0	0	0	0	0	0	0	0	0	0	1	0					1	
11:45	0	0	0	0	0	0	0	0	0	0	0	0					0	
12:00	0	0	0	0	0	0	0	0	0	0	0	0					0	
12:15	0	0	0	0	0	0	0	0	0	0	0	0					0	
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0	
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0	

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

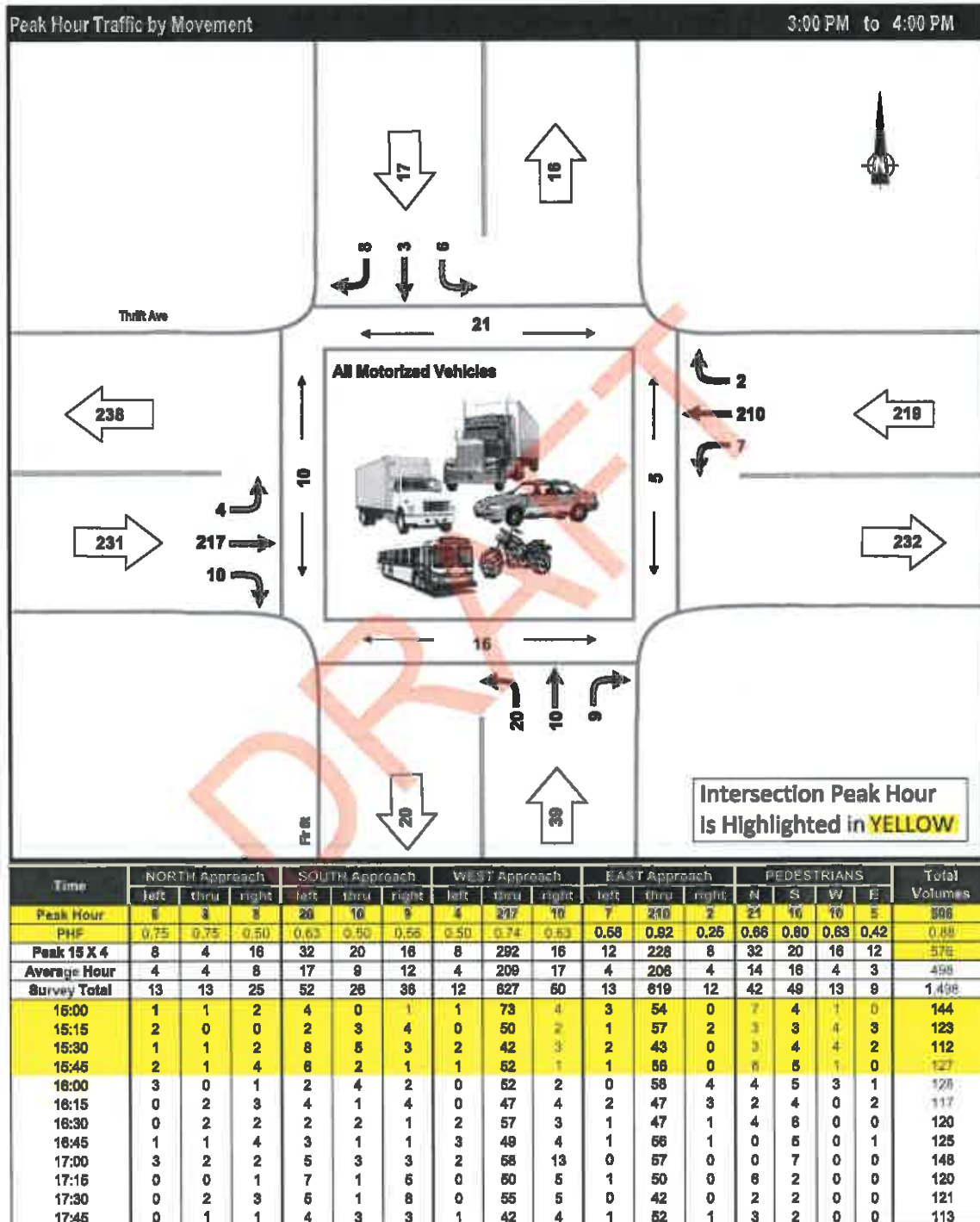
Midday Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
Survey Total	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
11:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
11:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles



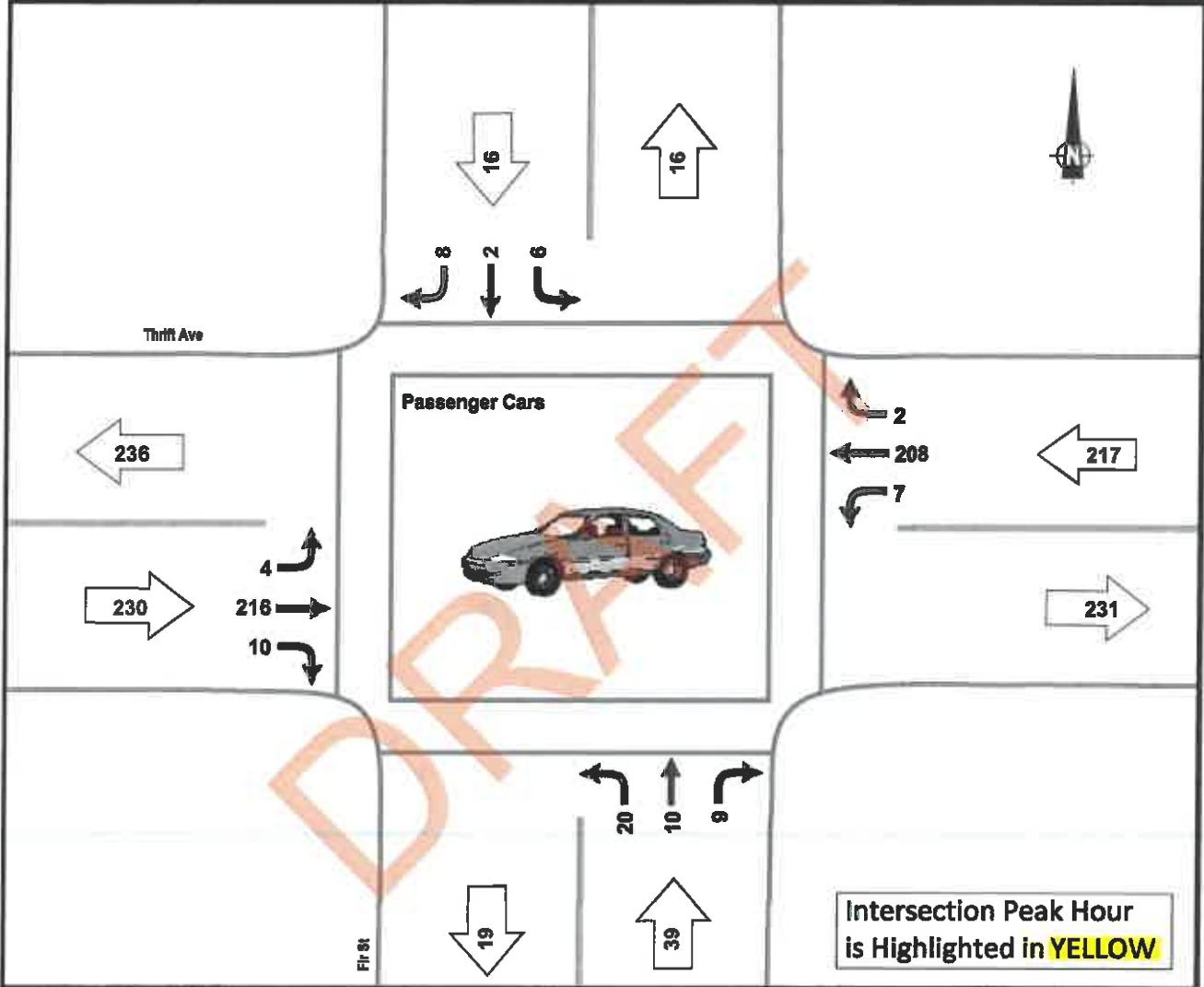


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period

Peak Hour Traffic by Movement

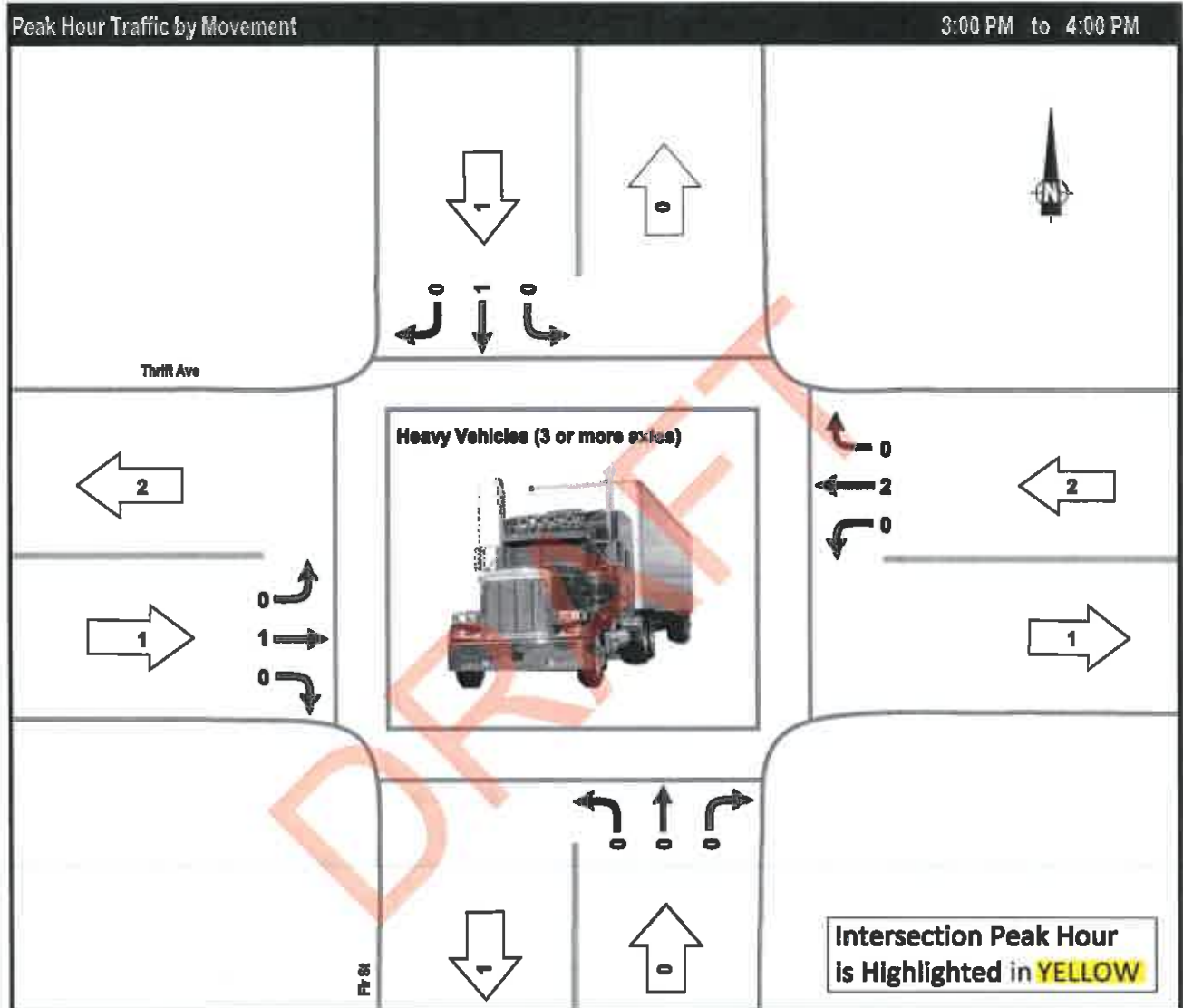
3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	6	2	8	20	10	9	4	216	10	7	206	2					502
PHF	0.75	0.50	0.50	0.83	0.50	0.56	0.50	0.75	0.63	0.58	0.91	0.25					0.89
Peak 15 X 4	8	4	16	32	20	16	8	288	16	12	228	8					564
Average Hour	4	4	8	17	8	12	4	209	17	4	206	4					497
Survey Total	13	12	25	52	25	36	12	626	50	13	617	12					1,493
15:00	1	0	2	4	0	1	1	72	4	3	53	0					141
15:15	2	0	0	2	3	4	0	50	2	1	57	2					123
15:30	1	1	2	8	5	3	2	42	3	2	43	0					112
15:45	2	1	4	6	2	1	1	52	1	1	55	0					126
16:00	3	0	1	2	3	2	0	52	2	0	58	4					127
16:15	0	2	3	4	1	4	0	47	4	2	47	3					117
16:30	0	2	2	2	2	1	2	57	3	1	47	1					120
16:45	1	1	4	3	1	1	3	49	4	1	56	1					125
17:00	3	2	2	5	3	3	2	58	13	0	57	0					148
17:15	0	0	1	7	1	5	0	50	5	1	50	0					120
17:30	0	2	3	5	1	8	0	55	5	0	42	0					121
17:45	0	1	1	4	3	3	1	42	4	1	52	1					113

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	1	0	0	0	0	0	1	0	0	2	0					4
PHF	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.50	0.00					0.33
Peak 15 X 4	0	4	0	0	0	0	0	4	0	0	4	0					12
Average Hour	0	0	0	0	0	0	0	0	0	0	1	0					1
Survey Total	0	1	0	0	1	0	0	1	0	0	2	0					5
15:00	0	1	0	0	0	0	0	1	0	0	1	0					3
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	1	0					1
16:00	0	0	0	0	1	0	0	0	0	0	0	0					1
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

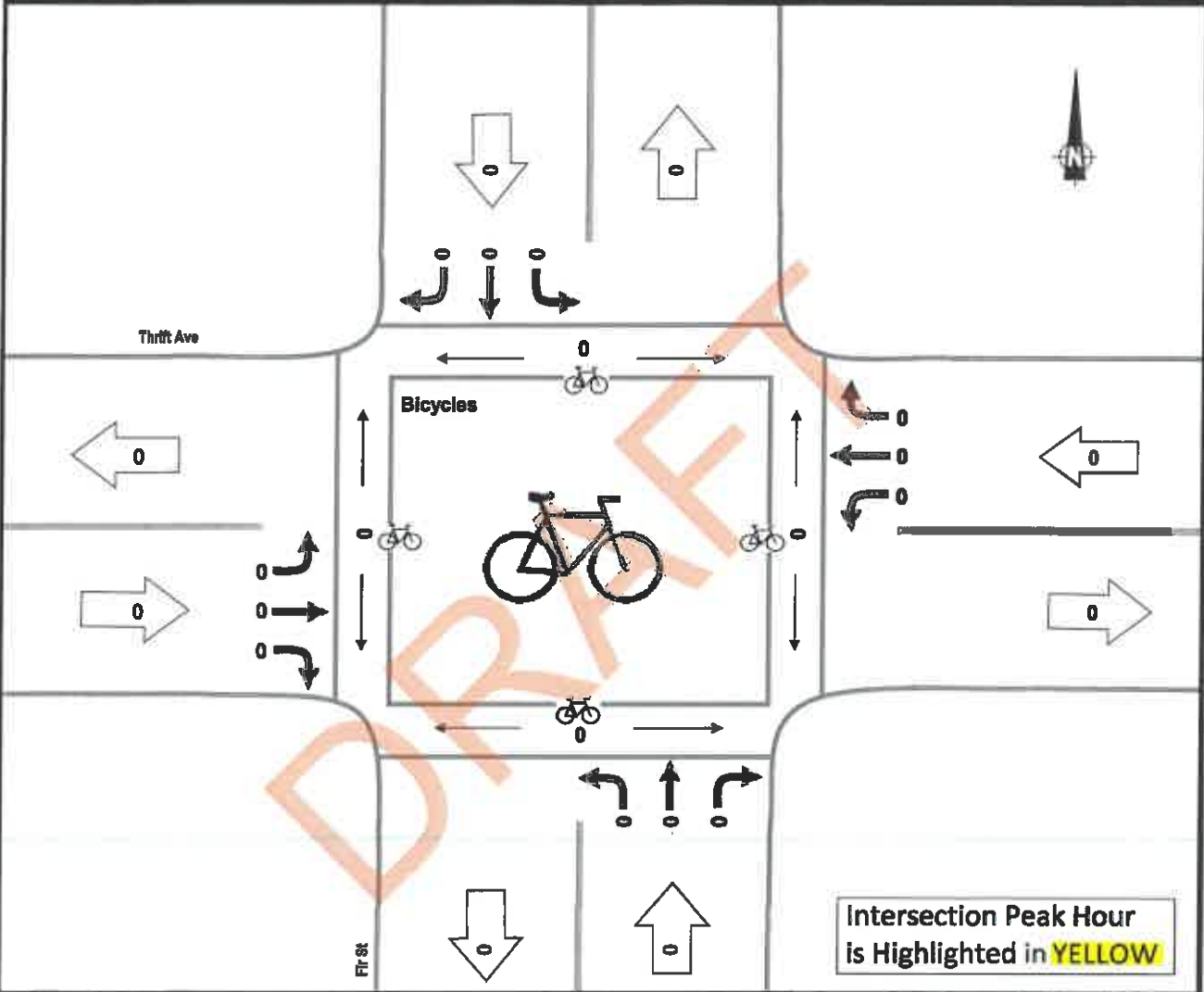
Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Survey Total	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
15:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1



George Lane & Russell Ave

Wednesday, April 03, 2019

Vehicle Classification Summary

Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

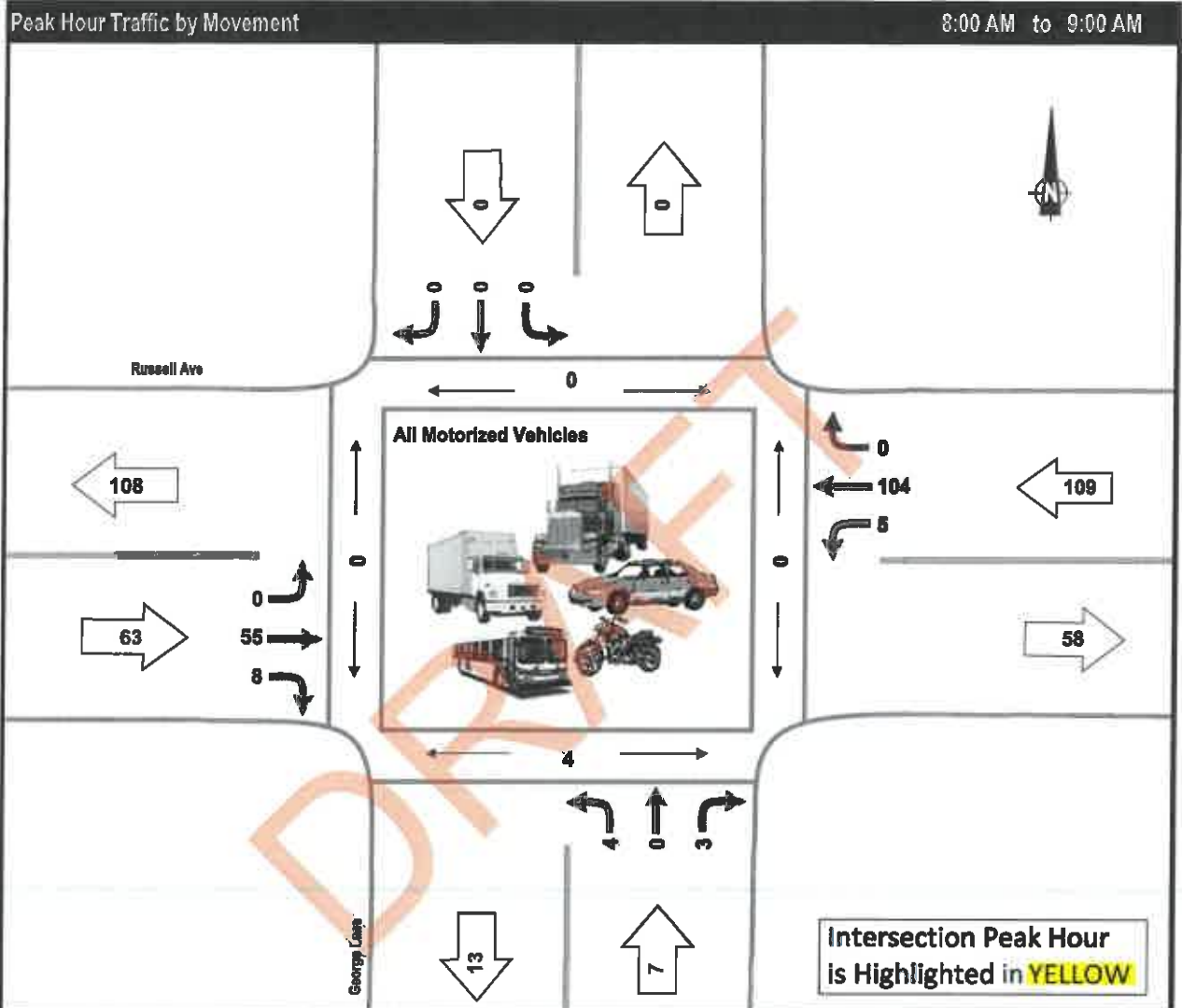
Z:\7

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 09:00)	Volume	284	0			284
	%	100.0%	0.0%			100.0%
Midday (11:00 - 13:00)	Volume	596	3			599
	%	99.5%	0.5%			100.0%
Afternoon (15:00 - 18:00)	Volume	854	0			854
	%	100.0%	0.0%			100.0%
Total (7 Hours)	Volume	1,734	3			1,737
	%	99.8%	0.2%			100.0%

DRAFT

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

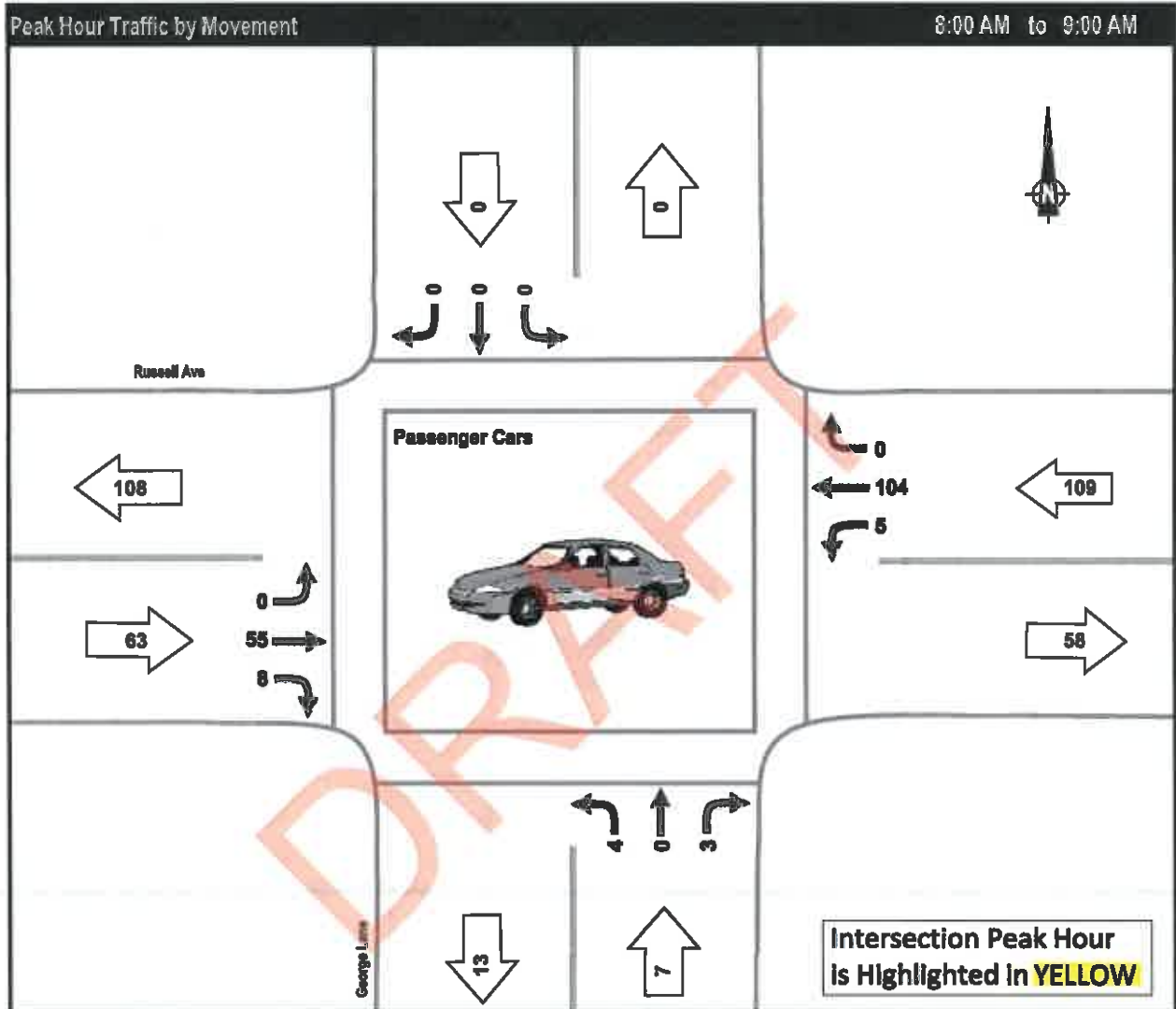
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	4	0	3	0	55	8	5	104	0	0	4	0	0	179
PHF	0.00	0.00	0.00	0.50	0.00	0.38	0.00	0.92	0.67	0.63	0.81	0.00	0.00	0.50	0.00	0.00	0.81
Peak 15 X 4	0	0	0	8	0	8	0	60	12	8	128	0	0	8	0	0	220
Average Hour	0	0	0	3	0	2	0	55	5	4	74	0	0	5	0	0	143
Survey Total	0	0	0	6	0	4	0	110	9	8	147	0	0	9	0	0	284
7:00	0	0	0	2	0	0	0	10	0	0	12	0	0	1	0	0	24
7:15	0	0	0	0	0	1	0	21	1	1	9	0	0	2	0	0	33
7:30	0	0	0	0	0	0	0	12	0	0	7	0	0	1	0	0	19
7:45	0	0	0	0	0	0	0	12	0	2	15	0	0	1	0	0	29
8:00	0	0	0	0	0	0	0	12	2	1	20	0	0	2	0	0	35
8:15	0	0	0	1	0	0	0	15	0	0	25	0	0	0	0	0	41
8:30	0	0	0	1	0	2	0	13	3	2	27	0	0	0	0	0	48
8:45	0	0	0	2	0	1	0	15	3	2	32	0	0	2	0	0	55

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

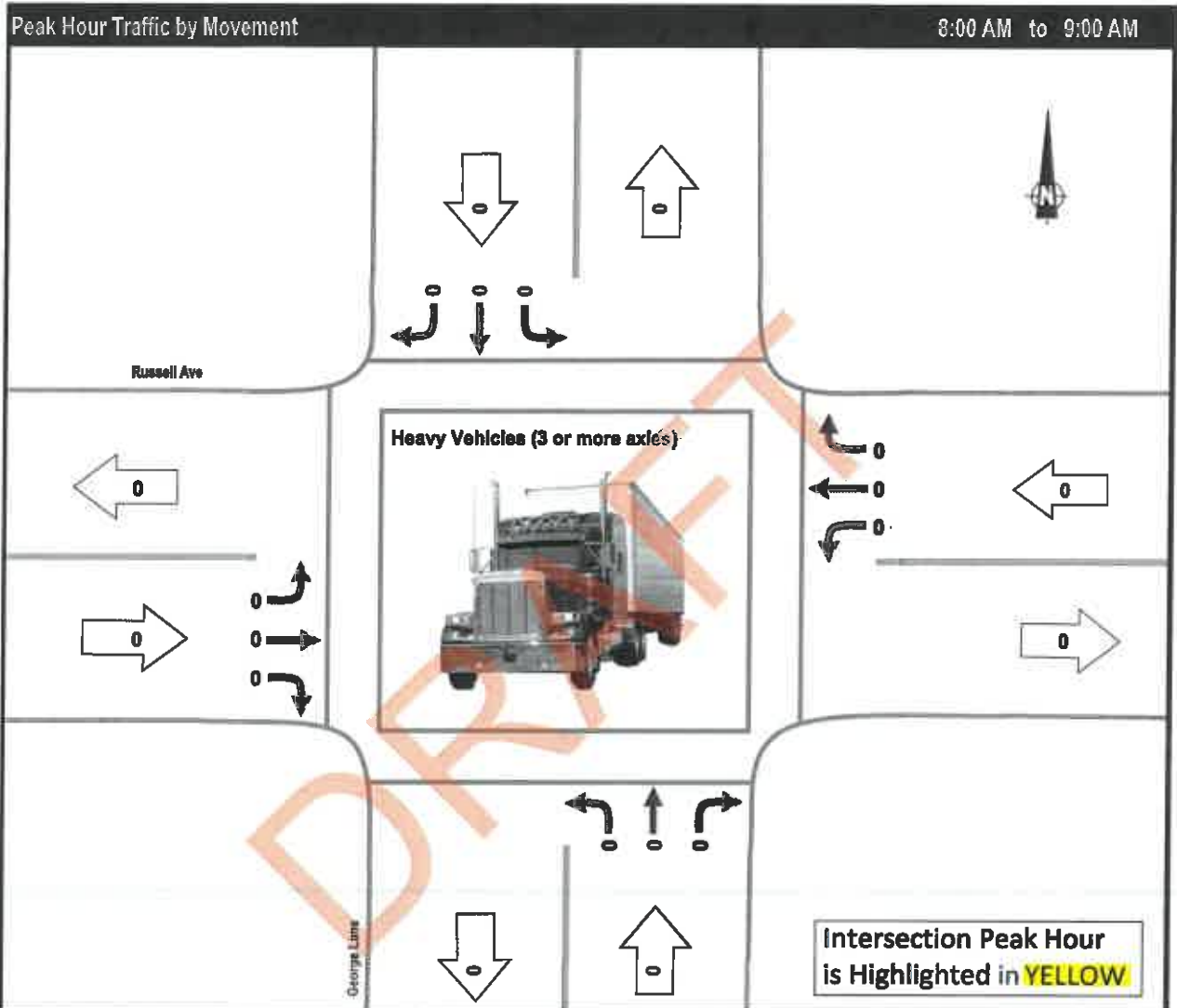
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	4	0	3	0	55	8	5	104	0					179
PHF	0.00	0.00	0.00	0.50	0.00	0.38	0.00	0.92	0.67	0.63	0.61	0.00					0.61
Peak 15 X 4	0	0	0	8	0	8	0	60	12	8	128	0					220
Average Hour	0	0	0	3	0	2	0	55	5	4	74	0					143
Survey Total	0	0	0	8	0	4	0	110	9	8	147	0					284
7:00	0	0	0	2	0	0	0	10	0	0	12	0					24
7:15	0	0	0	0	0	1	0	21	1	1	9	0					33
7:30	0	0	0	0	0	0	0	12	0	0	7	0					19
7:45	0	0	0	0	0	0	0	12	0	2	15	0					29
8:00	0	0	0	0	0	0	0	12	2	1	20	0					35
8:15	0	0	0	1	0	0	0	15	0	0	25	0					41
8:30	0	0	0	1	0	2	0	13	3	2	27	0					48
8:45	0	0	0	2	0	1	0	15	3	2	32	0					55

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

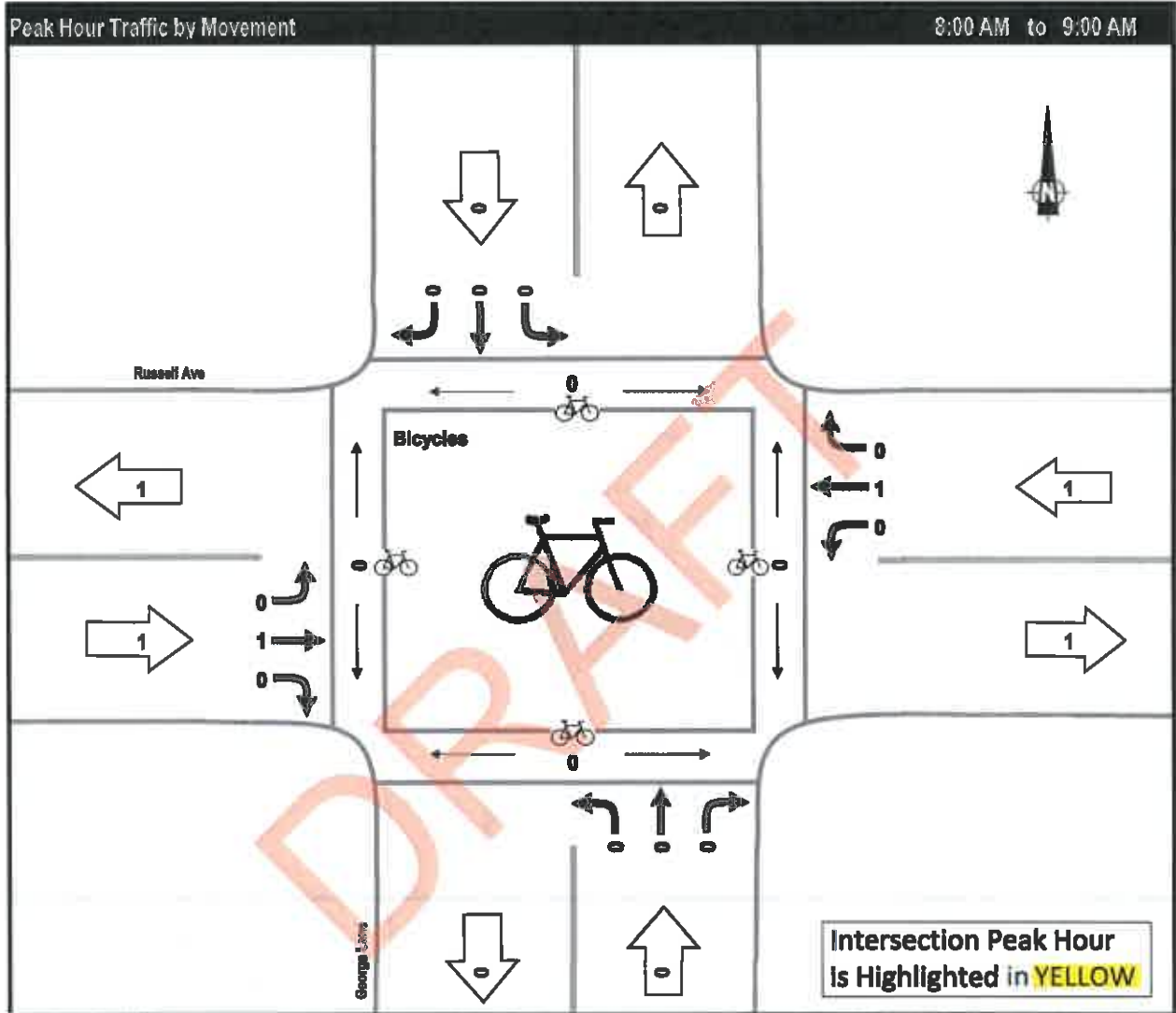
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0					0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0					0
7:00	0	0	0	0	0	0	0	0	0	0	0	0					0
7:15	0	0	0	0	0	0	0	0	0	0	0	0					0
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	0	0					0
8:30	0	0	0	0	0	0	0	0	0	0	0	0					0
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

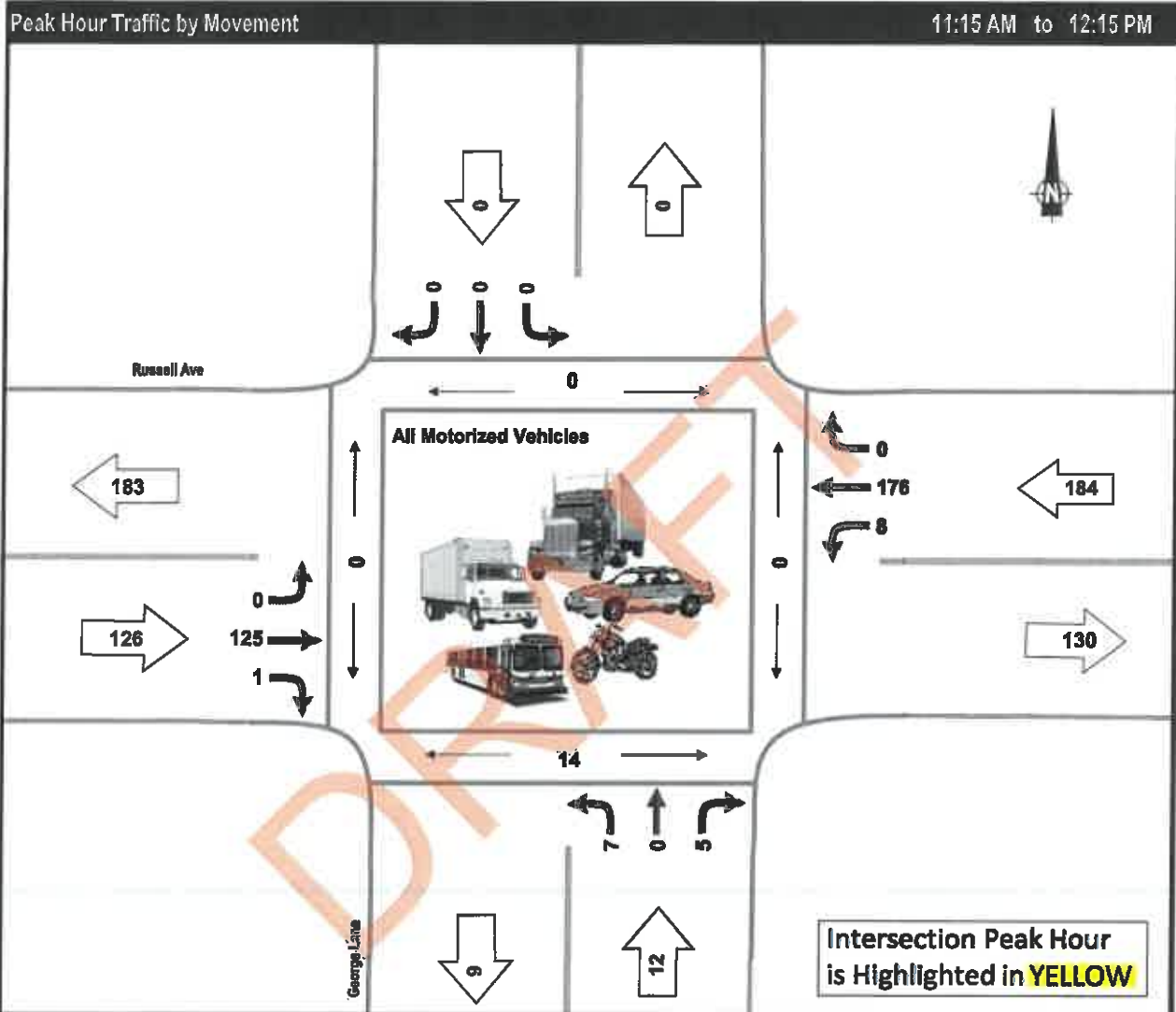
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes	
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E		
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

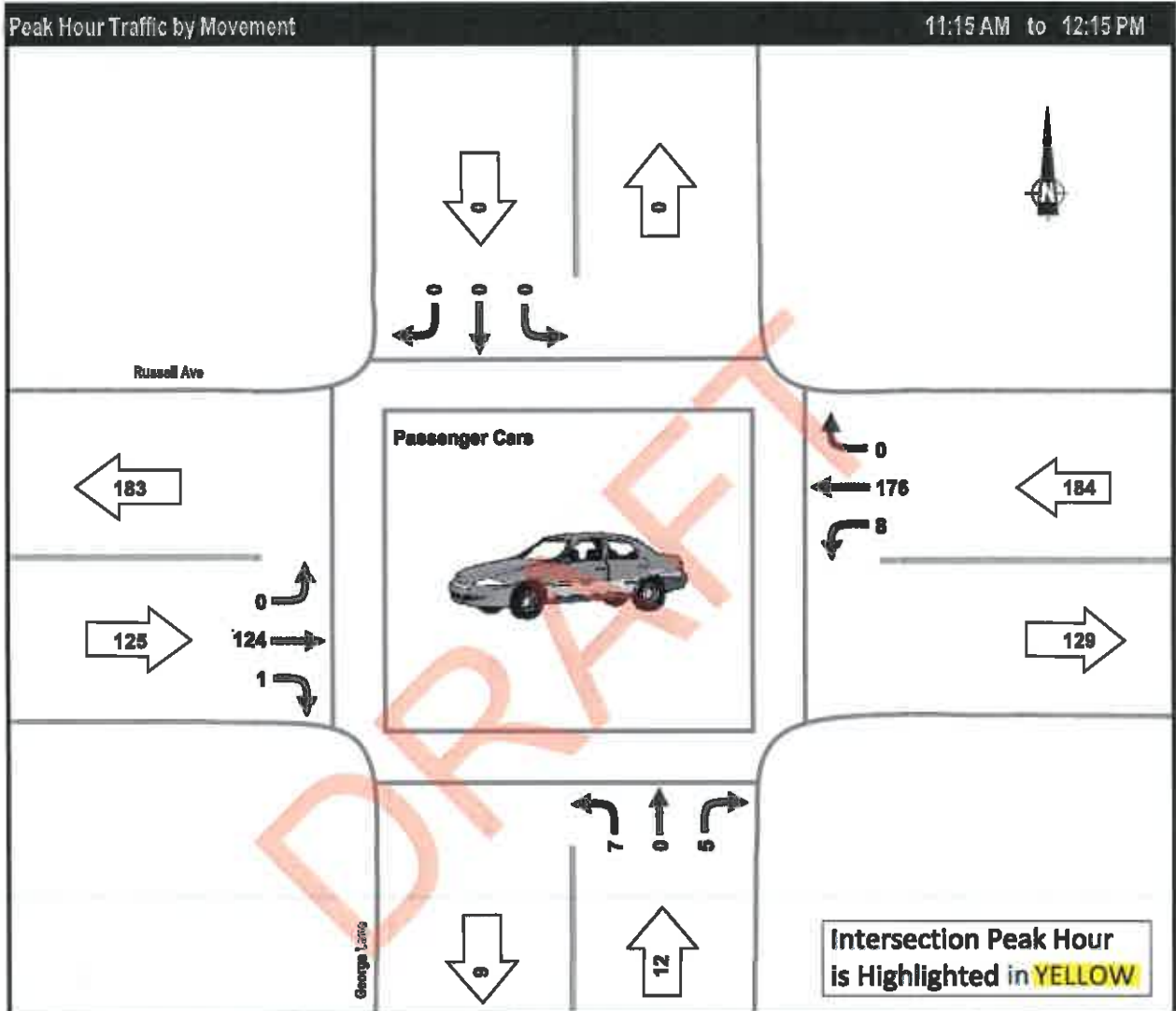
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	7	0	5	0	125	1	0	176	0	0	14	0	0	322
PH Factor	0.00	0.00	0.00	0.44	0.00	0.42	0.00	0.89	0.25	1.00	0.77	0.00	0.00	0.58	0.00	0.00	0.85
PHF	0	0	0	16	0	12	0	140	4	8	228	0	0	24	0	0	380
Average Hour	0	0	0	5	0	4	0	112	2	5	173	0	0	9	0	0	301
Survey Total	0	0	0	9	0	8	0	224	3	9	346	0	0	18	0	0	599
11:00	0	0	0	1	0	0	0	18	0	0	51	0	0	0	0	0	70
11:15	0	0	0	3	0	0	0	26	0	2	40	0	0	3	0	0	71
11:30	0	0	0	4	0	1	0	31	0	2	38	0	0	2	0	0	76
11:45	0	0	0	0	0	1	0	35	0	2	57	0	0	6	0	0	85
12:00	0	0	0	0	0	3	0	33	1	2	41	0	0	3	0	0	80
12:15	0	0	0	0	0	1	0	18	0	1	28	0	0	1	0	0	48
12:30	0	0	0	0	0	1	0	33	1	0	51	0	0	3	0	0	86
12:45	0	0	0	1	0	1	0	29	1	0	40	0	0	0	0	0	72

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

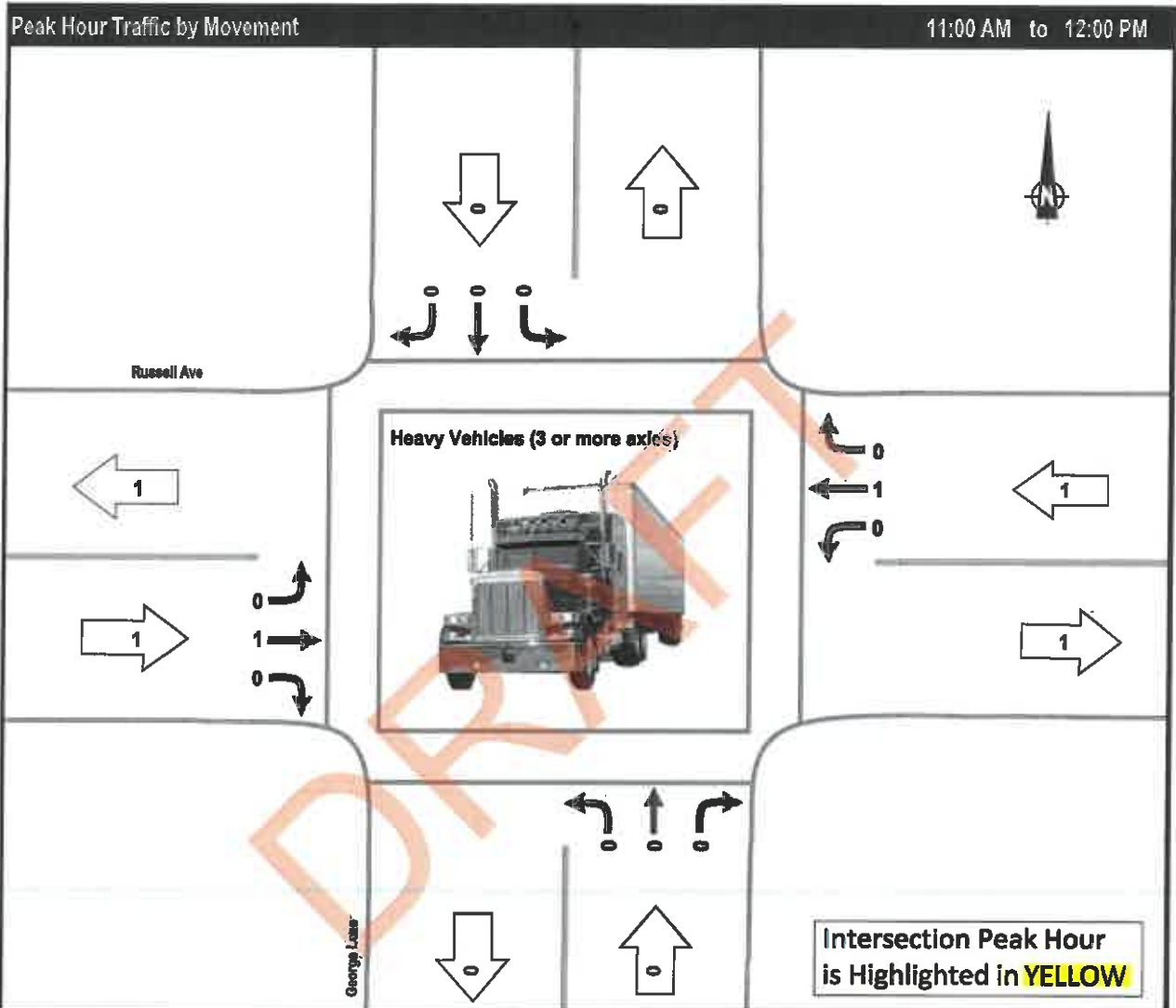
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	7	0	5	0	124	1	8	176	0					321
PHF	0.00	0.00	0.00	0.44	0.00	0.42	0.00	0.91	0.25	1.00	0.77	0.00					0.85
Peak 15 X 4	0	0	0	16	0	12	0	136	4	8	228	0					376
Average Hour	0	0	0	5	0	4	0	112	2	4	173	0					300
Survey Total	0	0	0	9	0	8	0	223	3	8	345	0					596
11:00	0	0	0	1	0	0	0	18	0	0	50	0					69
11:15	0	0	0	3	0	0	0	26	0	2	40	0					71
11:30	0	0	0	4	0	1	0	31	0	2	38	0					76
11:45	0	0	0	0	0	1	0	34	0	2	57	0					94
12:00	0	0	0	0	0	3	0	33	1	2	41	0					80
12:15	0	0	0	0	0	1	0	19	0	0	28	0					48
12:30	0	0	0	0	0	1	0	33	1	0	51	0					86
12:45	0	0	0	1	0	1	0	29	1	0	40	0					72

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

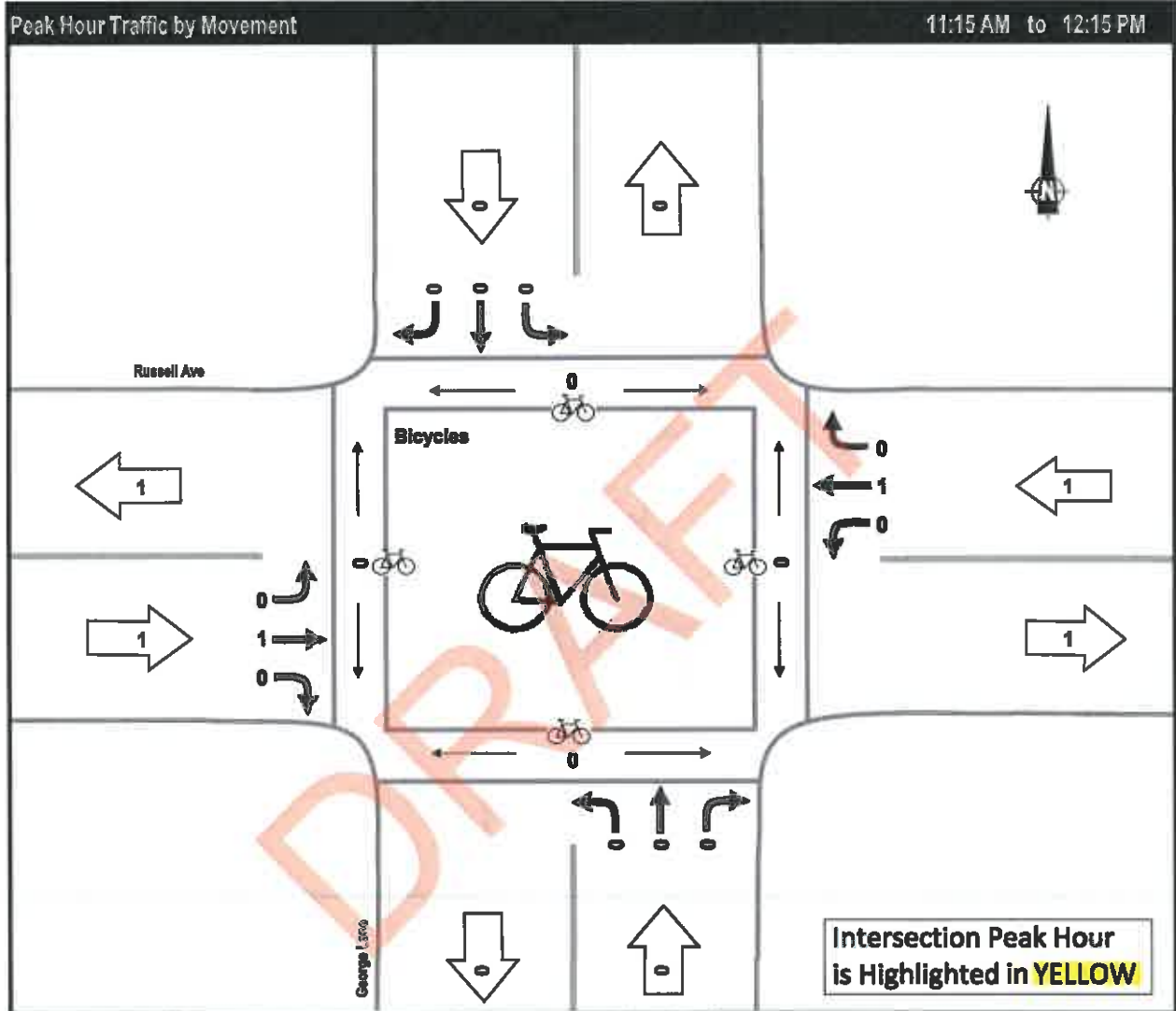
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					4
Average Hour	0	0	0	0	0	0	0	1	0	1	1	0					3
Survey Total	0	0	0	0	0	0	0	1	0	1	1	0					3
11:00	0	0	0	0	0	0	0	0	0	0	1	0					1
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0
11:30	0	0	0	0	0	0	0	0	0	0	0	0					0
11:45	0	0	0	0	0	0	0	1	0	0	0	0					1
12:00	0	0	0	0	0	0	0	0	0	0	0	0					0
12:15	0	0	0	0	0	0	0	0	0	1	0	0					1
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

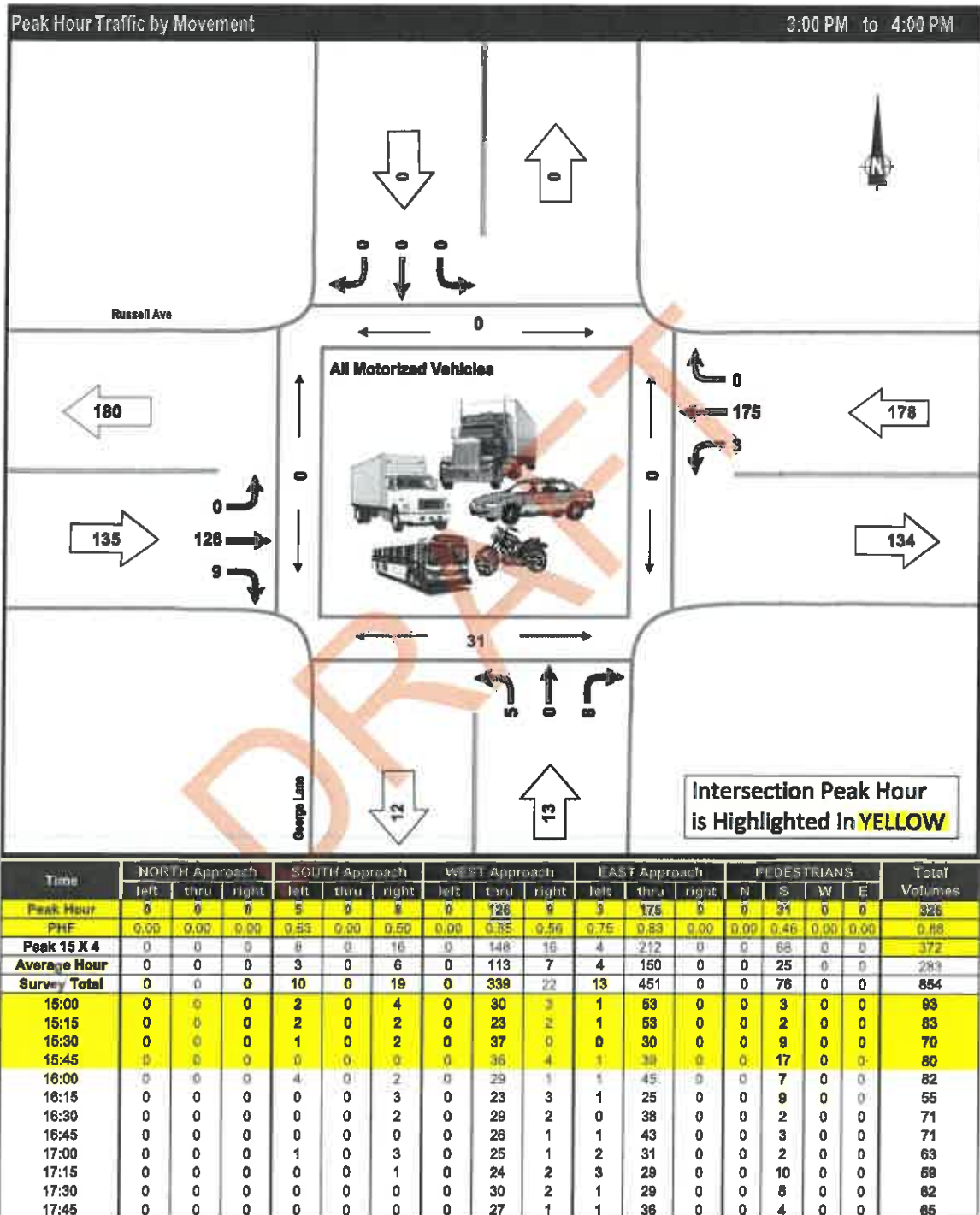
Note: Crosswalk bike volume shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
11:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

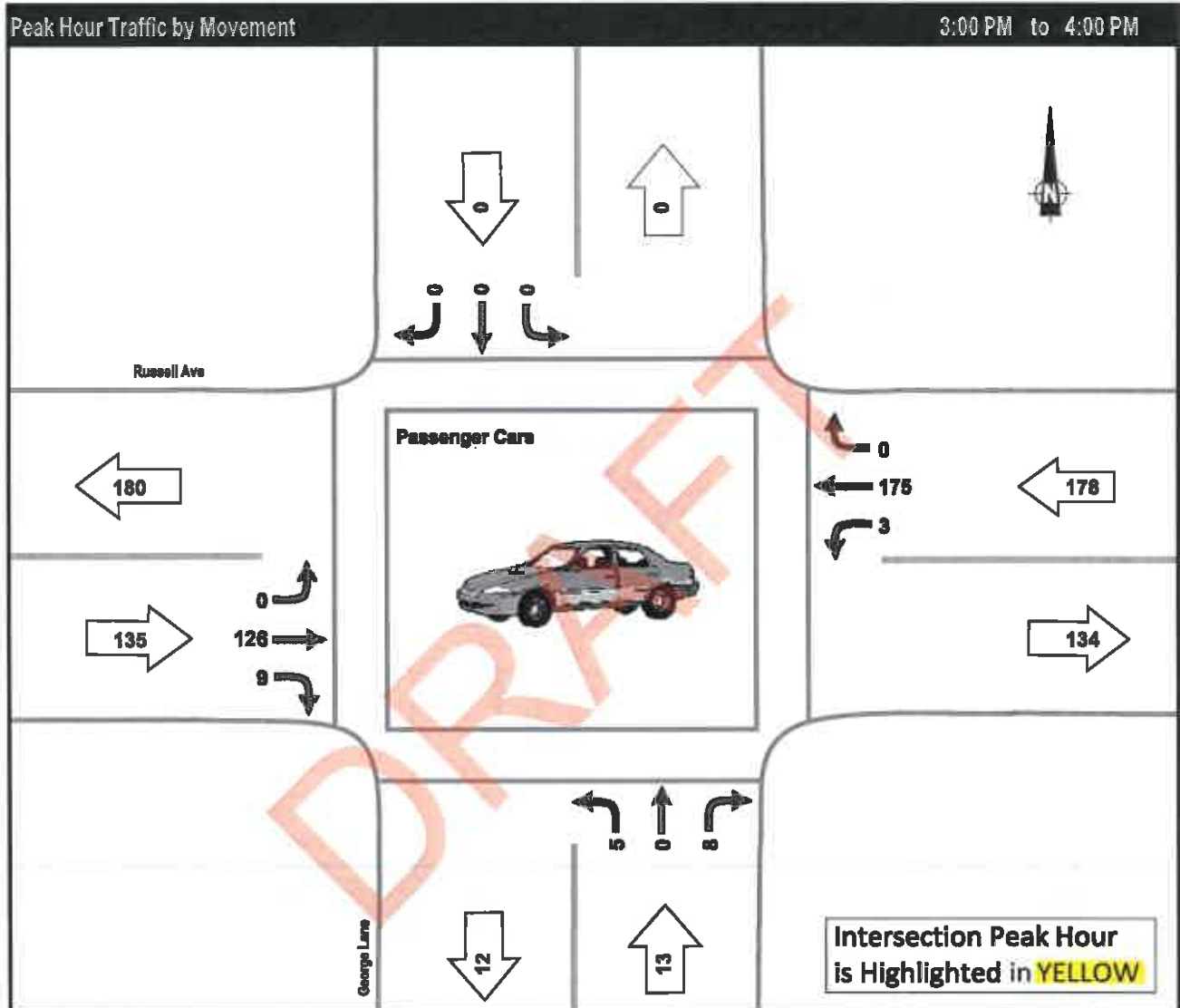
Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

Afternoon Peak Period



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	5	0	8	0	126	0	3	175	0					326
PHF	0.00	0.00	0.00	0.63	0.00	0.50	0.00	0.85	0.56	0.75	0.83	0.00					0.88
Peak 15 X 4	0	0	0	8	0	16	0	148	16	4	212	0					372
Average Hour	0	0	0	3	0	6	0	113	7	4	150	0					283
Survey Total	0	0	0	10	0	19	0	339	22	13	451	0					854
15:00	0	0	0	2	0	4	0	30	3	1	53	0					93
15:15	0	0	0	2	0	2	0	23	2	1	53	0					83
15:30	0	0	0	1	0	2	0	37	0	0	30	0					70
15:45	0	0	0	0	0	0	0	36	4	1	39	0					80
16:00	0	0	0	4	0	2	0	29	1	1	45	0					82
16:15	0	0	0	0	0	3	0	23	3	1	25	0					55
16:30	0	0	0	0	0	2	0	29	2	0	38	0					71
16:45	0	0	0	0	0	0	0	26	1	1	43	0					71
17:00	0	0	0	1	0	3	0	25	1	2	31	0					63
17:15	0	0	0	0	0	1	0	24	2	3	29	0					59
17:30	0	0	0	0	0	0	0	30	2	1	29	0					62
17:45	0	0	0	0	0	0	0	27	1	1	36	0					65

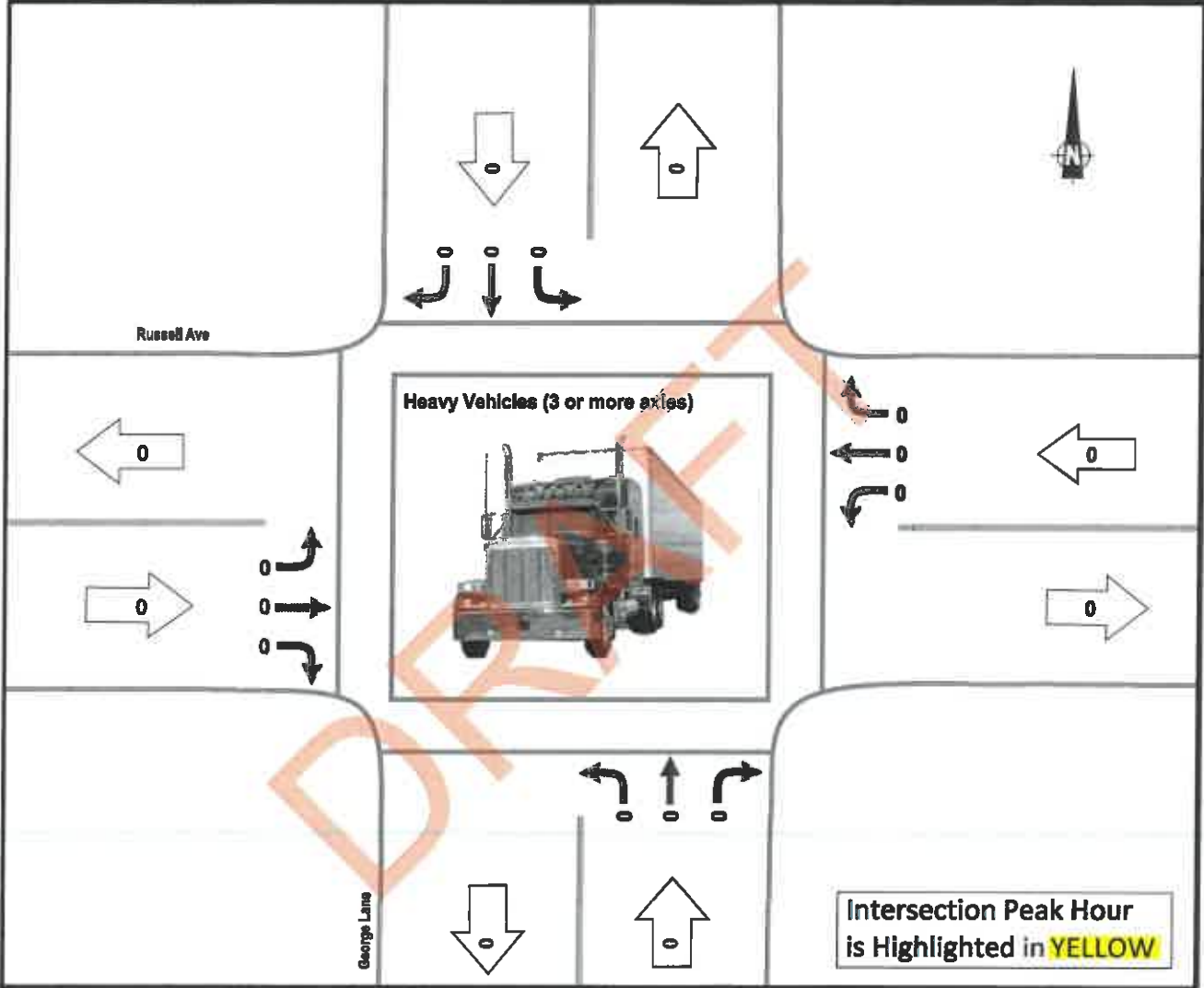


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Afternoon Peak Period

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM

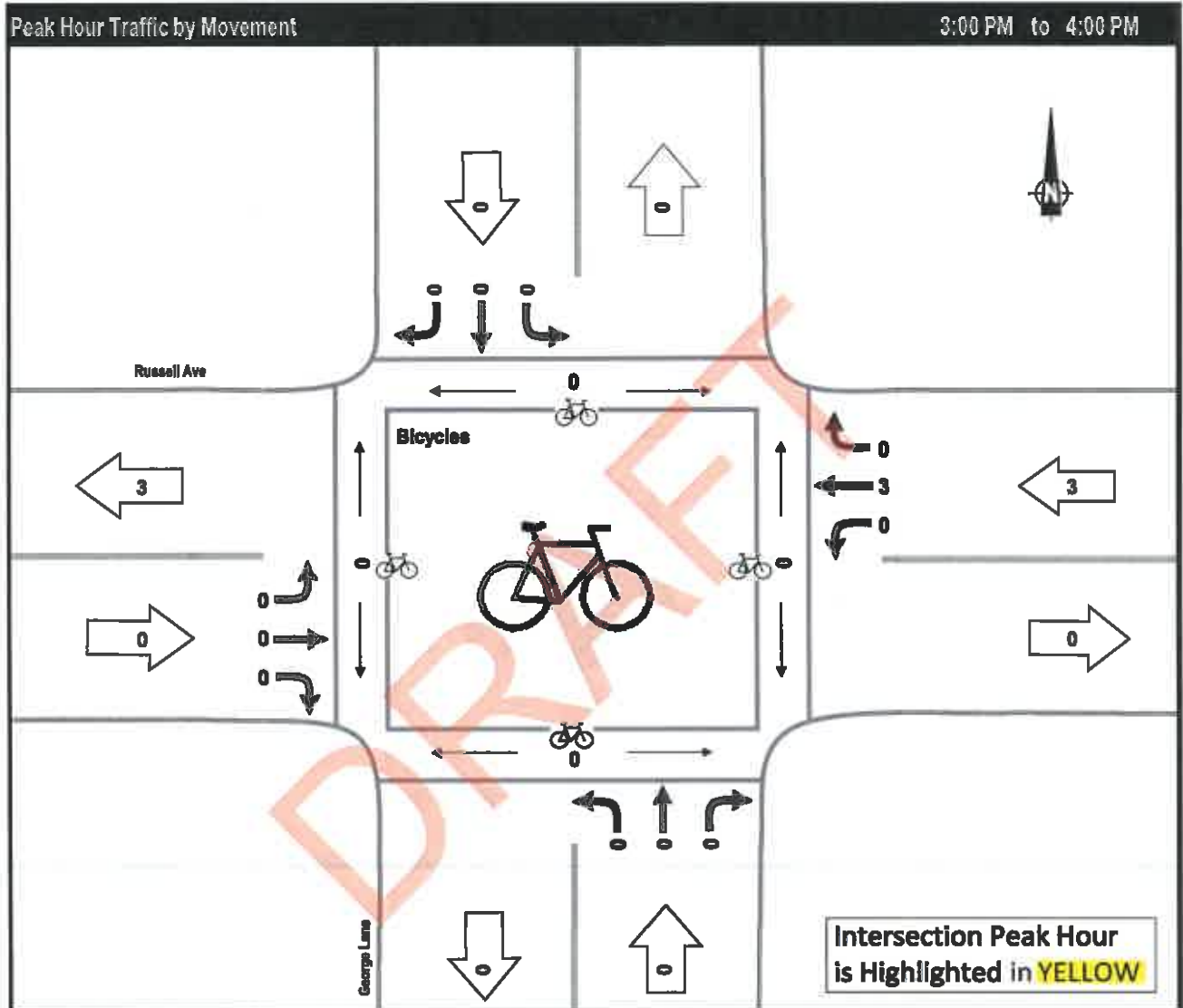


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0					0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0					0
15:00	0	0	0	0	0	0	0	0	0	0	0	0					0
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	0	0					0
16:00	0	0	0	0	0	0	0	0	0	0	0	0					0
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals.



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	3
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.38
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	8
Average Hour	0	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	3
Survey Total	0	0	0	0	0	0	0	3	0	0	5	0	0	0	0	0	8
15:00	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2
15:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0



George Lane & Thrift Ave

Wednesday, April 03, 2019

Vehicle Classification Summary

Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

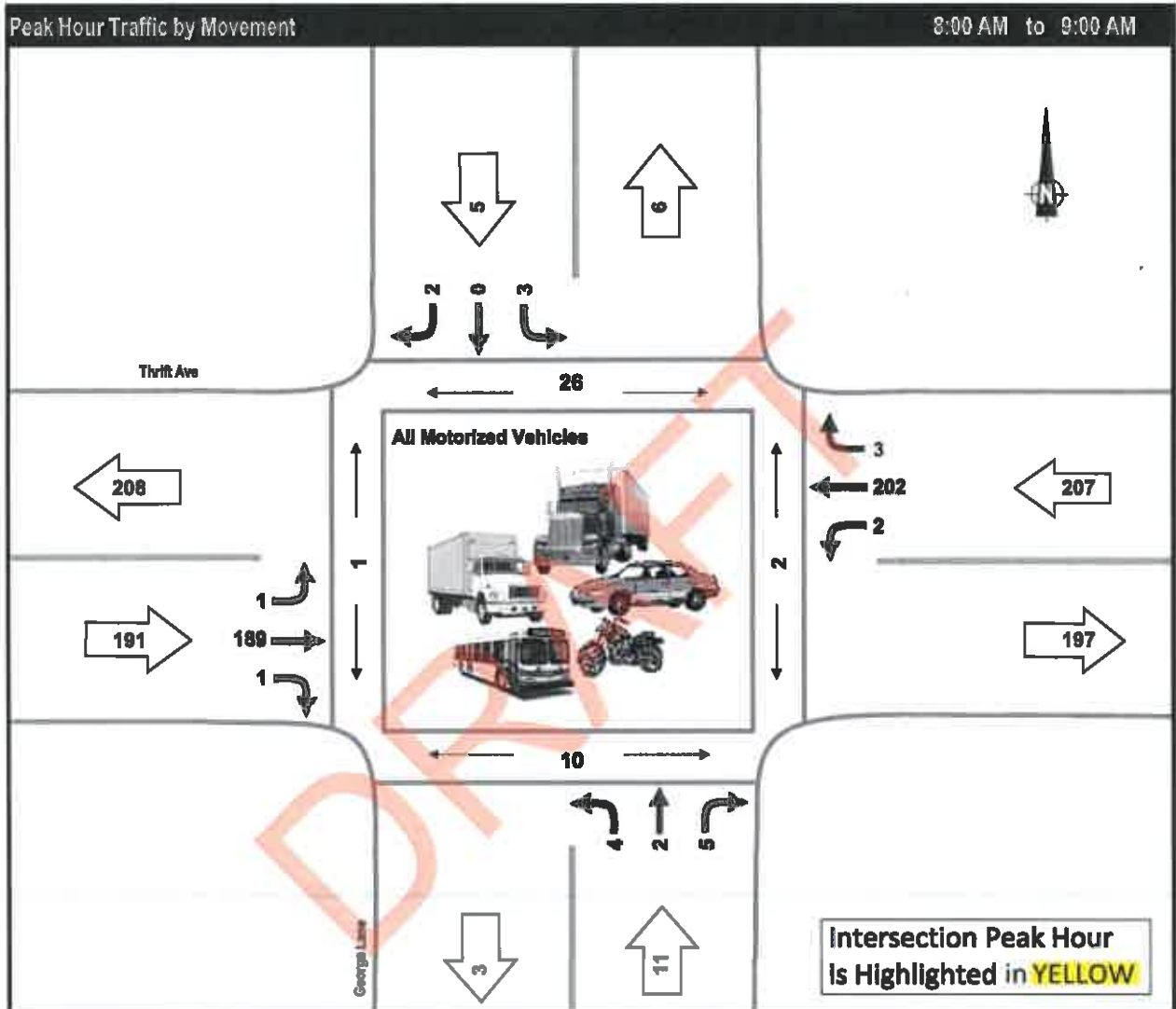
Z:\

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axes)			
Morning (07:00 - 09:00)	Volume	597	3			600
	%	99.5%	0.5%			100.0%
Midday (11:00 - 13:00)	Volume	787	2			789
	%	99.7%	0.3%			100.0%
Afternoon (15:00 - 18:00)	Volume	1,424	2			1,426
	%	99.9%	0.1%			100.0%
Total (7 Hours)	Volume	2,808	7			2,815
	%	99.8%	0.2%			100.0%

DRAFT

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

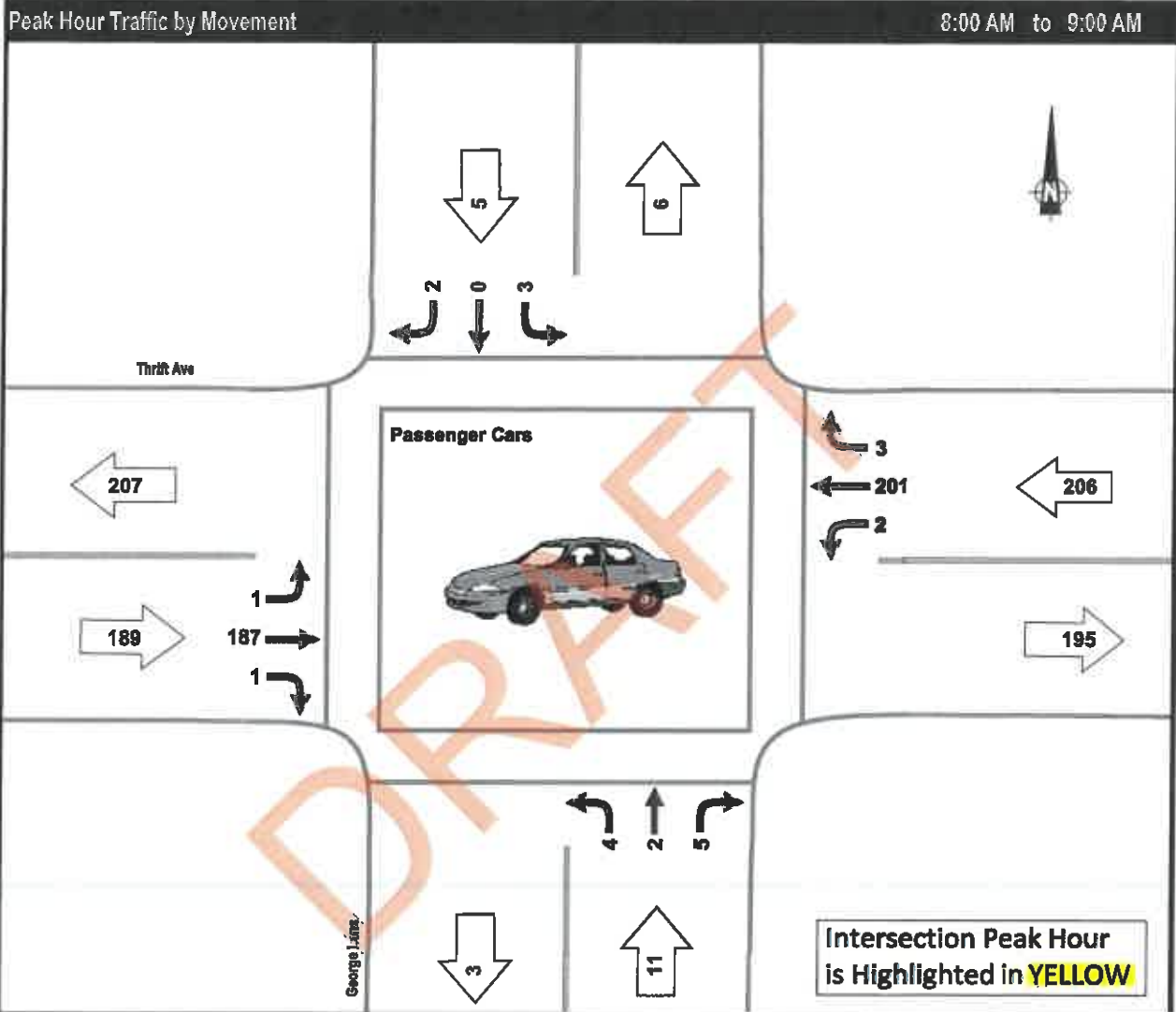
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	3	0	2	4	2	5	1	189	1	2	202	3	26	16	1	2	494
PHF	0.38	0.00	0.50	1.00	0.50	0.63	0.25	0.84	0.25	0.50	0.79	0.38	0.50	0.83	0.25	0.50	0.83
Peak 15 X 4	8	0	4	4	4	8	4	224	4	4	258	8	52	12	4	4	496
Average Hour	3	0	3	4	1	5	1	135	2	2	143	3	25	13	2	1	302
Survey Total	6	0	8	8	2	10	2	289	3	4	285	6	50	26	3	2	600
7:00	0	0	0	1	0	1	0	20	0	2	15	3	5	7	0	0	42
7:15	0	0	1	2	0	1	0	18	2	0	19	0	11	2	2	0	41
7:30	0	0	1	1	0	2	0	22	0	0	20	0	8	3	0	0	46
7:45	2	0	2	0	0	1	1	22	0	0	29	0	2	4	0	0	57
8:00	0	0	0	1	0	1	0	41	0	1	40	0	4	2	0	1	84
8:15	1	0	1	1	1	2	0	64	0	0	84	0	13	3	1	0	124
8:30	0	0	1	1	0	1	1	56	1	1	47	2	4	3	0	1	111
8:45	2	0	0	1	1	1	0	38	0	0	51	1	5	2	0	0	95

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars.

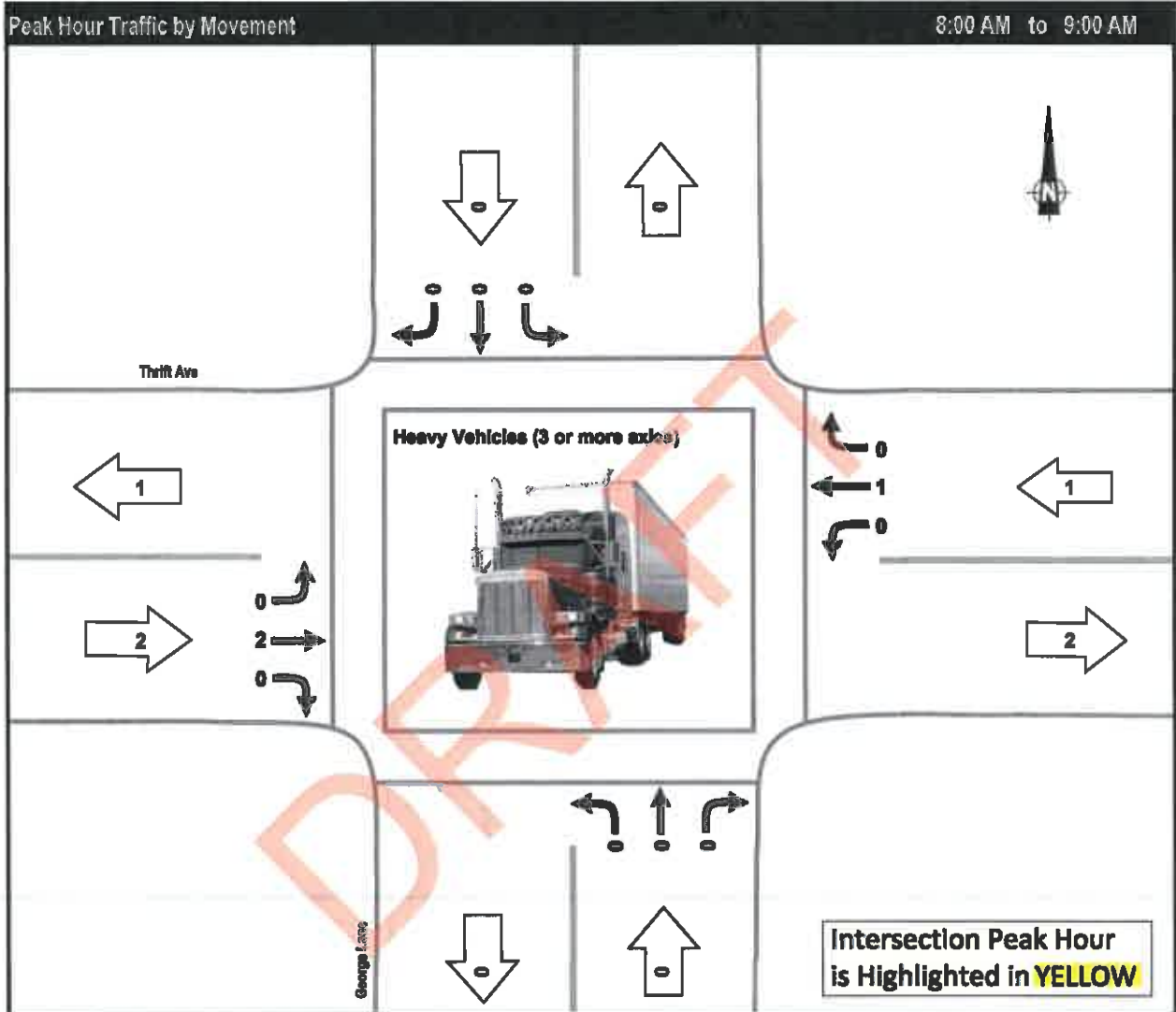
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	3	0	2	4	2	5	1	187	1	2	201	3					411
PHF	0.38	0.00	0.50	1.00	0.50	0.63	0.25	0.87	0.25	0.50	0.60	0.38					0.84
Peak 15 X 4	5	0	4	4	4	5	4	216	4	4	252	6					492
Average Hour	3	0	3	4	1	5	1	134	2	2	142	3					300
Survey Total	5	0	6	8	2	10	2	267	3	4	284	6					597
7:00	0	0	0	1	0	1	0	20	0	2	15	3					42
7:15	0	0	1	2	0	1	0	16	2	0	19	0					41
7:30	0	0	1	1	0	2	0	22	0	0	20	0					46
7:45	2	0	2	0	0	1	1	22	0	0	29	0					57
8:00	0	0	0	1	0	1	0	41	0	1	40	0					84
8:15	1	0	1	1	1	2	0	54	0	0	63	0					123
8:30	0	0	1	1	0	1	1	54	1	1	47	2					109
8:45	2	0	0	1	1	1	0	38	0	0	51	1					95

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	2	0	0	1	0					3
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.38
Peak 15 X 4	0	0	0	0	0	0	0	5	0	0	4	0					8
Average Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
Survey Total	0	0	0	0	0	0	0	2	0	0	1	0					3
7:00	0	0	0	0	0	0	0	0	0	0	0	0					0
7:15	0	0	0	0	0	0	0	0	0	0	0	0					0
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	1	0					1
8:30	0	0	0	0	0	0	0	2	0	0	0	0					2
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0



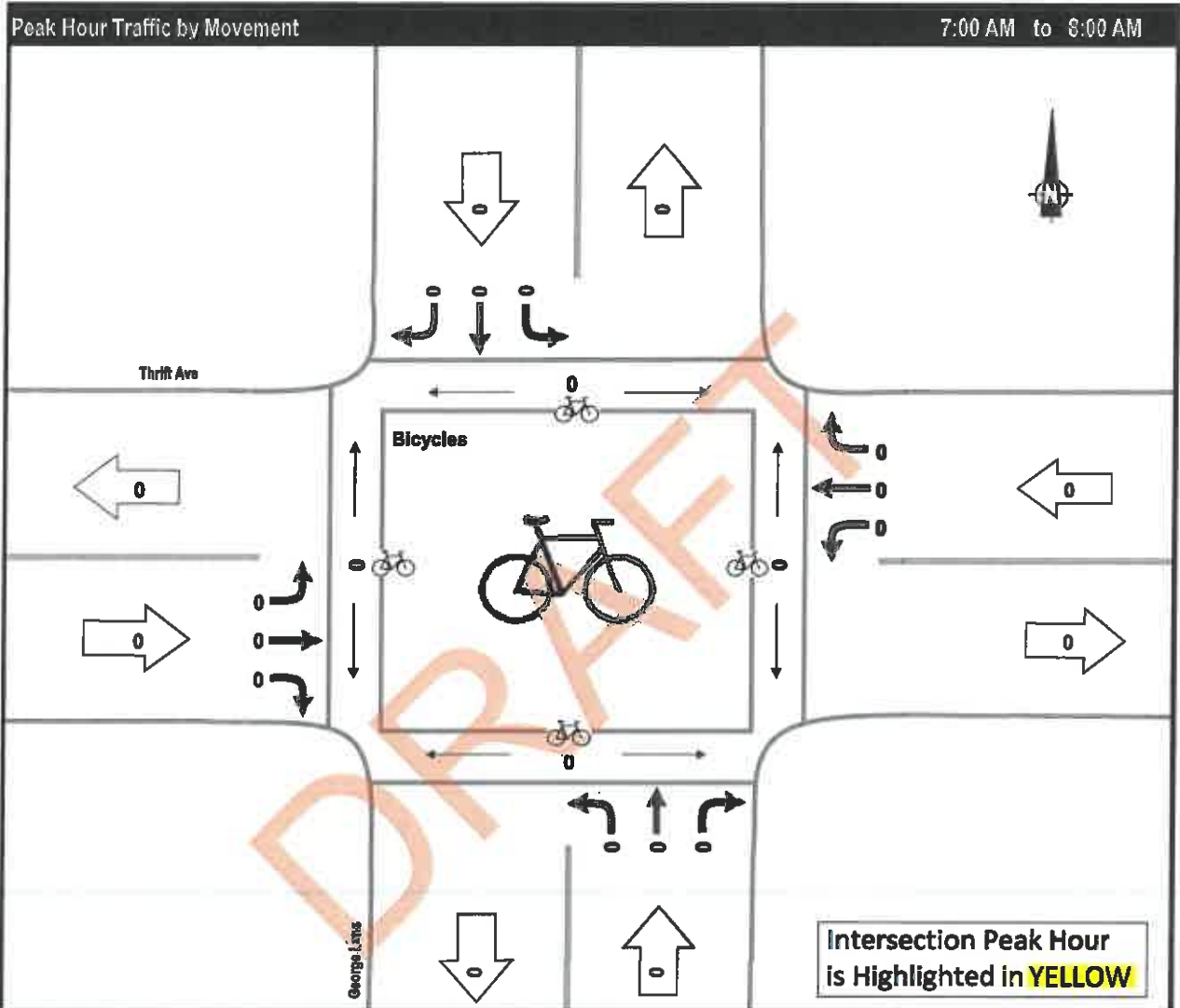
George Lane & Thrift Ave

Wednesday, April 03, 2019

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Morning Peak Period

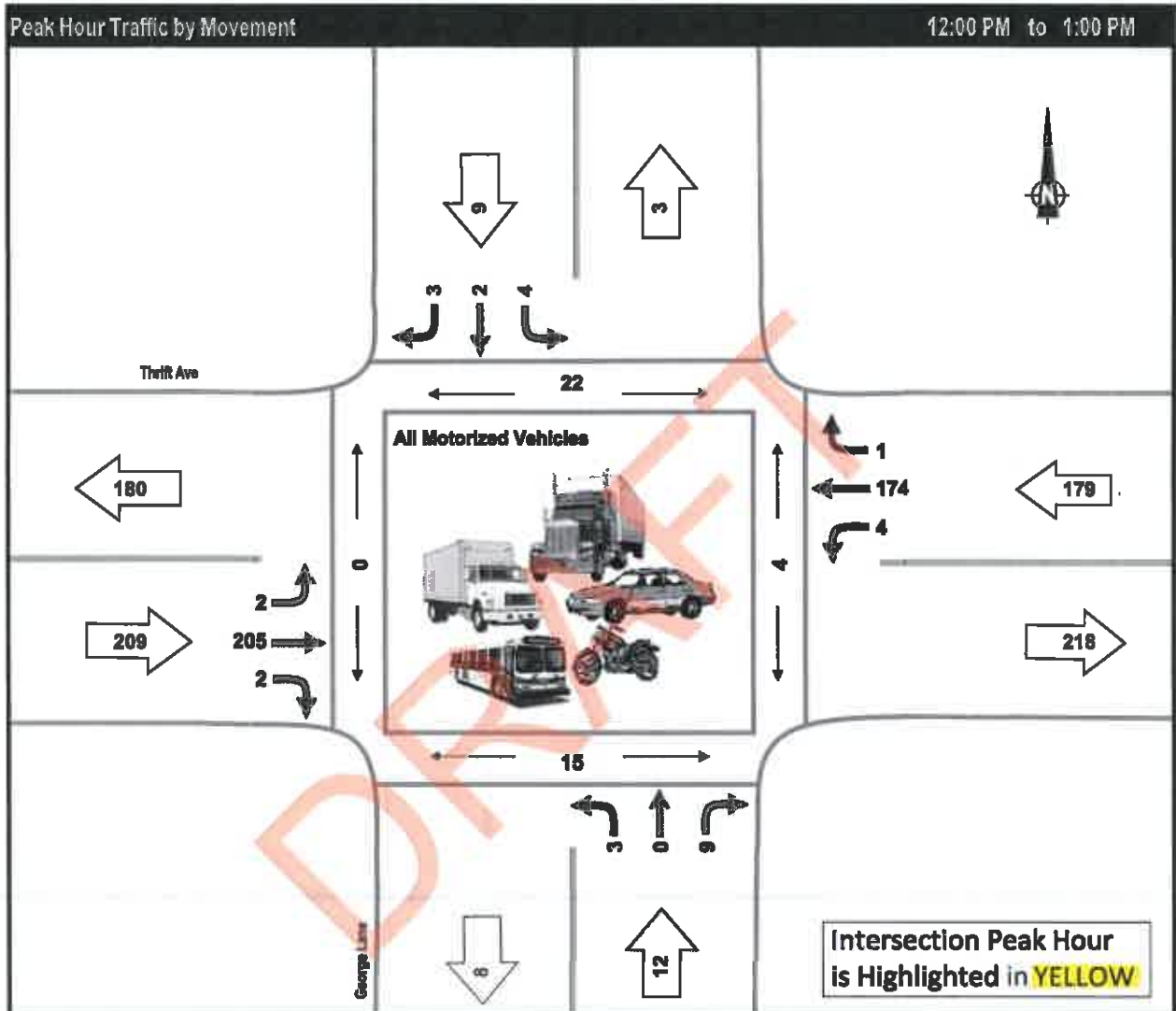
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7026: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

Midday Peak Period

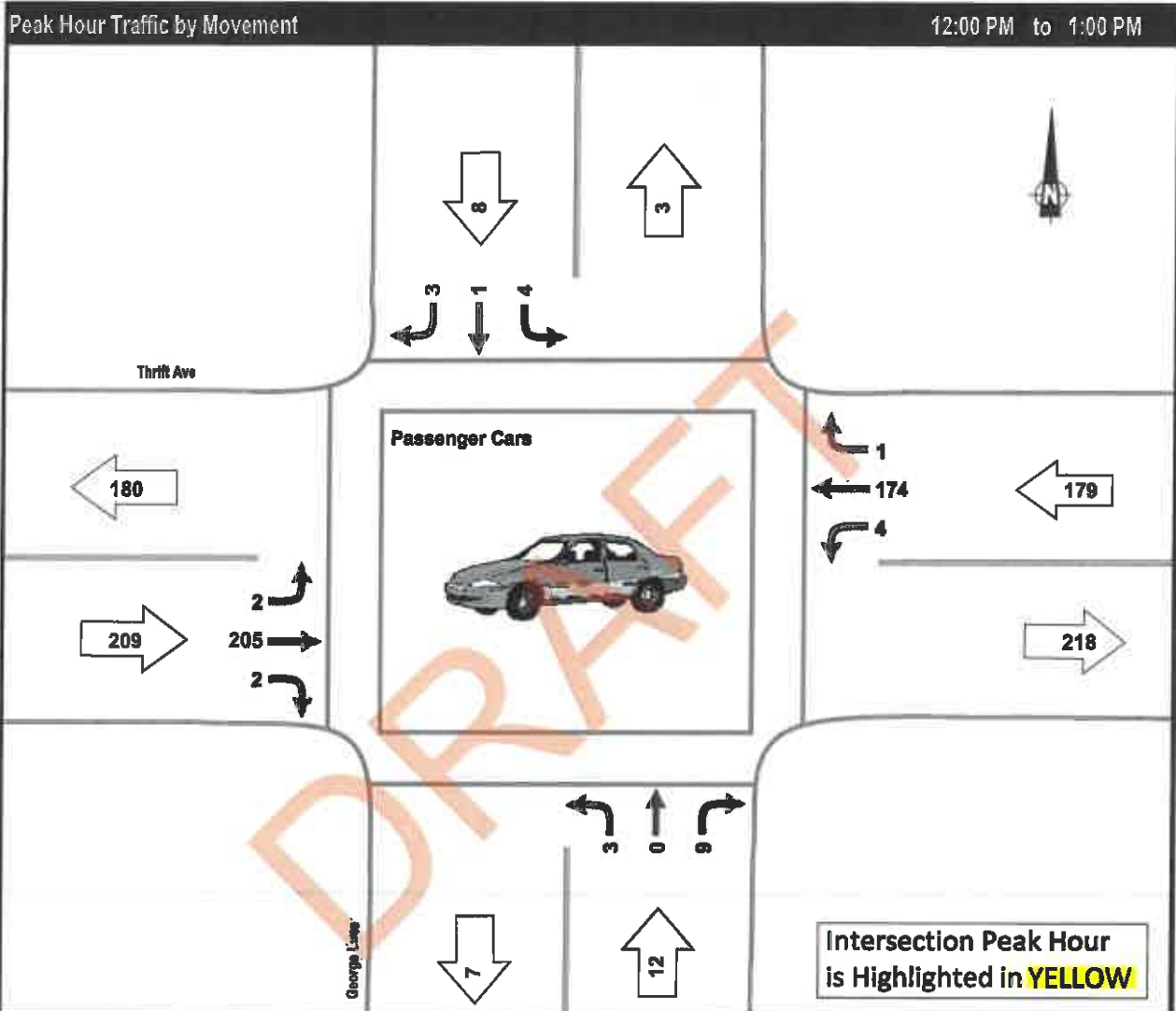


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	4	2	3	3	0	9	2	205	2	4	174	1	22	15	0	4	409
PH Factor	0.50	0.50	0.75	0.38	0.00	0.75	0.50	0.95	0.25	0.50	0.89	0.25	0.81	0.83	0.00	0.50	0.92
PHF	8	4	4	8	0	12	4	218	8	8	198	4	38	24	0	8	444
Average Hour	3	1	4	3	0	5	4	184	3	3	185	2	22	30	1	4	397
Survey Total	5	2	8	5	0	10	8	387	6	6	369	3	43	59	2	7	789
11:00	0	0	1	0	0	1	1	32	1	0	51	2	10	10	0	0	89
11:15	0	0	0	1	0	0	3	47	2	0	59	0	7	7	0	2	112
11:30	1	0	2	0	0	0	1	43	1	1	45	0	2	21	1	0	94
11:45	0	0	2	1	0	0	1	40	0	1	40	0	2	6	1	1	95
12:00	0	1	0	1	0	1	1	52	0	1	48	0	2	2	0	1	105
12:15	0	1	1	0	0	3	1	51	2	2	29	0	5	3	0	1	90
12:30	2	0	1	0	0	3	0	48	0	1	48	0	6	4	0	0	103
12:45	2	0	1	2	0	2	0	54	0	0	48	1	9	6	0	2	111



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

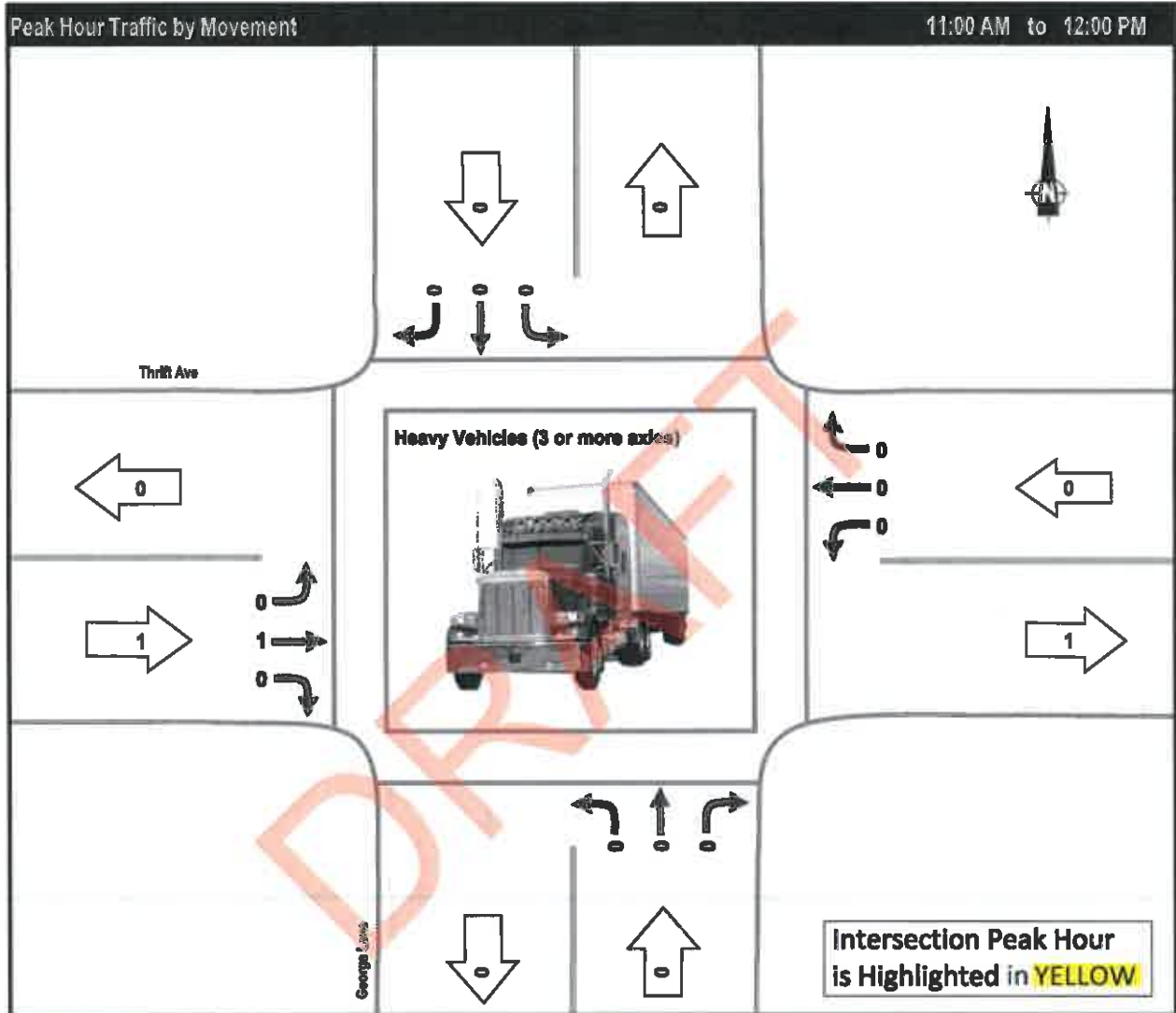
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	4	1	3	3	0	9	2	205	2	4	174	1					408
PHF	0.50	0.25	0.75	0.38	0.00	0.75	0.60	0.95	0.35	0.50	0.89	0.25					0.92
Peak 15 X 4	8	4	4	8	0	12	4	216	8	8	196	4					444
Average Hour	3	1	4	3	0	5	4	183	3	3	185	2					396
Survey Total	5	1	8	5	0	10	8	366	6	6	369	3					787
11:00	0	0	1	0	0	1	1	31	1	0	51	2					88
11:15	0	0	0	1	0	0	3	47	2	0	59	0					112
11:30	1	0	2	0	0	0	1	43	1	1	45	0					94
11:45	0	0	2	1	0	0	1	40	0	1	40	0					85
12:00	0	1	0	1	0	1	1	52	0	1	48	0					105
12:15	0	0	1	0	0	3	1	51	2	2	29	0					89
12:30	2	0	1	0	0	3	0	48	0	1	48	0					103
12:45	2	0	1	2	0	2	0	54	0	0	49	1					111

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Midday Peak Period



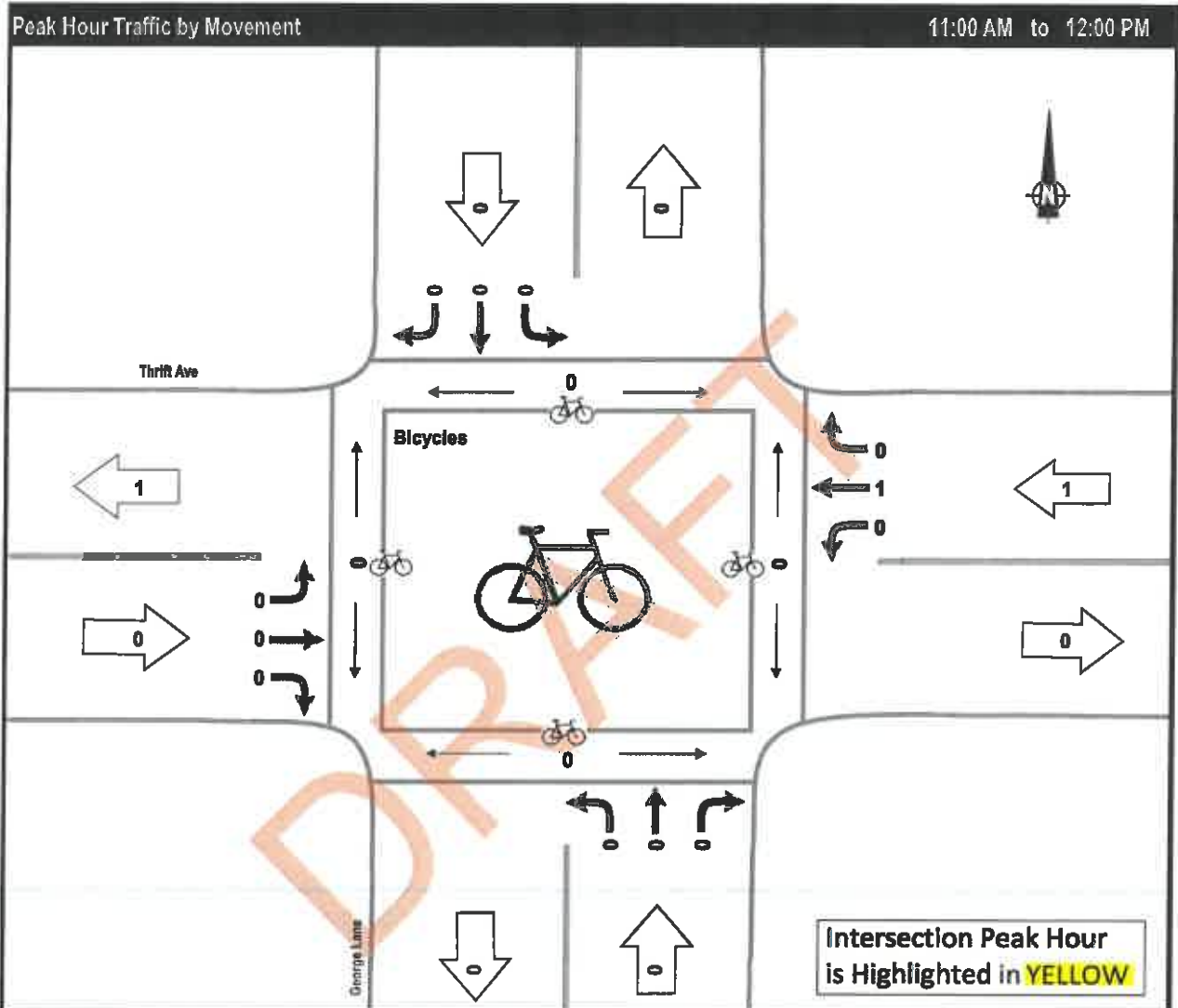
Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	0	0					1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00					0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	0	0					4
Average Hour	0	1	0	0	0	0	0	1	0	0	0	0					2
Survey Total	0	1	0	0	0	0	0	1	0	0	0	0					2
11:00	0	0	0	0	0	0	0	1	0	0	0	0					1
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0
11:30	0	0	0	0	0	0	0	0	0	0	0	0					0
11:45	0	0	0	0	0	0	0	0	0	0	0	0					0
12:00	0	0	0	0	0	0	0	0	0	0	0	0					0
12:15	0	1	0	0	0	0	0	0	0	0	0	0					1
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Midday Peak Period

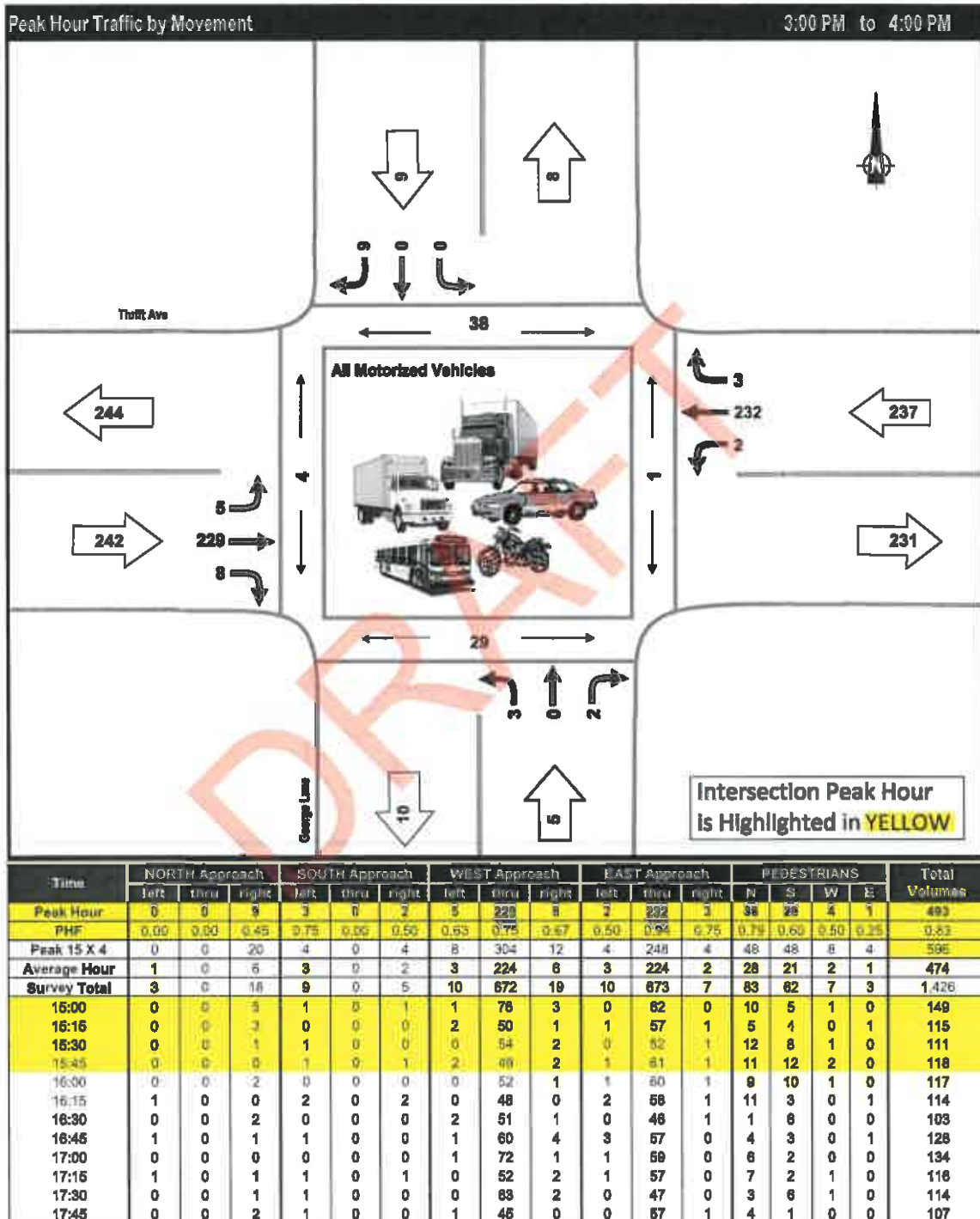
Note: Crosswalk bike volumes shown are cyclists who rode their bikes across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
Survey Total	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

Afternoon Peak Period

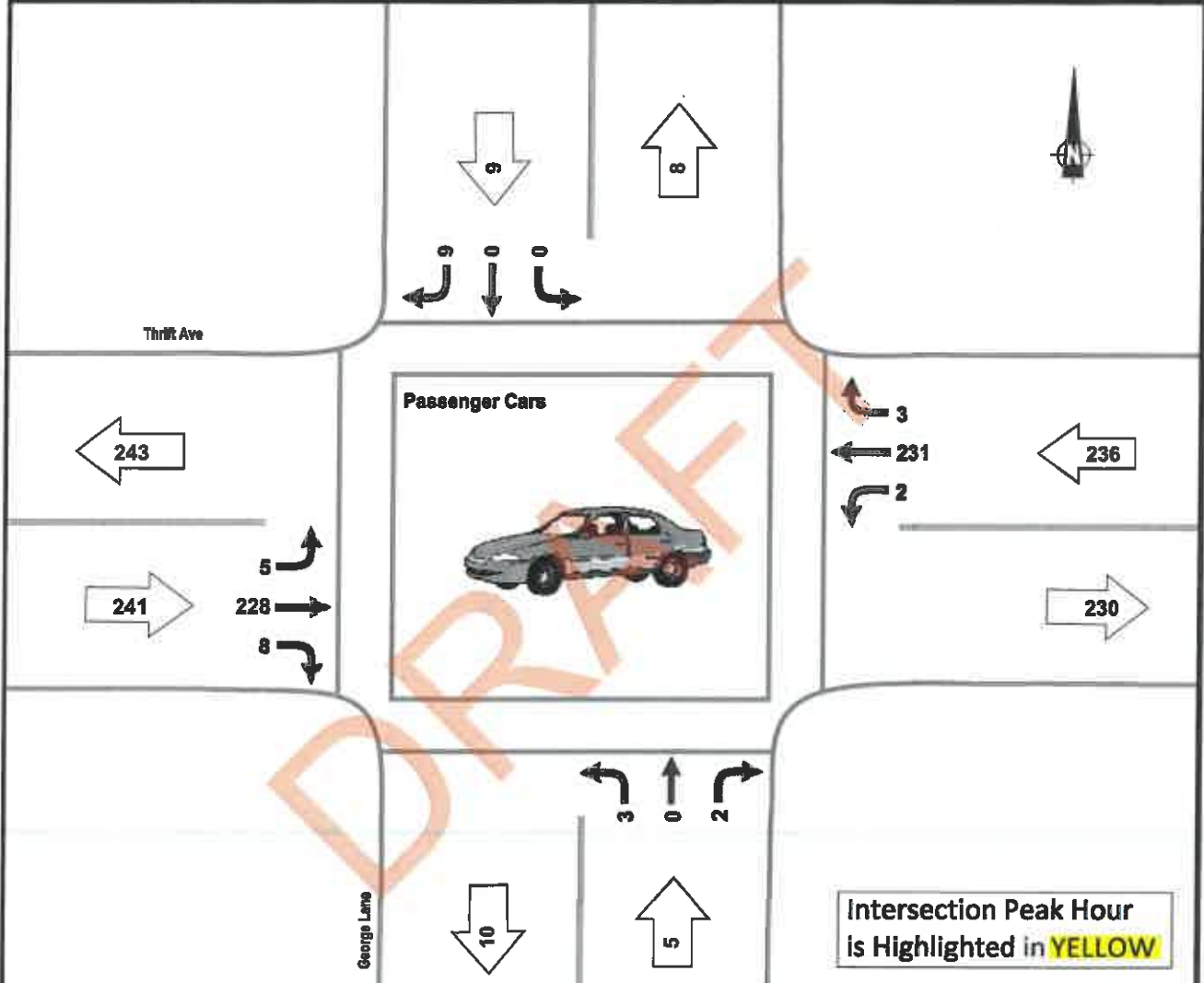


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period

Peak Hour Traffic by Movement

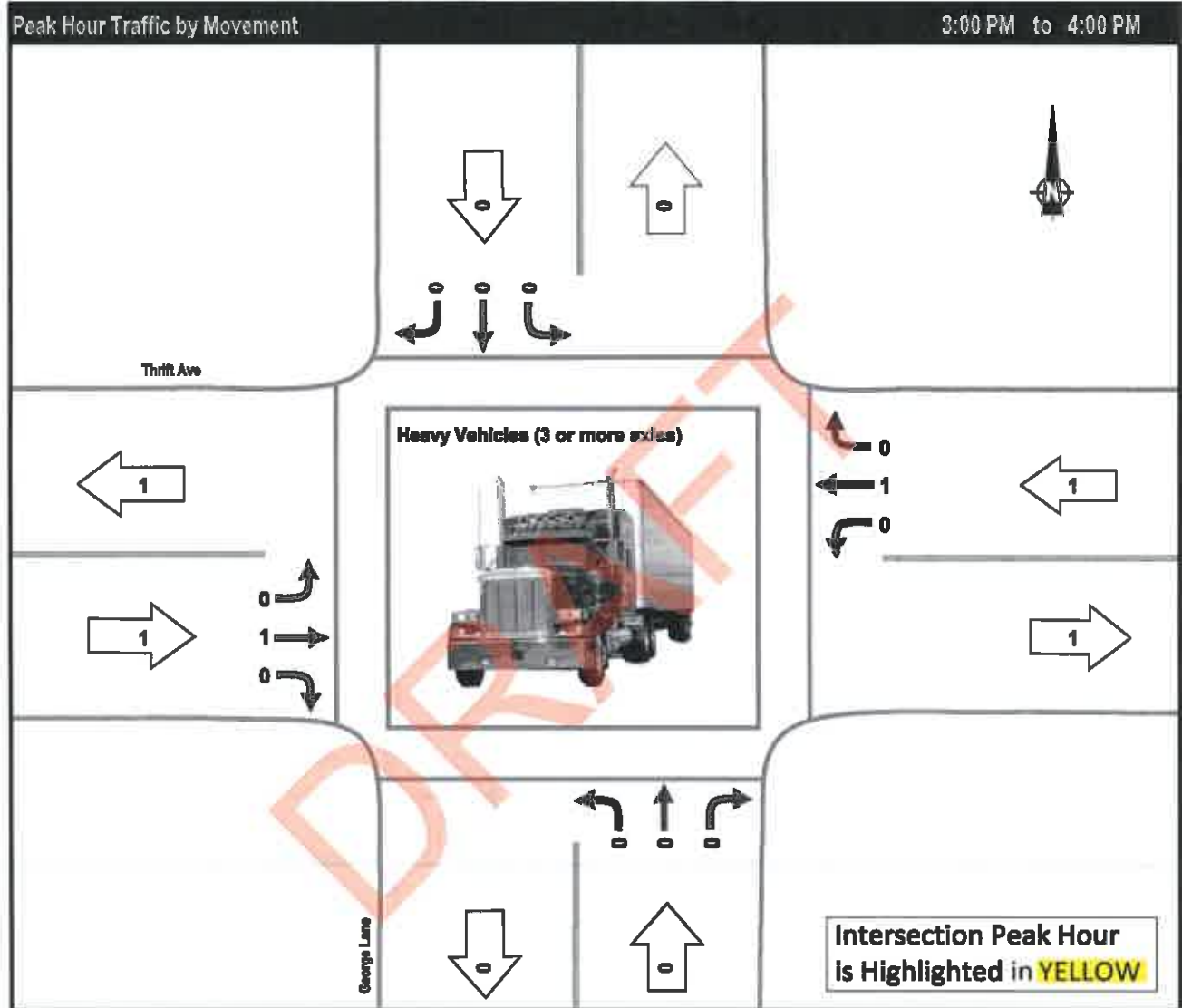
3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	9	3	0	2	5	228	8	2	231	3					491
PHF	0.00	0.00	0.45	0.75	0.00	0.50	0.63	0.76	0.67	0.50	0.95	0.75					0.84
Peak 15 X 4	0	0	20	4	0	4	8	300	12	4	244	4					588
Average Hour	1	0	6	3	0	2	3	224	6	3	224	2					474
Survey Total	3	0	18	9	0	5	10	671	19	10	672	7					1,424
15:00	0	0	5	1	0	1	1	75	3	0	61	0					147
15:15	0	0	3	0	0	0	2	50	1	1	57	1					116
15:30	0	0	1	1	0	0	0	54	2	0	52	1					111
15:45	0	0	0	1	0	1	2	49	2	1	61	1					118
16:00	0	0	2	0	0	0	0	52	1	1	60	1					117
16:15	1	0	0	2	0	2	0	48	0	2	58	1					114
16:30	0	0	2	0	0	0	2	51	1	0	46	1					103
16:45	1	0	1	1	0	0	1	60	4	3	57	0					128
17:00	0	0	0	0	0	0	1	72	1	1	59	0					134
17:15	1	0	1	1	0	1	0	52	2	1	57	0					116
17:30	0	0	1	1	0	0	0	63	2	0	47	0					114
17:45	0	0	2	1	0	0	1	45	0	0	57	1					107

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					8
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0					2
15:00	0	0	0	0	0	0	0	1	0	0	1	0					2
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	0	0					0
16:00	0	0	0	0	0	0	0	0	0	0	0	0					0
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

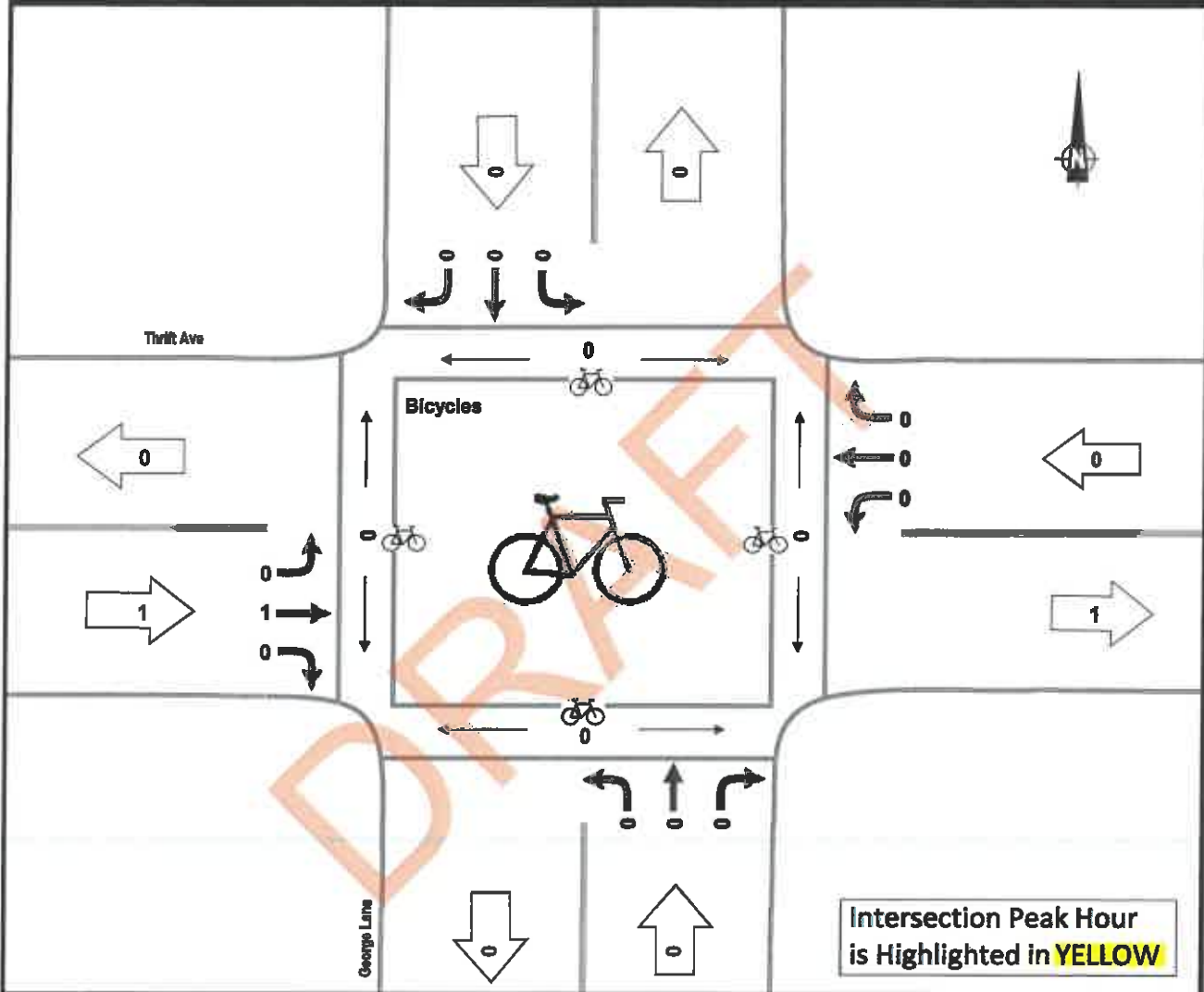
Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes	
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E		
Peak Hour	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Survey Total	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
15:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:15	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0



George St & Russell Ave

Wednesday, April 03, 2019

Vehicle Classification Summary

Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

Z1

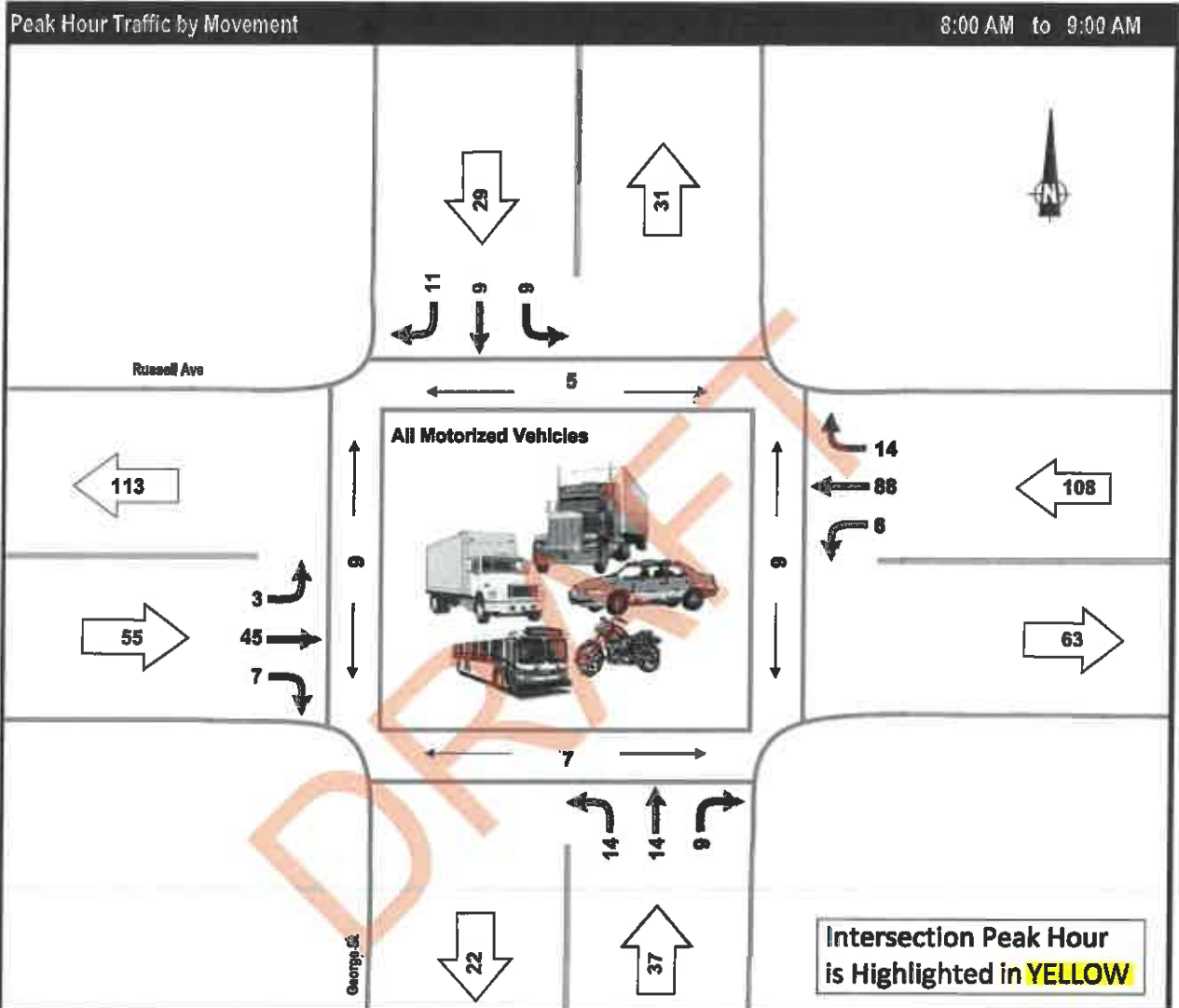
Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 08:00)	Volume	377	0			377
	%	100.0%	0.0%			100.0%
Midday (11:00 - 13:00)	Volume	981	1			982
	%	99.8%	0.1%			100.0%
Afternoon (15:00 - 18:00)	Volume	1 248	0			1 248
	%	100.0%	0.0%			100.0%
Total (7 Hours)	Volume	2 606	1			2 607
	%	100.0%	0.0%			100.0%

DRAFT



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

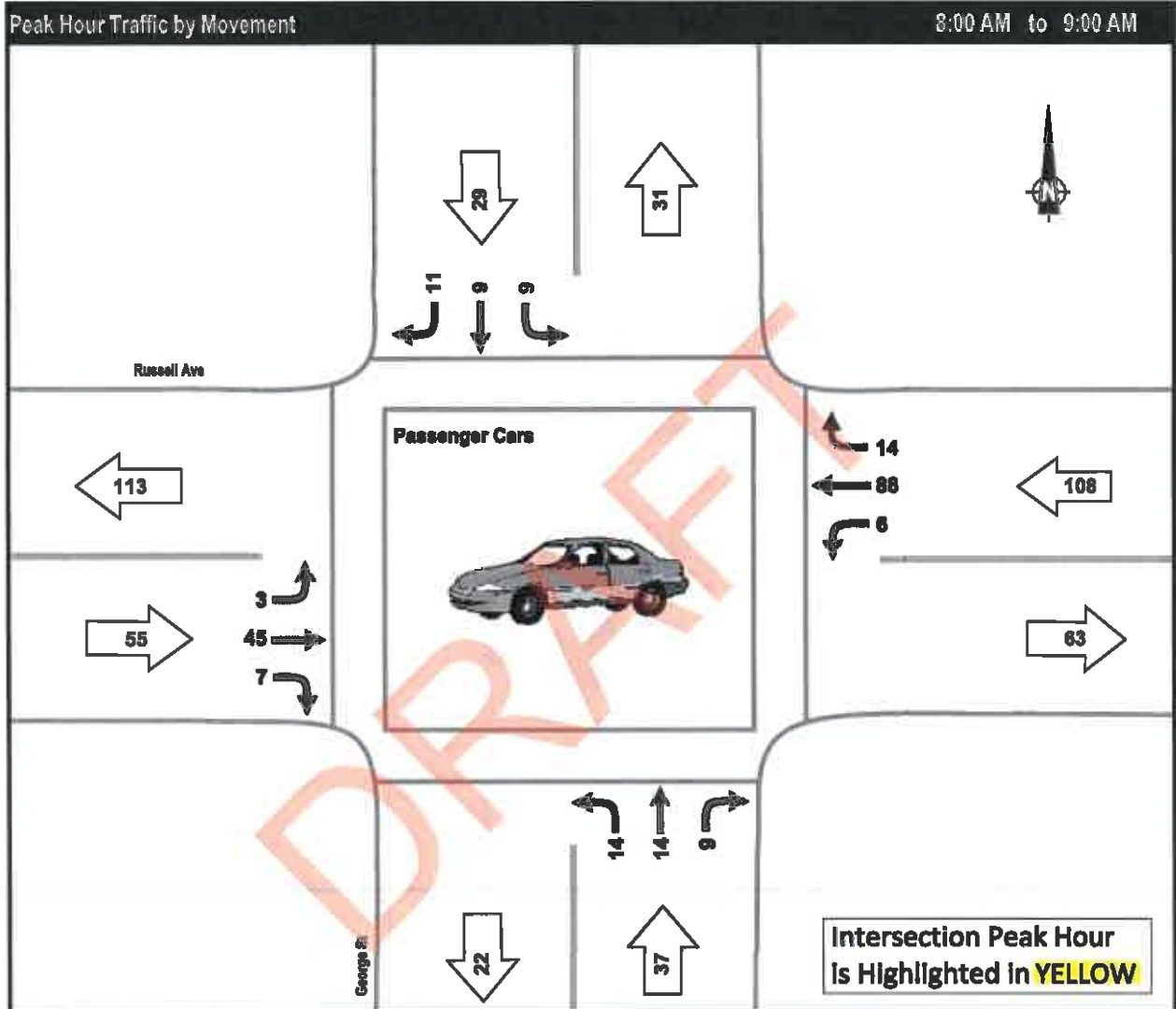
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	9	9	11	14	14	9	3	45	7	6	88	14	5	7	9	9	229
PHF	0.55	0.45	0.69	0.88	0.70	0.75	0.25	0.94	0.58	0.75	0.81	0.58	0.63	0.44	0.56	0.45	0.84
Peak 15 X 4	16	20	16	16	20	12	12	48	12	8	108	24	6	16	16	20	272
Average Hour	11	9	13	11	11	9	5	40	7	4	63	9	10	7	9	11	192
Survey Total	22	17	25	22	21	18	10	70	13	8	125	17	19	14	17	21	377
7:00	3	0	8	1	0	0	0	7	3	1	9	0	10	2	1	4	30
7:15	8	5	3	3	1	3	4	11	1	0	9	0	4	2	5	6	48
7:30	1	1	4	1	5	4	2	7	2	1	7	0	0	2	1	1	35
7:45	1	2	1	3	1	2	1	9	0	0	12	3	0	1	1	1	35
8:00	1	1	0	3	2	3	0	10	0	2	14	6	1	2	2	1	42
8:15	0	0	3	3	4	3	0	12	1	1	22	2	0	1	1	2	51
8:30	4	3	4	4	5	1	3	11	3	1	27	2	2	0	4	5	68
8:45	4	5	4	4	3	2	0	12	3	2	25	4	2	4	2	1	68

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

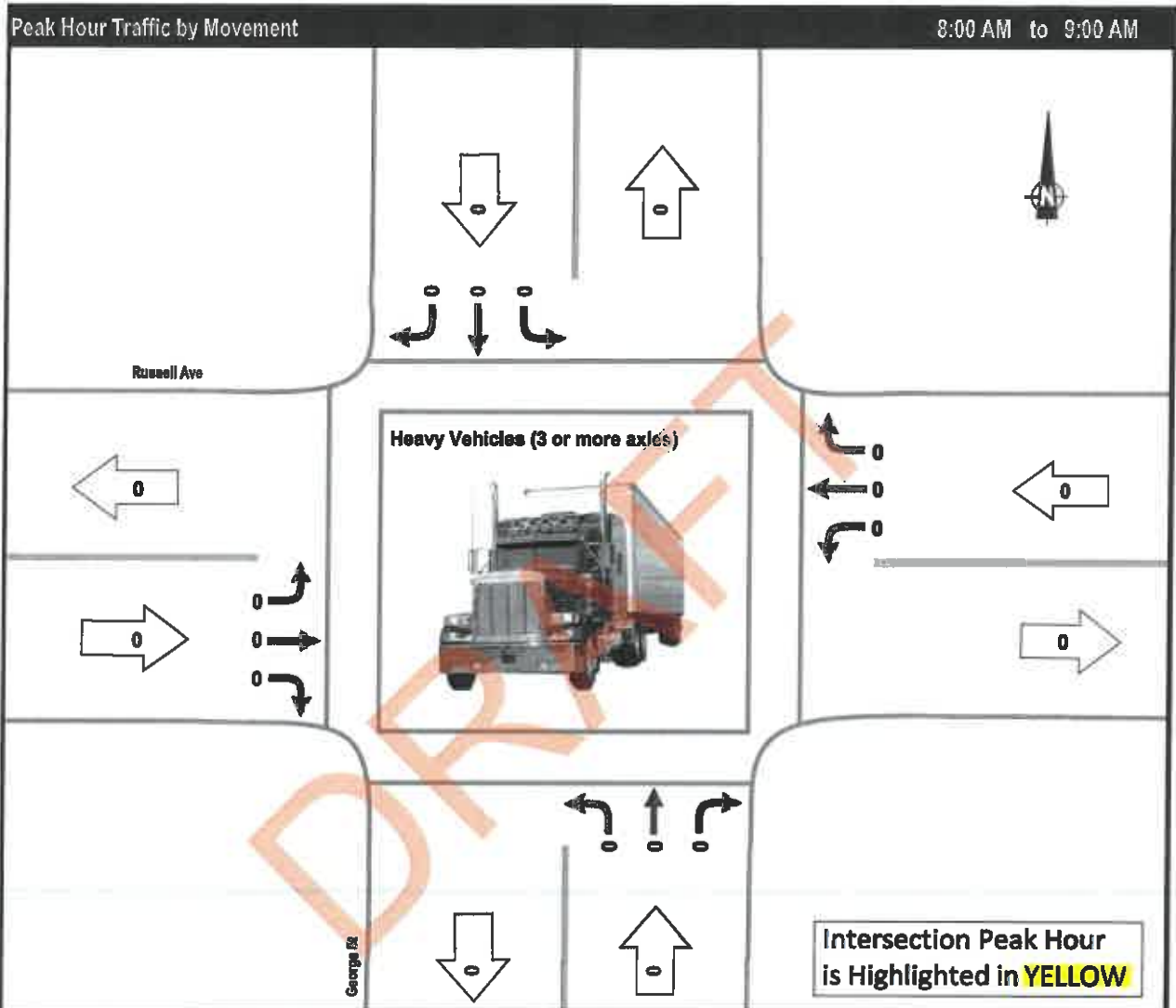
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	9	9	11	14	14	9	5	45	7	8	88	14					220
PHF	0.56	0.45	0.69	0.88	0.70	0.75	0.25	0.94	0.58	0.75	0.81	0.58					0.84
Peak 15 X 4	16	20	16	16	20	12	12	48	12	8	108	24					272
Average Hour	11	9	13	11	11	9	5	40	7	4	63	9					192
Survey Total	22	17	25	22	21	18	10	79	13	8	125	17					377
7:00	3	0	6	1	0	0	0	7	3	1	9	0					30
7:15	8	5	3	3	1	3	4	11	1	0	9	0					48
7:30	1	1	4	1	5	4	2	7	2	1	7	0					35
7:45	1	2	1	3	1	2	1	9	0	0	12	3					35
8:00	1	1	0	3	2	3	0	10	0	2	14	6					42
8:15	0	0	3	3	4	3	0	12	1	1	22	2					51
8:30	4	3	4	4	5	1	3	11	3	1	27	2					68
8:45	4	5	4	4	3	2	0	12	3	2	25	4					68

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

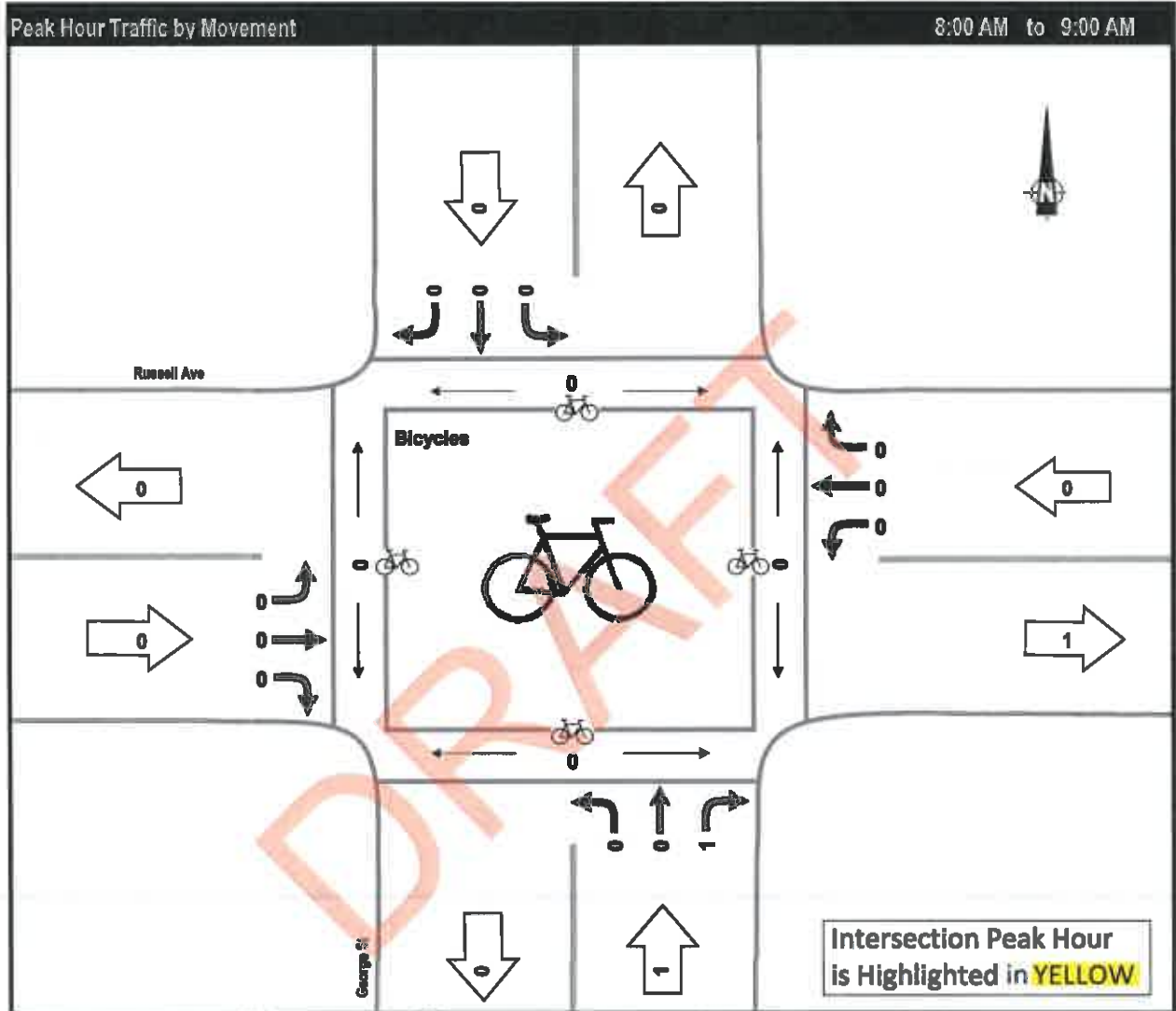
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0					0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0					0
7:00	0	0	0	0	0	0	0	0	0	0	0	0					0
7:15	0	0	0	0	0	0	0	0	0	0	0	0					0
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	0	0					0
8:30	0	0	0	0	0	0	0	0	0	0	0	0					0
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

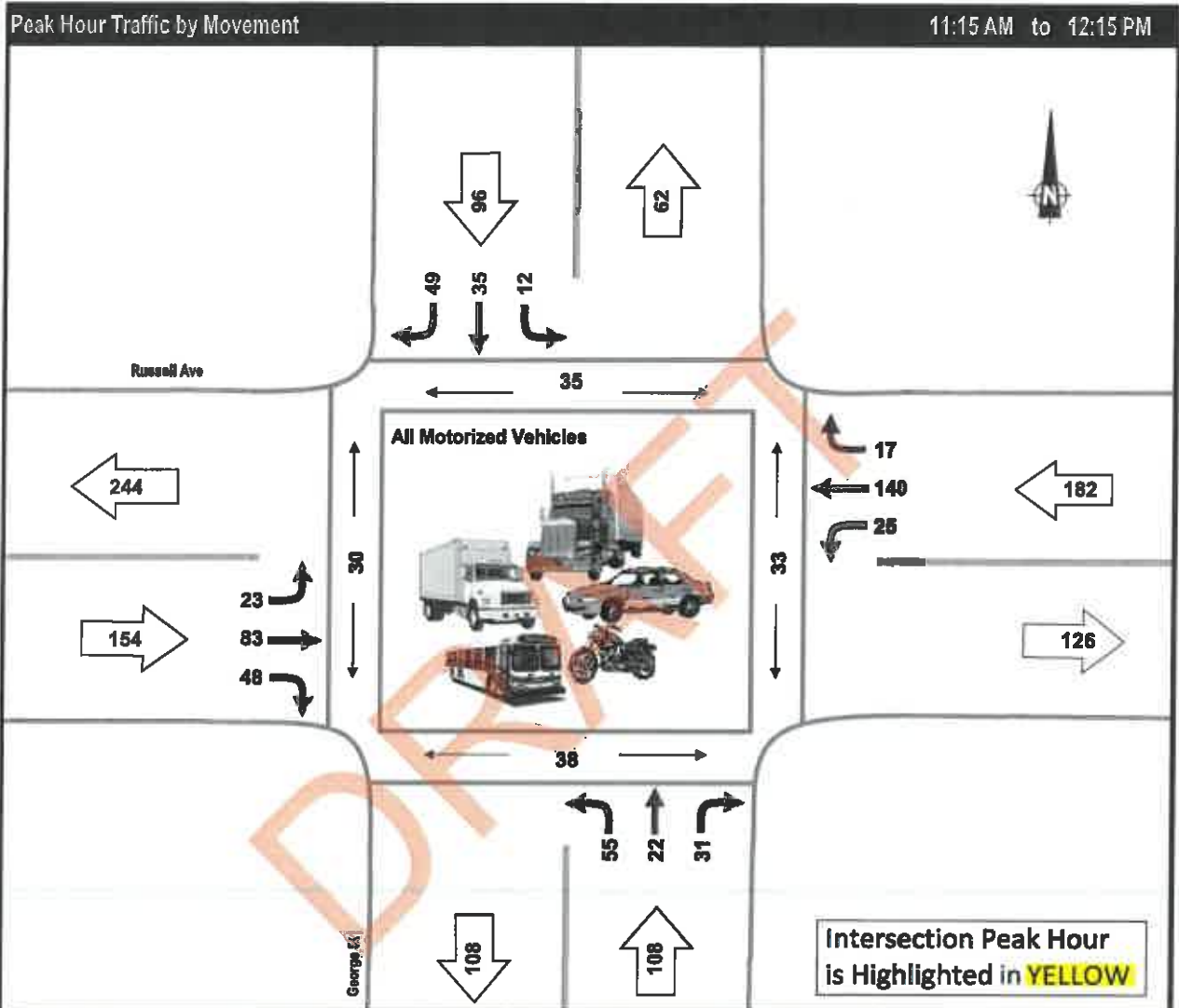
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	4
Average Hour	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2
Survey Total	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
8:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

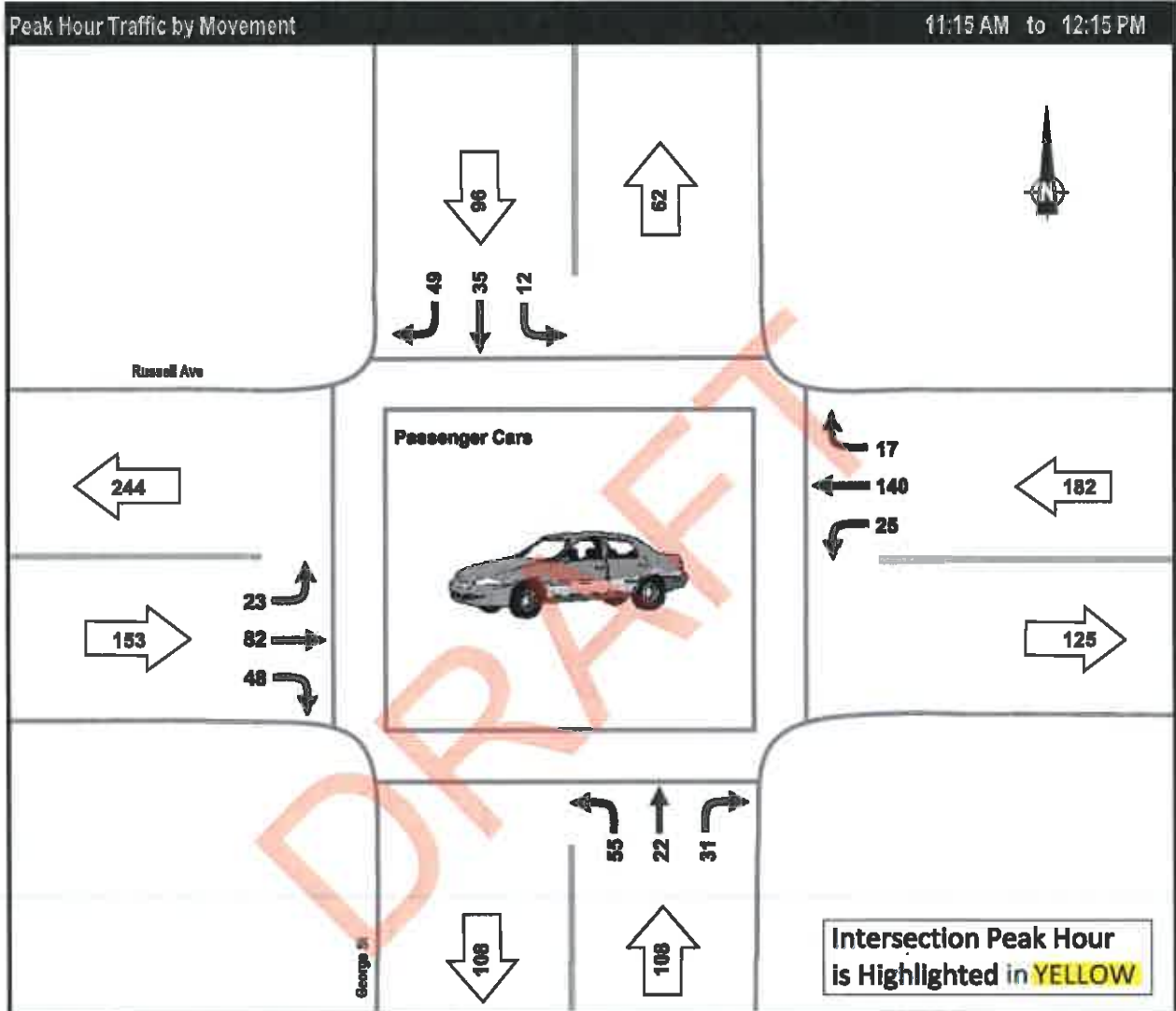
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	12	35	49	55	22	31	23	83	48	25	140	17	35	38	30	33	540
PH Factor	0.60	0.80	0.68	0.72	0.69	0.97	0.64	0.83	0.80	0.69	0.85	0.53	0.67	0.63	0.63	0.43	0.93
PHF	20	44	72	76	32	32	38	100	60	36	164	32	52	60	48	76	580
Average Hour	11	28	38	46	22	26	20	77	47	24	134	19	31	31	27	28	492
Survey Total	21	56	76	92	44	52	40	154	94	47	268	38	61	61	54	51	982
11:00	1	5	6	9	8	5	5	12	11	8	39	7	5	9	10	1	114
11:15	2	7	12	11	4	7	6	17	11	8	33	3	13	6	10	3	121
11:30	4	8	9	13	8	8	4	19	14	9	32	3	11	10	12	19	131
11:45	5	9	10	12	5	8	4	22	15	6	41	8	8	15	4	6	145
12:00	1	11	18	19	5	8	9	25	8	2	34	3	6	7	4	5	143
12:15	1	3	7	9	7	4	4	14	10	5	27	1	3	3	2	5	92
12:30	2	2	9	9	5	6	2	26	14	4	32	8	9	7	5	4	119
12:45	5	11	5	10	4	6	6	19	11	5	30	5	9	4	7	8	117

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

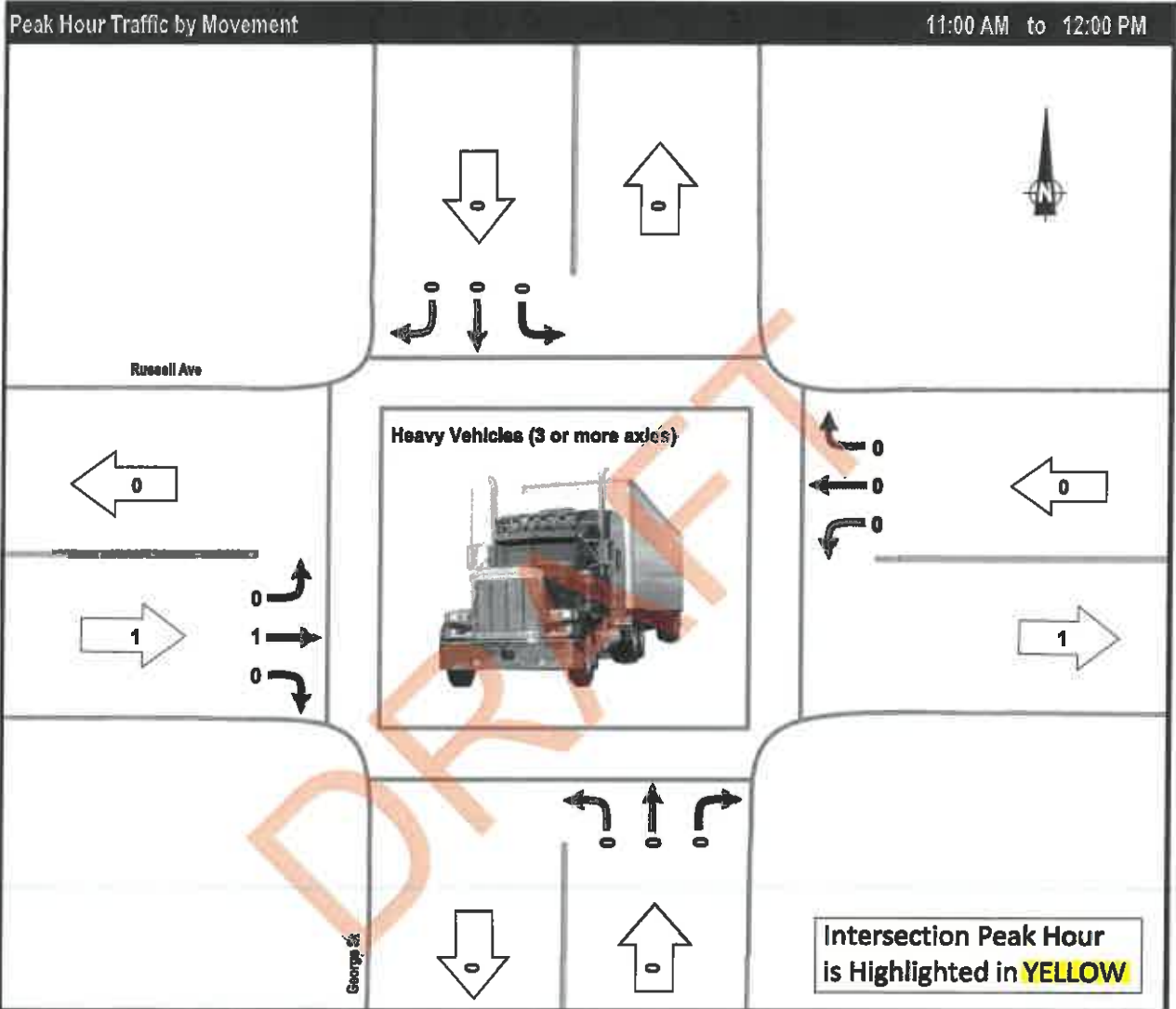
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	12	36	46	36	22	31	23	82	40	25	140	17					539
PHF	0.60	0.80	0.66	0.72	0.69	0.97	0.64	0.82	0.80	0.69	0.85	0.53					0.94
Peak 15 X 4	20	44	72	76	32	32	36	100	60	36	164	32					576
Average Hour	11	28	38	48	22	28	20	77	47	24	134	19					492
Survey Total	21	56	76	92	44	52	40	153	94	47	268	38					981
11:00	1	5	5	9	6	5	5	12	11	8	39	7					114
11:15	2	7	12	11	4	7	6	17	11	8	33	3					121
11:30	4	8	9	13	8	8	4	19	14	9	32	3					131
11:45	5	9	10	12	5	8	4	21	16	6	41	8					144
12:00	1	11	18	19	5	8	9	26	8	2	34	3					143
12:15	1	3	7	9	7	4	4	14	10	5	27	1					92
12:30	2	2	9	9	5	6	2	26	14	4	32	8					119
12:45	5	11	5	10	4	6	6	19	11	5	30	5					117

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Midday Peak Period

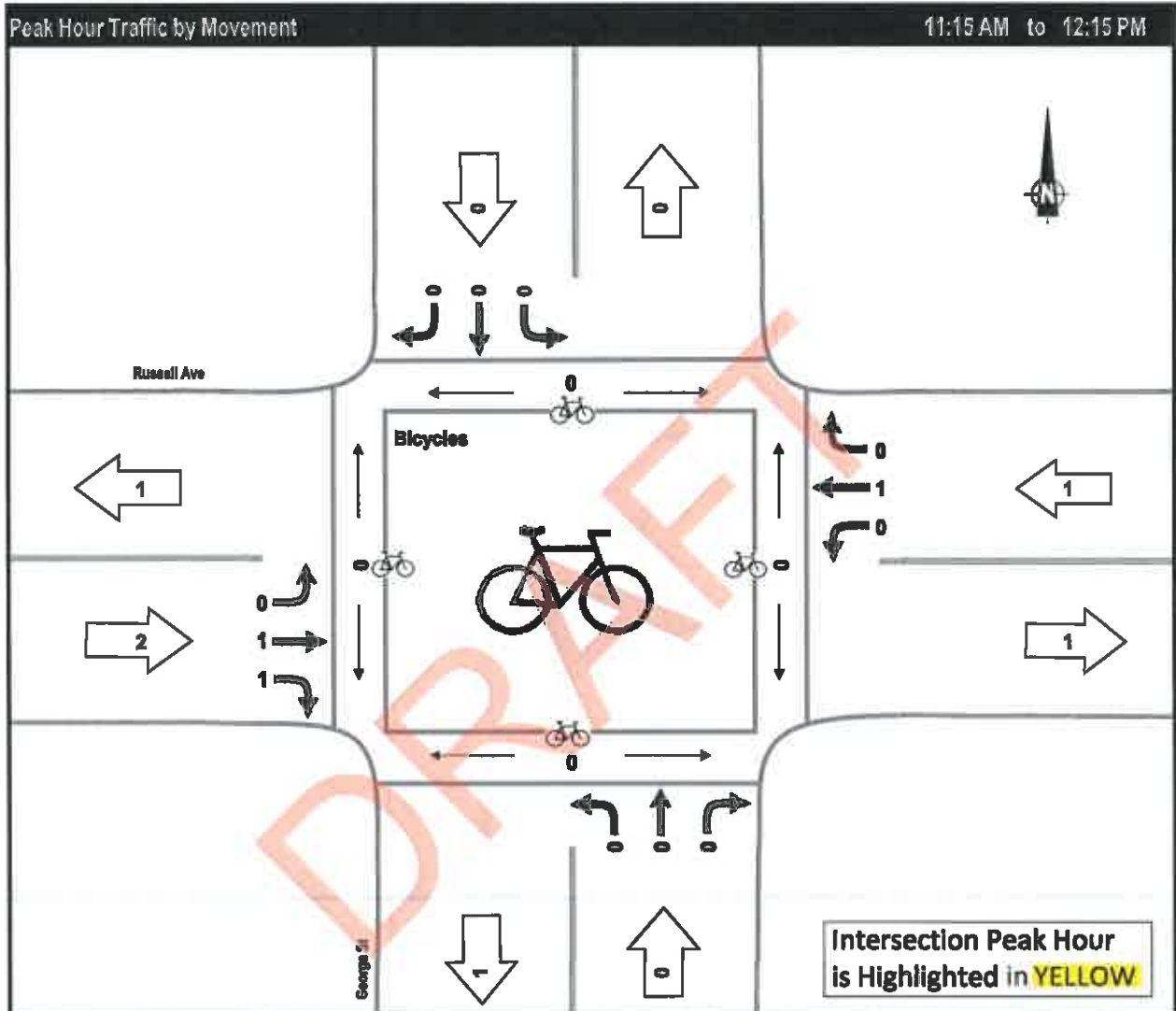


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	0	0					1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00					0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	0	0					4
Average Hour	0	0	0	0	0	0	0	1	0	0	0	0					1
Survey Total	0	0	0	0	0	0	0	1	0	0	0	0					1
11:00	0	0	0	0	0	0	0	0	0	0	0	0					0
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0
11:30	0	0	0	0	0	0	0	0	0	0	0	0					0
11:45	0	0	0	0	0	0	0	1	0	0	0	0					1
12:00	0	0	0	0	0	0	0	0	0	0	0	0					0
12:15	0	0	0	0	0	0	0	0	0	0	0	0					0
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Midday Peak Period

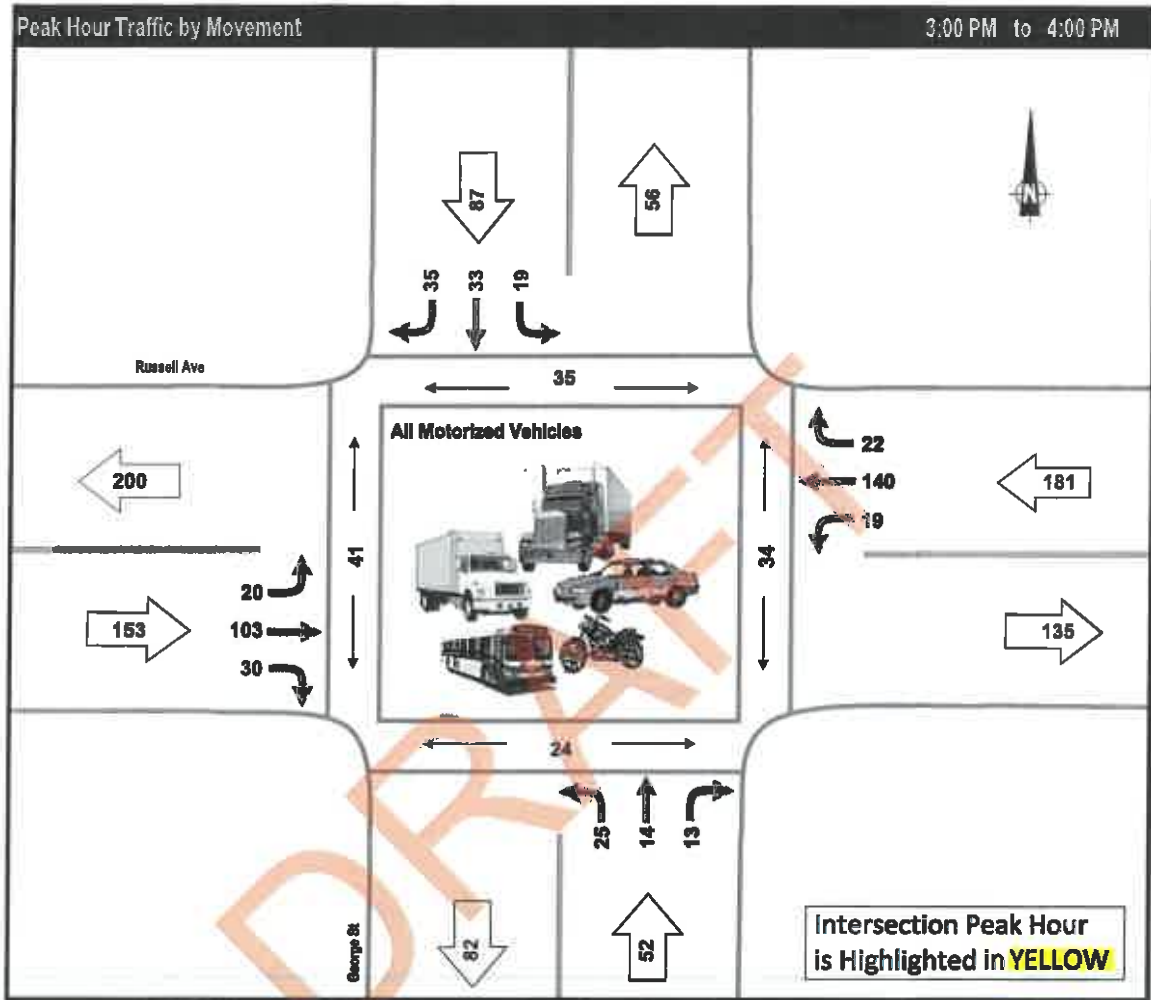
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes	
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E		
Peak Hour	0	0	0	0	0	0	0	1	1	0	1	0	0	0	0	0	0	3
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.25	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.75
Peak 15 X 4	0	0	0	0	0	0	0	4	4	0	4	0	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	1	1	0	1	0	0	0	0	0	0	3
Survey Total	0	0	0	0	0	0	0	1	1	0	1	0	0	0	0	0	0	3
11:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
12:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

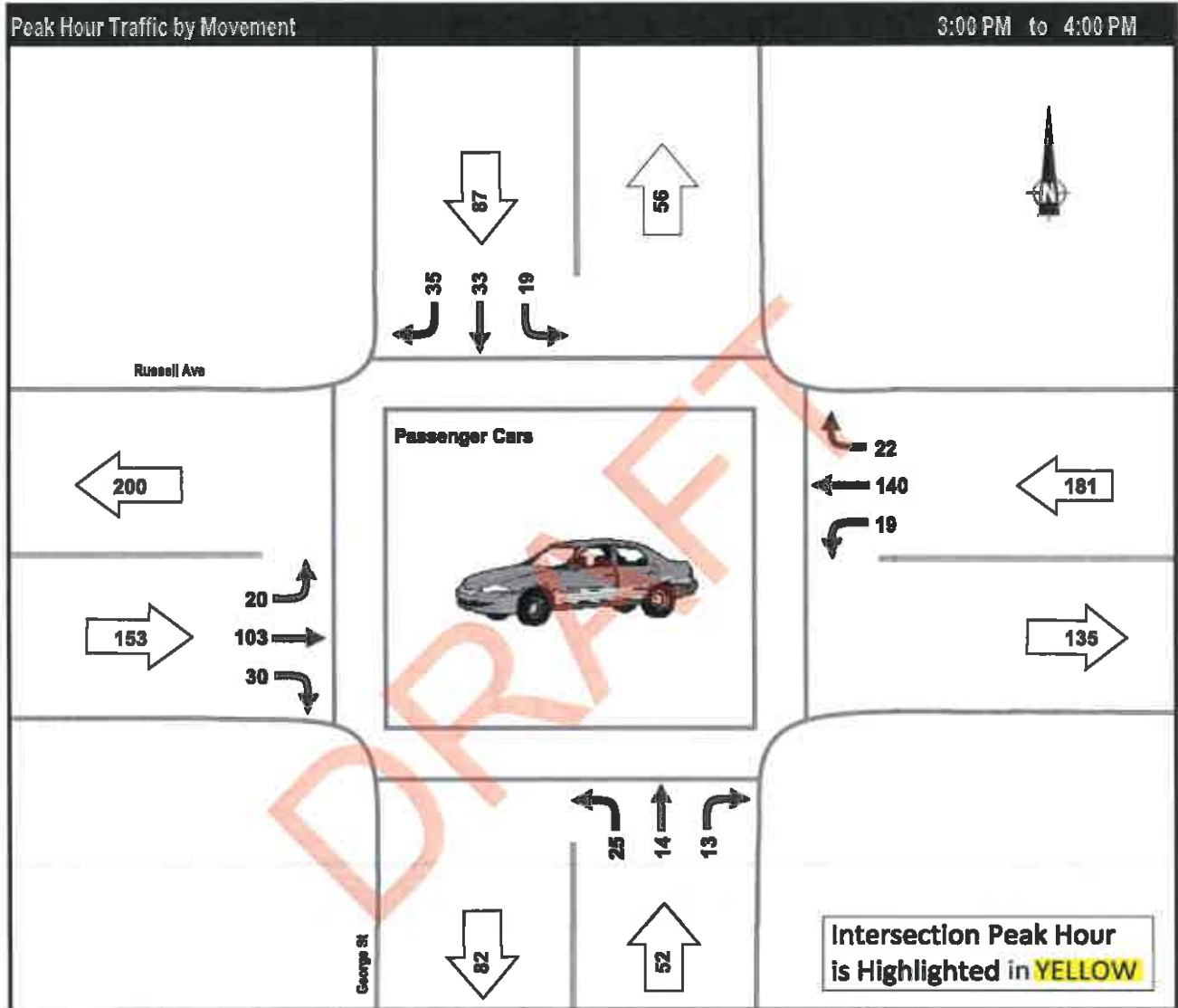
Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	19	33	35	25	14	13	20	103	30	19	140	22	36	24	41	34	473
PHF	0.79	0.82	0.73	0.69	0.70	0.46	0.71	0.83	0.68	0.60	0.60	0.60	0.67	0.75	0.85	0.85	0.94
Peak 15 X 4	24	36	48	36	20	28	28	124	44	32	176	32	52	32	48	40	504
Average Hour	16	23	34	22	15	13	19	92	30	16	119	18	29	21	27	25	417
Survey Total	48	70	101	65	46	38	58	275	89	49	356	53	86	62	80	75	1 248
15:00	3	8	5	8	4	7	6	23	11	8	40	3	8	8	11	9	128
15:15	4	9	7	3	3	0	7	21	5	8	44	7	8	3	9	10	118
15:30	6	7	11	6	6	3	5	28	6	2	21	4	6	6	8	7	103
15:45	6	9	12	8	2	3	3	31	8	1	35	8	13	8	12	8	126
16:00	3	6	7	6	3	3	6	24	11	4	36	7	8	1	4	11	118
16:15	4	3	7	3	1	0	4	22	5	3	23	3	11	4	8	7	78
16:30	3	4	9	2	3	1	4	27	11	6	28	3	12	2	6	5	101
16:45	1	7	7	7	2	5	4	21	7	6	35	5	7	6	6	4	107
17:00	2	5	7	2	1	1	5	23	2	6	21	4	1	3	7	1	79
17:15	6	3	7	6	10	2	2	18	13	3	22	1	8	11	6	3	93
17:30	7	4	10	6	7	8	7	17	4	1	23	5	3	9	2	7	99
17:45	3	5	12	6	5	6	6	20	6	1	28	3	1	2	1	3	100

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	18	33	95	25	14	19	20	103	30	19	140	22					473
PHF	0.79	0.92	0.73	0.58	0.70	0.46	0.71	0.83	0.68	0.59	0.80	0.89					0.94
Peak 15 X 4	24	36	48	36	20	28	28	124	44	32	176	32					504
Average Hour	16	23	34	22	15	13	19	92	30	16	119	18					417
Survey Total	48	70	101	65	46	38	58	275	89	49	356	53					1,248
15:00	3	8	5	9	4	7	5	23	11	8	40	3					126
15:15	4	9	7	3	3	0	7	21	5	8	44	7					118
15:30	6	7	11	5	5	3	5	28	6	2	21	4					103
15:45	6	9	12	8	2	3	3	31	8	1	35	8					126
16:00	3	6	7	8	3	3	8	24	11	4	36	7					118
16:15	4	3	7	3	1	0	4	22	5	3	23	3					78
16:30	3	4	9	2	3	1	4	27	11	6	28	3					101
16:45	1	7	7	7	2	5	4	21	7	6	35	5					107
17:00	2	5	7	2	1	1	5	23	2	6	21	4					79
17:15	6	3	7	6	10	2	2	18	13	3	22	1					93
17:30	7	4	10	6	7	8	7	17	4	1	23	5					89
17:45	3	5	12	6	5	5	6	20	6	1	28	3					100

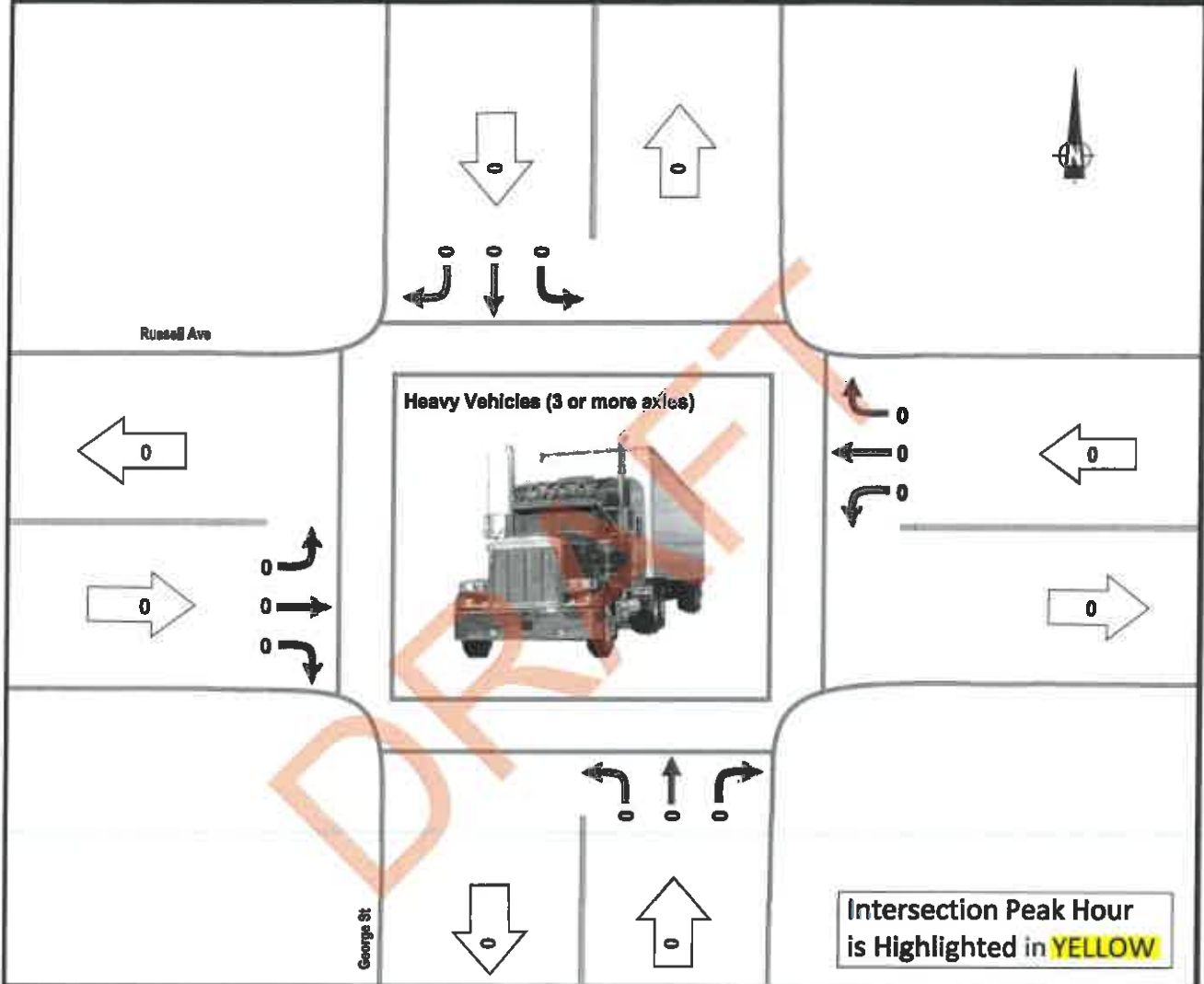


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Afternoon Peak Period

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM

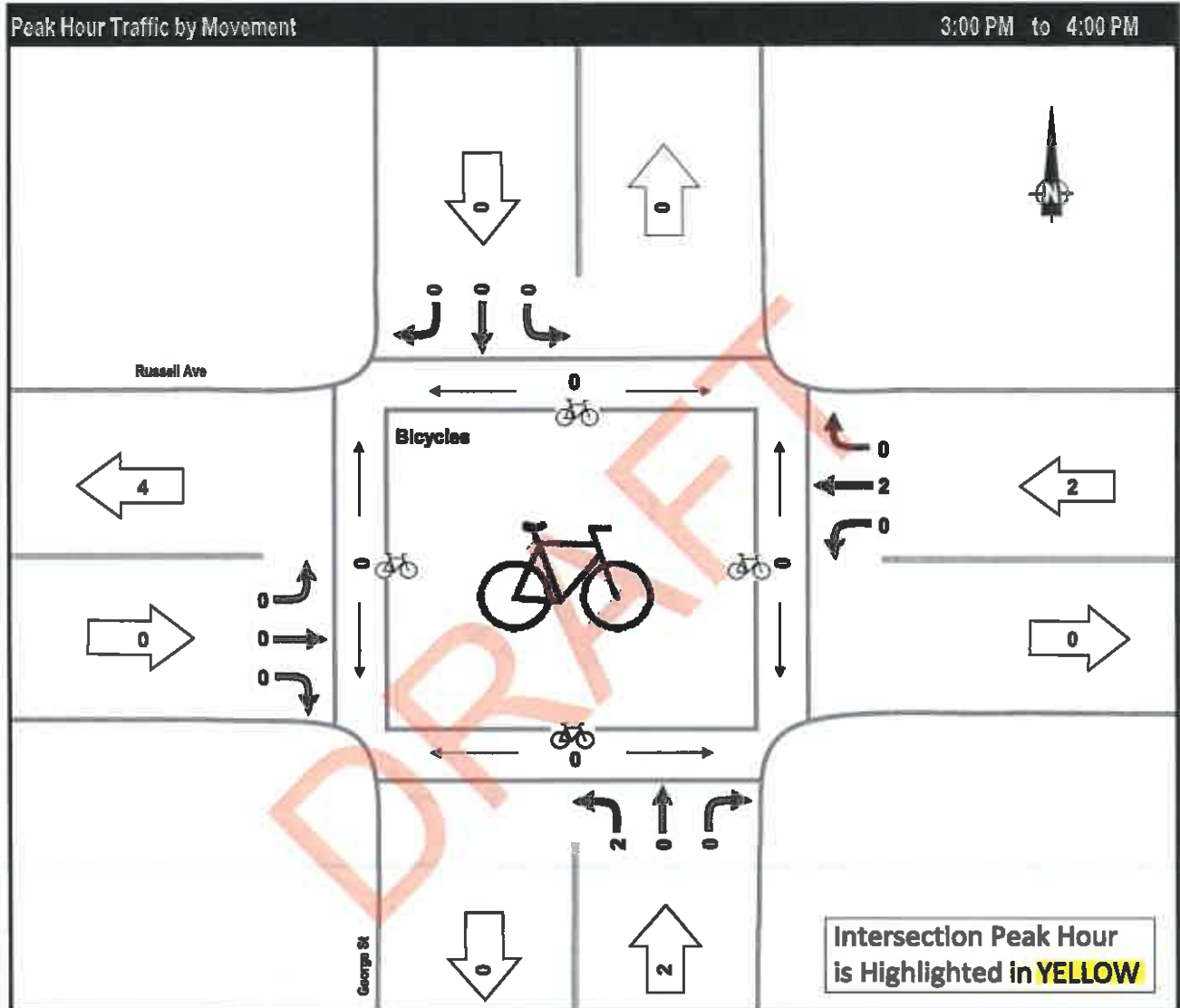


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0					0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0					0
15:00	0	0	0	0	0	0	0	0	0	0	0	0					0
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	0	0					0
16:00	0	0	0	0	0	0	0	0	0	0	0	0					0
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	2	0	0	0	0	0	0	2	0	0	0	0	0	4
PHF	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.50
Peak 15 X 4	0	0	0	5	0	0	0	0	0	0	4	0	0	0	0	0	8
Average Hour	1	0	0	1	0	0	0	0	0	0	1	0	0	0	0	0	3
Survey Total	2	0	1	2	0	0	0	1	0	0	3	1	0	0	0	0	10
15:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
15:15	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
16:00	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	1	0	0	0	0	0	0	0	1	1	0	0	0	0	3
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0



Vehicle Classification Summary

Project: #7025: 1485 Fir Street Traffic Impact Study
Municipality: White Rock
Weather: Rain

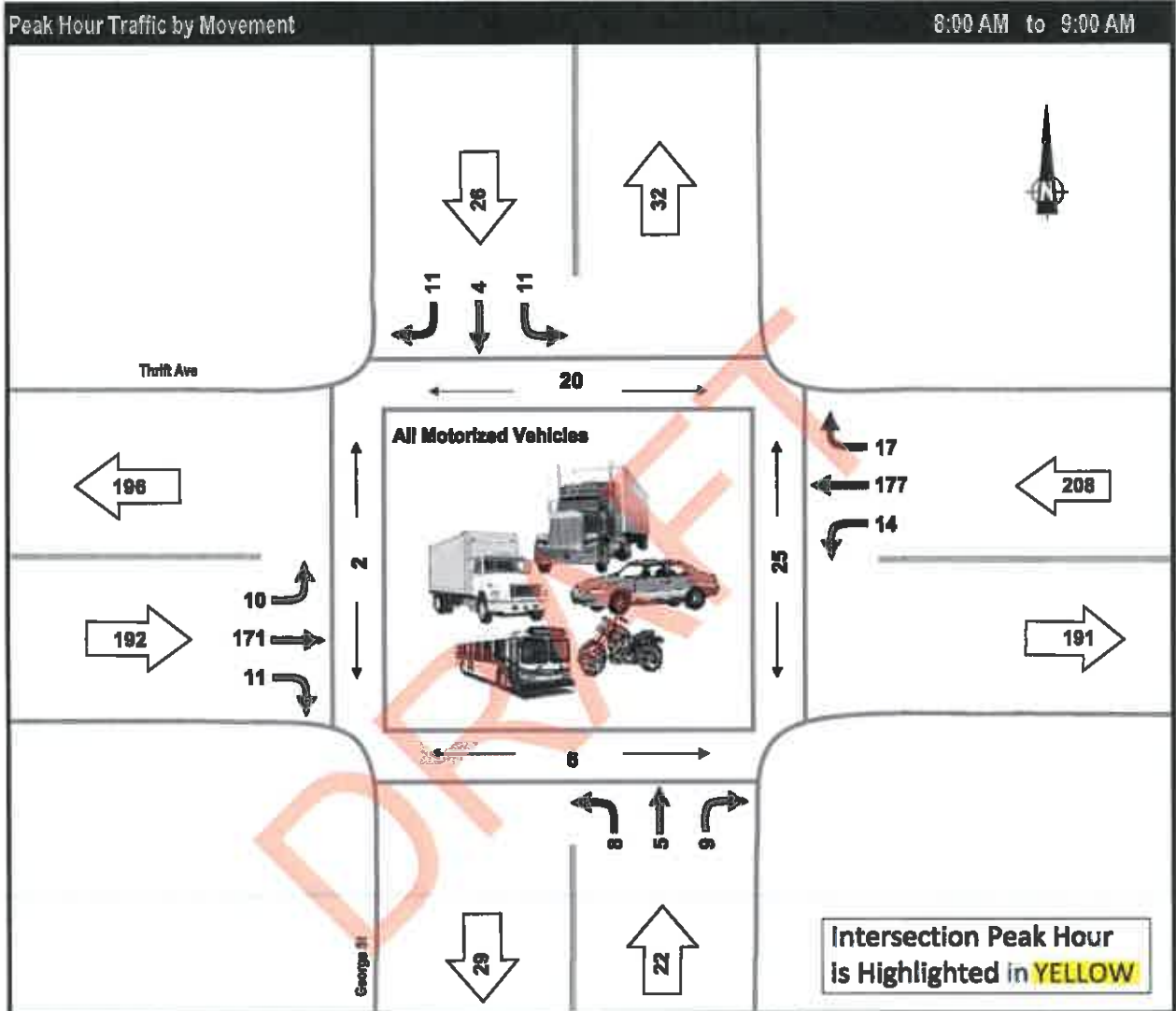
Z

Time Period	Entering Intersection	Vehicle Classification				Total
		Passenger Cars	Heavy Vehicles (3 or more axles)			
Morning (07:00 - 09:00)	Volume	635	2			637
	%	99.7%	0.3%			100.0%
Midday (11:00 - 13:00)	Volume	963	4			967
	%	99.6%	0.4%			100.0%
Afternoon (15:00 - 18:00)	Volume	1,629	2			1,631
	%	99.9%	0.1%			100.0%
Total (7 Hours)	Volume	3,227	8			3,235
	%	99.8%	0.2%			100.0%

DRAFT

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

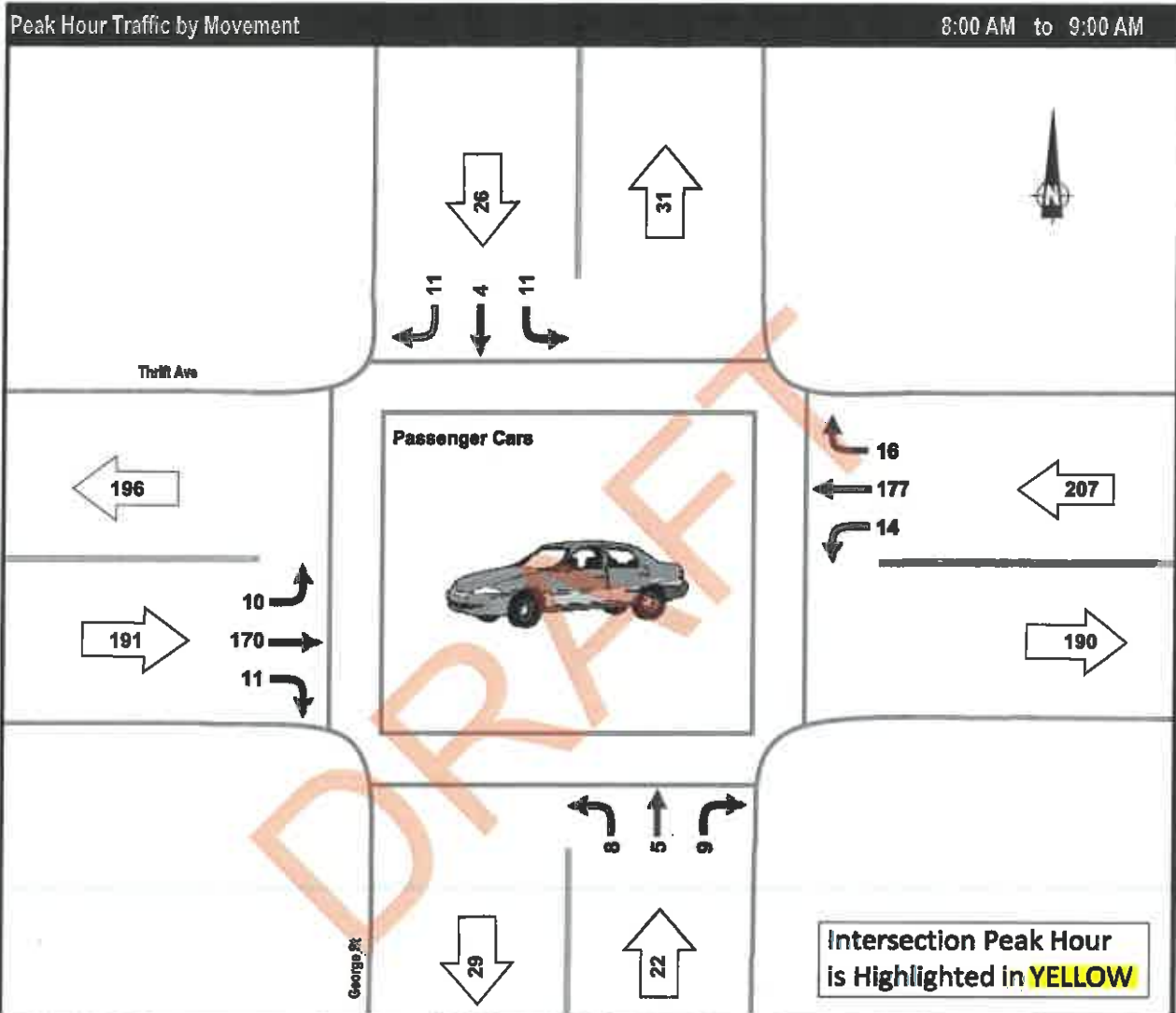
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	11	4	11	8	5	9	16	171	11	14	177	17	26	6	2	25	448
PHF	0.46	0.33	0.46	0.50	0.63	0.56	0.63	0.79	0.69	0.50	0.79	0.71	0.71	0.50	0.50	0.48	0.81
Peak 15 X 4	24	12	24	16	8	16	16	216	16	28	224	24	28	12	4	62	552
Average Hour	7	4	8	4	4	8	7	122	8	13	124	13	22	8	8	20	322
Survey Total	14	7	15	8	8	15	13	244	15	25	248	25	43	15	15	38	637
7:00	0	0	3	0	0	3	1	14	1	6	11	1	6	0	2	4	40
7:15	2	1	0	0	0	2	1	16	1	3	16	2	4	1	6	5	44
7:30	1	1	0	0	2	1	1	20	1	0	16	1	12	0	1	2	44
7:45	0	1	1	0	1	0	0	23	1	2	28	4	1	8	4	3	61
8:00	6	3	2	1	1	2	1	31	1	2	35	3	4	0	0	1	89
8:15	0	1	3	2	1	4	2	54	3	7	58	5	7	3	0	6	138
8:30	3	0	0	4	2	3	4	50	4	4	43	3	4	1	1	13	120
8:45	2	0	6	1	1	0	3	36	3	1	42	6	5	2	1	5	101

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

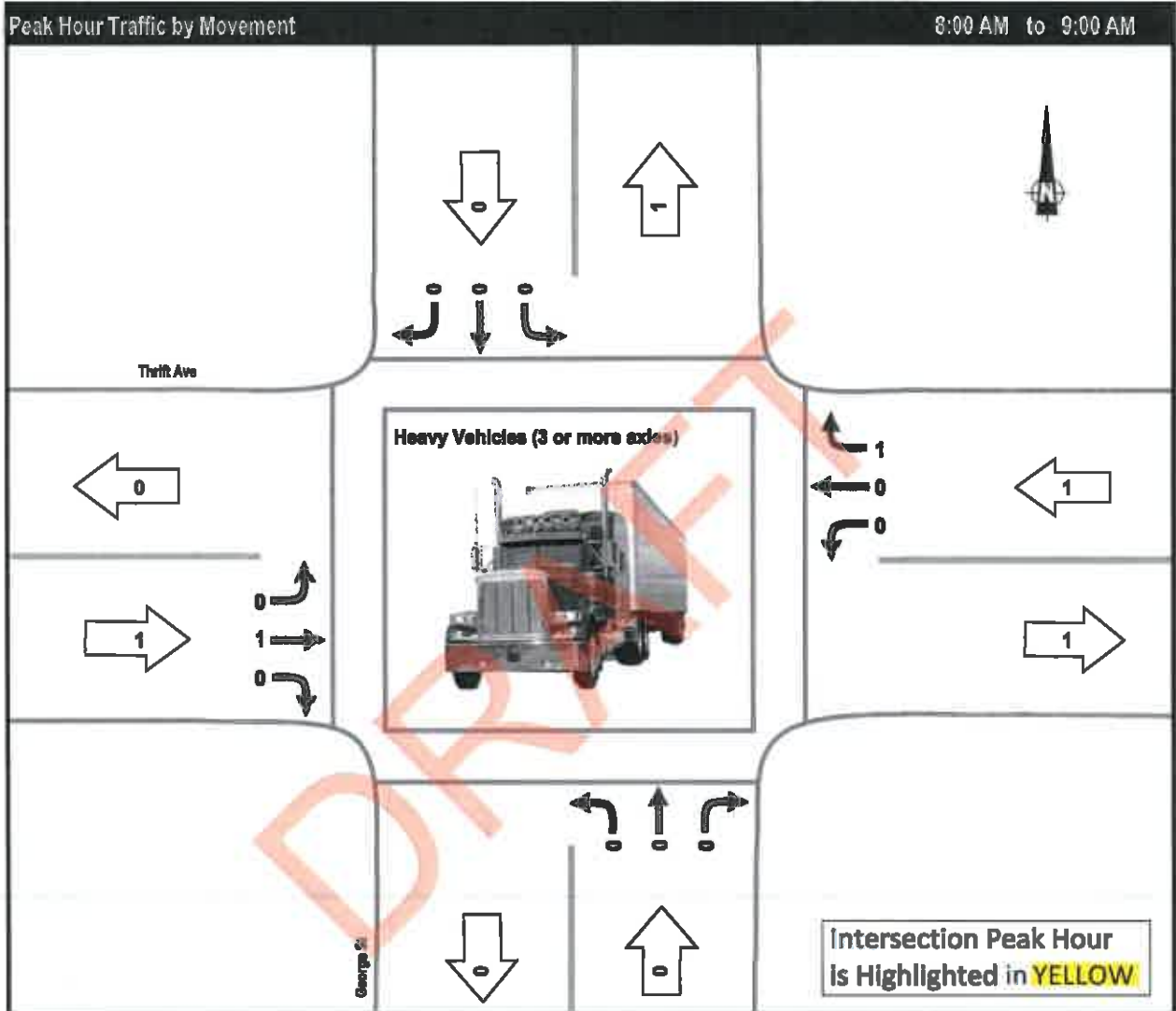
Morning Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	11	4	11	8	5	9	10	170	11	14	177	16					446
PHF	0.48	0.33	0.48	0.50	0.63	0.56	0.63	0.79	0.69	0.50	0.79	0.67					0.81
Peak 15 X 4	24	12	24	16	8	16	16	216	18	28	224	24					646
Average Hour	7	4	8	4	4	8	7	122	8	13	124	12					321
Survey Total	14	7	15	8	8	15	13	243	15	25	248	24					635
7:00	0	0	3	0	0	3	1	14	1	6	11	1					40
7:15	2	1	0	0	0	2	1	16	1	3	16	2					44
7:30	1	1	0	0	2	1	1	20	1	0	16	1					44
7:45	0	1	1	0	1	0	0	23	1	2	28	4					61
8:00	6	3	2	1	1	2	1	31	1	2	36	3					89
8:15	0	1	3	2	1	4	2	54	3	7	56	4					137
8:30	3	0	0	4	2	3	4	49	4	4	43	3					119
8:45	2	0	6	1	1	0	3	36	3	1	42	6					101

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axes)

Morning Peak Period

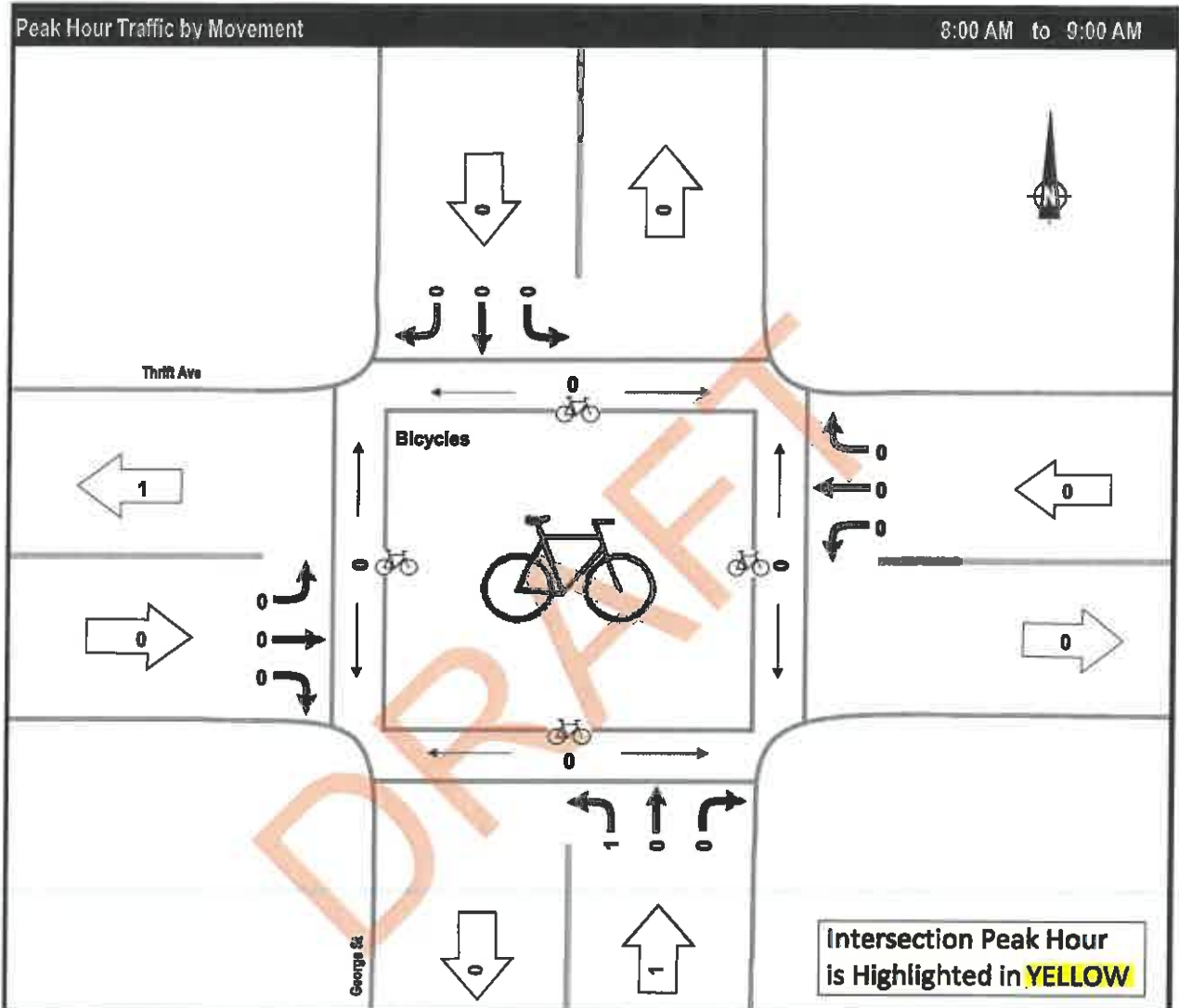


Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	0	1					2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.25					0.50
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	0	4					4
Average Hour	0	0	0	0	0	0	0	1	0	0	0	1					2
Survey Total	0	0	0	0	0	0	0	1	0	0	0	1					2
7:00	0	0	0	0	0	0	0	0	0	0	0	0					0
7:15	0	0	0	0	0	0	0	0	0	0	0	0					0
7:30	0	0	0	0	0	0	0	0	0	0	0	0					0
7:45	0	0	0	0	0	0	0	0	0	0	0	0					0
8:00	0	0	0	0	0	0	0	0	0	0	0	0					0
8:15	0	0	0	0	0	0	0	0	0	0	0	1					1
8:30	0	0	0	0	0	0	0	1	0	0	0	0					1
8:45	0	0	0	0	0	0	0	0	0	0	0	0					0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Morning Peak Period

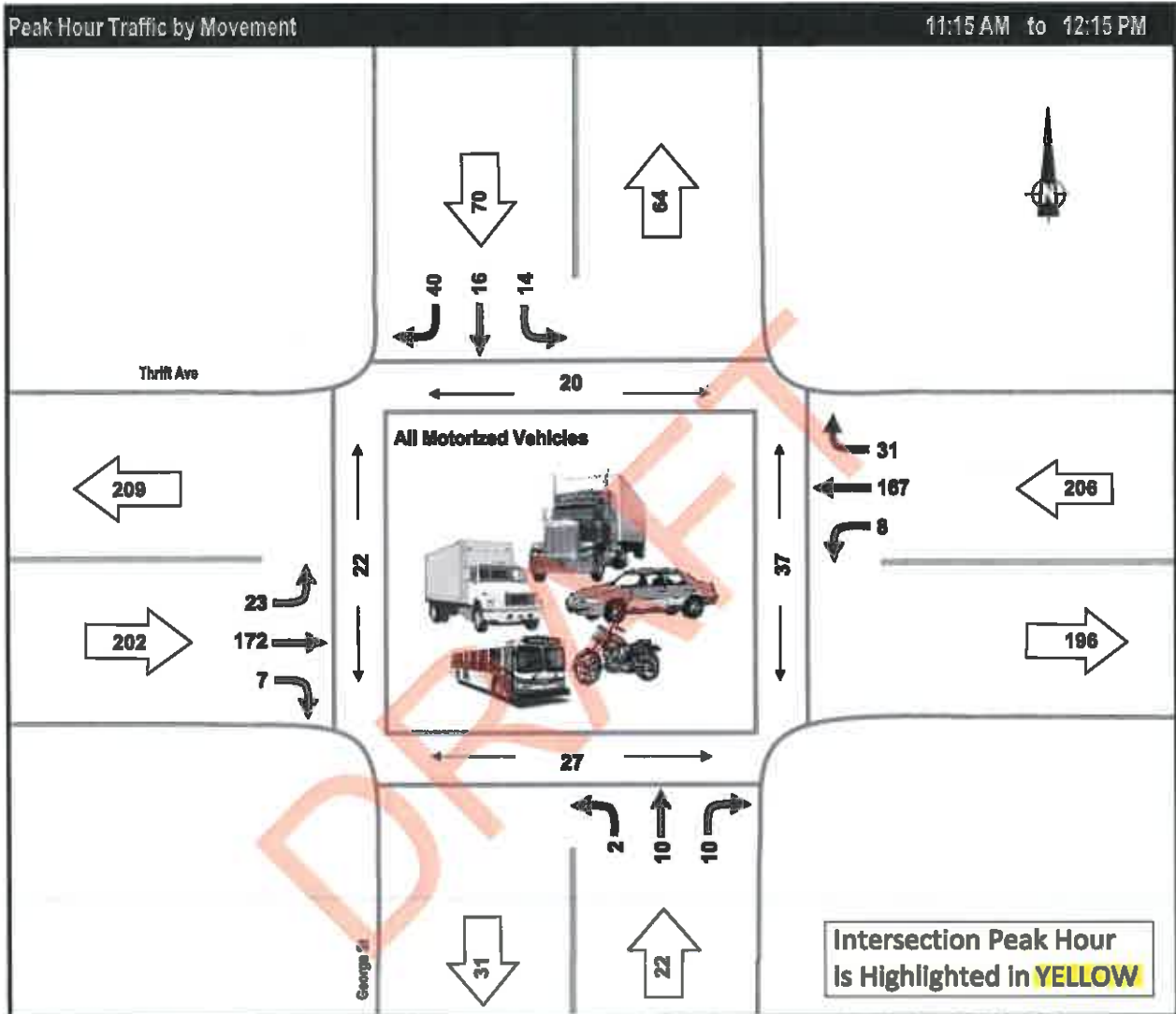
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes	
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E		
Peak Hour	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	4
Average Hour	0	0	0	1	0	0	0	0	0	0	1	0	0	0	0	0	0	2
Survey Total	0	0	0	1	0	0	0	0	0	0	2	0	0	0	0	0	0	3
7:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2
7:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
8:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7026: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

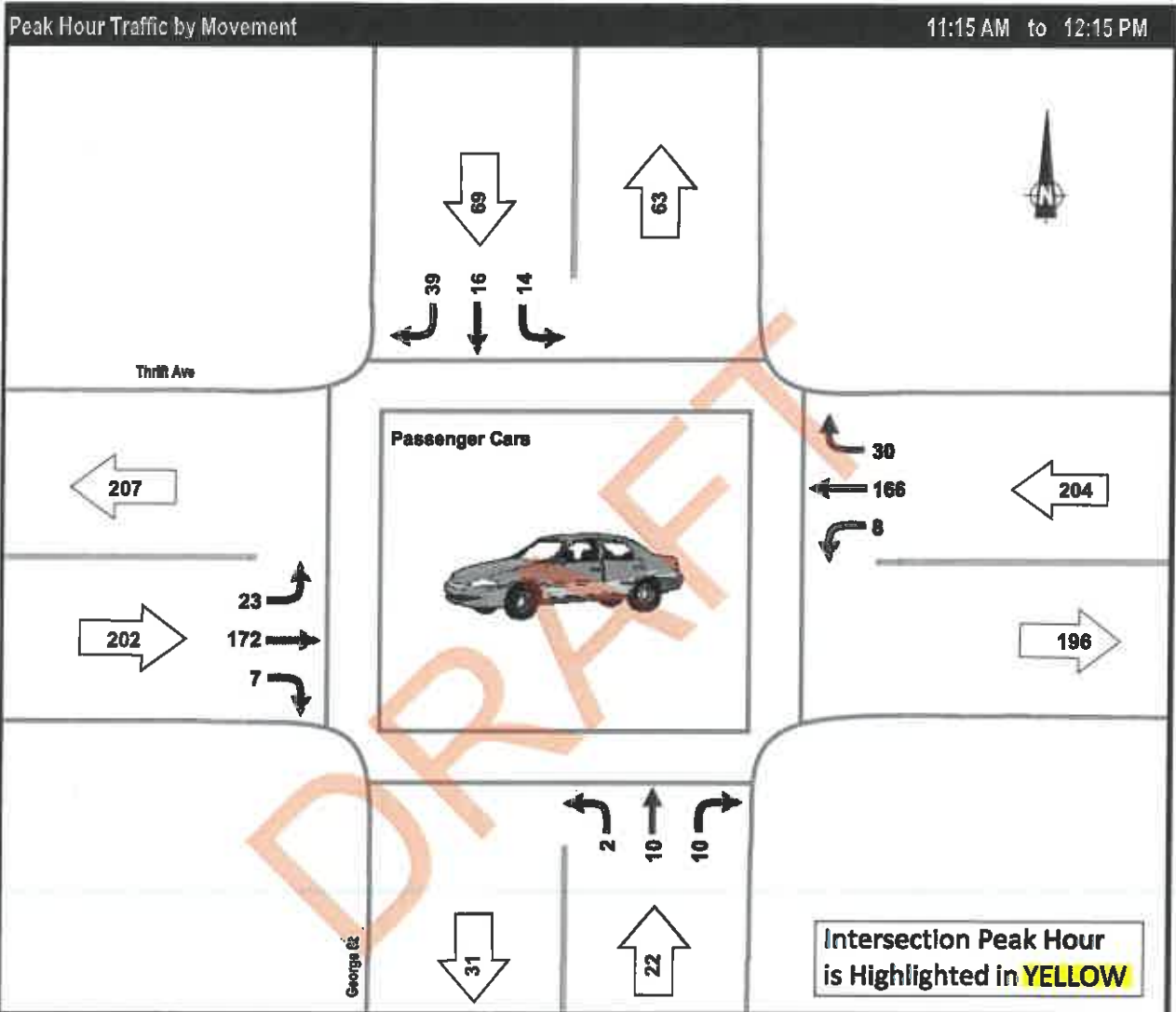
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	14	16	40	2	10	10	23	172	7	8	167	31	20	27	22	37	500
PH Factor	0.70	0.67	0.77	0.25	0.63	0.50	0.72	0.93	0.88	0.50	0.80	0.65	0.56	0.75	0.50	0.58	0.91
PHF	20	24	52	8	18	20	32	184	8	18	208	48	36	36	44	64	548
Average Hour	16	12	37	4	8	8	25	173	8	7	165	24	24	22	18	35	487
Survey Total	31	24	74	7	15	15	49	345	15	14	330	48	48	43	31	69	967
11:00	3	3	10	0	3	1	5	33	1	2	45	5	12	1	2	9	111
11:15	4	4	7	0	2	3	8	48	2	4	52	5	4	9	5	9	137
11:30	5	3	8	0	4	2	7	43	2	1	38	12	2	9	1	18	125
11:45	2	6	13	0	0	0	5	38	2	2	32	11	9	4	11	10	111
12:00	3	3	12	2	4	5	3	45	1	1	45	3	5	5	5	2	127
12:15	4	1	7	3	1	2	8	51	3	0	27	4	4	4	1	4	111
12:30	4	1	8	0	0	0	5	43	0	2	45	4	4	5	4	9	112
12:45	6	3	9	2	1	2	8	48	4	2	48	4	7	7	2	10	133

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

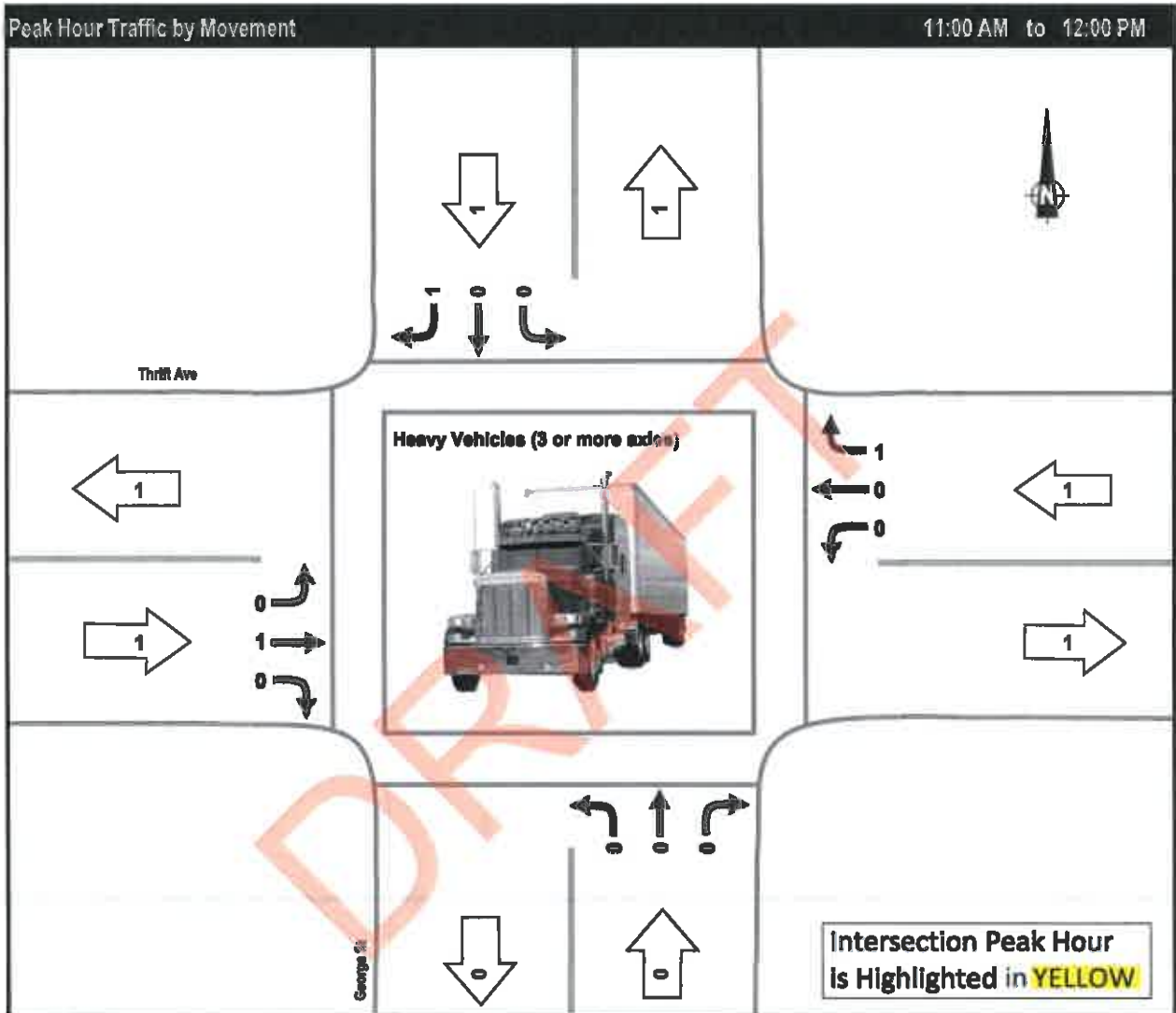
Midday Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	14	16	39	2	10	10	23	172	7	8	166	30					497
PHF	0.70	0.67	0.81	0.25	0.63	0.50	0.72	0.93	0.88	0.50	0.80	0.68					0.91
Peak 15 X 4	20	24	48	8	16	20	32	184	8	16	208	44					545
Average Hour	16	12	37	4	8	8	25	172	8	7	165	24					486
Survey Total	31	24	73	7	15	15	49	344	15	14	329	47					963
11:00	3	3	10	0	3	1	5	32	1	2	45	5					110
11:15	4	4	7	0	2	3	8	46	2	4	52	5					137
11:30	5	3	8	0	4	2	7	43	2	1	38	11					124
11:45	2	6	12	0	0	0	5	38	2	2	32	11					110
12:00	3	3	12	2	4	5	8	45	1	1	44	3					126
12:15	4	1	7	3	1	2	8	51	3	0	27	4					111
12:30	4	1	8	0	0	0	5	43	0	2	45	4					112
12:45	6	3	9	2	1	2	8	46	4	2	46	4					133

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Midday Peak Period



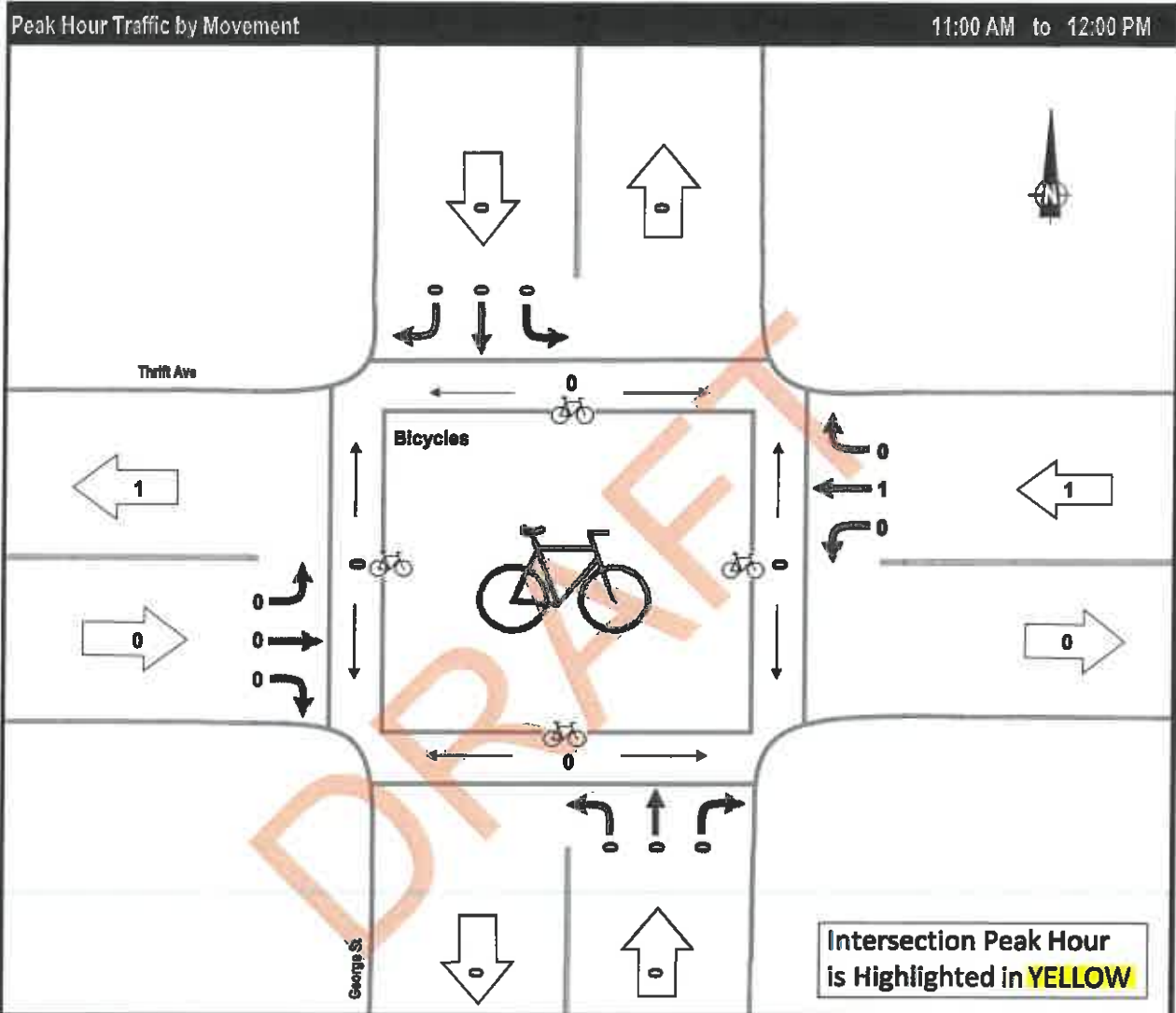
Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	1	0	0	0	0	1	0	0	0	1					3
PHF	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.25					0.75
Peak 15 X 4	0	0	4	0	0	0	0	4	0	0	0	4					4
Average Hour	0	0	1	0	0	0	0	1	0	0	0	1					4
Survey Total	0	0	1	0	0	0	0	1	0	0	0	1					4
11:00	0	0	0	0	0	0	0	1	0	0	0	0					1
11:15	0	0	0	0	0	0	0	0	0	0	0	0					0
11:30	0	0	0	0	0	0	0	0	0	0	0	1					1
11:45	0	0	1	0	0	0	0	0	0	0	0	0					1
12:00	0	0	0	0	0	0	0	0	0	0	1	0					1
12:15	0	0	0	0	0	0	0	0	0	0	0	0					0
12:30	0	0	0	0	0	0	0	0	0	0	0	0					0
12:45	0	0	0	0	0	0	0	0	0	0	0	0					0



Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Midday Peak Period

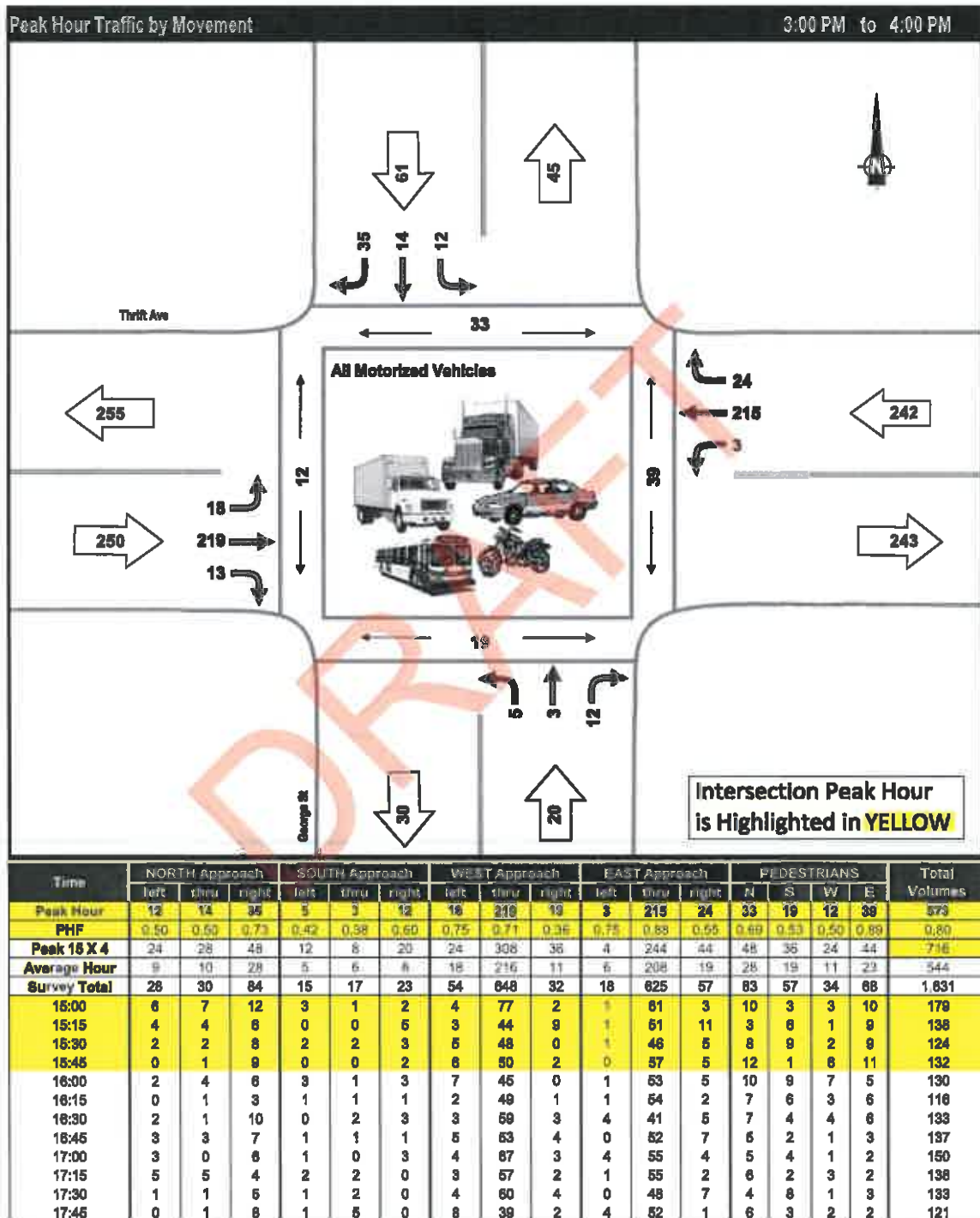
Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.25
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	4
Average Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
Survey Total	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1
11:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: All Motorized Vehicles

Afternoon Peak Period

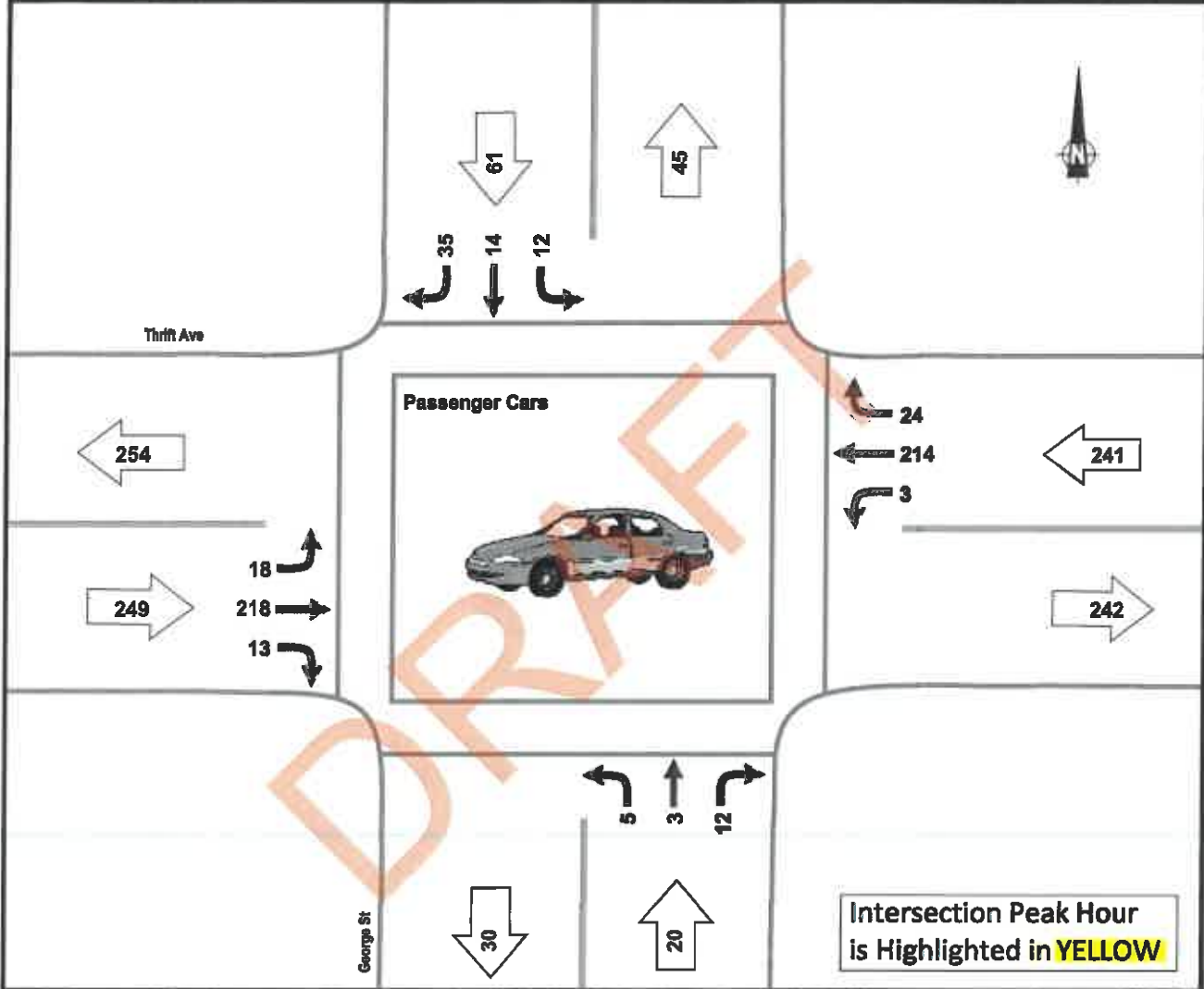


Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Passenger Cars

Afternoon Peak Period

Peak Hour Traffic by Movement

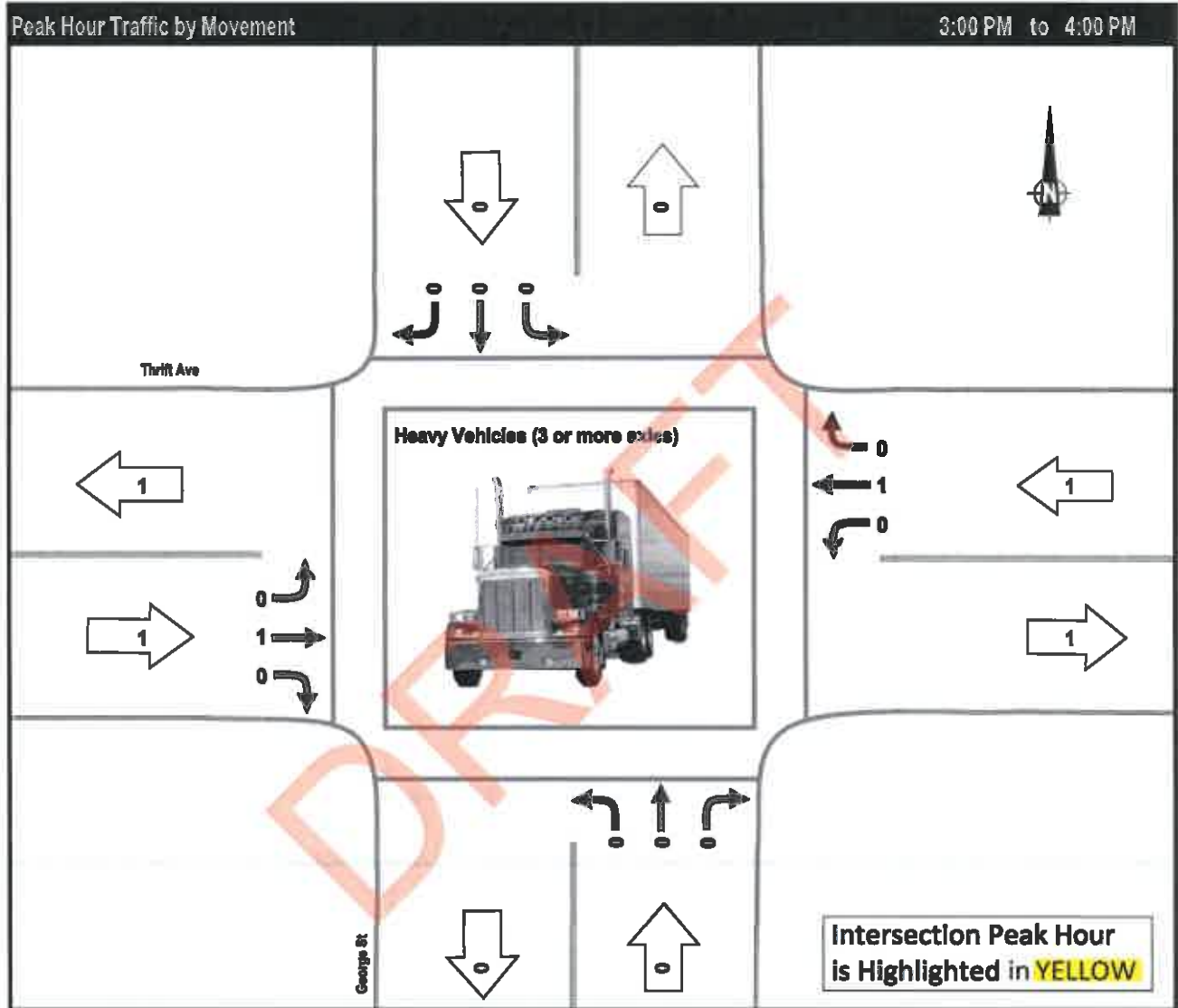
3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	12	14	35	5	3	12	18	218	13	3	214	24					571
PHF	0.50	0.50	0.73	0.42	0.38	0.80	0.75	0.72	0.36	0.75	0.89	0.55					0.81
Peak 15 X 4	24	28	48	12	8	20	24	304	36	4	240	44					708
Average Hour	9	10	28	5	6	8	18	216	11	6	208	19					544
Survey Total	28	30	84	15	17	23	54	647	32	18	624	57					1,629
15:00	6	7	12	3	1	2	4	76	2	1	60	3					177
15:16	4	4	6	0	0	5	3	44	9	1	51	11					138
15:30	2	2	8	2	2	3	5	48	0	1	46	5					124
15:45	0	1	9	0	0	2	6	50	2	0	57	6					132
16:00	2	4	6	3	1	3	7	45	0	1	53	5					130
16:15	0	1	3	1	1	1	2	49	1	1	54	2					116
16:30	2	1	10	0	2	3	3	59	3	4	41	5					133
16:45	3	3	7	1	1	1	5	53	4	0	52	7					137
17:00	3	0	6	1	0	3	4	67	3	4	55	4					150
17:15	5	5	4	2	2	0	3	57	2	1	55	2					136
17:30	1	1	5	1	2	0	4	60	4	0	48	7					133
17:45	0	1	8	1	5	0	8	39	2	4	52	1					121

Project: #7025: 1486 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Heavy Vehicles (3 or more axles)

Afternoon Peak Period



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			PEDESTRIANS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	1	0	0	1	0					2
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.25	0.00					0.25
Peak 15 X 4	0	0	0	0	0	0	0	4	0	0	4	0					8
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0					0
Survey Total	0	0	0	0	0	0	0	1	0	0	1	0					2
15:00	0	0	0	0	0	0	0	1	0	0	1	0					2
15:15	0	0	0	0	0	0	0	0	0	0	0	0					0
15:30	0	0	0	0	0	0	0	0	0	0	0	0					0
15:45	0	0	0	0	0	0	0	0	0	0	0	0					0
16:00	0	0	0	0	0	0	0	0	0	0	0	0					0
16:15	0	0	0	0	0	0	0	0	0	0	0	0					0
16:30	0	0	0	0	0	0	0	0	0	0	0	0					0
16:45	0	0	0	0	0	0	0	0	0	0	0	0					0
17:00	0	0	0	0	0	0	0	0	0	0	0	0					0
17:15	0	0	0	0	0	0	0	0	0	0	0	0					0
17:30	0	0	0	0	0	0	0	0	0	0	0	0					0
17:45	0	0	0	0	0	0	0	0	0	0	0	0					0

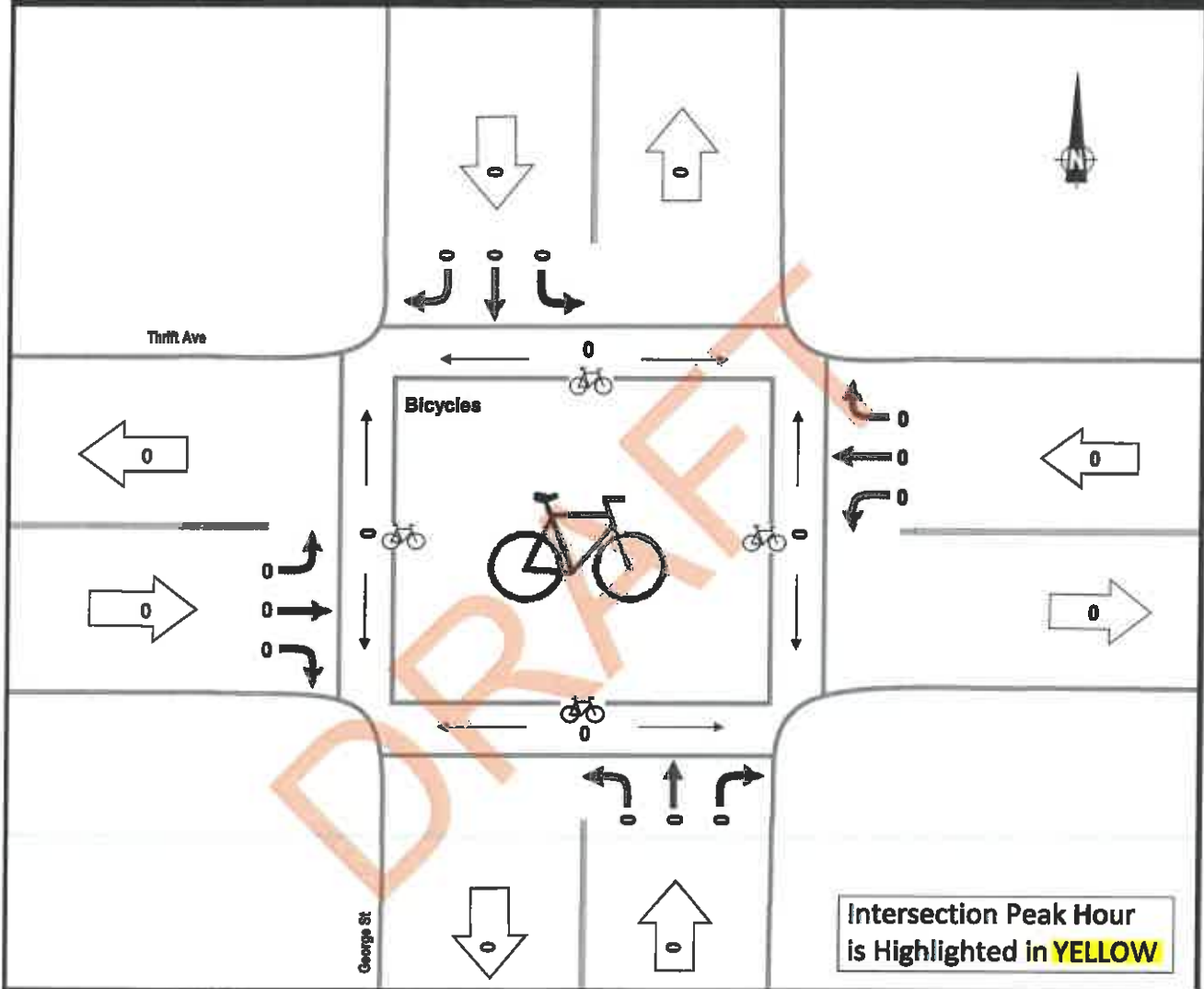
Project: #7025: 1485 Fir Street Traffic Impact Study
 Municipality: White Rock
 Weather: Rain
 Vehicle Class: Bicycles

Afternoon Peak Period

Note: Crosswalk bike volumes shown are cyclists who rode their bike across the crosswalk and are not included in the pedestrian volume totals.

Peak Hour Traffic by Movement

3:00 PM to 4:00 PM



Time	NORTH Approach			SOUTH Approach			WEST Approach			EAST Approach			BIKES in X-WALKS				Total Volumes
	left	thru	right	left	thru	right	left	thru	right	left	thru	right	N	S	W	E	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PHF	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Peak 15 X 4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Average Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Survey Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
15:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Appendix D
Intersection Capacity Analysis

DRAFT

General Information		Site Information	
Analyst	JLL	Intersection	George St at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analyst Year	2019	North/South Street	George Street
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83
Time Analyzed	AM Base		
Project Description	702S: 148S Fr Street TIA		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	3	45	7	6	88	14	14	14	11	9	9	11
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR
Flow Rate, v (veh/h)	68	130	45	45	45	45	45	45	45	45	45	45
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2

Departure Headway and Service Time

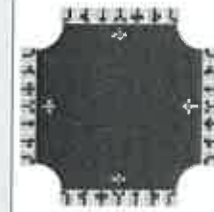
Initial Departure Headway, hd (s)	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20
Initial Degree of Utilization, x	0.059	0.116	0.040	0.040	0.040	0.040	0.040	0.040	0.040	0.040	0.040	0.040
Final Departure Headway, hd (s)	4.17	4.11	4.32	4.32	4.24	4.24	4.24	4.24	4.24	4.24	4.24	4.24
Final Degree of Utilization, x	0.077	0.149	0.054	0.054	0.041	0.041	0.041	0.041	0.041	0.041	0.041	0.041
Move-Up Time, m (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Service Time, ts (s)	2.17	2.11	2.32	2.32	2.24	2.24	2.24	2.24	2.24	2.24	2.24	2.24

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	68	130	45	45	45	45	45	45	45	45	45	45
Capacity	863	863	863	863	863	863	863	863	863	863	863	863
95% Queue Length, Q ₉₅ (veh)	0.2	0.5	0.2	0.2	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	7.5	7.8	7.5	7.5	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4
Level of Service, LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	7.5	7.8	7.5	7.5	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	A
Intersection Delay, s/veh LOS	7.7 A											
Intersection Delay, s/veh LOS	8.3 A											

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General Information		Site Information	
Analyst	JLL	Intersection	George St at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analyst Year	2019	North/South Street	George Street
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.87
Time Analyzed	PM Base		
Project Description	702S: 148S Fr Street TIA		



Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	20	103	30	19	140	22	25	14	13	19	33	35
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR	UTR
Flow Rate, v (veh/h)	176	208	208	208	208	208	60	60	60	100	100	100
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2

Departure Headway and Service Time

Initial Departure Headway, hd (s)	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20
Initial Degree of Utilization, x	0.155	0.185	0.053	0.053	0.053	0.053	0.053	0.053	0.053	0.053	0.053	0.053
Final Departure Headway, hd (s)	4.48	4.48	4.59	4.59	4.59	4.59	4.71	4.71	4.71	4.71	4.71	4.71
Final Degree of Utilization, x	0.219	0.219	0.081	0.081	0.081	0.081	0.131	0.131	0.131	0.131	0.131	0.131
Move-Up Time, m (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Service Time, ts (s)	2.48	2.48	2.48	2.48	2.48	2.48	2.71	2.71	2.71	2.71	2.71	2.71

Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	176	208	208	208	208	208	60	60	60	100	100	100
Capacity	863	863	863	863	863	863	863	863	863	863	863	863
95% Queue Length, Q ₉₅ (veh)	0.8	1.0	1.0	1.0	1.0	1.0	0.3	0.3	0.3	0.4	0.4	0.4
Control Delay (s/veh)	8.7	9.0	9.0	9.0	9.0	9.0	8.1	8.1	8.1	8.4	8.4	8.4
Level of Service, LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	8.3	9.0	9.0	9.0	9.0	9.0	8.3	8.3	8.3	8.4	8.4	8.4
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	A
Intersection Delay, s/veh LOS	8.8 A											
Intersection Delay, s/veh LOS	8.8 A											

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HCS7 All-Way Stop Control Report												
General Information			Site Information									
Analyst	JLL	George St at Russell Ave	Interaction									
Agency/Co.	URS	White Rock	Introduction									
Date Performed	4/10/2019	Russell Avenue	East/West Street									
Analysis Year	2012	George Street	North/South Street									
Analysis Time Period (hrs)	0.25	0.81	Peak Hour Factor									
Time Analyzed	3:14 PM											
Project Description	7025: 1485 Ft Street TIA											
Notes:												
Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Volume	4	48	8	7	91	15	11	35	10	10	10	12
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/h)	72	148	148	48								
Percent Heavy Vehicles	2	2	2	2								
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.064			0.064			0.064			0.064		
Final Departure Headway, hf (s)	4.21			4.21			4.21			4.21		
Final Degree of Utilization, x	0.084			0.084			0.084			0.084		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.21			2.21			2.21			2.21		
Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	72			140			48			38		
Capacity	816			888			312			240		
95% Queue Length, Q95 (veh)	0.3			0.6			0.2			0.1		
Control Delay (s/veh)	7.6			7.9			7.6			7.5		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	7.6			7.8			7.6			7.5		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	7.7			A			A			A		

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HCS7 All-Way Stop Control Report												
General Information			Site Information									
Analyst	JLL	George St at Russell Ave	Interaction									
Agency/Co.	URS	White Rock	Introduction									
Date Performed	4/10/2019	Russell Avenue	East/West Street									
Analysis Year	2012	George Street	North/South Street									
Analysis Time Period (hrs)	0.25	0.87	Peak Hour Factor									
Time Analyzed	3:14 PM											
Project Description	7025: 1485 Ft Street TIA											
Notes:												
Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Volume	22	130	32	21	349	24	27	113	14	21	35	38
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/h)	189	223	223	72								
Percent Heavy Vehicles	2	2	2	2								
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.168			0.168			0.168			0.168		
Final Departure Headway, hf (s)	4.54			4.54			4.54			4.54		
Final Degree of Utilization, x	0.218			0.218			0.218			0.218		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.56			2.56			2.56			2.56		
Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	189			223			64			108		
Capacity	210			223			77			120		
95% Queue Length, Q95 (veh)	0.9			1.2			0.3			0.5		
Control Delay (s/veh)	9.0			9.3			8.5			8.2		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	9.0			9.3			8.5			8.2		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh LOS	9.1			9.1			8.6			8.6		

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 Generated: 4/23/2019 3:55:54 PM

HCS7 All-Way Stop Control Report												
General Information						Site Information						
Analyst	JLL	Intersection	George St at Russell Ave			Analyst	JLL	Intersection	George St at Russell Ave			
Agency/Co	CTS	Jurisdiction	White Rock			Agency/Co	CTS	Jurisdiction	White Rock			
Date Performed	4/17/2019	East/West Street	Russell Avenue			Date Performed	4/17/2019	East/West Street	Russell Avenue			
Analysis Year	2022	North/South Street	George Street			Analysis Year	2022	North/South Street	George Street			
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83			Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83			
Time Analyzed	AM Base + Site					Time Analyzed	AM Base + Site					
Project Description	702S, 148S Ft Street TIA					Project Description	702S, 148S Ft Street TIA					
Lanes						Lanes						
Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Movement	L	T	R	L	T	R	L	T	R	L	T	R
Volume	4	49	9	97	16	15	15	15	13	10	20	22
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/h)	73	147	147	147	147	147	147	147	147	147	147	147
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.065			0.133			0.133			0.133		
Final Departure Headway, hf (s)	4.22			4.35			4.35			4.35		
Final Degree of Utilization, x	0.086			0.176			0.176			0.176		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.22			2.15			2.15			2.15		
Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	73			147			147			147		
Capacity	83			407			407			407		
95% Queue Length, Q ₉₅ (veh)	0.3			0.6			0.6			0.6		
Control Delay (s/veh)	7.6			8.0			8.0			8.0		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	7.6			8.0			8.0			8.0		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh | LOS	7.8 | A			8.1 | A			8.1 | A			8.1 | A		

HCS7 All-Way Stop Control Report												
General Information						Site Information						
Analyst	JLL	Intersection	George St at Russell Ave			Analyst	JLL	Intersection	George St at Russell Ave			
Agency/Co	CTS	Jurisdiction	White Rock			Agency/Co	CTS	Jurisdiction	White Rock			
Date Performed	4/17/2019	East/West Street	Russell Avenue			Date Performed	4/17/2019	East/West Street	Russell Avenue			
Analysis Year	2022	North/South Street	George Street			Analysis Year	2022	North/South Street	George Street			
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83			Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83			
Time Analyzed	AM Base + Site					Time Analyzed	AM Base + Site					
Project Description	702S, 148S Ft Street TIA					Project Description	702S, 148S Ft Street TIA					
Lanes						Lanes						
Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Movement	L	T	R	L	T	R	L	T	R	L	T	R
Volume	4	49	9	97	16	15	15	15	13	10	20	22
% Thru in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/h)	73	147	147	147	147	147	147	147	147	147	147	147
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2
Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.065			0.133			0.133			0.133		
Final Departure Headway, hf (s)	4.22			4.35			4.35			4.35		
Final Degree of Utilization, x	0.086			0.176			0.176			0.176		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.22			2.15			2.15			2.15		
Capacity, Delay and Level of Service												
Flow Rate, v (veh/h)	73			147			147			147		
Capacity	83			407			407			407		
95% Queue Length, Q ₉₅ (veh)	0.3			0.6			0.6			0.6		
Control Delay (s/veh)	7.6			8.0			8.0			8.0		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	7.6			8.0			8.0			8.0		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh | LOS	7.8 | A			8.1 | A			8.1 | A			8.1 | A		

HCS7 All-Way Stop Control Report											
General Information						Site Information					
Analyst	JL	Intersection	George St at Russell Ave			White Mark					
Agency/Cc.	CTE	East/West Street	Russell Avenue			Russell Avenue					
Date Performed	4/10/2019	North/South Street	George Street			George Street					
Analysis Year	2027	Peak Hour Factor	0.83								
Analysis Time Period (Hrs)	0.25										
Time Analyzed	AM Rows										
Project Description	7025-1485 PE Street TIA										
Lanes											

Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	4	59	9	7	105	17	17	17	11	11	11	13
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/H)	80	153	54	54	54	54	54	54	54	54	54	54
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2

Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20
Initial Degree of Utilization, x	0.071	0.138	0.042	0.138	0.138	0.138	0.138	0.138	0.138	0.138	0.138	0.138
Final Departure Headway, hd (s)	4.28	4.17	4.42	4.17	4.17	4.17	4.17	4.17	4.17	4.17	4.17	4.17
Final Degree of Utilization, x	0.094	0.066	0.051	0.066	0.066	0.066	0.066	0.066	0.066	0.066	0.066	0.066
Move-Up Time, m (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Service Time, ts (s)	2.24	2.23	2.23	2.23	2.23	2.23	2.23	2.23	2.23	2.23	2.23	2.23

Capacity, Delay and Level of Service												
Flow Rate, v (veh/H)	80	153	54	54	54	54	54	54	54	54	54	54
Capacity	141	141	141	141	141	141	141	141	141	141	141	141
95% Queue Length, Q ₉₅ (veh)	0.3	0.5	0.2	0.2	0.2	0.2	0.2	0.2	0.2	0.2	0.2	0.2
Control Delay (s/veh)	7.7	8.1	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7
Level of Service, LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)												
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	A

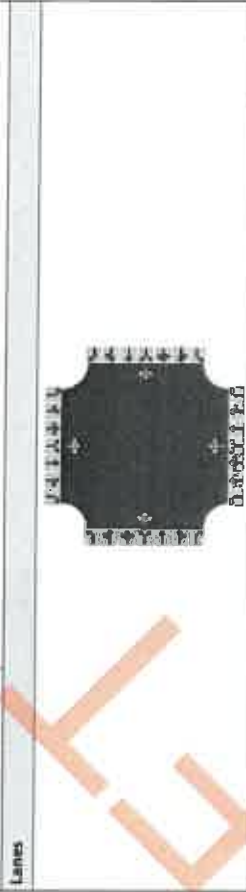
HCS7 All-Way Stop Control Report											
General Information						Site Information					
Analyst	JL	Intersection	George St at Russell Ave			White Mark					
Agency/Cc.	CTE	East/West Street	Russell Avenue			Russell Avenue					
Date Performed	4/10/2019	North/South Street	George Street			George Street					
Analysis Year	2027	Peak Hour Factor	0.87								
Analysis Time Period (Hrs)	0.25										
Time Analyzed	PM Rows										
Project Description	7025-1485 PE Street TIA										
Lanes											

Vehicle Volume and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	24	120	35	23	163	26	29	17	16	17	39	41
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Flow Rate, v (veh/H)	205	244	244	244	244	244	244	244	244	244	244	244
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2

Departure Headway and Service Time												
Initial Departure Headway, hd (s)	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20	3.20
Initial Degree of Utilization, x	0.183	0.177	0.177	0.177	0.177	0.177	0.177	0.177	0.177	0.177	0.177	0.177
Final Departure Headway, hd (s)	4.63	4.63	4.63	4.63	4.63	4.63	4.63	4.63	4.63	4.63	4.63	4.63
Final Degree of Utilization, x	0.255	0.255	0.255	0.255	0.255	0.255	0.255	0.255	0.255	0.255	0.255	0.255
Move-Up Time, m (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Service Time, ts (s)	2.63	2.63	2.63	2.63	2.63	2.63	2.63	2.63	2.63	2.63	2.63	2.63

Capacity, Delay and Level of Service												
Flow Rate, v (veh/H)	205	244	244	244	244	244	244	244	244	244	244	244
Capacity	777	777	777	777	777	777	777	777	777	777	777	777
95% Queue Length, Q ₉₅ (veh)	1.1	1.1	1.1	1.1	1.1	1.1	1.1	1.1	1.1	1.1	1.1	1.1
Control Delay (s/veh)	9.3	9.3	9.3	9.3	9.3	9.3	9.3	9.3	9.3	9.3	9.3	9.3
Level of Service, LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)												
Approach LOS	A	A	A	A	A	A	A	A	A	A	A	A

General Information		Site Information	
Analyst	JLL	Intersection	George St at Russell Ave
Agency/Co.	URS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	George Street
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83
Time Analyzed	AM Base + Site		
Project Description	7025, 7485 Fir Street TIA		



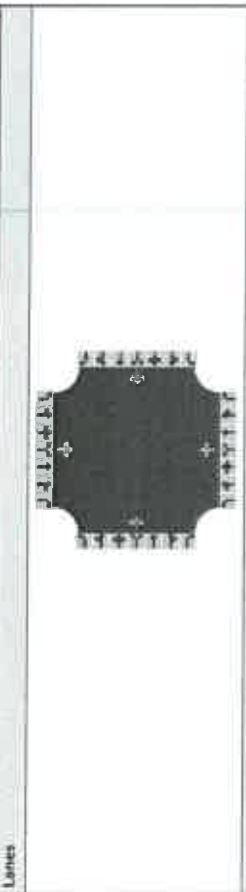
Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	24	13	35	25	163	27	29	17	21	24	39	41
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18
Flow Rate, v (veh/h)	207	207	207	207	207	207	207	207	207	207	207	207
Percent Heavy Vehicles	7	7	7	7	7	7	7	7	7	7	7	7

Vehicle Volume and Adjustments	
Initial Departure Headway, hd (s)	3.20
Initial Degree of Utilization, x	0.186
Final Departure Headway, hd (s)	4.68
Final Degree of Utilization, x	0.271
Move-Up Time, m (s)	2.0
Service Time, Ts (s)	2.66

Departure Headway and Service Time	
Initial Departure Headway, hd (s)	3.20
Initial Degree of Utilization, x	0.222
Final Departure Headway, hd (s)	4.65
Final Degree of Utilization, x	0.322
Move-Up Time, m (s)	2.0
Service Time, Ts (s)	2.66

Capacity, Delay and Level of Service	
Flow Rate, v (veh/h)	209
Capacity	774
95% Queue Length, Q ₉₅ (veh)	1.1
Control Delay (s/veh)	9.4
Level of Service, LOS	A
Approach Delay (s/veh)	9.4
Approach LOS	A

General Information		Site Information	
Analyst	JLL	Intersection	George St at Russell Ave
Agency/Co.	URS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	George Street
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.83
Time Analyzed	AM Base + Site		
Project Description	7025, 7485 Fir Street TIA		



Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Volume	4	54	18	306	18	17	17	17	17	11	11	13
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18	1.18
Flow Rate, v (veh/h)	41	41	41	41	41	41	41	41	41	41	41	41
Percent Heavy Vehicles	2	2	2	2	2	2	2	2	2	2	2	2

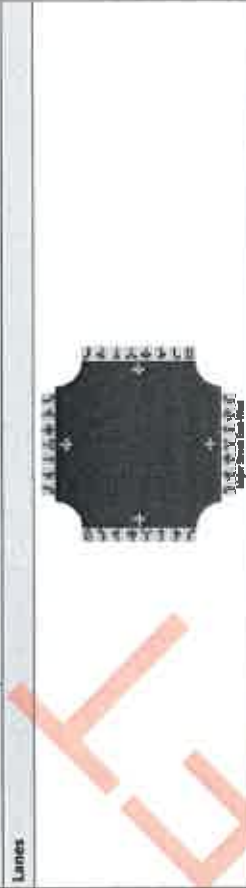
Vehicle Volume and Adjustments	
Initial Departure Headway, hd (s)	3.20
Initial Degree of Utilization, x	0.051
Final Departure Headway, hd (s)	4.40
Final Degree of Utilization, x	0.071
Move-Up Time, m (s)	2.0
Service Time, Ts (s)	2.66

Departure Headway and Service Time	
Initial Departure Headway, hd (s)	3.20
Initial Degree of Utilization, x	0.162
Final Departure Headway, hd (s)	4.18
Final Degree of Utilization, x	0.186
Move-Up Time, m (s)	2.0
Service Time, Ts (s)	2.66

Capacity, Delay and Level of Service	
Flow Rate, v (veh/h)	160
Capacity	816
95% Queue Length, Q ₉₅ (veh)	0.2
Control Delay (s/veh)	7.7
Level of Service, LOS	A
Approach Delay (s/veh)	7.7
Approach LOS	A

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St. at Thrift Ave
Agency/Co.	CS	Jurisdiction	White Road
Date Performed	4/12/2019	East/West Street	Thrift Avenue
Analysis Year	2019	North/South Street	George Street
Time Analyzed	AM Base	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (Day)	0.25
Project Description	7007-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Measurement	30	1	2	40	4	5	6	7	8	9	10	11
Priority	3	0	1	0	0	1	0	0	1	0	0	1
Number of Lanes	LTR			LTR			LTR			LTR		
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	18	17	11	18	17	17	17	8	3	9	11	11
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

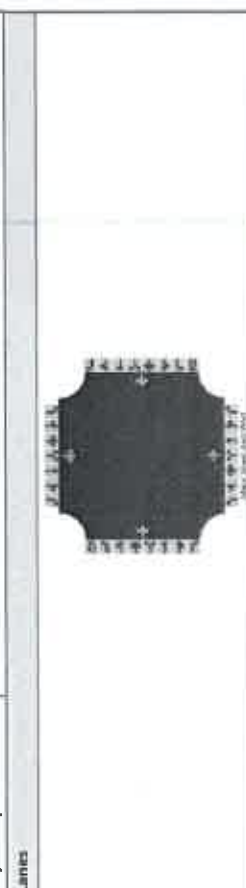
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	21	21	21	21	21	21	21	21	21	21	21	21
Capacity, c (veh/h)	1271	1271	1271	1271	1271	1271	1271	1271	1271	1271	1271	1271
v/c Ratio	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02
95% Queue Length, Q ₉₅ (veh)	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.7											
Approach LOS	B											

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St. at Thrift Ave
Agency/Co.	CS	Jurisdiction	White Road
Date Performed	4/12/2019	East/West Street	Thrift Avenue
Analysis Year	2019	North/South Street	George Street
Time Analyzed	AM Base	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (Day)	0.25
Project Description	7007-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Measurement	30	1	2	40	4	5	6	7	8	9	10	11
Priority	3	0	1	0	0	1	0	0	1	0	0	1
Number of Lanes	LTR			LTR			LTR			LTR		
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	18	17	11	18	17	17	17	8	3	9	11	11
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

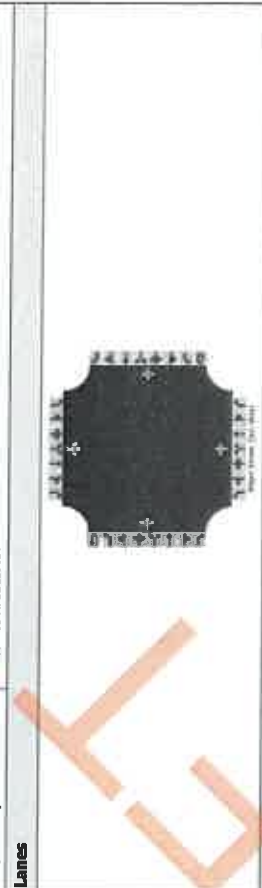
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	12	12	12	12	12	12	12	12	12	12	12	12
Capacity, c (veh/h)	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285
v/c Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.5											
Approach LOS	B											

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HCS7 Two-Way Stop Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St at Thrift Ave
Agency	GIS	Jurisdiction	White Rock
Date Performed	4/23/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	George Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 3485 Fr Street TA		



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	L	T	R	L	L	T	R	L	L	T	R	L	L	T	R	L
Movement	10	1	2	3	40	4	5	6	7	8	9	10	11	12		
Priority	0	0	1	0	0	0	0	1	0	1	0	0	0	1	0	0
Number of Lanes	LTR				LTR				LTR				LTR			
Volume (veh/h)	11	18	22	18	20	213	14	8	278	24	6	4	13	15	38	
Percent Heavy Vehicles (P)	2				2				2				2			
Proportion Time Blocked																
Percent Grade (G)	0															
Right Turn Channelized	Unbid															
Median Type | Storage	Unbid															

Critical and Follow-up Headways

	4.1	4.12	2.2	2.22	4.1	4.12	2.2	2.22	7.1	6.5	6.2	7.1	6.5	6.2
Base Critical Headway (sec)														
Critical Headway (sec)														
Base Follow-Up Headway (sec)														
Follow-Up Headway (sec)														

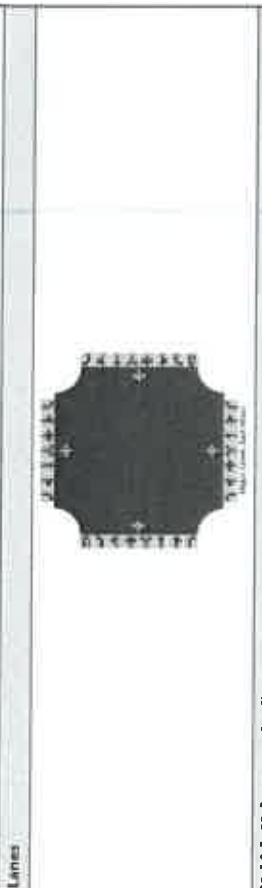
Delay, Queue Length, and Level of Service

	29	5	26	76
Flow Rate, v (veh/h)				
Capacity, c (veh/h)	1194	1134	461	416
v/c Ratio	0.02	0.00	0.06	0.16
95% Queue Length, Q ₉₅ (ft)	0.1	0.0	0.1	0.6
Control Delay (s/veh)	6.1	7.9	13.3	14.0
Level of Service (LOS)	A	A	B	B
Approach Delay (s/veh)	0.8	0.2	13.3	14.0
Approach LOS	B	B	B	B

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HCS7 Two-Way Stop Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St at Thrift Ave
Agency	GIS	Jurisdiction	White Rock
Date Performed	4/23/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	George Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 3485 Fr Street TA		



Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	L	T	R	L	L	T	R	L	L	T	R	L	L	T	R	L
Movement	10	1	2	3	40	4	5	6	7	8	9	10	11	12		
Priority	0	0	1	0	0	0	0	1	0	1	0	0	0	1	0	0
Number of Lanes	LTR				LTR				LTR				LTR			
Volume (veh/h)	11	18	22	18	20	213	14	8	278	24	6	4	13	15	38	
Percent Heavy Vehicles (P)	2				2				2				2			
Proportion Time Blocked																
Percent Grade (G)	0															
Right Turn Channelized	Unbid															
Median Type | Storage	Unbid															

Critical and Follow-up Headways

	4.1	4.12	2.2	2.22	4.1	4.12	2.2	2.22	7.1	6.5	6.2	7.1	6.5	6.2
Base Critical Headway (sec)														
Critical Headway (sec)														
Base Follow-Up Headway (sec)														
Follow-Up Headway (sec)														

Delay, Queue Length, and Level of Service

	13	30	30	35
Flow Rate, v (veh/h)				
Capacity, c (veh/h)	1194	1134	461	416
v/c Ratio	0.01	0.01	0.06	0.07
95% Queue Length, Q ₉₅ (ft)	0.0	0.0	0.2	0.2
Control Delay (s/veh)	7.9	7.8	12.3	12.9
Level of Service (LOS)	A	A	B	B
Approach Delay (s/veh)	0.5	0.6	12.5	12.9
Approach LOS	B	B	B	B

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HCS7 Two-Way Stop-Control Report												
General Information						Site Information						
Analyst	JLL	Intersection	George St at Thrift Ave									
Agency/Co	CTD	Jurisdiction	White Rock									
Date Performed	4/12/2019	East/West Street	Thrift Avenue									
Analysis Year	2022	North/South Street	George Street									
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7007: 1465 Fr Street TIA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Volume (veh/h)	17	187	17	16	180	23	9	6	10	119	34	
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Unbid			Unbid			Unbid			Unbid		
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	
Base Follow-up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	
Follow-up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	34	39	39	30	30	37						
Capacity, c (veh/h)	1751	1713	1713	101	101	677						
v/c Ratio	0.02	0.02	0.02	0.06	0.06	0.08						
95% Queue Length, Q ₉₅ (veh)	0	0	0	0.2	0.2	0.7						
Control Delay (s/veh)	7.9	7.8	7.8	12.6	12.6	11.8						
Level of Service (LOS)	A	A	A	B	B	B						
Approach Delay (s/veh)	0.6			0.7			12.6			12.8		
Approach LOS	B			B			B			B		

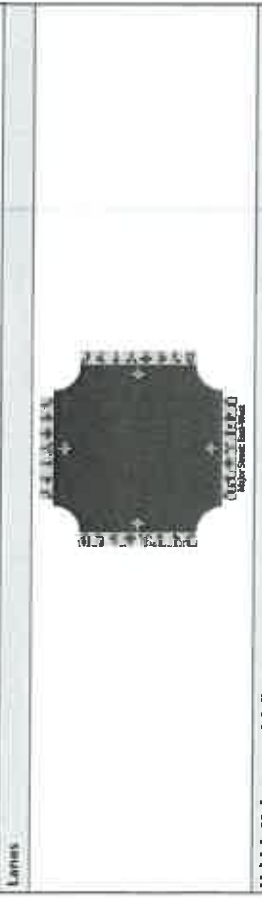
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HCS7 Two-Way Stop-Control Report												
General Information						Site Information						
Analyst	JLL	Intersection	George St at Thrift Ave									
Agency/Co	CTD	Jurisdiction	White Rock									
Date Performed	4/12/2019	East/West Street	Thrift Avenue									
Analysis Year	2022	North/South Street	George Street									
Time Analyzed	PM Base + Site	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025: 1465 Fr Street TIA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Volume (veh/h)	21	214	21	4	230	28	7	8	9	10	11	
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Unbid			Unbid			Unbid			Unbid		
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	
Base Follow-up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	
Follow-up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	34	39	39	30	30	37						
Capacity, c (veh/h)	1751	1713	1713	101	101	677						
v/c Ratio	0.02	0.02	0.02	0.06	0.06	0.08						
95% Queue Length, Q ₉₅ (veh)	0	0	0	0.2	0.2	0.7						
Control Delay (s/veh)	7.9	7.8	7.8	12.6	12.6	11.8						
Level of Service (LOS)	A	A	A	B	B	B						
Approach Delay (s/veh)	0.6			0.7			12.6			12.8		
Approach LOS	B			B			B			B		

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General Information

Analyst	JLL	Intersection	George St at Thrift Ave
Agency/Co.	CTE	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	George Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 FR Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	1U	1	2	3	4U	4	5	6	7	8	9	10
Priority	0	0	0	0	0	0	1	0	0	0	0	0
Number of Lanes	LTR			LTR			LTR			LTR		
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	0	0	0	0	0	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0	0	0	0	0	0	0	0	0	0	0	0
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	No			No			No			No		
Median Type | Storage	Unchannelized			Unchannelized			Unchannelized			Unchannelized		

Critical and Follow-up Headways

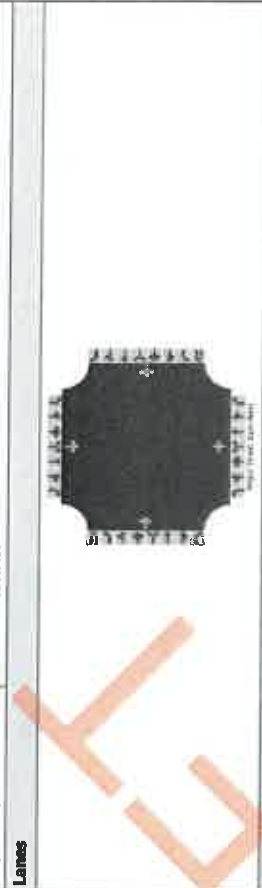
	U	L	T	U	L	T	U	L	T	U	L	T
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

Delay, Queue Length, and Level of Service

	U	L	T	U	L	T	U	L	T	U	L	T
Flow Rate, v (veh/h)	14	14	14	14	14	14	14	14	14	14	14	14
Capacity, c (veh/h)	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285
v/c Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9
Level of Service (LOS)	B	B	B	B	B	B	B	B	B	B	B	B
Approach Delay (s/veh)	0.5	0.5	0.5	0.5	0.5	0.5	0.5	0.5	0.5	0.5	0.5	0.5
Approach LOS	B	B	B	B	B	B	B	B	B	B	B	B

General Information

Analyst	JLL	Intersection	George St at Thrift Ave
Agency/Co.	CTE	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	George Street
Time Analyzed	PM Base	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 FR Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	1U	1	2	3	4U	4	5	6	7	8	9	10
Priority	0	0	0	0	0	0	1	0	0	0	0	0
Number of Lanes	LTR			LTR			LTR			LTR		
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	21	215	19	4	250	18	6	4	14	14	17	41
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	No			No			No			No		
Median Type | Storage	Unchannelized			Unchannelized			Unchannelized			Unchannelized		

Critical and Follow-up Headways

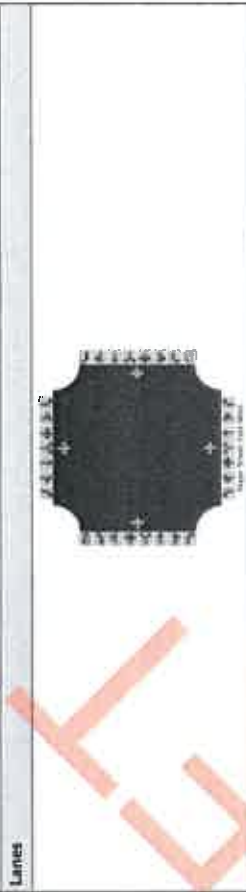
	U	L	T	U	L	T	U	L	T	U	L	T
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

Delay, Queue Length, and Level of Service

	U	L	T	U	L	T	U	L	T	U	L	T
Flow Rate, v (veh/h)	24	24	24	24	24	24	24	24	24	24	24	24
Capacity, c (veh/h)	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285	1285
v/c Ratio	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02
95% Queue Length, Q ₉₅ (veh)	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	6.2	6.2	6.2	6.2	6.2	6.2	6.2	6.2	6.2	6.2	6.2	6.2
Level of Service (LOS)	B	B	B	B	B	B	B	B	B	B	B	B
Approach Delay (s/veh)	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8	0.8
Approach LOS	B	B	B	B	B	B	B	B	B	B	B	B

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St at Thrift Ave
Agency/Co.	CTA	Jurisdiction	White Rock
Date Performed	4/12/2019	Northbound	Thrift Avenue
Analysis Year	2019	East/West Street	George Street
Time Analyzed	AM Peak + Site	North/South Street	George Street
Intersection Orientation	East-West	Peak Hour Factor	0.89
Project Description	7025 1485 Ft Street TIA	Analysis Time Period (hrs)	0.25



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound			
	M	T	R	M	T	R	M	T	R	M	T	R	
Approach	10	1	3	4	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	1	0	0	1	0	0	0
Number of Lanes	LTR			LTR			LTR			LTR			
Configuration	LTR			LTR			LTR			LTR			
Volume (veh/h)	13	108	33	18	208	22	18	6	31	13	5	15	43
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked													
Percent Grade (%)	0			0			0			0			
Right Turn Channelized	Unchannelized			Unchannelized			Unchannelized			Unchannelized			
Median Type | Storage	Unchannelized			Unchannelized			Unchannelized			Unchannelized			

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

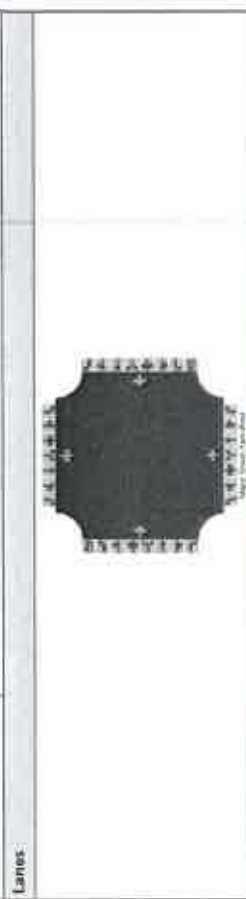
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	16	22	22	22	22	22	22	22	22
Capacity, c (veh/h) <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td> <td>3370</td>	3370	3370	3370	3370	3370	3370	3370	3370	3370
v/c Ratio	0.01	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02
95% Queue Length, Q ₉₅ (veh)	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	7.9	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8
Level of Service (LOS)	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.6	0.7	0.7	0.7	0.7	0.7	0.7	0.7	0.7
Approach LOS	B	B	B	B	B	B	B	B	B

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George St at Thrift Ave
Agency/Co.	CTA	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Thrift Avenue
Analysis Year	2019	North/South Street	George Street
Time Analyzed	AM Peak + Site	Peak Hour Factor	0.89
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7007 1485 Ft Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound			
	M	T	R	M	T	R	M	T	R	M	T	R	
Approach	10	1	3	4	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	1	0	0	1	0	0	0
Number of Lanes	LTR			LTR			LTR			LTR			
Configuration	LTR			LTR			LTR			LTR			
Volume (veh/h)	13	108	33	18	208	22	18	6	31	13	5	15	43
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked													
Percent Grade (%)	0			0			0			0			
Right Turn Channelized	Unchannelized			Unchannelized			Unchannelized			Unchannelized			
Median Type | Storage	Unchannelized			Unchannelized			Unchannelized			Unchannelized			

Critical and Follow-up Headways

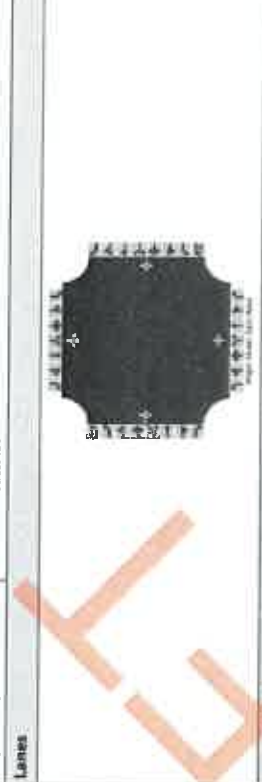
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	16	22	22	22	22	22	22	22	22
Capacity, c (veh/h)	3370	3370	3370	3370	3370	3370	3370	3370	3370
v/c Ratio	0.01	0.02	0.02	0.02	0.02	0.02	0.02	0.02	0.02
95% Queue Length, Q ₉₅ (veh)	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	7.9	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8
Level of Service (LOS)	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.6	0.7	0.7	0.7	0.7	0.7	0.7	0.7	0.7
Approach LOS	B	B	B	B	B	B	B	B	B

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General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CH	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2019	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



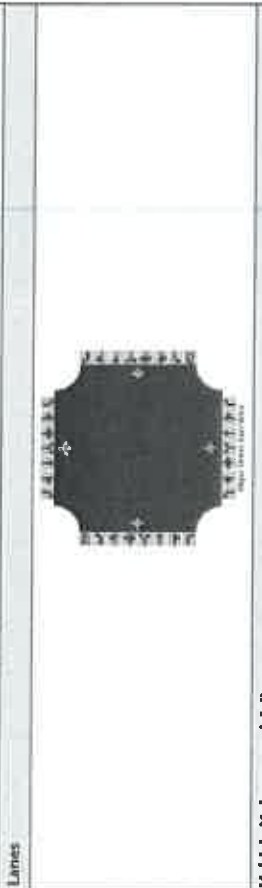
Vehicle Volumes and Adjustments		Eastbound		Westbound		Northbound		Southbound	
Approach	Volume	U	T	U	T	U	T	U	T
Minor Street	30	1	2	3	4	5	6	7	8
Major Street	0	0	1	0	1	0	1	0	1
Number of Lanes		LTR		LTR		LTR		LTR	
Volume (veh/h)	8	11	7	5	15	7	10	9	16
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2
Proportion Time Blocked		0		0		0		0	
Percent Grade (%)		0		0		0		0	
Right Turn Channelized		Unbid		Unbid		Unbid		Unbid	
Median Type | Storage		Unbid		Unbid		Unbid		Unbid	

Critical and Follow-up Headways		Eastbound		Westbound		Northbound		Southbound	
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	
Base Follow-up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	
Follow-up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	

Delay, Queue Length, and Level of Service		Eastbound		Westbound		Northbound		Southbound	
Flow Rate, v (veh/h)	10	6	6	6	6	6	6	6	
Capacity, c (veh/h)	1276	1349	1349	1349	1349	1349	1349	1349	
v/c Ratio	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (s/veh)	7.8	7.7	7.7	7.7	7.7	7.7	7.7	7.7	
Level of Service (LOS)	A	A	A	A	A	A	A	A	
Approach Delay (s/veh)		0.6	0.6	0.6	0.6	0.6	0.6	0.6	
Approach LOS		B	B	B	B	B	B	B	

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General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CH	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2019	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



Vehicle Volumes and Adjustments		Eastbound		Westbound		Northbound		Southbound	
Approach	Volume	U	T	U	T	U	T	U	T
Minor Street	11	1	2	3	4	5	6	7	8
Major Street	0	0	1	0	1	0	1	0	1
Number of Lanes		LTR		LTR		LTR		LTR	
Volume (veh/h)	6	11	7	3	11	7	3	11	14
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2
Proportion Time Blocked		0		0		0		0	
Percent Grade (%)		0		0		0		0	
Right Turn Channelized		Unbid		Unbid		Unbid		Unbid	
Median Type | Storage		Unbid		Unbid		Unbid		Unbid	

Critical and Follow-up Headways		Eastbound		Westbound		Northbound		Southbound	
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	
Base Follow-up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	
Follow-up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	

Delay, Queue Length, and Level of Service		Eastbound		Westbound		Northbound		Southbound	
Flow Rate, v (veh/h)	11	4	4	4	4	4	4	4	
Capacity, c (veh/h)	1318	1349	1349	1349	1349	1349	1349	1349	
v/c Ratio	0.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Control Delay (s/veh)	7.6	7.4	7.4	7.4	7.4	7.4	7.4	7.4	
Level of Service (LOS)	A	A	A	A	A	A	A	A	
Approach Delay (s/veh)	1.2	0.2	0.2	0.2	0.2	0.2	0.2	0.2	
Approach LOS		A	A	A	A	A	A	A	

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CTS	Approach	Wide Rect
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	3.0	1	2	3	4	5	6	7	8	9	10	11
Priority	0	0	1	0	0	1	0	0	1	0	0	1
Number of Lanes	0	0	1	0	0	1	0	0	1	0	0	1
Configuration	LTR											
Volume (veh/h)	10	127	0	6	165	13	0	4	20	2	2	2
Percent Heavy Vehicles (%)	0											
Proportion Time Restricted	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2
Base Follow-Up Headway (sec)	2.2	2.2	4.0	3.3	3.5	4.0	3.3	3.5
Follow-Up Headway (sec)	2.2	2.2	4.0	3.3	3.5	4.0	3.3	3.5

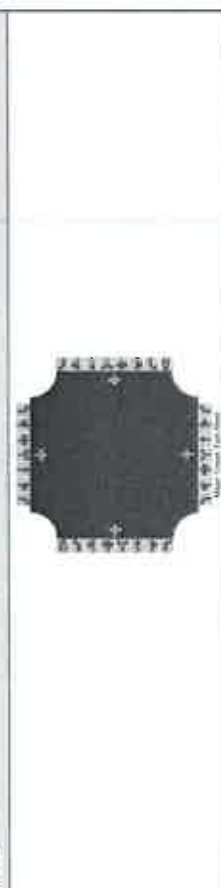
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	11	7	37	37	38
Capacity, c (veh/h)	1100	1100	1100	1100	1100
v/c Ratio	0.01	0.01	0.03	0.03	0.03
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.9	7.7	11.9	11.9	11.7
Level of Service (LOS)	A	A	B	B	B
Approach Delay (s/veh)	0.6	0.9	11.0	11.0	11.7
Approach LOS	A	A	B	B	B

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CTS	Approach	Wide Rect
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	3.0	1	2	3	4	5	6	7	8	9	10	11
Priority	0	0	1	0	0	1	0	0	1	0	0	1
Number of Lanes	0	0	1	0	0	1	0	0	1	0	0	1
Configuration	LTR											
Volume (veh/h)	10	127	0	6	165	13	0	4	20	2	2	2
Percent Heavy Vehicles (%)	0											
Proportion Time Restricted	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2
Base Follow-Up Headway (sec)	2.2	2.2	4.0	3.3	3.5	4.0	3.3	3.5
Follow-Up Headway (sec)	2.2	2.2	4.0	3.3	3.5	4.0	3.3	3.5

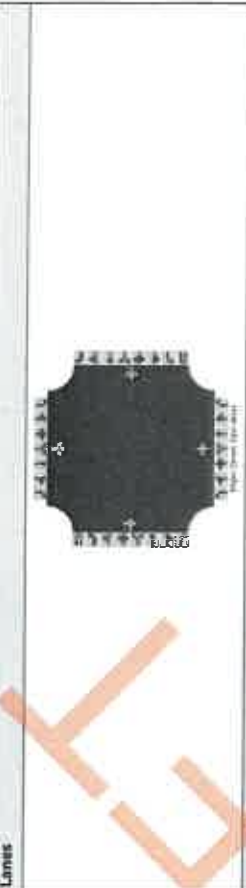
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	12	5	14	14	40
Capacity, c (veh/h)	1100	1100	1100	1100	1100
v/c Ratio	0.01	0.01	0.02	0.02	0.05
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.6	7.4	11.0	11.0	10.0
Level of Service (LOS)	A	A	B	B	A
Approach Delay (s/veh)	1.3	0.3	9.9	9.9	10.0
Approach LOS	A	A	B	B	A

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HCS2 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	Fr Street
Time Analyzed	PM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	L	T	R	L	T	R
Volume (veh/h)	11	139	9	6	180	14	9	4	21	1	7	19
Percent Heavy Vehicles (%)												
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Undivided											

Critical and Follow-up Headways

	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

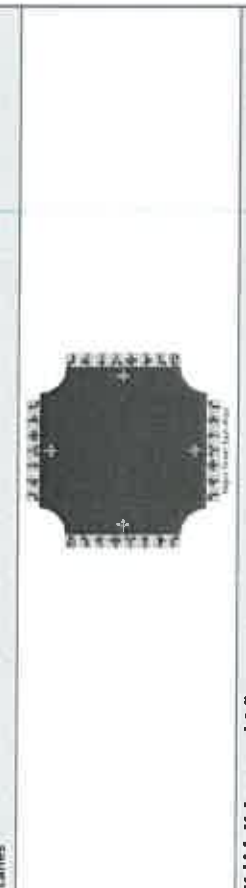
Delay, Queue Length, and Level of Service

	13	13	13	13	13	13	13	13	13	13	13	13
Flow Rate, v (veh/h)	13	13	13	13	13	13	13	13	13	13	13	13
Capacity, c (veh/h)	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246
V/C Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.6	0.6	0.6	0.6	0.6	0.6	0.6	0.6	0.6	0.6	0.6	0.6
Approach LOS												

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HCS2 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	L	T	R	L	T	R
Volume (veh/h)	11	139	9	6	180	14	9	4	21	1	7	19
Percent Heavy Vehicles (%)												
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Undivided											

Critical and Follow-up Headways

	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2

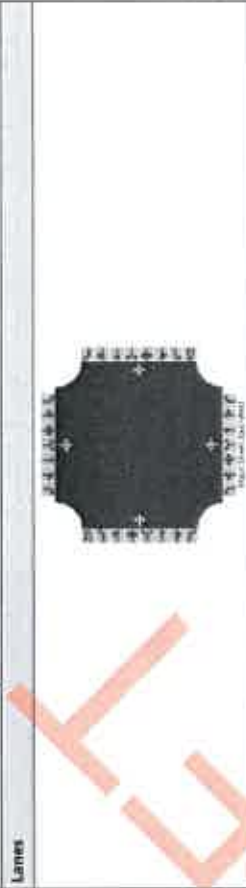
Delay, Queue Length, and Level of Service

	13	13	13	13	13	13	13	13	13	13	13	13
Flow Rate, v (veh/h)	13	13	13	13	13	13	13	13	13	13	13	13
Capacity, c (veh/h)	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246	1246
V/C Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6	7.6
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	1.3	1.3	1.3	1.3	1.3	1.3	1.3	1.3	1.3	1.3	1.3	1.3
Approach LOS												

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HCS7 Two-Way Stop Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	PM Peak + Site	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.75
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	U	L	T	R	L	T	R	L	T	R	L	T	R	
Priority	3U	1	2	3	4U	4	5	6	7	8	9	10	11	12
Number of Lanes	0	0	0	0	0	0	1	0	0	0	1	0	0	0
Configuration	LTR			LTR			LTR			LTR				
Volume (veh/h)	10	10	8	6	368	13			11	8	10	7	10	
Percent Heavy Vehicles (%)	2			2					2	2	2	2	2	
Proportion Time Blocked														
Percent Grade (%)														
Right Turn Channelized														
Median Type | Storage	Unobstructed													

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12			4.12			4.12			4.12			4.12
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2			2.2
Follow-Up Headway (sec)	2.22			2.22			2.22			2.22			2.22

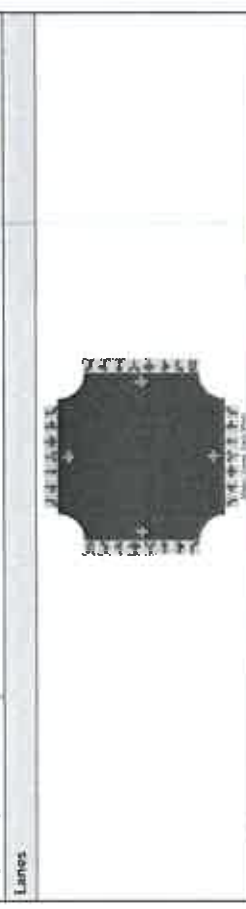
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	11	7	40	40	40	40	40	40	40	40	40	40	40
Capacity, c (veh/h)	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500
v/c Ratio	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.6	0.3	11.3	11.3	11.3	11.3	11.3	11.3	11.3	11.3	11.3	11.3	11.3
Approach LOS	A	B	F	F	F	F	F	F	F	F	F	F	F

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HCS7 Two-Way Stop Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Peak + Site	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	U	L	T	R	L	T	R	L	T	R	L	T	R	
Priority	3U	1	2	3	4U	4	5	6	7	8	9	10	11	12
Number of Lanes	0	0	0	0	0	0	1	0	0	0	1	0	0	0
Configuration	LTR			LTR			LTR			LTR				
Volume (veh/h)	11	10	8	6	368	13			11	8	10	7	10	
Percent Heavy Vehicles (%)	2			2					2	2	2	2	2	
Proportion Time Blocked														
Percent Grade (%)														
Right Turn Channelized														
Median Type | Storage	Unobstructed													

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12			4.12			4.12			4.12			4.12
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2			2.2
Follow-Up Headway (sec)	2.22			2.22			2.22			2.22			2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	13	5	14	14	14	14	14	14	14	14	14	14	14
Capacity, c (veh/h)	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500	1500
v/c Ratio	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.6	7.4	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9	7.9
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	1.3	0.3	11.0	11.0	11.0	11.0	11.0	11.0	11.0	11.0	11.0	11.0	11.0
Approach LOS	A	B	F	F	F	F	F	F	F	F	F	F	F

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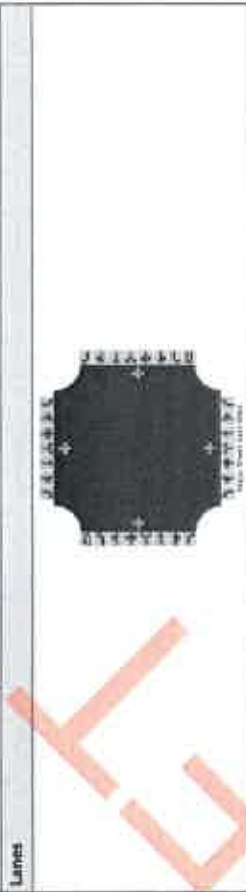
HCS7 Two-Way Stop-Control Right											
General Information						Site Information					
Analyst	JLL	Intersection	Fr St at Russell Ave			Fr St at Russell Ave					
Agency/Co.	CH	Analysis	White Rock			White Rock					
Date Performed	4/10/2019	East/West Street	Russell Avenue			Russell Avenue					
Analysis Year	2027	North/South Street	Fr Street			Fr Street					
Time Analyzed	AM Base + SIB	Peak Hour Factor	0.83			0.87					
Intersection Orientation	East-West	Analysis Time Period (Hrs)	0.25			0.21					
Project Description	7025, 1485 Fr Street TIA										
Vehicle Volumes and Adjustments											
Approach	Southbound		Westbound		Northbound		Southbound				
Movement	U	L	T	R	L	T	R	L	T	R	
Priority	1U	2	3	4	5	6	7	8	9	10	
Number of Lanes	0	0	0	0	0	0	0	0	0	0	
Configuration	LTR		LTR		LTR		LTR				
Volume (veh/h)	12	57	8	107	13	11	4	11	2	17	
Percent Heavy Vehicles (%)	2										
Proportion Time Blocked											
Percent Grade (%)											
Right Turn Channelized	Unidirectional										
Median Type | Storage	Unidirectional										
Critical and Follow-up Headways											
Base Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2	7.1	6.5	
Critical Headway (sec)	4.12	4.12	7.12	6.52	6.22	7.12	6.52	6.22	7.12	6.52	
Base Follow-Up Headway (sec)	2.2	2.2	3.5	4.0	3.3	3.5	4.0	3.3	3.5	4.0	
Follow-Up Headway (sec)	2.22	2.22	3.52	4.02	3.32	3.52	4.02	3.32	3.52	4.02	
Delay, Queue Length, and Level of Service											
Flow Rate, v (veh/h)	14	5	11	11	11	45	45	45	45	45	
Capacity, c (veh/h)	1350	1500	710	710	710	742	742	742	742	742	
v/c Ratio	0.01	0.00	0.02	0.02	0.02	0.06	0.06	0.06	0.06	0.06	
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.1	0.1	0.1	0.2	0.2	0.2	0.2	0.2	
Control Delay (s/veh)	7.6	7.4	10.0	10.0	10.0	10.2	10.2	10.2	10.2	10.2	
Level of Service (LOS)	A	A	B	B	B	B	B	B	B	B	
Approach Delay (s/veh)	1.3	0.3	10.0	10.0	10.0	10.2	10.2	10.2	10.2	10.2	
Approach LOS	B	B	B	B	B	B	B	B	B	B	

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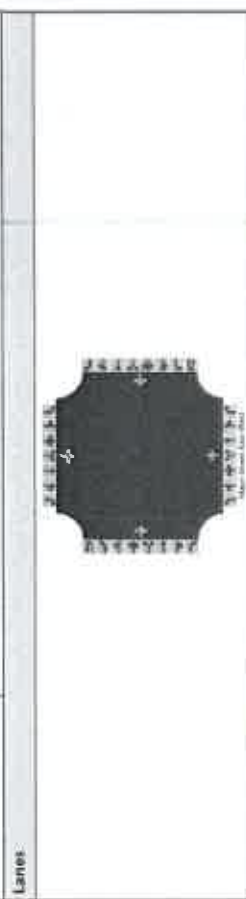
HCS7 Two-Way Stop-Control Right											
General Information						Site Information					
Analyst	JLL	Intersection	Fr St at Russell Ave			Fr St at Russell Ave					
Agency/Co.	CH	Analysis	White Rock			White Rock					
Date Performed	4/10/2019	East/West Street	Russell Avenue			Russell Avenue					
Analysis Year	2027	North/South Street	Fr Street			Fr Street					
Time Analyzed	PM Base + SIB	Peak Hour Factor	0.87			0.87					
Intersection Orientation	East-West	Analysis Time Period (Hrs)	0.21			0.21					
Project Description	7025, 1485 Fr Street TIA										
Vehicle Volumes and Adjustments											
Approach	Eastbound		Westbound		Northbound		Southbound				
Movement	U	L	T	R	L	T	R	L	T	R	
Priority	1U	2	3	4	5	6	7	8	9	10	
Number of Lanes	0	0	0	0	0	0	0	0	0	0	
Configuration	LTR		LTR		LTR		LTR				
Volume (veh/h)	11	140	18	183	14	12	21	1	7	20	
Percent Heavy Vehicles (%)	2										
Proportion Time Blocked											
Percent Grade (%)											
Right Turn Channelized	Unidirectional										
Median Type | Storage	Unidirectional										
Critical and Follow-up Headways											
Base Critical Headway (sec)	4.1	4.1	7.1	6.5	6.2	7.1	6.5	6.2	7.1	6.5	
Critical Headway (sec)	4.12	4.12	7.12	6.52	6.22	7.12	6.52	6.22	7.12	6.52	
Base Follow-Up Headway (sec)	2.2	2.2	3.5	4.0	3.3	3.5	4.0	3.3	3.5	4.0	
Follow-Up Headway (sec)	2.22	2.22	3.52	4.02	3.32	3.52	4.02	3.32	3.52	4.02	
Delay, Queue Length, and Level of Service											
Flow Rate, v (veh/h)	13	7	7	7	7	45	45	45	45	45	
Capacity, c (veh/h)	1245	1315	710	710	710	742	742	742	742	742	
v/c Ratio	0.01	0.01	0.01	0.01	0.01	0.06	0.06	0.06	0.06	0.06	
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.2	0.2	0.2	0.2	0.2	
Control Delay (s/veh)	7.9	7.7	10.2	10.2	10.2	10.2	10.2	10.2	10.2	10.2	
Level of Service (LOS)	A	A	B	B	B	B	B	B	B	B	
Approach Delay (s/veh)	0.6	0.3	10.0	10.0	10.0	10.2	10.2	10.2	10.2	10.2	
Approach LOS	B	B	B	B	B	B	B	B	B	B	

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General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co.	ETS	Approach	White Road
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2019	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1495 Fr Street TIA		



General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co.	ETS	Approach	White Road
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2019	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1495 Fr Street TIA		



Approach	Eastbound				Westbound				Northbound				Southbound			
	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	U
Volume	311	1	2	3	40	4	5	6	7	8	9	10	11	12	13	14
Priority	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0	0
Number of Lanes	LTR				LTR				LTR				LTR			
Configuration	LTR				LTR				LTR				LTR			
Volume (veh/h)	37				38				4				2			
Percent Heavy Vehicles (%)	2				2				2				2			
Proportion Time Blocked	0				0				0				0			
Percent Grade (%)	0															
Right Turn Channelized	Unchannelized															
Median Type | Storage	Unchannelized															

Approach	Eastbound				Westbound				Northbound				Southbound			
	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	U
Volume	311	1	2	3	40	4	5	6	7	8	9	10	11	12	13	14
Priority	0	1	0	0	0	1	0	0	0	1	0	0	0	1	0	0
Number of Lanes	LTR				LTR				LTR				LTR			
Configuration	LTR				LTR				LTR				LTR			
Volume (veh/h)	37				38				4				2			
Percent Heavy Vehicles (%)	2				2				2				2			
Proportion Time Blocked	0				0				0				0			
Percent Grade (%)	0															
Right Turn Channelized	Unchannelized															
Median Type | Storage	Unchannelized															

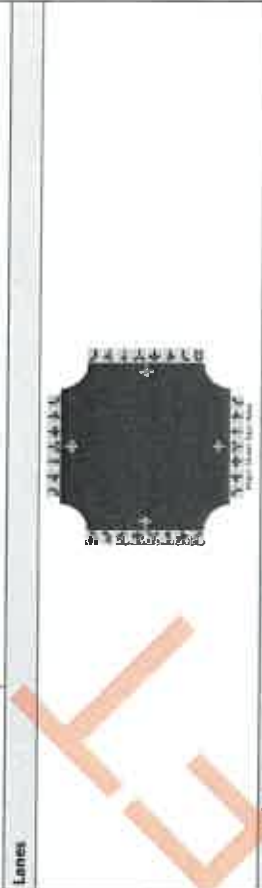
Metric	Eastbound		Westbound		Northbound		Southbound	
	Value	Unit	Value	Unit	Value	Unit	Value	Unit
Base Critical Headway (sec)	4.1		4.1		7.1	6.5	7.1	6.5
Critical Headway (sec)	4.17		4.17		7.17	6.57	7.17	6.57
Base Follow-Up Headway (sec)	2.2		2.2		3.5	4.0	3.5	4.0
Follow-Up Headway (sec)	2.22		2.22		3.52	4.02	3.52	4.02
Flow Rate, v (veh/h)	45		45		45		45	
Capacity, c (veh/h)	465		465		465		465	
v/c Ratio	0.03		0.03		0.03		0.03	
95% Queue Length, Q ₉₅ (veh)	0.0		0.0		0.0		0.0	
Control Delay (s/veh)	7.8		7.8		13.1		13.1	
Level of Service (LOS)	A		A		B		B	
Approach Delay (s/veh)	0.2		0.2		0.3		0.3	
Approach LOS	B		B		B		B	

Metric	Eastbound		Westbound		Northbound		Southbound	
	Value	Unit	Value	Unit	Value	Unit	Value	Unit
Base Critical Headway (sec)	4.1		4.1		7.1	6.5	7.1	6.5
Critical Headway (sec)	4.17		4.17		7.17	6.57	7.17	6.57
Base Follow-Up Headway (sec)	2.2		2.2		3.5	4.0	3.5	4.0
Follow-Up Headway (sec)	2.22		2.22		3.52	4.02	3.52	4.02
Flow Rate, v (veh/h)	45		45		45		45	
Capacity, c (veh/h)	465		465		465		465	
v/c Ratio	0.03		0.03		0.03		0.03	
95% Queue Length, Q ₉₅ (veh)	0.0		0.0		0.0		0.0	
Control Delay (s/veh)	7.8		7.8		13.1		13.1	
Level of Service (LOS)	A		A		B		B	
Approach Delay (s/veh)	0.2		0.2		0.3		0.3	
Approach LOS	B		B		B		B	

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 Fr St at Thrift Ave 201906mmr

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/13/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	L	T	R	L	T	R	L	T	R	L	T	R		
Movement	11	1	2	3	4U	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	1	1	1	1	1	1	1	1	1	1	1	1	1	1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	9	183	89	40	185	5	28	2	15	2	2	2	2	2
Percent Heavy Vehicles (%)	2													
Proportion Time Blocked														
Percent Grade (%)														
Right Turn Channelized														
Median Type | Storage	Undivided													

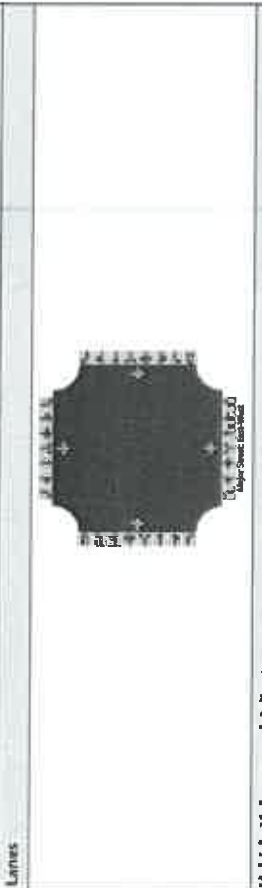
Critical and Follow-up Headways

	Eastbound	Westbound	Northbound	Southbound
Base Critical Headway (sec)	4.1	4.1	7.1	6.5
Critical Headway (sec)	4.1	4.1	7.1	6.5
Base Follow-Up Headway (sec)	2.2	2.2	3.5	4.0
Follow-Up Headway (sec)	2.2	2.2	3.5	4.0

Delay, Queue Length, and Level of Service

	Eastbound	Westbound	Northbound	Southbound
Flow Rate, v (veh/h)	6	9	49	23
Capacity, c (veh/h)	1215	1215	411	490
v/c Ratio	0.00	0.01	0.11	0.06
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.4	0.1
Control Delay (s/veh)	7.9	7.9	14.2	12.7
Level of Service (LOS)	A	A	B	B
Approach Delay (s/veh)	0.2	0.3	14.2	12.7
Approach LOS			B	B

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/13/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	L	T	R	L	T	R	L	T	R	L	T	R		
Movement	11	1	2	3	4U	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	1	1	1	1	1	1	1	1	1	1	1	1	1	1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	9	183	89	40	185	5	28	2	15	2	2	2	2	2
Percent Heavy Vehicles (%)	2													
Proportion Time Blocked														
Percent Grade (%)														
Right Turn Channelized														
Median Type | Storage	Undivided													

Critical and Follow-up Headways

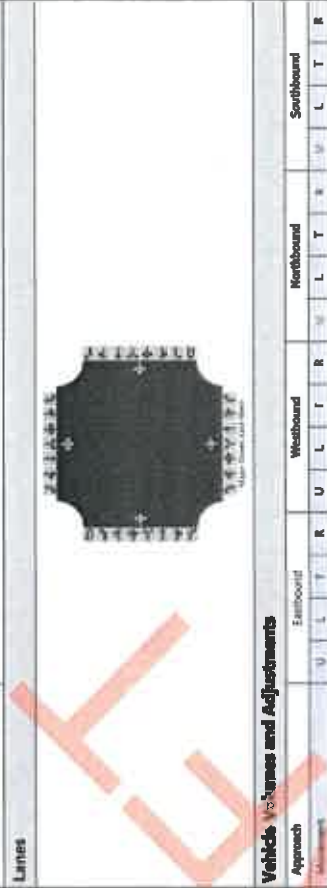
	Eastbound	Westbound	Northbound	Southbound
Base Critical Headway (sec)	4.1	4.1	7.1	6.5
Critical Headway (sec)	4.1	4.1	7.1	6.5
Base Follow-Up Headway (sec)	2.2	2.2	3.5	4.0
Follow-Up Headway (sec)	2.2	2.2	3.5	4.0

Delay, Queue Length, and Level of Service

	Eastbound	Westbound	Northbound	Southbound
Flow Rate, v (veh/h)	7	48	54	8
Capacity, c (veh/h)	1264	1264	464	425
v/c Ratio	0.01	0.04	0.12	0.02
95% Queue Length, Q ₉₅ (veh)	0.0	0.1	0.4	0.1
Control Delay (s/veh)	7.9	7.9	13.8	13.6
Level of Service (LOS)	A	A	B	B
Approach Delay (s/veh)	0.3	1.6	13.8	13.6
Approach LOS			B	B

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Cz	CTK	Jurisdiction	White Rock
Date Performed	4/21/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street-TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	30	1	2	40	4	5	7	8	9	10	11	12
Number of Lanes	3	1	2	4	1	2	3	3	3	3	3	3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	6	187	29	40	195	5	38	3	15	1	9	7
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Unsignalized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	7.1	6.5	6.2	7.1	6.5	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.12	7.12	6.52	6.22	7.12	6.52	6.22	7.12	6.52	6.22
Base Follow-Up Headway (sec)	2.2	3.5	4.0	3.3	3.5	4.0	3.3	3.5	4.0	3.3
Follow-Up Headway (sec)	2.22	3.52	4.02	3.32	3.52	4.02	3.32	3.52	4.02	3.32

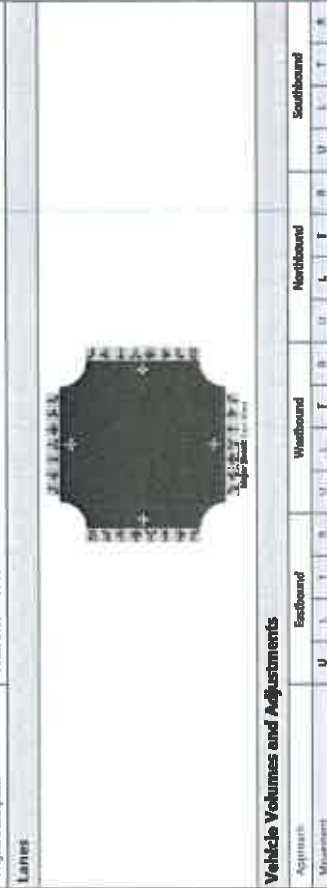
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	7	46	54	54	51	24
Capacity, c (veh/h)	1160	1280	486	486	486	486
v/c Ratio	0.01	0.04	0.12	0.12	0.12	0.05
95% Queue Length, Q ₉₅ (veh)	0.0	0.1	0.4	0.4	0.4	0.2
Control Delay (s/veh)	7.9	7.9	13.9	13.9	14.4	12.9
Level of Service (LOS)	A	A	B	B	B	B
Approach Delay (s/veh)	0.3	1.8	1.9	1.9	14.3	12.9
Approach LOS						

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Cz	CTK	Jurisdiction	White Rock
Date Performed	4/21/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	Fr Street
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Fr Street-TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	30	1	2	40	4	5	7	8	9	10	11	12
Number of Lanes	3	1	2	4	1	2	3	3	3	3	3	3
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	6	187	29	40	195	5	38	3	15	1	9	7
Percent Heavy Vehicles (%)	2	2	2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Unsignalized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	7.1	6.5	6.2	7.1	6.5	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.12	7.12	6.52	6.22	7.12	6.52	6.22	7.12	6.52	6.22
Base Follow-Up Headway (sec)	2.2	3.5	4.0	3.3	3.5	4.0	3.3	3.5	4.0	3.3
Follow-Up Headway (sec)	2.22	3.52	4.02	3.32	3.52	4.02	3.32	3.52	4.02	3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	7	46	54	54	51	24
Capacity, c (veh/h)	1160	1280	486	486	486	486
v/c Ratio	0.01	0.04	0.12	0.12	0.12	0.05
95% Queue Length, Q ₉₅ (veh)	0.0	0.1	0.4	0.4	0.4	0.2
Control Delay (s/veh)	7.9	7.9	13.9	13.9	14.4	12.9
Level of Service (LOS)	A	A	B	B	B	B
Approach Delay (s/veh)	0.3	1.8	1.9	1.9	14.3	12.9
Approach LOS						

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General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co.	CH	Jurisdiction	White Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 Fr Street TIA		



Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Number of Lanes	0	0	1	0	1	0	0	1	0	0	1	0	0	1	0	0
Configuration	LTR				LTR				LTR				LTR			
Volume (veh/h)	6	202	21	43	210	5	31	2	17	2	2	2	2	2	2	2
Percent Heavy Vehicles (%)	2															
Proportion Time Blocked																
Percent Grade (%)																
Right Turn Channelized	Undivided															
Median Type | Storage	Undivided															

Vehicle Volumes and Adjustments

Approach	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

Delay, Queue Length, and Level of Service

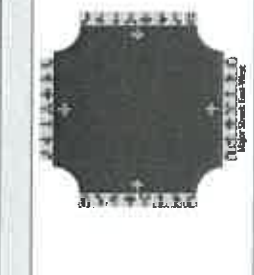
Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)
6	1229	0.00	0.0	7.9	A
1229	1229	1.00	14.3	14.3	B
0.00	0.00	0.00	0.0	0.0	A
0.0	0.0	0.0	0.0	0.0	A
7.9	14.3	0.55	14.3	14.3	B
A	A	A	A	A	A

Approach LOS

Approach	LOS
Eastbound	A
Westbound	A
Northbound	B
Southbound	B

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Fr St at Thrift Ave 2027/am,3w

General Information		Site Information	
Analyst	JLL	Intersection	Fr St at Thrift Ave
Agency/Co.	CH	Jurisdiction	White Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	Fr Street
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 Fr Street TIA		



Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Number of Lanes	0	0	1	0	1	0	0	1	0	0	1	0	0	1	0	0
Configuration	LTR				LTR				LTR				LTR			
Volume (veh/h)	6	202	21	43	210	5	31	2	17	2	2	2	2	2	2	2
Percent Heavy Vehicles (%)	2															
Proportion Time Blocked																
Percent Grade (%)																
Right Turn Channelized	Undivided															
Median Type | Storage	Undivided															

Vehicle Volumes and Adjustments

Approach	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)
7	1241	0.01	0.0	7.9	A
1241	1241	1.00	14.3	14.3	B
0.01	0.01	0.01	0.0	0.0	A
0.0	0.0	0.0	0.0	0.0	A
7.9	14.3	0.55	14.3	14.3	B
A	A	A	A	A	A

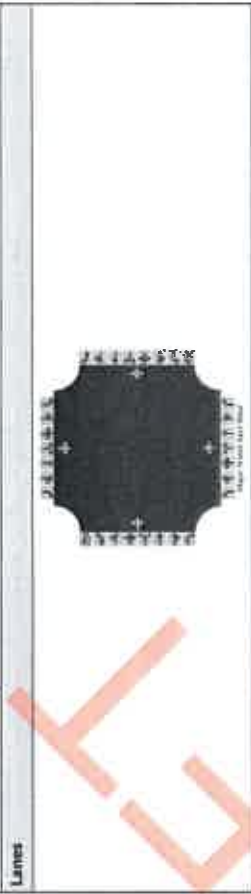
Approach LOS

Approach	LOS
Eastbound	A
Westbound	A
Northbound	B
Southbound	B

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Fr St at Thrift Ave 2027/am,3w

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	FR St at Thrift Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	FR Street
Time Analyzed	AM Base + Site	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1465 FR Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Volume (veh/h)	310	2	3	40	4	5	6	7	8	9	10	11
Percent Heavy Vehicles (%)	0	0	1	0	0	1	0	0	1	0	0	1
Proportion Time Blocked	LTR			LTR			LTR			LTR		
Percent Grade (%)	0			0			0			0		

Critical and Follow-up Headways

Base Critical Headway (sec)	Critical Headway (sec)	Base Follow-Up Headway (sec)	Follow-Up Headway (sec)
4.1	4.12	2.2	2.22

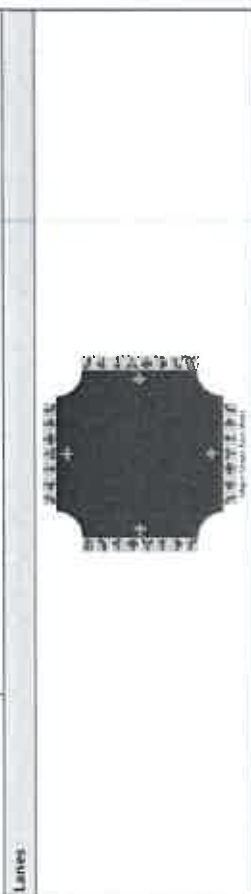
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)	Approach Delay (s/veh)
52	1377	0.04	0.0	7.9	A	0.3
60	429	0.14	0.0	14.8	B	1.5
62	622	0.1	0.0	15.1	B	1.5
6.5	402	0.16	0.0	15.1	C	1.5

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FR St at Thrift Ave 2027/beam.cub

HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	FR St at Thrift Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	FR Street
Time Analyzed	AM Base + Site	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1465 FR Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Volume (veh/h)	310	2	3	40	4	5	6	7	8	9	10	11
Percent Heavy Vehicles (%)	0	0	1	0	0	1	0	0	1	0	0	1
Proportion Time Blocked	LTR			LTR			LTR			LTR		
Percent Grade (%)	0			0			0			0		

Critical and Follow-up Headways

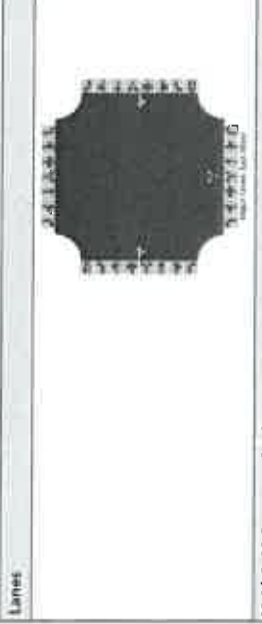
Base Critical Headway (sec)	Critical Headway (sec)	Base Follow-Up Headway (sec)	Follow-Up Headway (sec)
4.1	4.12	2.2	2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)	Approach Delay (s/veh)
52	1377	0.04	0.0	7.9	A	0.3
60	429	0.14	0.0	14.8	B	1.5
62	622	0.1	0.0	15.1	B	1.5
6.5	402	0.16	0.0	15.1	C	1.5

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FR St at Thrift Ave 2027/beam.cub

General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2019	North/South Street	George Lane
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	U	T	R	L	T	R	L	T	R	L	T	R		
Movement	1U	1	2	3	4U	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	TR			LT			UR			LR				
Volume (veh/h)	108	8	3	104	4	2	2	2	4	2	2	2	2	0
Percent Heavy Vehicles (%)	0													
Proportion Time Blocked	0													
Percent Grade (%)	0													
Right Turn Channelized	0													
Median Type | Storage	Unfringed													

Critical and Follow-up Headways

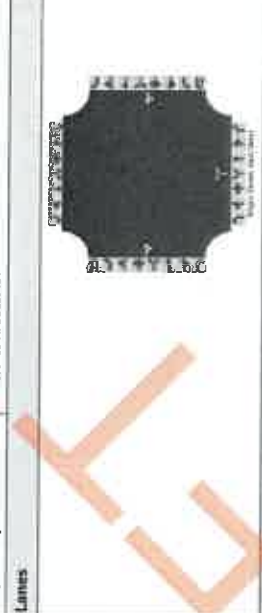
Base Critical Headway (sec)	4.1	6.1	6.2
Critical Headway (sec)	4.12	6.42	6.72
Base Follow-Up Headway (sec)	2.2	3.5	3.3
Follow-Up Headway (sec)	2.22	3.52	3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	6	8
Capacity, c (veh/h)	112	848
v/c Ratio	0.06	0.01
95% Queue Length, Q ₉₅ (veh)	0	0
Control Delay (s/veh)	7.4	0.3
Level of Service (LOS)	F	A
Approach Delay (s/veh)	6.4	9.3
Approach LOS	F	A

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 George Ln at Russell Ave 201904m.vdw
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General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTS	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2019	North/South Street	George Lane
Time Analyzed	PM Base	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025; 1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound				
	U	T	R	L	T	R	L	T	R	L	T	R		
Movement	1U	1	2	3	4U	4	5	6	7	8	9	10	11	12
Priority	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	TR			LT			UR			LR				
Volume (veh/h)	108	8	3	104	4	2	2	2	4	2	2	2	2	0
Percent Heavy Vehicles (%)	0													
Proportion Time Blocked	0													
Percent Grade (%)	0													
Right Turn Channelized	0													
Median Type | Storage	Unfringed													

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	7.1	6.2
Critical Headway (sec)	4.12	7.42	6.22
Base Follow-Up Headway (sec)	2.2	3.5	3.3
Follow-Up Headway (sec)	2.22	3.52	3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	3	15
Capacity, c (veh/h)	145	722
v/c Ratio	0.09	0.02
95% Queue Length, Q ₉₅ (veh)	0	0.8
Control Delay (s/veh)	7.7	10.1
Level of Service (LOS)	F	B
Approach Delay (s/veh)	6.2	10.1
Approach LOS	F	B

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 George Ln at Russell Ave 201904m.vdw
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HCS? Two-Way Stop Control Report												
General Information						Site Information						
Analyst	ALL	Intersection	George Ln at Russell Ave									
Agency/Co	CTL	Jurisdiction	White Oak									
Date Performed	4/10/2019	East/West Street	Russell Avenue									
Analysis Year	2022	North/South Street	George Lane									
Time Analyzed	AM Base	Peak Hour Factor	0.83									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025 1485 Rt Street TIA											
Vehicle Volumes and Adjustments												
Approach	Eastbound		Westbound		Northbound		Southbound					
Measurements	L	T	L	T	L	T	L	T	L	T	R	
Priority	3	0	3	0	0	0	7	8	9	10	11	12
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LR		LR		LR		LR					
Volume (veh/h)	111		111		111		111					
Percent Heavy Vehicles (%)	2		2		2		2					
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unbid											
Median Type | Storage	Unbid											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1		4.1		4.1		4.1					
Critical Headway (sec)	4.12		4.12		4.12		4.12					
Base Follow-Up Headway (sec)	2.2		2.2		2.2		2.2					
Follow-Up Headway (sec)	2.22		2.22		2.22		2.22					
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	7		7		7		7					
Capacity, c (veh/h)	1336		1336		1336		1336					
v/c Ratio	0.00		0.00		0.00		0.00					
95% Queue Length, Q ₉₅ (veh)	0.0		0.0		0.0		0.0					
Control Delay (s/veh)	7.4		7.4		7.4		7.4					
Level of Service (LOS)	A		A		A		A					
Approach Delay (s/veh)	6.4		6.4		6.4		6.4					
Approach LOS	A											

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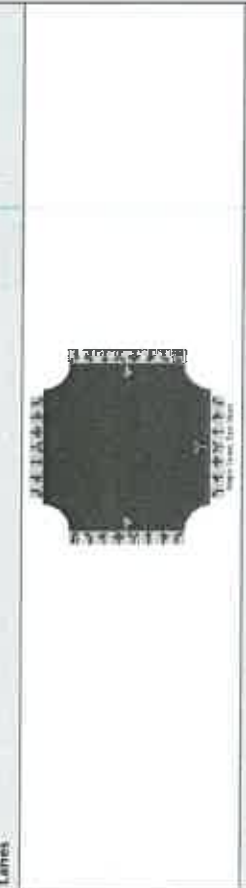
HCS? Two-Way Stop Control Report												
General Information						Site Information						
Analyst	ALL	Intersection	George Ln at Russell Ave									
Agency/Co	CTL	Jurisdiction	White Oak									
Date Performed	4/10/2019	East/West Street	Russell Avenue									
Analysis Year	2022	North/South Street	George Lane									
Time Analyzed	PM Base	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025 1485 Rt Street TIA											
Vehicle Volumes and Adjustments												
Approach	Eastbound		Westbound		Northbound		Southbound					
Measurements	L	T	L	T	L	T	L	T	L	T	R	
Priority	3	0	3	0	0	0	7	8	9	10	11	12
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LR		LR		LR		LR					
Volume (veh/h)	111		111		111		111					
Percent Heavy Vehicles (%)	2		2		2		2					
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unbid											
Median Type | Storage	Unbid											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1		4.1		4.1		4.1					
Critical Headway (sec)	4.12		4.12		4.12		4.12					
Base Follow-Up Headway (sec)	2.2		2.2		2.2		2.2					
Follow-Up Headway (sec)	2.22		2.22		2.22		2.22					
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	7		7		7		7					
Capacity, c (veh/h)	1336		1336		1336		1336					
v/c Ratio	0.00		0.00		0.00		0.00					
95% Queue Length, Q ₉₅ (veh)	0.0		0.0		0.0		0.0					
Control Delay (s/veh)	7.4		7.4		7.4		7.4					
Level of Service (LOS)	A		A		A		A					
Approach Delay (s/veh)	6.4		6.4		6.4		6.4					
Approach LOS	A											

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 George Ln at Russell Ave 202204m.dwg
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HCS7 Two-Way Stop-Control Report

General Information

Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTL	Jurisdiction	White Rock
Date Performed	4/19/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.85
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025 1465 Ft Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4U	4	5	6	7	8	9	10	11
Number of Lanes	1	1	1	0	0	0	0	1	0	0	0	0
Configuration	UR											
Volume (veh/h)	56	13	1	1	11	11	10	11	10	10	10	10
Percent Heavy Vehicles (%)	0											
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

Delay, Queue Length, and Level of Service

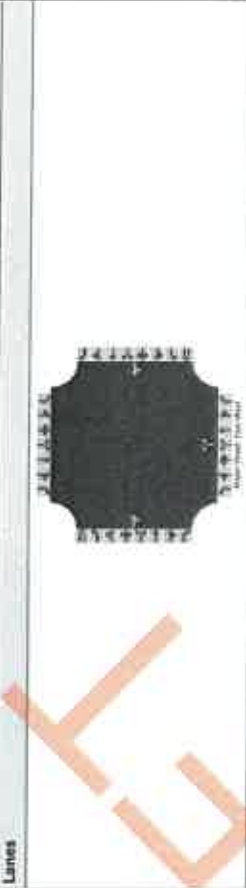
Flow Rate, v (veh/h)	8	8	8	8	8	8	8	8	8	8	8	8
Capacity, c (veh/h)	1468	1468	1468	1468	1468	1468	1468	1468	1468	1468	1468	1468
v/c Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4	7.4
Level of Service (LOS)	A											
Approach Delay (s/veh)	0.5											
Approach LOS	A											

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 George Ln at Russell Ave 2022bprmsrwb
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HCS7 Two-Way Stop-Control Report

General Information

Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTL	Jurisdiction	White Rock
Date Performed	4/19/2019	East/West Street	Russell Avenue
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	PM Base + Site	Peak Hour Factor	0.87
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025 1465 Ft Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4U	4	5	6	7	8	9	10	11
Number of Lanes	0	0	0	0	0	0	0	1	0	0	0	0
Configuration	TR											
Volume (veh/h)	11	11	11	11	11	11	11	11	11	11	11	11
Percent Heavy Vehicles (%)	0											
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											

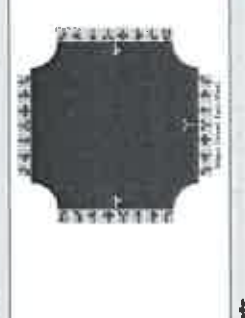
Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22


Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	11	11	11	11	11	11	11	11	11	11	11	11
Capacity, c (veh/h)	1321	1321	1321	1321	1321	1321	1321	1321	1321	1321	1321	1321
v/c Ratio	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01	0.01
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7	7.7
Level of Service (LOS)	B											
Approach Delay (s/veh)	0.5											
Approach LOS	B											

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 George Ln at Russell Ave 2022bprmsrwb
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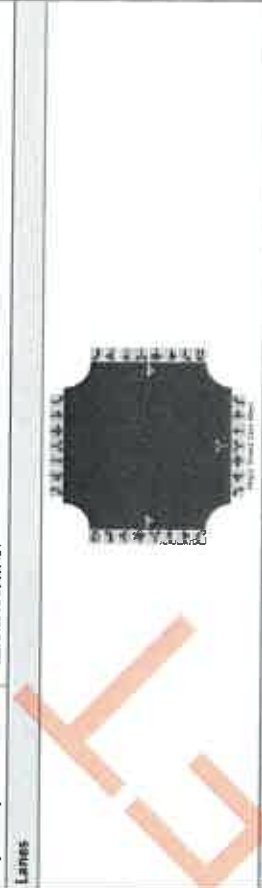
HCS7 Two-Way Stop-Control Report												
General Information						Site Information						
Analyst	ALL	Intersection	George Ln at Russell Ave									
Agency/Co	CS	Jurisdiction	White Rock									
Date Performed	4/10/2019	East/West Street	Russell Avenue									
Analysis Year	2019	North/South Street	George Lane									
Time Analyzed	AM Base	Peak Hour Factor	0.83									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025-1485 Fr Street TIA											
												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Movement	L	T	R	L	T	R	L	T	R	L	T	R
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	1	1	1	0	1	0	0	1	0	0	1	0
Configuration	TR			LT			LR			LR		
Volume (veh/h)	10	0	0	0	121	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0	0	0	0	0	0	0	0	0	0	0	0
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0											
Right Turn Channelized	No											
Median Type | Storage	Unobstructed											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			4.1			4.1		
Critical Headway (sec)	4.12			4.12			4.12			4.12		
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2		
Follow-Up Headway (sec)	2.21			2.21			2.21			2.21		
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	7			7			7			7		
Capacity, c (veh/h)	1000			1000			1000			1000		
v/c Ratio	0.00			0.00			0.00			0.00		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	7.4			7.4			7.4			7.4		
Level of Service (LOS)	A			A			A			A		
Approach Delay (s/veh)	0.4			0.4			0.4			0.4		
Approach LOS	A			A			A			A		

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 George Ln at Russell Ave 2027/turntab
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HCS7 Two-Way Stop-Control Report												
General Information						Site Information						
Analyst	ALL	Intersection	George Ln at Russell Ave									
Agency/Co	CS	Jurisdiction	White Rock									
Date Performed	4/10/2019	East/West Street	Russell Avenue									
Analysis Year	2019	North/South Street	George Lane									
Time Analyzed	PM Base	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025-1485 Fr Street TIA											
												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Movement	L	T	R	L	T	R	L	T	R	L	T	R
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	1	1	1	0	0	1	0	1	0	0	1	0
Configuration	TR			LT			LR			LR		
Volume (veh/h)	10	0	0	0	0	203	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0	0	0	0	0	0	0	0	0	0	0	0
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0											
Right Turn Channelized	No											
Median Type | Storage	Unobstructed											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			4.1			4.1		
Critical Headway (sec)	4.12			4.12			4.12			4.12		
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2		
Follow-Up Headway (sec)	2.21			2.21			2.21			2.21		
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	5			5			5			5		
Capacity, c (veh/h)	1000			1000			1000			1000		
v/c Ratio	0.00			0.00			0.00			0.00		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	7.7			7.7			7.7			7.7		
Level of Service (LOS)	A			A			A			A		
Approach Delay (s/veh)	0.2			0.2			0.2			0.2		
Approach LOS	A			A			A			A		

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 George Ln at Russell Ave 2027/turntab
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General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTE	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	702S 148S Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Volume (veh/h)	44	18	7	121	11	10	2	2	2	2	2	2
Percent Heavy Vehicles (%)												
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Undivided											

Critical and Follow-up Headways

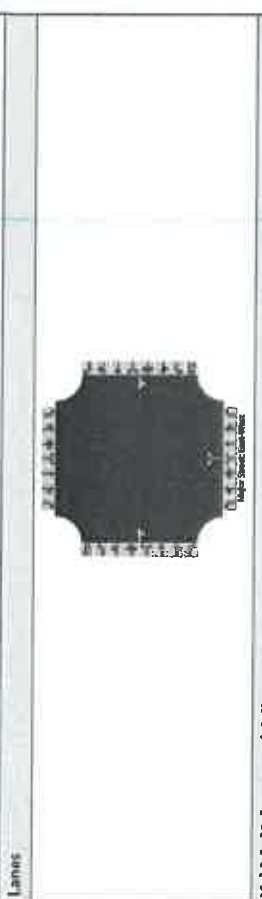
Base Critical Headway (sec)	Critical Headway (sec)	Base Follow-Up Headway (sec)	Follow-Up Headway (sec)
4.1	4.1	2.2	2.2
6.2	6.2	3.5	3.5

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)	Approach Delay (s/veh)
8	1469	0.01	0.0	7.4	A	0.4
25	1025	0.03	0.1	9.5	A	0.5
10.8	645	0.04	0.1	11.0	B	10.8

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 George Ln at Russell Ave 2027bomnator
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General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Russell Ave
Agency/Co.	CTE	Jurisdiction	White Rock
Date Performed	4/10/2019	East/West Street	Russell Avenue
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	702S 148S Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Volume (veh/h)	44	18	7	121	11	10	2	2	2	2	2	2
Percent Heavy Vehicles (%)												
Proportion Time Blocked												
Percent Grade (%)												
Right Turn Channelized												
Median Type | Storage	Undivided											

Critical and Follow-up Headways

Base Critical Headway (sec)	Critical Headway (sec)	Base Follow-Up Headway (sec)	Follow-Up Headway (sec)
4.1	4.1	2.2	2.2
6.2	6.2	3.5	3.5

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	Capacity, c (veh/h)	v/c Ratio	95% Queue Length, Q ₉₅ (veh)	Control Delay (s/veh)	Level of Service (LOS)	Approach Delay (s/veh)
8	1469	0.01	0.0	7.4	A	0.4
25	1025	0.03	0.1	9.5	A	0.5
10.8	645	0.04	0.1	11.0	B	10.8

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 George Ln at Russell Ave 2027bomnator
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HCS/Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JL	Intersection	George Ln at Site Access
Agency/Co.	City	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Site Access
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.87
Intersection Orientation	North-South	Analysis Time Period (hr)	0.25
Project Description	7025-1485 Fr Street TIA		

HCS/Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JL	Intersection	George Ln at Site Access
Agency/Co.	City	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Site Access
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.87
Intersection Orientation	North-South	Analysis Time Period (hr)	0.25
Project Description	7025-1485 Fr Street TIA		

Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	R	T	U	L	R	T	U	L	T	R	U	L	T	R
Volume	10	31	12	0	7	9	30	1	2	3	40	6	6	6	1	0
Percent Heavy Vehicles (%)																
Proportion Time Blocked																
Percent Grade (%)																
Right Turn Channelized																
Median Type | Storage	Unfielded															

Critical and Follow-up Headways

Base Critical Headway (sec)	7.1	6.2
Critical Headway (sec)	6.42	5.27
Base Follow-Up Headway (sec)	3.5	3.3
Follow-Up Headway (sec)	3.27	3.17

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	28	16
Capacity, c (veh/h)	1015	1024
v/c Ratio	0.03	0.02
95% Queue Length, Q ₉₅ (veh)	0.1	0.0
Control Delay (s/veh)	4.1	2.4
Level of Service (LOS)	A	A
Approach Delay (s/veh)	4.1	2.4

Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	R	T	U	L	R	T	U	L	T	R	U	L	T	R
Volume	10	31	12	0	7	9	30	1	2	3	40	6	6	6	1	0
Percent Heavy Vehicles (%)																
Proportion Time Blocked																
Percent Grade (%)																
Right Turn Channelized																
Median Type | Storage	Unfielded															

Critical and Follow-up Headways

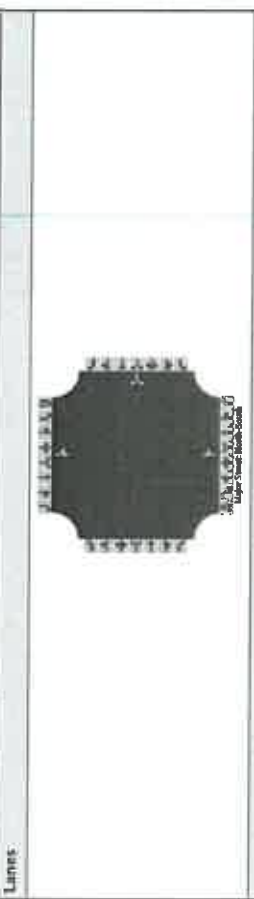
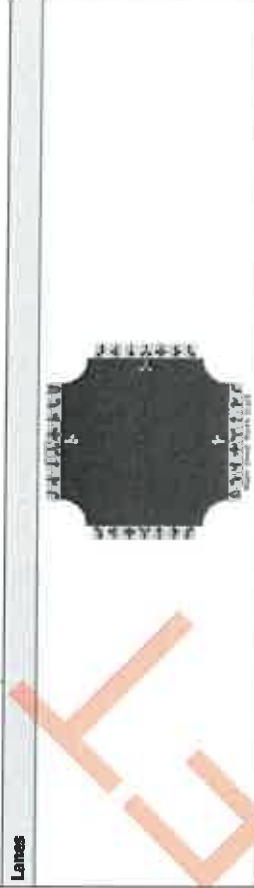
Base Critical Headway (sec)	7.1	6.2
Critical Headway (sec)	6.42	5.27
Base Follow-Up Headway (sec)	3.5	3.3
Follow-Up Headway (sec)	3.27	3.17

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	28	16
Capacity, c (veh/h)	1015	1024
v/c Ratio	0.03	0.02
95% Queue Length, Q ₉₅ (veh)	0.1	0.0
Control Delay (s/veh)	4.1	2.4
Level of Service (LOS)	A	A
Approach Delay (s/veh)	4.1	2.4

General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Site Access
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Site Access
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	702E: 1485 Fir Street TIA		

General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Site Access
Agency/Co	CTS	Jurisdiction	White Rock
Date Performed	4/12/2019	East/West Street	Site Access
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM Base + Site	Peak Hour Factor	0.83
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	702E: 1485 Fir Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement	10	11	12	7	8	9	10	11	12	13	14	15
Priority	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LR			LR			LR			LR		
Volume (veh/h)	0	0	0	0	0	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0			0			0			0		
Proportion Time Blocked	0			0			0			0		
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	Unbid			Unbid			Unbid			Unbid		
Median Type | Storage	Unbid			Unbid			Unbid			Unbid		

Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement	10	11	12	7	8	9	10	11	12	13	14	15
Priority	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LR			LR			LR			LR		
Volume (veh/h)	0	0	0	0	0	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0			0			0			0		
Proportion Time Blocked	0			0			0			0		
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	Unbid			Unbid			Unbid			Unbid		
Median Type | Storage	Unbid			Unbid			Unbid			Unbid		

Critical and Follow-up Headways

Base Critical Headway (sec)	7.1	6.2	6.2	6.2	6.2
Critical Headway (sec)	8.42	6.22	6.22	6.22	6.22
Base Follow-Up Headway (sec)	3.3	3.3	3.3	3.3	3.3
Follow-Up Headway (sec)	3.32	3.32	3.32	3.32	3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	26	16	16	16	16
Capacity, c (veh/h) <td>1048</td> <td>1048</td> <td>1048</td> <td>1048</td> <td>1048</td>	1048	1048	1048	1048	1048
v/c Ratio	0.025	0.015	0.015	0.015	0.015
95% Queue Length, Q ₉₅ (veh)	0	0	0	0	0
Control Delay (s/veh)	0	0	0	0	0
Level of Service (LOS)	A	A	A	A	A
Approach Delay (s/veh)	8.5	8.6	8.6	8.6	8.6
Approach LOS	A	A	A	A	A

Critical and Follow-up Headways

Base Critical Headway (sec)	7.1	6.2	6.2	6.2	6.2
Critical Headway (sec)	8.42	6.22	6.22	6.22	6.22
Base Follow-Up Headway (sec)	3.3	3.3	3.3	3.3	3.3
Follow-Up Headway (sec)	3.32	3.32	3.32	3.32	3.32

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	26	16	16	16	16
Capacity, c (veh/h)	1048	1048	1048	1048	1048
v/c Ratio	0.025	0.015	0.015	0.015	0.015
95% Queue Length, Q ₉₅ (veh)	0	0	0	0	0
Control Delay (s/veh)	0	0	0	0	0
Level of Service (LOS)	A	A	A	A	A
Approach Delay (s/veh)	8.5	8.6	8.6	8.6	8.6
Approach LOS	A	A	A	A	A

HCS Two-Way Stop Control Report												
General Information						Site Information						
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency/Co.	CH	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2019	North/South Street	George Lane									
Time Analyzed	AM Base	Peak Hour Factor	0.83									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025; 1485 Fr Street TIA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Measurements	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	0	0	1	0	0	1	0	1	0	0	0	0
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	1	189	1	2	262	3	4	2	5	3	0	3
Percent Heavy Vehicles (%)	2		2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked												
Percent Grade (%)	0											
Right Turn Channelized	Undeclared											
Median Type | Storage	Undeclared											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			7.1	6.3	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.12			4.12			7.12	6.32	6.22	7.12	6.52	6.22
Base Follow-Up Headway (sec)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
Follow-Up Headway (sec)	2.22			2.22			3.52	4.02	3.32	3.52	4.02	3.32
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	1			2			19			6		
Capacity, c (veh/h)	1256			1294			1204			689		
v/c Ratio	0.00			0.00			0.00			0.01		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	7.9			7.7			11.8			11.8		
Level of Service (LOS)	A			A			B			B		
Approach Delay (s/veh)	0.0			0.1			11.4			11.8		
Approach LOS				B			B			B		

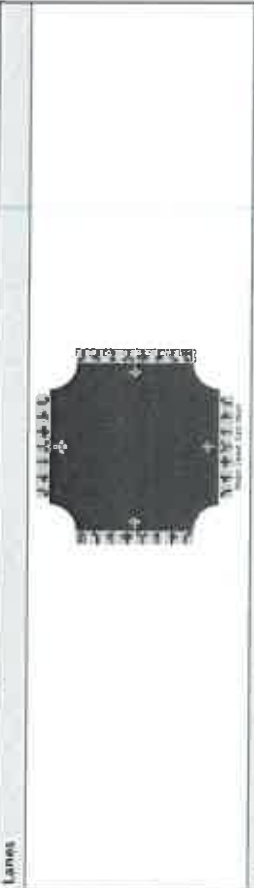
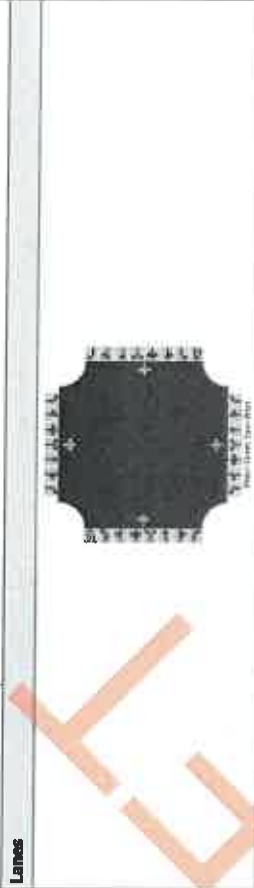
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HCS Two-Way Stop Control Report												
General Information						Site Information						
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency/Co.	CH	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2019	North/South Street	George Lane									
Time Analyzed	PM Base	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025; 1485 Fr Street TIA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
Measurements	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	0	0	1	0	0	1	0	1	0	0	0	0
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	1	229	1	2	292	3	4	2	2	3	0	3
Percent Heavy Vehicles (%)	2		2	2	2	2	2	2	2	2	2	2
Proportion Time Blocked												
Percent Grade (%)	0											
Right Turn Channelized	Undeclared											
Median Type | Storage	Undeclared											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
Critical Headway (sec)	4.12			4.12			7.12	6.52	6.22	7.12	6.52	6.22
Base Follow-Up Headway (sec)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
Follow-Up Headway (sec)	2.22			2.22			3.52	4.02	3.32	3.52	4.02	3.32
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	6			2			6			6		
Capacity, c (veh/h)	1294			1204			1204			689		
v/c Ratio	0.00			0.00			0.00			0.01		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	8.0			7.8			12.5			10.2		
Level of Service (LOS)	A			A			B			B		
Approach Delay (s/veh)	0.2			0.1			12.5			10.2		
Approach LOS				B			B			B		

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General Information		Site Information	
Analyst	ILL	Intersection	George Ln at Thrift Ave
Agency/Co.	CFE	Jurisdiction	White Rock
Date Performed	4/17/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 Fr Street TIA		

General Information		Site Information	
Analyst	ILL	Intersection	George Ln at Thrift Ave
Agency/Co.	CFE	Jurisdiction	White Rock
Date Performed	4/17/2019	East/West Street	Thrift Avenue
Analysis Year	2022	North/South Street	George Lane
Time Analyzed	AM Base	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025, 1485 Fr Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	10	1	2	40	4	5	6	7	8	9	10	11
Priority	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	0	0	0	0	0	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0	0	0	0	0	0	0	0	0	0	0	0
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0	0	0	0	0	0	0	0	0	0	0	0
Right Turn Channelized	Undivided											
Median Type | Storage	Undivided											

Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Movement	10	1	2	40	4	5	6	7	8	9	10	11
Priority	0	0	0	0	0	0	0	0	0	0	0	0
Number of Lanes	0	0	0	0	0	0	0	0	0	0	0	0
Configuration	LTR			LTR			LTR			LTR		
Volume (veh/h)	0	0	0	0	0	0	0	0	0	0	0	0
Percent Heavy Vehicles (%)	0	0	0	0	0	0	0	0	0	0	0	0
Proportion Time Blocked	0	0	0	0	0	0	0	0	0	0	0	0
Percent Grade (%)	0	0	0	0	0	0	0	0	0	0	0	0
Right Turn Channelized	Undivided											
Median Type | Storage	Undivided											

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1
Critical Headway (sec)	4.12
Base Follow-Up Headway (sec)	2.2
Follow-Up Headway (sec)	2.22

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1
Critical Headway (sec)	4.12
Base Follow-Up Headway (sec)	2.2
Follow-Up Headway (sec)	2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	1187
Capacity, c (veh/h) <td>0.01</td>	0.01
v/c Ratio <td>0.0</td>	0.0
95% Queue Length, Q ₉₅ (veh) <td>0.1</td>	0.1
Control Delay (s/veh) <td>7.9</td>	7.9
Level of Service (LOS) <td>A</td>	A
Approach Delay (s/veh) <td>0.1</td>	0.1
Approach LOS <td>B</td>	B

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	17
Capacity, c (veh/h) <td>539</td>	539
v/c Ratio <td>0.03</td>	0.03
95% Queue Length, Q ₉₅ (veh) <td>0.1</td>	0.1
Control Delay (s/veh) <td>11.9</td>	11.9
Level of Service (LOS) <td>A</td>	A
Approach Delay (s/veh) <td>0.1</td>	0.1
Approach LOS <td>B</td>	B

HCS7 Two-Way Stop-Control Report												
General Information			Site Information									
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency	CTS	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2022	North/South Street	George Lane									
Time Analyzed	AM Peak + Site	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (h:m)	0.25									
Project Description	7025; 1485 Ft Street TIA											
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Configuration	LTR											
Volume (veh/h)	7	201	2	1	117	5	3	6	4	4	4	4
Percent Heavy Vehicles (%)	0											
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unhibited											
Median Type | Storage	Unhibited											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	2	4	17	17	17	17	17	17	17	17	17	17
Capacity, c (veh/h)	225	177	177	177	177	177	177	177	177	177	177	177
v/c Ratio	0.009	0.023	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	7.9	7.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9
Level of Service (LOS)	A	A	B	B	B	B	B	B	B	B	B	B
Approach Delay (s/veh)	0.1	0.1	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9
Approach LOS	B											

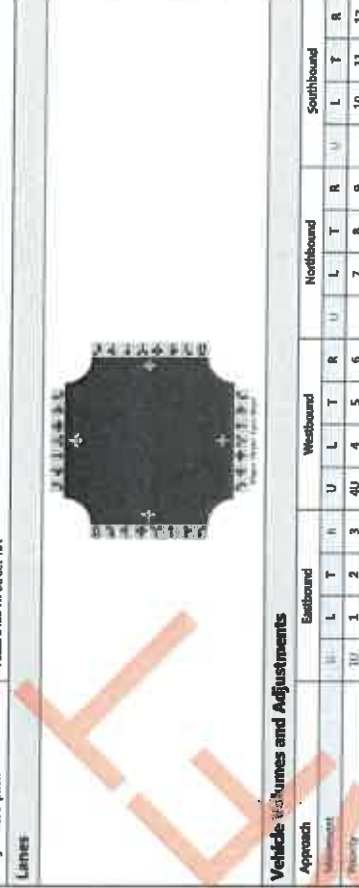
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 George Ln at Thrift Ave 2022sum.sab
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HCS7 Two-Way Stop-Control Report												
General Information			Site Information									
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency	CTS	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2022	North/South Street	George Lane									
Time Analyzed	PM Peak + Site	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (h:m)	0.25									
Project Description	7025; 1485 Ft Street TIA											
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
	U	L	T	U	L	T	U	L	T	U	L	T
Priority	1	2	3	4	5	6	7	8	9	10	11	12
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Configuration	LTR											
Volume (veh/h)	7	201	2	1	117	5	3	6	4	4	4	4
Percent Heavy Vehicles (%)	0											
Proportion Time Blocked	0											
Percent Grade (%)	0											
Right Turn Channelized	Unhibited											
Median Type | Storage	Unhibited											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	2	4	17	17	17	17	17	17	17	17	17	17
Capacity, c (veh/h)	225	177	177	177	177	177	177	177	177	177	177	177
v/c Ratio	0.009	0.023	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096	0.096
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Control Delay (s/veh)	7.9	7.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9	11.9
Level of Service (LOS)	A	A	B	B	B	B	B	B	B	B	B	B
Approach Delay (s/veh)	0.1	0.1	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9	12.9
Approach LOS	B											

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Thrift Ave
Agency	CTS	Jurisdiction	Wilab Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM EST	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Ft Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound					
	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1	2	3	4U	4	5	6	7	8	9	10	11	12		
Number of Lanes	1	1	1	1	1	1	1	0	1	0	1	0	1	0	1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	2	2	2	3	2	2	2	2	2	2	2	2	2	2	2
Percent Heavy Vehicles (%)															
Proportion Time Blocked															
Percent Grade (%)															
Right Turn Channelized															
Median Type | Storage	Unbid														

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

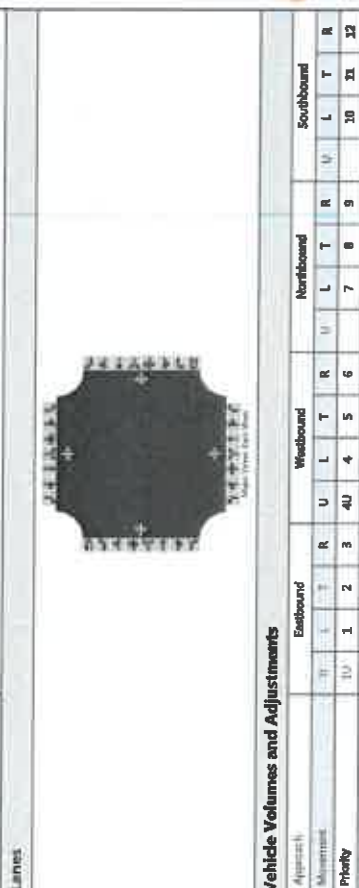
Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	2	4	4	4	4	4	4	4	4	4	4	4	4	4	4
Capacity, c (veh/h)	1214	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272
v/c Ratio	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	8.0	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Approach LOS	B	B	B	B	B	B	B	B	B	B	B	B	B	B	B

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HCS7 Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JLL	Intersection	George Ln at Thrift Ave
Agency	CTS	Jurisdiction	Wilab Rock
Date Performed	4/11/2019	East/West Street	Thrift Avenue
Analysis Year	2027	North/South Street	George Lane
Time Analyzed	AM EST	Peak Hour Factor	0.83
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	7025-1485 Ft Street TIA		



Vehicle Volumes and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound					
	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1	2	3	4U	4	5	6	7	8	9	10	11	12		
Number of Lanes	1	1	1	1	1	1	1	0	1	0	1	0	1	0	1
Configuration	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR	LTR
Volume (veh/h)	2	2	2	3	2	2	2	2	2	2	2	2	2	2	2
Percent Heavy Vehicles (%)															
Proportion Time Blocked															
Percent Grade (%)															
Right Turn Channelized															
Median Type Storage	Unbid														

Critical and Follow-up Headways

Base Critical Headway (sec)	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1	4.1
Critical Headway (sec)	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12	4.12
Base Follow-Up Headway (sec)	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2	2.2
Follow-Up Headway (sec)	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22	2.22

Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)	2	4	4	4	4	4	4	4	4	4	4	4	4	4	4
Capacity, c (veh/h)	1214	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272	1272
v/c Ratio	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
95% Queue Length, Q ₉₅ (veh)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (s/veh)	8.0	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8	7.8
Level of Service (LOS)	A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
Approach Delay (s/veh)	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1	0.1
Approach LOS	B	B	B	B	B	B	B	B	B	B	B	B	B	B	B

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HCS/Two-Way-Stop-Control-Report												
General Information			Site Information									
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency/Co.	CTS	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2027	North/South Street	George Lane									
Time Analyzed	AM Run + Site	Peak Hour Factor	0.83									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025 1485 Fr Street TA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
U	L	T	R	U	L	T	R	U	L	T	R	
10	1	2	1	4	5	6	7	8	9	10	11	
0	0	0	0	0	0	0	0	0	0	0	0	
LTR												
Volume (veh/h)	237			237			237			237		
Percent Heavy Vehicles (%)	2			2			2			2		
Proportion Time Blocked	0			0			0			0		
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			4.1			4.1		
Critical Headway (sec)	4.12			4.12			4.12			4.12		
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2		
Follow-Up Headway (sec)	2.22			2.22			2.22			2.22		
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	2			2			2			2		
Capacity, c (veh/h)	1777			1777			1777			1777		
v/c Ratio	0.00			0.00			0.00			0.00		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	7.8			7.8			7.8			7.8		
Level of Service (LOS)	A			A			A			A		
Approach Delay (s/veh)	0.1			0.1			0.1			0.1		
Approach LOS	A			A			A			A		

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HCS/Two-Way-Stop-Control-Report												
General Information			Site Information									
Analyst	JL	Intersection	George Ln at Thrift Ave									
Agency/Co.	CTS	Jurisdiction	White Rock									
Date Performed	4/11/2019	East/West Street	Thrift Avenue									
Analysis Year	2027	North/South Street	George Lane									
Time Analyzed	PM Run + Site	Peak Hour Factor	0.87									
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25									
Project Description	7025 1485 Fr Street TA											
Lanes												
Vehicle Volumes and Adjustments												
Approach	Eastbound			Westbound			Northbound			Southbound		
U	L	T	R	U	L	T	R	U	L	T	R	
10	1	2	1	4	5	6	7	8	9	10	11	
0	0	0	0	0	0	0	0	0	0	0	0	
LTR												
Volume (veh/h)	265			265			265			265		
Percent Heavy Vehicles (%)	2			2			2			2		
Proportion Time Blocked	0			0			0			0		
Percent Grade (%)	0			0			0			0		
Right Turn Channelized	Unchannelized											
Median Type | Storage	Unchannelized											
Critical and Follow-up Headways												
Base Critical Headway (sec)	4.1			4.1			4.1			4.1		
Critical Headway (sec)	4.11			4.11			4.11			4.11		
Base Follow-Up Headway (sec)	2.2			2.2			2.2			2.2		
Follow-Up Headway (sec)	2.22			2.22			2.22			2.22		
Delay, Queue Length, and Level of Service												
Flow Rate, v (veh/h)	2			2			2			2		
Capacity, c (veh/h)	1777			1777			1777			1777		
v/c Ratio	0.00			0.00			0.00			0.00		
95% Queue Length, Q ₉₅ (veh)	0.0			0.0			0.0			0.0		
Control Delay (s/veh)	8.1			8.1			8.1			8.1		
Level of Service (LOS)	A			A			A			A		
Approach Delay (s/veh)	0.3			0.1			0.1			0.1		
Approach LOS	A			B			B			B		

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THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: October 19, 2020

TO: Land Use and Planning Committee

FROM: Carl Isaak, Director, Planning and Development Services

SUBJECT: Consideration of New City Hall and Theatre/Performance Facility in Proposed Town Centre Development Project

RECOMMENDATION

THAT the Land Use and Planning Committee provide an opportunity at this meeting for the owner of 1513 Johnston Road to discuss with the Committee their intentions to redevelop their property, including the feasibility of incorporating a new City Hall facility with public amenities into the redevelopment.

EXECUTIVE SUMMARY

Council has previously discussed options regarding the replacement or renovation of the existing City Hall building, which has been noted to have seismic (life/safety) issues, accessibility deficiencies, and space constraints, among other issues common to aging structures. The options presented to Council have focused on City Hall remaining within the present civic precinct area, which includes the White Rock Library, Evergreen Childcare building, City Hall Annex, and the RCMP and Fire Services buildings.

An alternate approach to renewing City Hall, and potentially freeing up space in the existing civic precinct for alternate uses, would be to develop or acquire space for the City Hall at another location. The notion of relocating City Hall in the Town Centre, specifically, is in the Official Community Plan (OCP) policy 9.2.2: "Explore the possibility of relocating City Hall to the Town Centre, establishing a new civic centre with other potential civic facilities."

The property owner of 1513 Johnston Road has approached staff inquiring about the redevelopment of their property, indicating that they intend to submit an application under the current Zoning Bylaw provisions in the form of a mixed-use tower development, which include bonus density where a community amenity contribution is provided. Under the current maximum density in the Zoning Bylaw (5.4 gross floor area times the lot area – FAR) and the related provisions in the Council Policy 511: Density Bonus and Community Amenity Contribution Policy, a property of this size built to a maximum density would generate an amenity value of approximately \$5.5M. Spaces that are City owned and operated are not included in the maximum FAR and are therefore not double-counted in determining the targeted amenity contribution. The owner is also aware that a potential outcome of the Town Centre OCP Review is that the development potential (e.g. permitted maximum height and density) may be reduced. Other recommendations, such as establishing minimum parkade setbacks and minimum areas of soil permeability may also have the effect of requiring deeper excavations to meet parking requirements.

Given the relatively small size of the Town Centre area and the limited number of opportunities to co-locate new public facilities such as a City Hall in close proximity to the existing White Rock Community Centre, staff requested that the property owner consider whether they would incorporate space for a new City Hall within their redevelopment as an on-site amenity contribution. The property owner is open to this approach to an amenity contribution and has worked with City staff to produce schematic drawings of how it could be configured, for discussion purposes. These drawings, inclusive of a Council Chambers space that could also serve as a theatre/performance facility, and a lobby/galleria area that could potentially host events outside of office hours, are included as Appendix A to this corporate report. A ground level site plan, second level floor plan (containing City Hall space), and building elevation drawings are included in the figures below for reference.

Figure 1: Ground Level Site Plan (dated October 13, 2020)

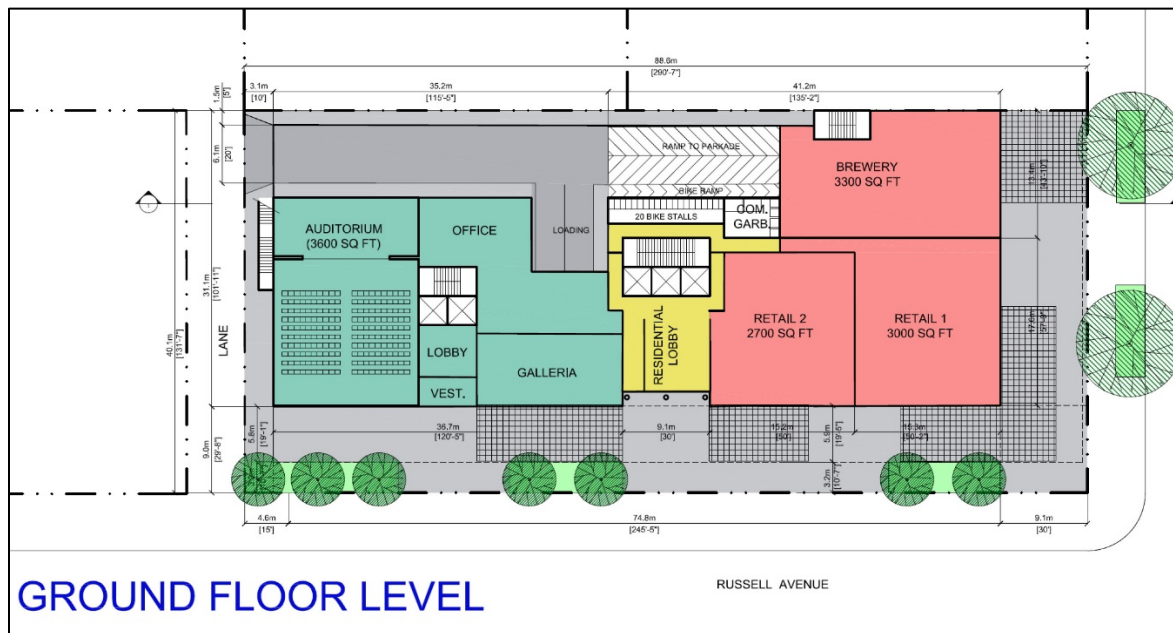


Figure 2: Second Level Floor Plan (dated October 13, 2020)

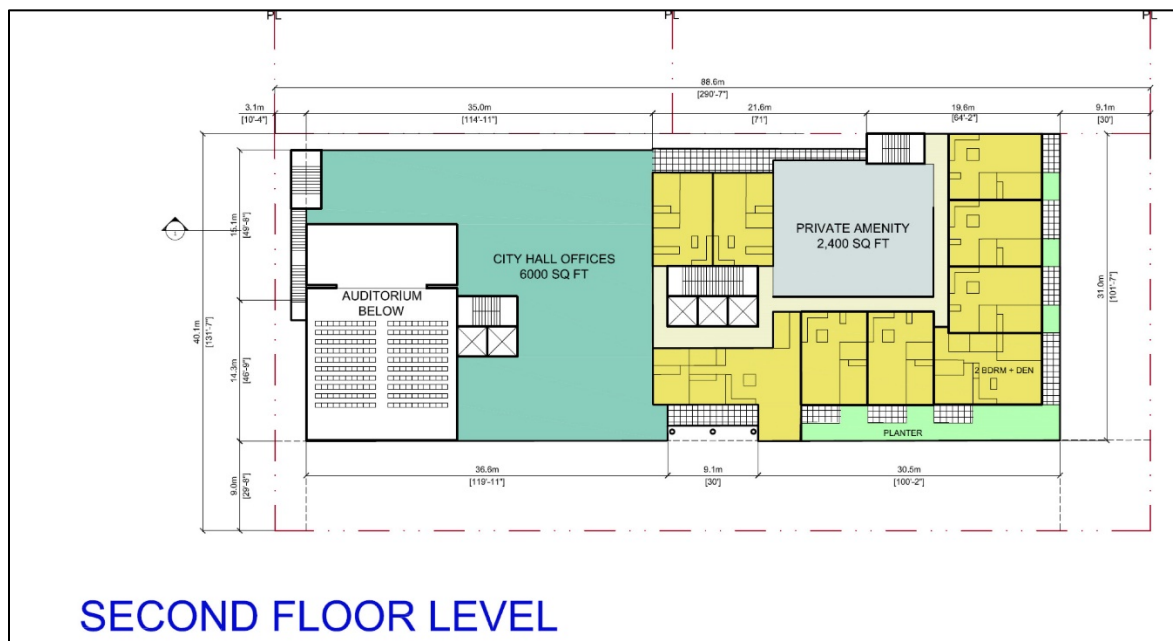


Figure 3: Building Section Illustrating Uses (from south) (dated October 13, 2020)

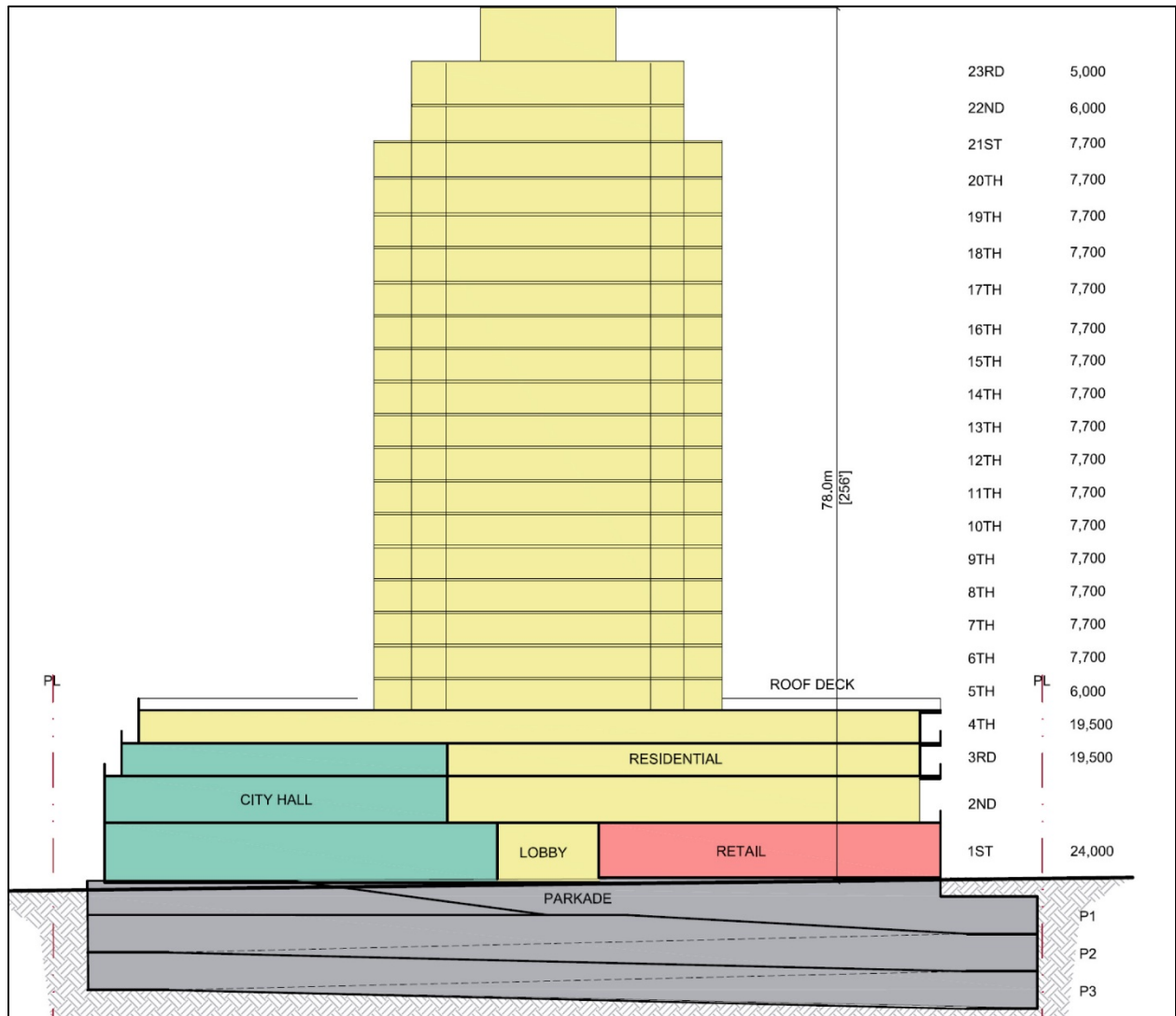
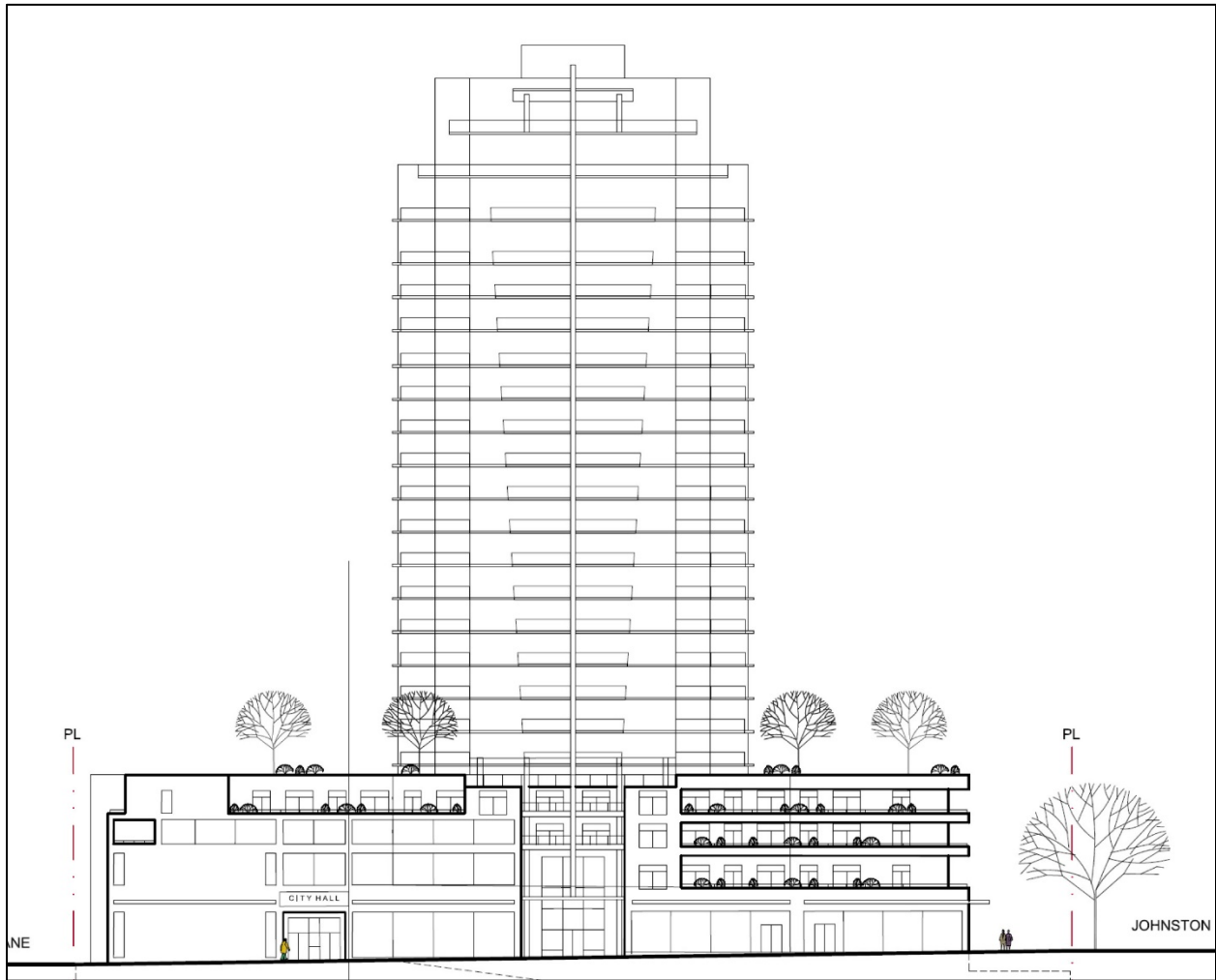


Figure 4: Building Elevation (from south) (dated October 13, 2020)



The property owner has discussed this option with staff at a high level, but prior to the preparing detailed architectural drawings to include with their application submission, it would be appropriate for Council to discuss whether there is support for continuing consideration of this option. The design and layout of the City Hall and any civic amenity areas would also likely require specialized architectural services that would be contracted by the City, in order to ensure the space meets the needs of the community. It is recommended that the LUPC allow the property owner to discuss this proposal with the Committee at this meeting and provide the opportunity for them to answer any questions of the Committee.

If Council is not interested at this time, the applicant would likely proceed with making an application for a similar sized building without a City Hall.

PREVIOUS COUNCIL DIRECTION

Resolution # and Date	Resolution Details
March 13, 2019 2019-107	That Council: <ol style="list-style-type: none"> <li data-bbox="592 1837 1404 1938">1. Receives for information the corporate report dated March 11, 2019 from the Chief Administrative Officer, titled “City Hall”; and

	2. Directs staff to prepare a business analysis, as described within the corporate report
January 13, 2020 2020-016	THAT Council directs staff to bring forward an estimate for a study to be done for City Hall and the City Annex building to make it habitable and safe during an earthquake.
February 24, 2020 2020-095	THAT Council directs staff to bring forward a corporate report that outlines the process, implementation and cost(s) of the City conducting a Referendum regarding a new City Hall.

INTRODUCTION/BACKGROUND

Some of the challenges associated with the current City Hall building were presented to Council in corporate reports dated March 11, 2019 and January 13, 2020. The issues as presented in the reports include seismic risk, accessibility deficiencies, and inadequate space. Space inadequacies not only relate to the City’s ability to accommodate community meetings but also the ability to house staff from various multidisciplinary branches of the corporation (e.g. Planning, Engineering, Operations, etc.) under one roof. This can create challenges in delivering local services, as customers may need to visit multiple City offices.

Anticipated Space Needs

While the trend toward working from home during the COVID-19 pandemic has created some uncertainty around post-pandemic space requirements for meetings and employees, staff undertook a survey of similar sized municipalities in the Metro Vancouver area several years ago and believe that approximately 25,000-35,000 square feet (~2,300-3,200 square metres) would be adequate for the meeting, lobby, and office space requirements for a new City Hall. Further, the space noted is believed to be sufficient to serve the population growth in White Rock over the next 30-50 years. Additional facilities that are commonly co-located with civic facilities include community theatres and child care spaces, which can benefit from sharing spaces with similar functions (e.g. Chambers space and performance space; this option is included in the drawings attached to this corporate report as Appendix A).

A more detailed needs assessment / space planning exercise would need to be conducted to validate these numbers, and should include consideration of the trend towards online services and work from home options for staff, as well as exploring opportunities to relocate staff at the Keil Street Operations Building to City Hall. This could potentially be undertaken within the Facilities Master Plan or done as a separate study.

Pending Council’s decision on whether this opportunity should be pursued, staff would undertake this study and potentially engage a design firm to assist with the interior layout and programming to be incorporated into the property owner’s development application. Staff could bring a corporate report to a future Regular Council meeting with identifying the anticipated costs of the study and sources of funding.

Official Community Plan Policy

In the Town Centre chapter of the OCP, there is a current objective that seeks “to enable the establishment of civic and community uses, as well as other important destinations, reinforcing the Town Centre as a centre for cultural, civic, and public life in the city.” In support of this objective, Policy 9.2.2 directs that the City “explore the possibility of relocating City Hall to the Town Centre, establishing a new civic centre with other potential civic facilities.”

Proposed Site – 1513 Johnston Road

The property at 1513 Johnston Road currently has a one-storey strip of commercial businesses including the Wooden Spoon restaurant and two micro-breweries (White Rock Beach Beer and 3 Dogs Brewing), among other tenants. It is situated on the north-west corner of the Johnston Road and Russell Avenue intersection, opposite the White Rock Community Centre and Bryant Park to the south, and across Johnston Road from the lot expropriated from Imperial Oil by the City for a future Town Square/Civic Plaza (1510 Johnston Road).

The current Town Centre policies and 2011 Town Centre Urban Design Plan call for a large new neighbourhood park in the block bounded by Johnston Road, Russell Avenue, Foster Street, and North Bluff Road, which would likely be located immediately north of the subject site.

Given the proximity to existing City assets (the White Rock Community Centre) and potential future assets (neighbourhood park and Town Square), staff believe this site offers a unique opportunity to create a civic heart in the growing Town Centre area.

Cost of Constructing new City Hall

The March 11, 2019 corporate report estimated a \$16-20M cost to construct a new City Hall, on the assumption that it would be a standalone building located in the civic precinct area, with an approximate floor area of 20,000-30,000 square feet. Should Council direct staff to pursue the option of relocating to a new mixed use development in the Town Centre, these costs would have to be re-evaluated following a space planning study. Given that the developer's approximate amenity contribution would be \$5.5M, there would be additional costs to constructing the City Hall. These additional costs could be partially covered by cash-in-lieu CACs from other projects, the sale of the City lane adjacent to the parcel to the developer (if considered surplus to the City's needs) or other surplus City properties, or other sources to be determined.

FINANCIAL IMPLICATIONS

The City's current Financial Plan includes \$3M for some renovations to the current City Hall. A project of the magnitude described in this report will require significant changes to the current capital program. Prior to committing to the Developer and the project, the full costs will need to be included in a Financial Plan Bylaw.

LEGAL IMPLICATIONS

Any partnership on a project of this scale entails risks, including those with legal implications, and any related agreements, including cost/maintenance agreements and Air Space Parcel subdivisions with the property owner would require legal review. Such agreements would need to be compliant with the *Community Charter* prohibitions on providing assistance to a business and other related legislation applicable to municipal dealings.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

On February 24, 2020, Council requested a corporate report on the process, implementation and cost of the City doing a referendum on a new City Hall. Managing the City's response to the COVID-19 pandemic has disrupted the work on this topic.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

This corporate report was discussed with the Engineering and Municipal Operations and Finance Departments, and no additional comments were provided.

CLIMATE CHANGE IMPLICATIONS

The opportunity to construct a new City Hall could include building and site features designed to reduce greenhouse gas emissions (electric vehicle charging infrastructure, building energy efficiency, etc.), as well as incorporate technology that facilitates increased work-from-home and/or online services.

By contributing to the mix of activities within walking distance in the city's growing Town Centre area, a relocated City Hall could encourage more residents in the area to walk or cycle to meet their daily needs. The Town Centre is also the transit hub for the community and may allow more employees to come to work by bus instead of single-occupant vehicle.

ALIGNMENT WITH STRATEGIC PRIORITIES

The review of the City Hall and Civic Precinct is included in Council's 2018-2022 Strategic Priorities as a "Next" project.

OPTIONS / RISKS / ALTERNATIVES

Alternatives

Land Use and Planning Committee can:

1. Allow the property owner to discuss the potential for incorporating a new City Hall and theatre facility in their proposed redevelopment with the Committee.
2. Direct staff not to pursue opportunities for relocating the City Hall to the Town Centre area, both generally and more specifically as part of the mixed-use redevelopment application at 1513 Johnston Road. This may result in a lost opportunity to co-locate City Hall in close proximity to the White Rock Community Centre and other future civic facilities (parks, etc.) in the Town Centre.

It is noted that the LUPC has already provided direction to staff to proceed with preparing implementation mechanisms (i.e. Zoning and OCP amendments) related to the recommendations in the Town Centre OCP Review which relate to the height and density of properties in this area, and that this may impact the development potential of this and other parcels in the Town Centre area.

CONCLUSION

The property owner of 1513 Johnston Road has indicated they are open to incorporating a new City Hall / community amenity space within the redevelopment proposal for their property, and staff are requesting that the owner be given an opportunity to discuss this possibility with the Committee. Following this discussion, the Committee may wish to provide staff with additional direction on proceeding with or discontinuing efforts on this option.

Respectfully submitted,



Carl Isaak, MCIP, RPP
Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendation of this report.

A handwritten signature in black ink, appearing to read 'Guillermo Ferrero', written over a light grey rectangular background.

Guillermo Ferrero
Chief Administrative Officer

Appendix A: Conceptual Drawings Submitted by Property Owner of 1513 Johnston Road

APPENDIX A

Conceptual Drawings Submitted by Property Owner of 1513 Johnston Road

STATISTICS

MUNICIPAL ADDRESS:

1513 JOHNSTON ROAD, WHITE ROCK, BC

LEGAL DESCRIPTION:

LOT 34 EXCEPT: FIRSTLY; PART SUBDIVIDED BY PLAN 34504
SECONDLY; PARCEL "P" (REFERENCE PLAN 36117),
SECTION 10, TOWNSHIP 1, NWD PLAN 32979

GROSS LOT AREA:

38,016 SQ. FT. (3531.7 SQ M) = 0.873 ACRE (0.353 Ha)

EXISTING ZONE: CR-1 TOWN CENTRE AREA

TYPE OF DEVELOPMENT:

MIXED USE

MAX. SITE COVERAGE: 65%

ALLOWED: 24,710 SQ FT (7531.73 SQ M)
PROPOSED: 20,300 SQ FT (53.4%)

BUILDING HEIGHT:

ALLOWED: 265'-0" (80.7 M) C/W AMENITY AGREEMENT
PROPOSED: 265'-0" (80.7 M)
23 STOREYS

PROPOSED SETBACKS:

EAST (FRONT): 30' (9 M)
WEST (REAR): 15' (4.5 M)
SOUTH (EXTERIOR SIDE YARD): 30' (9 M)
NORTH (INTERIOR SIDE YARD):
89'-6" (27.3 M) AT 0'-0"
160'-0" (47.5 M) AT 5'-0" (1.5 M)

FSR: (C/W AMENITY AGREEMENT)

ALLOWED: 5.4 = 205,286 SQ FT (19,071.1 SQ M)
PROPOSED: 5.37 = 204,000 SQ FT (1895 SQ M)
(EXCLUDING CITY HALL)

CITY HALL AMENITY (NOT INCLUDED IN FSR)

GROUND FLOOR: 9,500 SQ FT (880 SQ M)
SECOND FLOOR: 6,000 SQ FT (560 SQ M)
THIRD FLOOR: 11,000 SQ FT (1020 SQ M)
TOTAL AREA: 26,500 SQ FT (2460 SQ M)

BUILDING AREAS (INCLUDED IN FSR ONLY):

GROUND FLOOR PLATE: 16,600 SQ FT (1540 SQ M)
SECOND FLOOR PLATE: 11,000 SQ FT (1020 SQ M)
THIRD FLOOR PLATE: 11,000 SQ FT (1020 SQ M)
FOURTH FLOOR PLATE: 19,800 SQ FT (1840 SQ M)
TYPICAL TOWER PLATE: 7,700 SQ FT (715 SQ M)
(19 FLOORS)

COMMERCIAL SPACE:

RETAIL 1: 3000 SQ FT
RETAIL 2: 2700 SQ FT
BREWERY: 3300 SQ FT
TOTAL COMMERCIAL: 9000 SQ FT

RESIDENTIAL SUITES:

TOWER SUITES: 123
2ND, 3RD & 4TH FLOOR APARTMENTS: 35
TOTAL NO. OF UNITS: 158

PRIVATE INDOOR AMENITY PROVIDED (INCLUDED IN FSR):

2,400 SQ FT (220 SQ M)

PRIVATE OUTDOOR AMENITY PROVIDED:

12,300 SQ FT (1140 SQ M)

RESIDENTIAL STORAGE LOCKERS:

NO MINIMUM NUMBER REQUIRED
PROVIDED: 168

PARKING:

COMMERCIAL PARKING STALLS:

REQUIRED: 1 PER 37 SQ M (400 SQ.FT.)
= 9,000 SQ FT / 400 = 23
PROVIDED: = 23

CITY HALL PARKING:

REQUIRED: 1 PER 37 SQ M (400 SQ.FT.)
= 26,500 SQ FT / 400 = 66.25
PROVIDED: = 67

RESIDENTIAL PARKING STALLS:

REQUIRED FOR APARTMENTS: 1.2 PER UNIT + 0.3 VISITOR
PROVIDED: 158 X 1.2 = 190 SECURE RESIDENTIAL
158 X 0.3 = 48 VISITOR
TOTAL APARTMENT RESIDENTIAL = 238

TOTAL PARKING STALLS REQUIRED:

PUBLIC (COMMERCIAL + CITY HALL + RESIDENTIAL VISITORS)
= 23 + 67 + 48 = 138
SECURE RESIDENTIAL = 190
= 328

TOTAL PARKING STALLS PROVIDED:

PUBLIC: = 83 - 55 (RELAXATION REQUIRED)
SECURE: = 190
TOTAL: = 273

LOADING BAYS:

REQUIRED: 2 (1 SHARED WITH RESIDENTIAL)
PROVIDED: 2

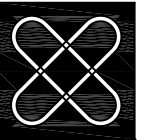
BICYCLE STALLS:

	CLASS 1	CLASS 2
CITY HALL	0.25 PER 200 SQ M 2320 / 200 X .25 = 3	0.75 PER 200 SQ M 2320 / 200 X .75 = 9
COMMERCIAL	0.25 PER 200 SQ M 1100 / 200 X .25 = 2	0.75 PER 200 SQ M 1100 / 200 X .75 = 4
SUITES	1 PER UNIT = 158	0.2 PER UNIT = 34

TOTAL CLASS 1 BICYCLE STALLS PROVIDED: 172
TOTAL CLASS 2 BICYCLE STALLS PROVIDED: 44

PROJECT STATISTICS

OPTION 1: CITY HALL



F. ADAB ARCHITECTS INC.

#130-1000 ROOSEVELT CRESCENT
NORTH VANCOUVER, BC V7P 3R4
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E-MAIL: mfa@multigonadab.com

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NO.	DATE	REVISION/ISSUED
8	OCT 13 2020	OPTION 1 REVISED FOR CITY REVIEW
7	AUG 19 2020	OPTION 1 REVISED FOR CLIENT REVIEW
6	JUL 29 2020	OPTIONS 1 & 2 REVIEW BY CITY
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4	JUL 15 2020	DESIGN DEVELOPMENT
3	JUL 08 2020	REVIEW BY CITY
2	JUN 30 2020	PRELIMINARY SCHEMATIC
1	JUN 18 2020	PRELIMINARY SCHEMATIC

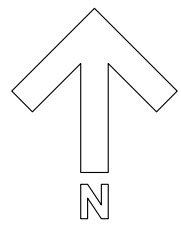
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MIXED USE DEVELOPMENT
1513 JOHNSTON ROAD,
WHITE ROCK, BC
FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:

PROJECT STATISTICS

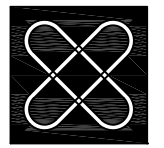
OPTION 1: CITY HALL

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SCALE:	N/A		
DESIGN:	FAA		
DRAWN:	NW		
PROJECT NO:	2003		



CONTEXT PLAN: TOWN CENTRE

OPTION 1: CITY HALL



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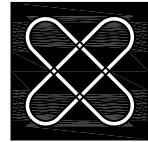
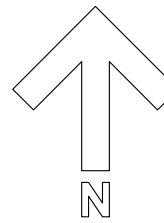
FOR:
**PRIME JOHNSTON
HOLDINGS LTS.
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6**

DRAWING TITLE:
**CONTEXT PLAN:
TOWN CENTRE**

OPTION 1: CITY HALL

DATE:	JULY 29, 2020	SHEET NO:	A-2
SCALE:	N.T.S.		
DESIGN:	FAA		
DRAWN:	NW		
PROJECT NO:	2003		





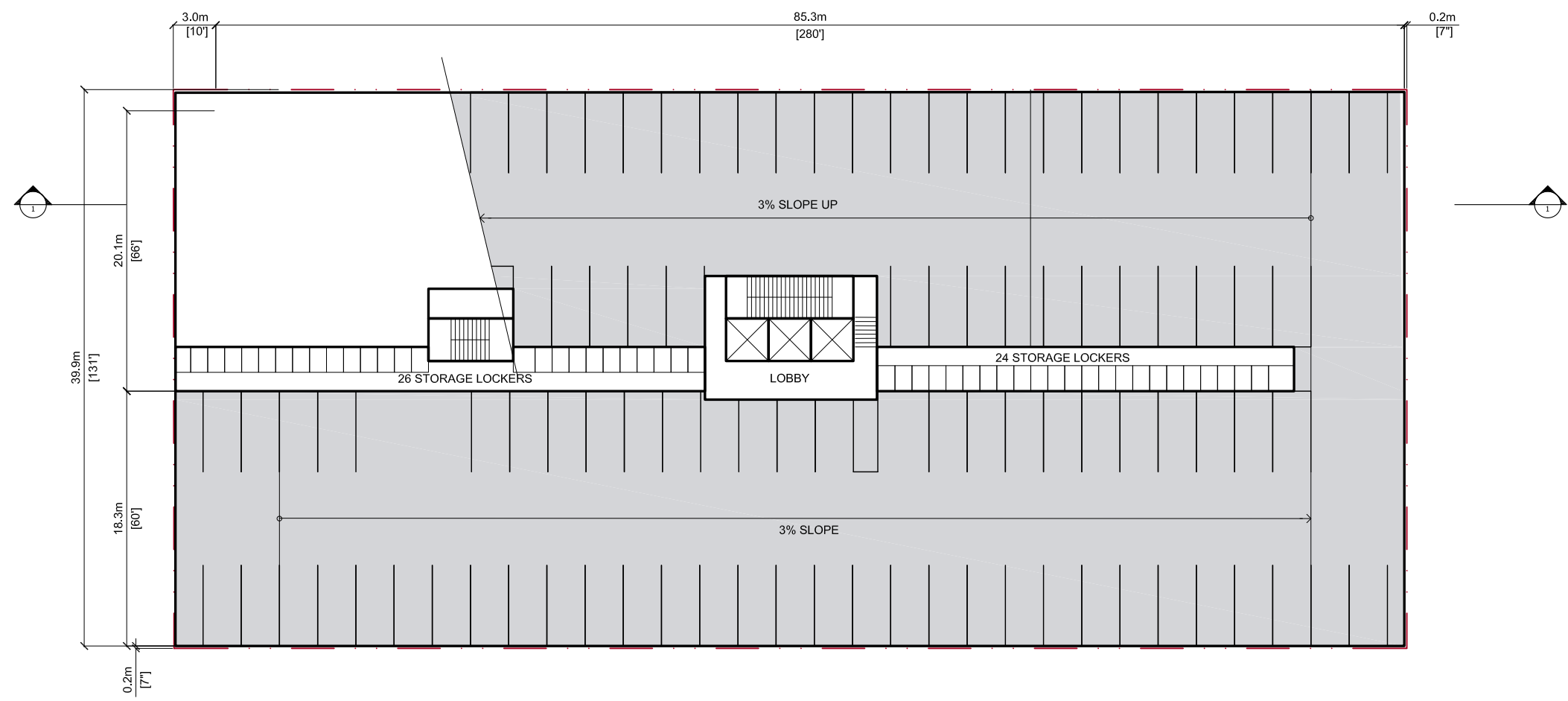
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**P3 PARKING LEVEL:
101 SECURE (RESIDENTIAL) PARKING STALLS**

OPTION 1: CITY HALL

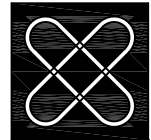
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VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
**P3 PARKING LEVEL
OPTION 1: CITY HALL**

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-4
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



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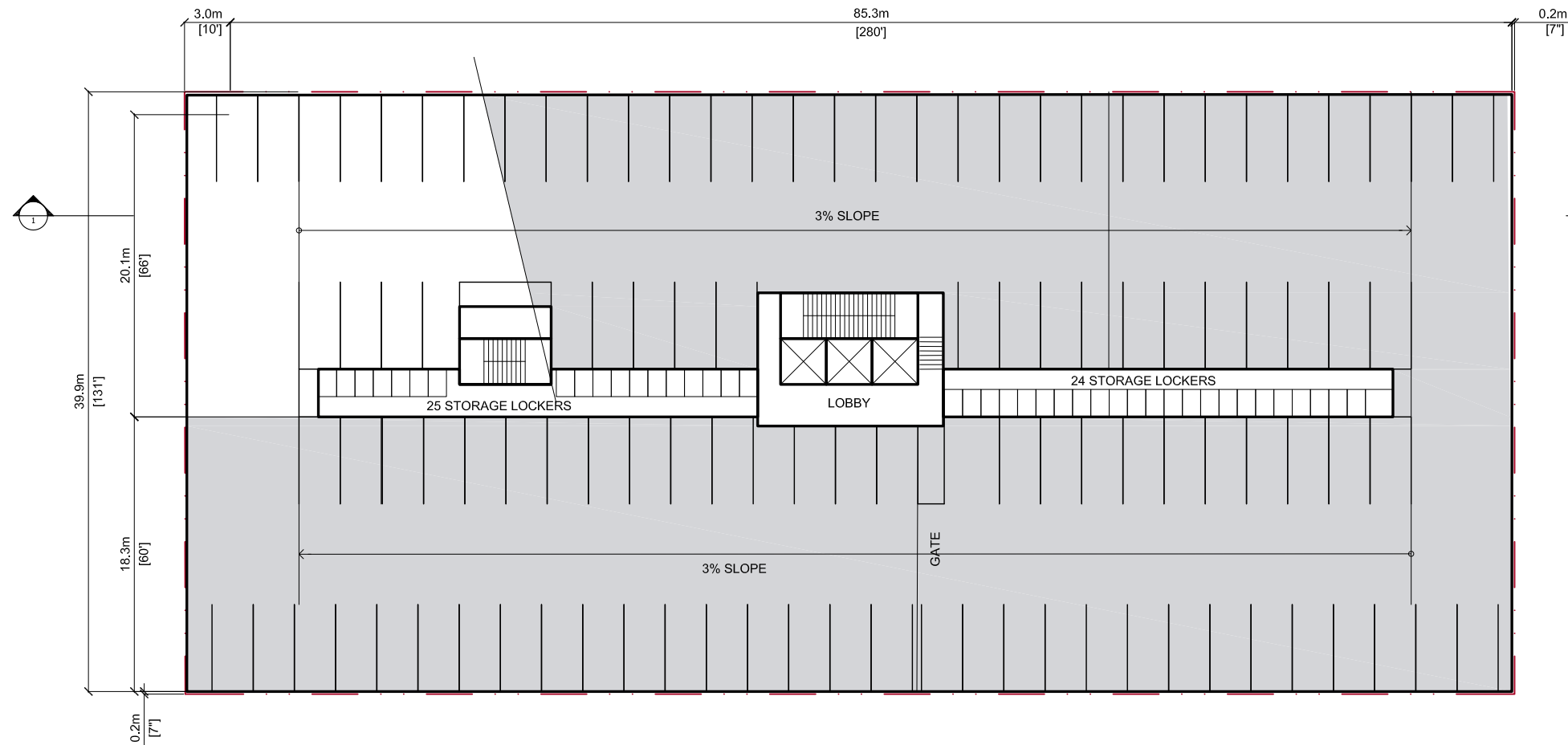
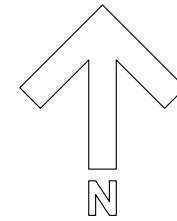
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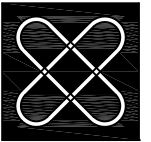
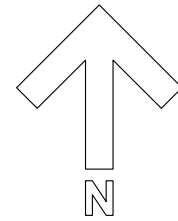
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P2 PARKING LEVEL
OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-5
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



P2 PARKING LEVEL: 101 SECURE (RESIDENTIAL) PARKING STALLS

OPTION 1: CITY HALL



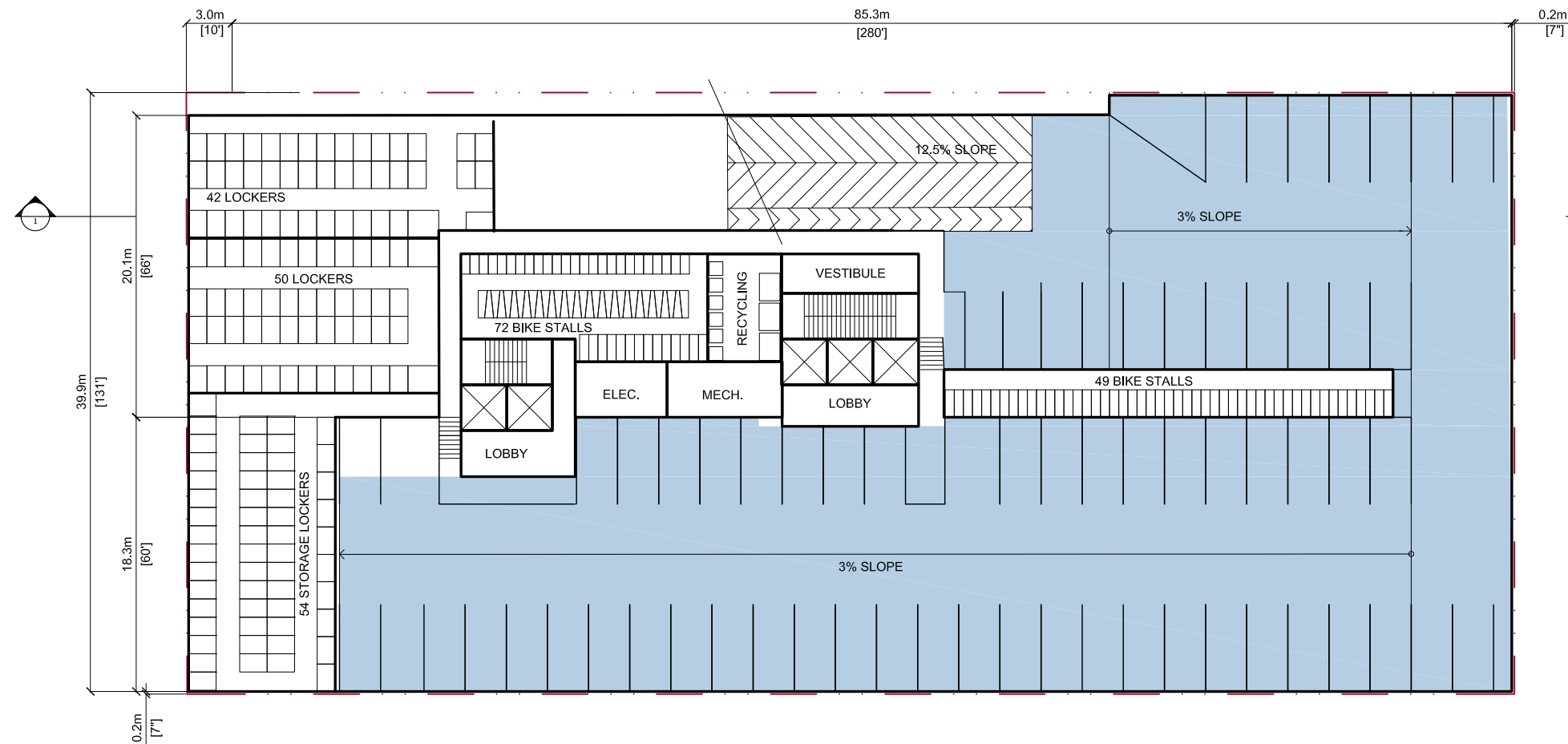
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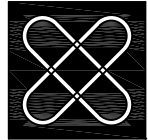
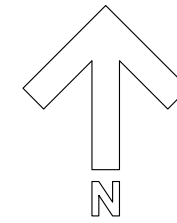
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P1 PARKING LEVEL
OPTION 1: CITY HALL

DATE:	JULY 29, 2020	SHEET NO:	
SCALE:	1/16" = 1'-0"		
DESIGN:	FAA		A-6
DRAWN:	NW		
PROJECT NO:	2003		

P1 PARKING LEVEL:

73 UNSECURED (PUBLIC) PARKING STALLS; CITY HALL: 68 PARKING STALLS IN BLUE

OPTION 1: CITY HALL



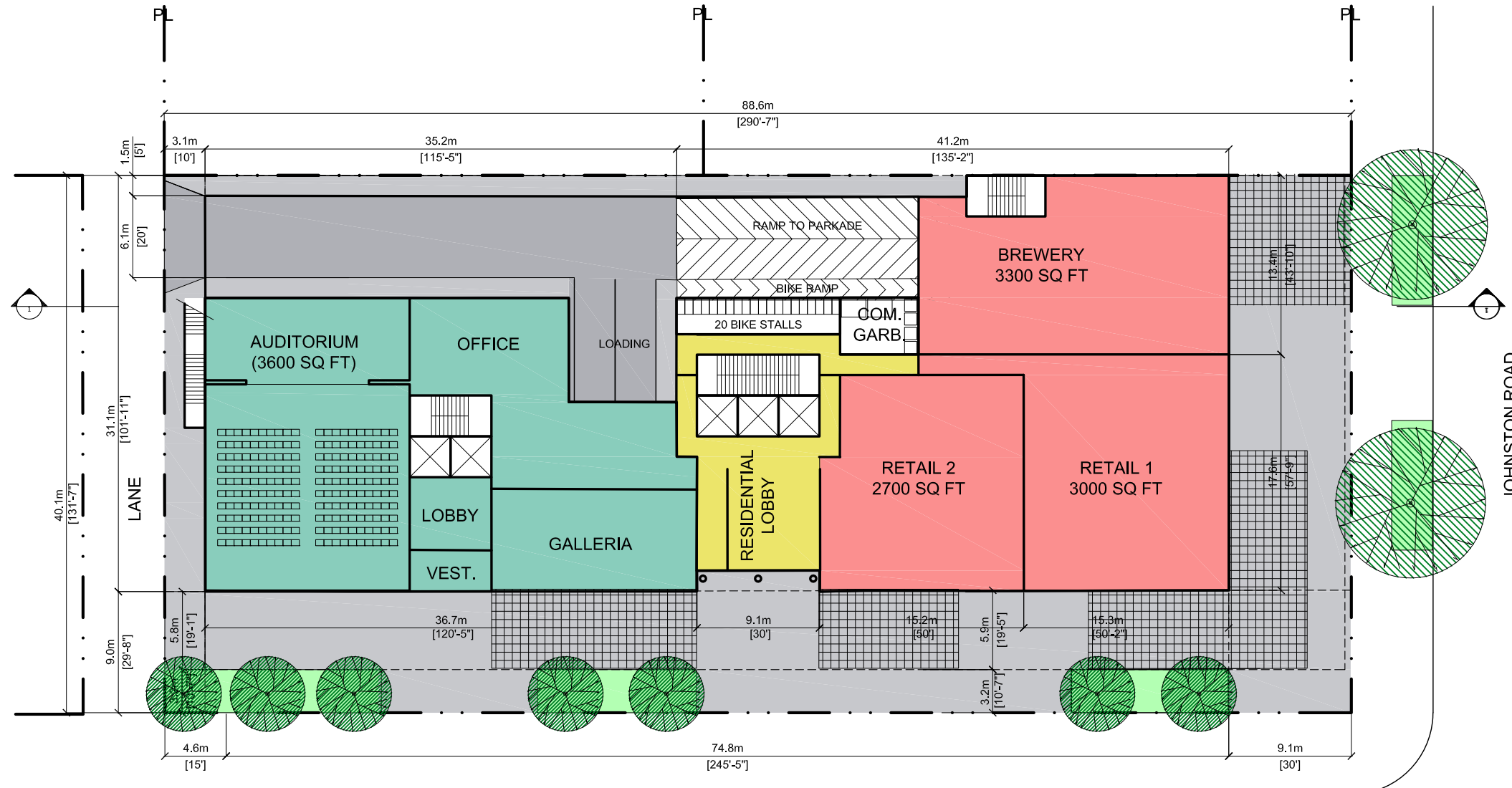
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GROUND FLOOR LEVEL

OPTION 1: CITY HALL

RUSSELL AVENUE

JOHNSTON ROAD

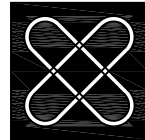
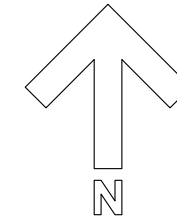
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WHITE ROCK, BC

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PRIME JOHNSTON HOLDINGS LTS.
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
GROUND FLOOR PLAN
OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
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DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



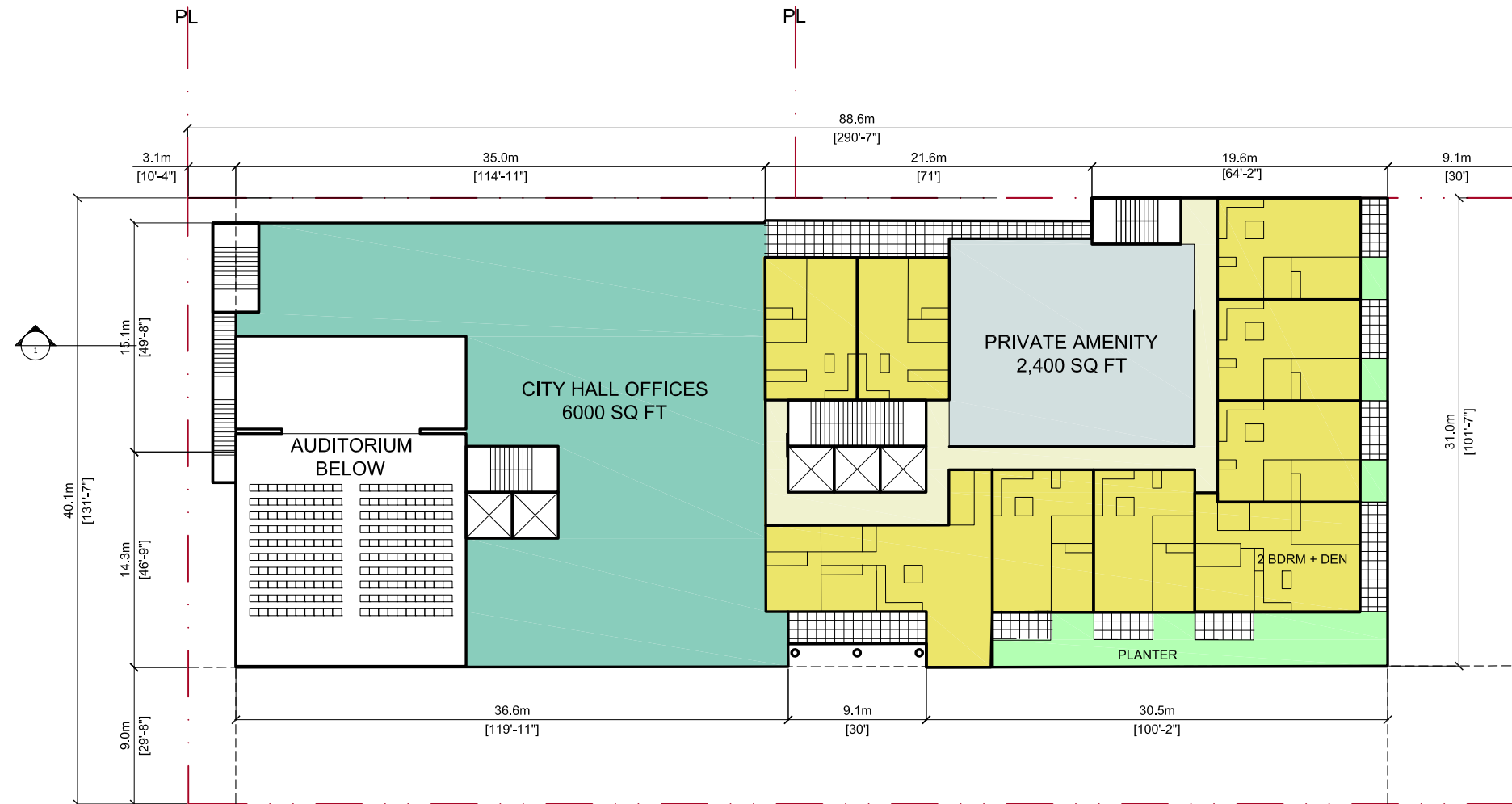
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SECOND FLOOR LEVEL
9 RESIDENTIAL UNITS
6000 SQ FT CITY HALL

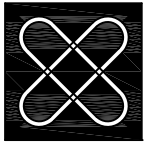
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DRAWING TITLE:
SECOND FLOOR LEVEL
OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-8
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



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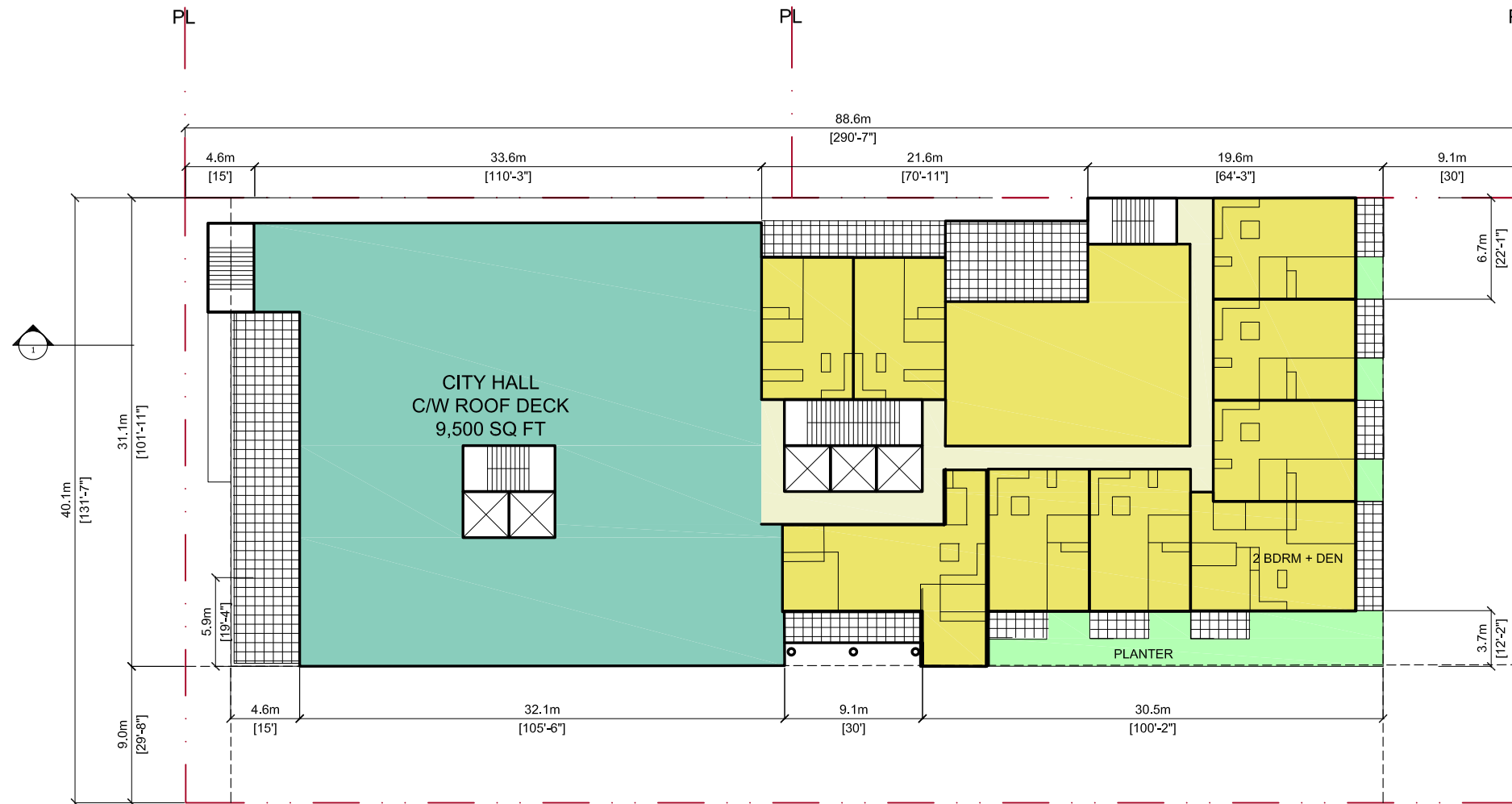
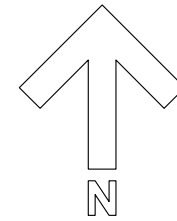
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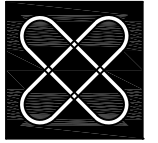
FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
THIRD FLOOR LEVEL
OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-9
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



THIRD FLOOR LEVEL
9 RESIDENTIAL UNITS
11,000 SQ FT CITY HALL



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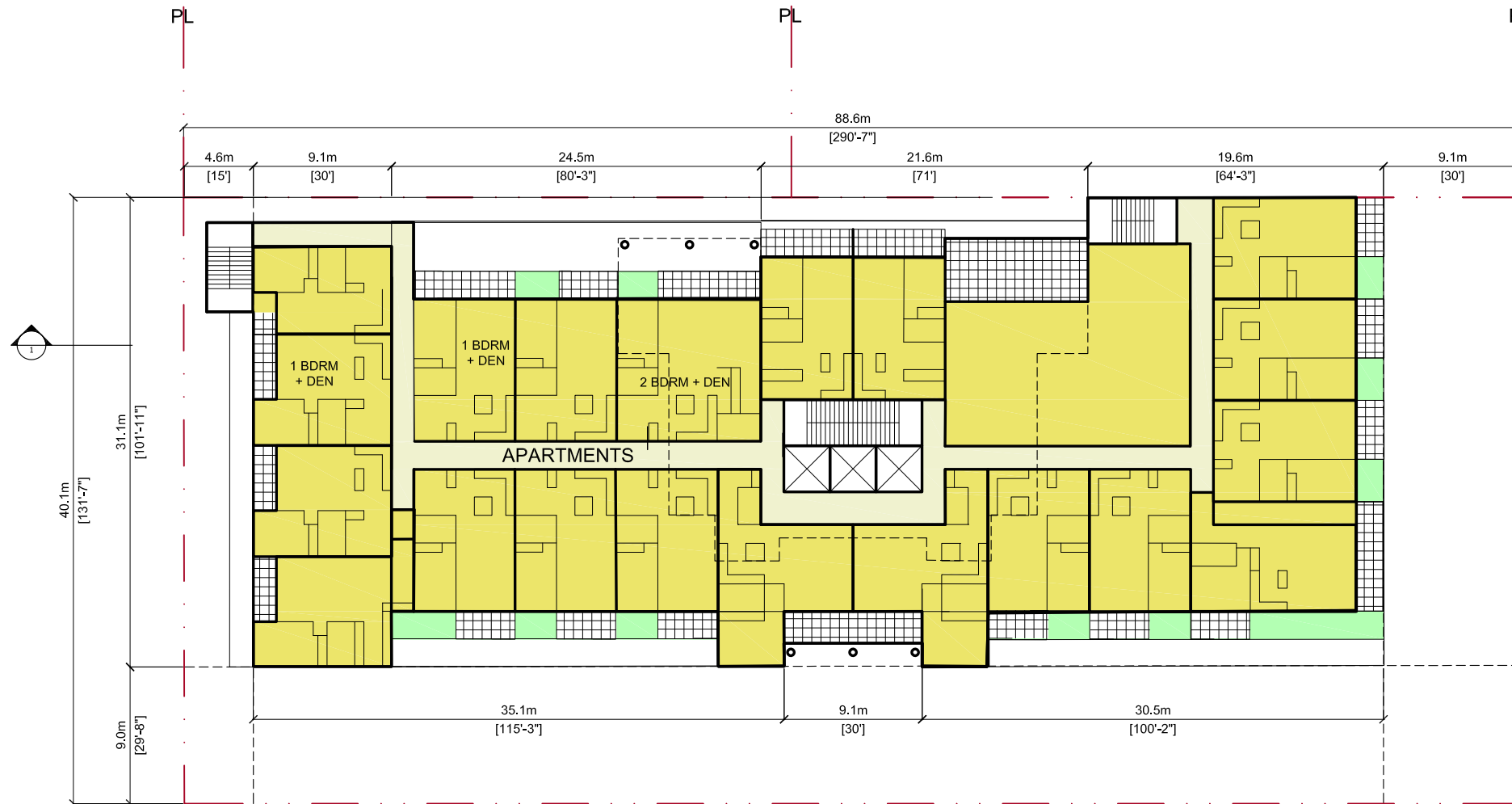
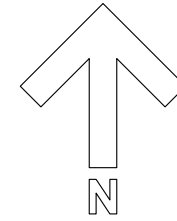
NO.	DATE	REVISION/ISSUED
8	OCT 13 2020	OPTION 1 REVISED FOR CITY REVIEW
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6	JUL 29 2020	OPTIONS 1 & 2 REVIEW BY CITY
5	JUL 22 2020	DESIGN DEVELOPMENT
4	JUL 15 2020	DESIGN DEVELOPMENT
3	JUL 08 2020	REVIEW BY CITY
2	JUN 30 2020	PRELIMINARY SCHEMATIC
1	JUN 18 2020	PRELIMINARY SCHEMATIC

PROJECT TITLE:
**MIXED USE DEVELOPMENT
1513 JOHNSTON ROAD,
WHITE ROCK, BC**

FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
**FOURTH FLOOR LEVEL
OPTION 1: CITY HALL**

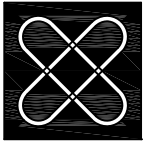
DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-10
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



FOURTH FLOOR LEVEL

17 RESIDENTIAL UNITS

OPTION 1: CITY HALL



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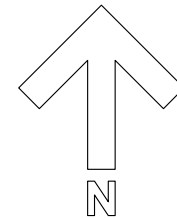
NO.	DATE	REVISION ISSUED
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1513 JOHNSTON ROAD,
WHITE ROCK, BC**

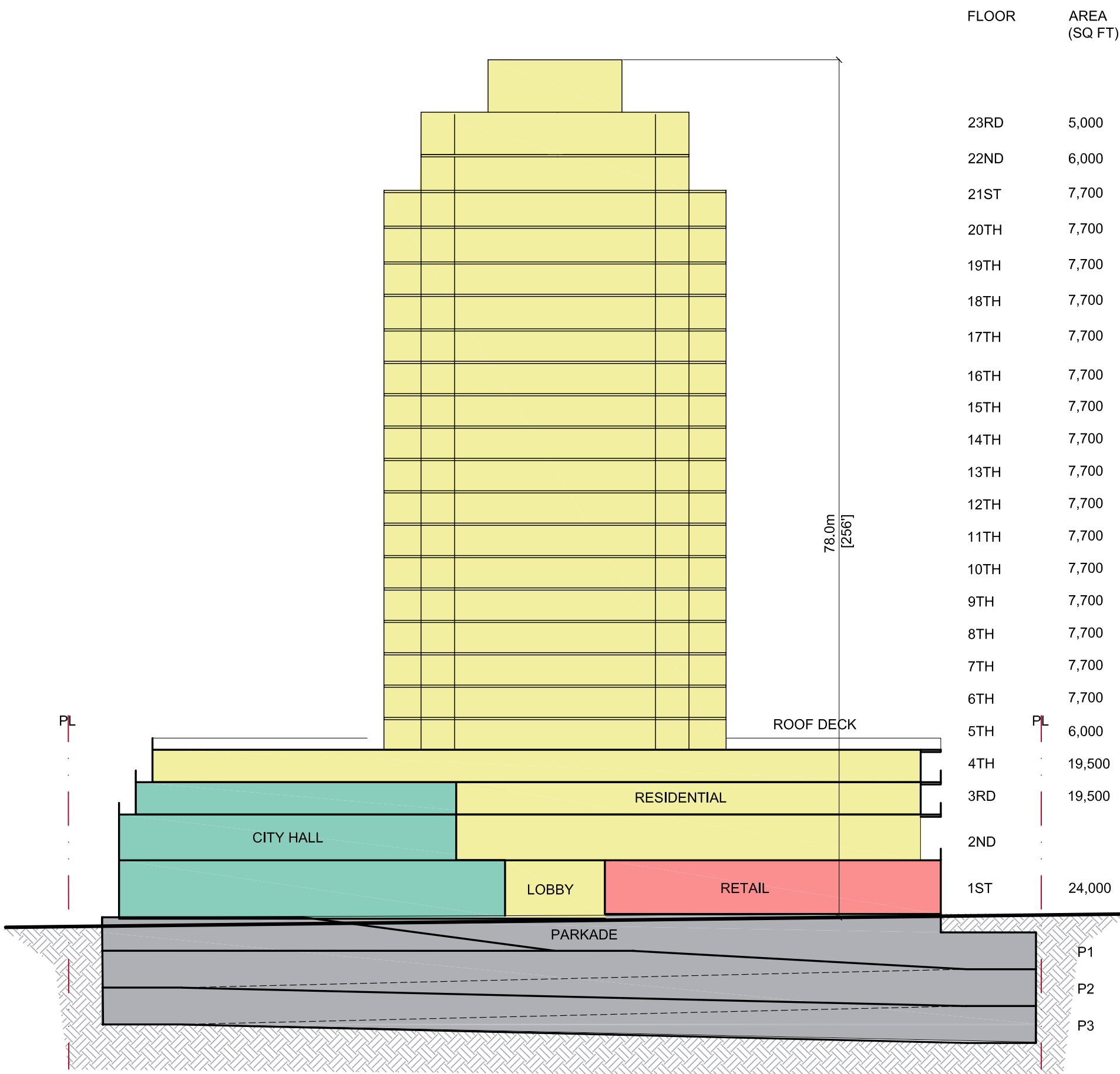
FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

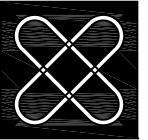
DRAWING TITLE:
**LONGITUDINAL SECTION
OPTION 1: CITY HALL**

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-11
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



LONGITUDINAL SECTION THROUGH TOWER OPTION 1: CITY HALL





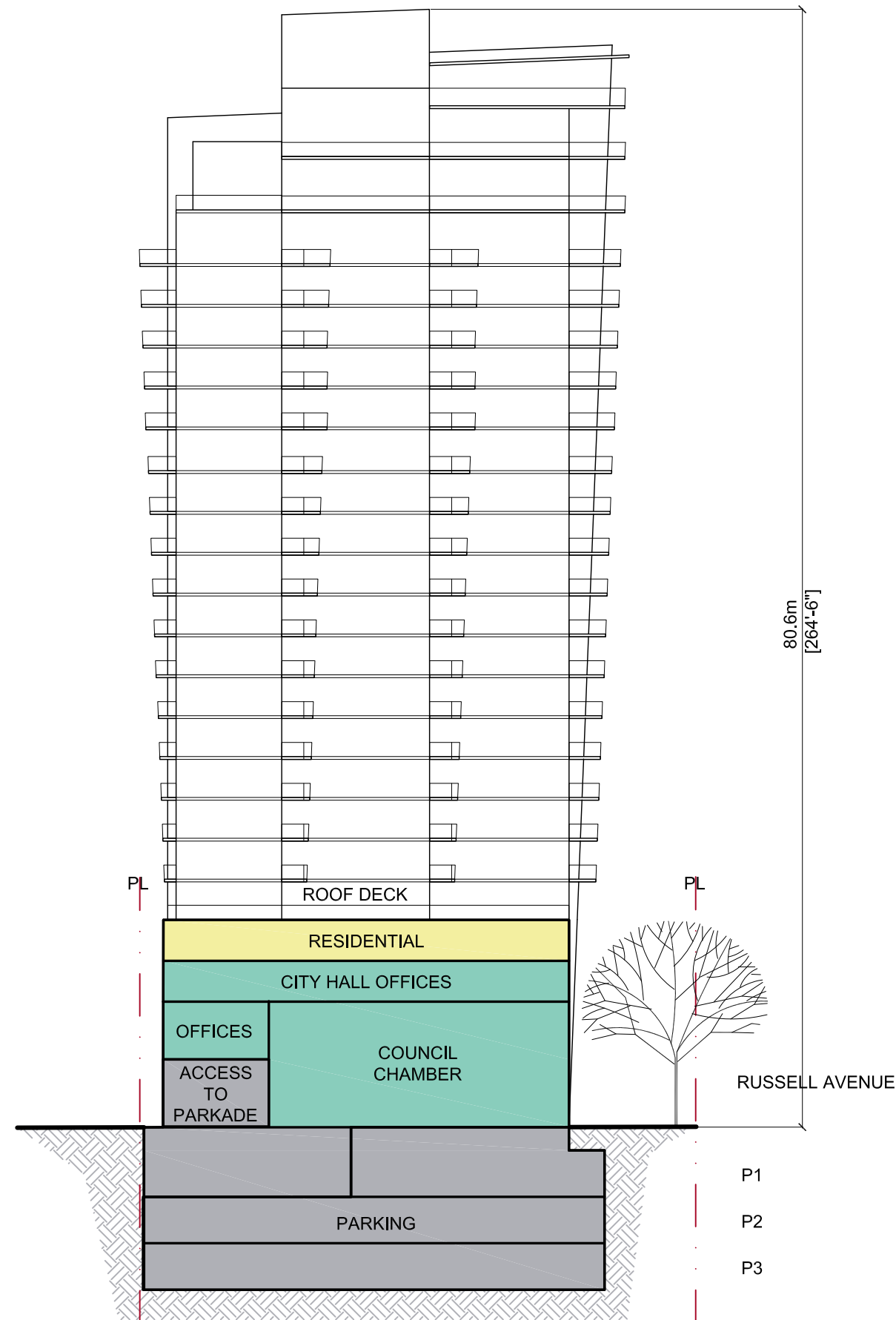
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CROSS SECTION THROUGH CITY HALL

OPTION 1: CITY HALL

NO.	DATE	REVISION/ ISSUED
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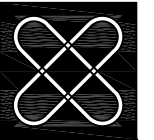
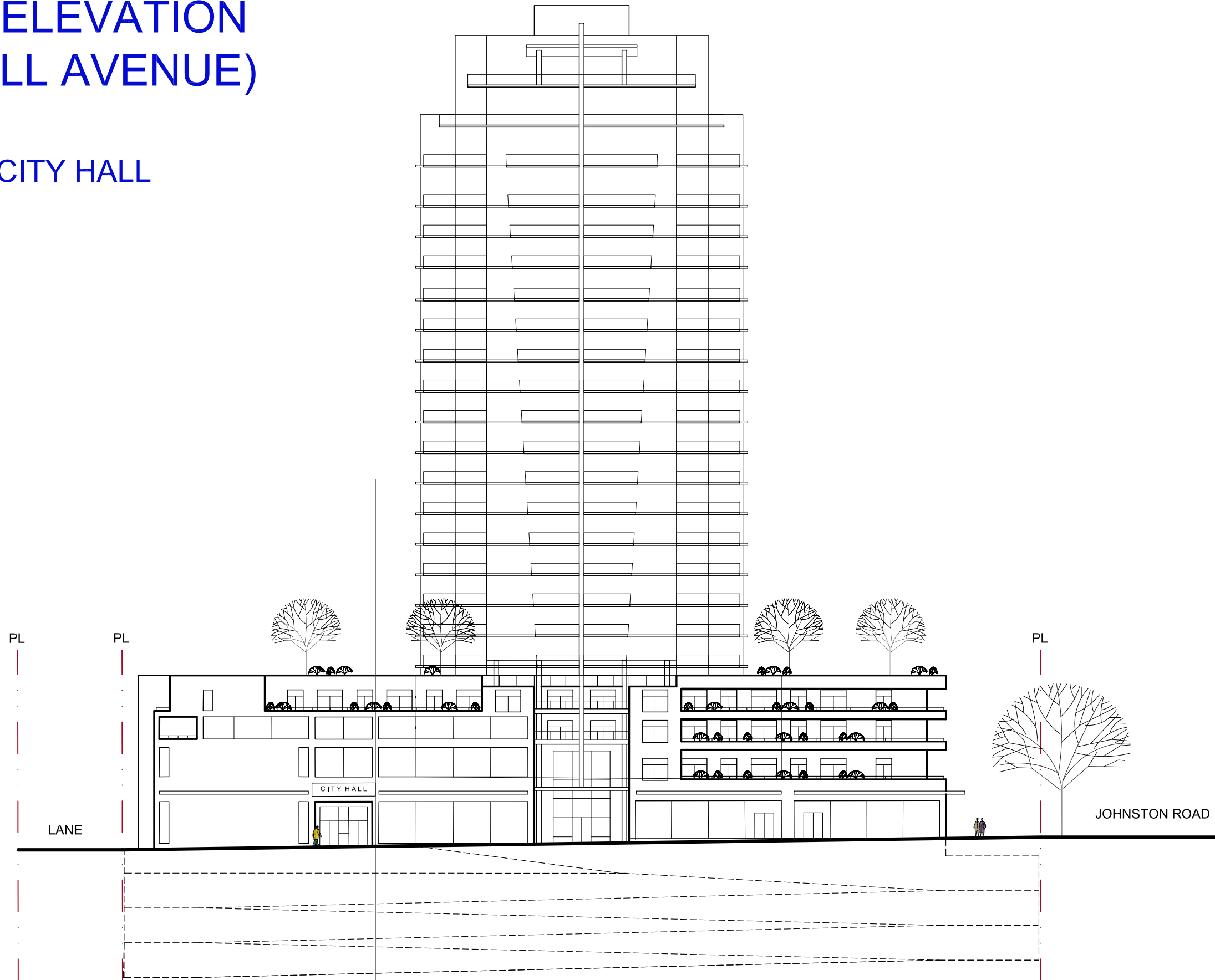
FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
SECTION
OPTION 1: CITY HALL

DATE:	JULY 29, 2020	SHEET NO:	
SCALE:	1/16" = 1'-0"		
DESIGN:	FAA		
DRAWN:	NW		A-12
PROJECT NO:	2003		

SOUTH ELEVATION (RUSSELL AVENUE)

OPTION 1: CITY HALL



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PROJECT TITLE:
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1513 JOHNSTON ROAD,
WHITE ROCK, BC**

FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

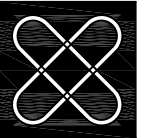
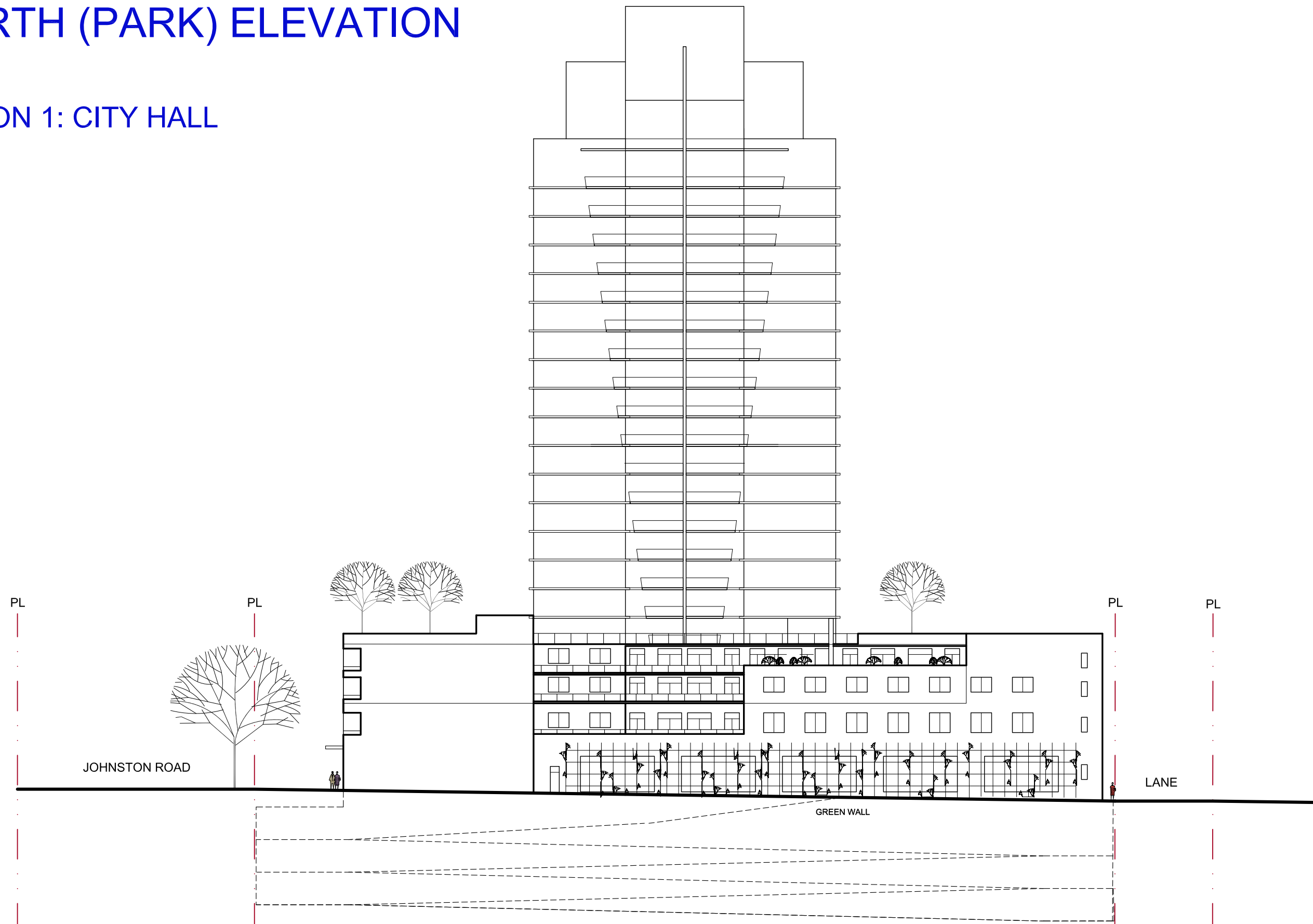
DRAWING TITLE:
**SOUTH ELEVATION
(RUSSELL AVENUE)**

OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-13
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	

NORTH (PARK) ELEVATION

OPTION 1: CITY HALL



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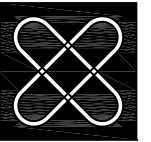
PROJECT TITLE:
**MIXED USE DEVELOPMENT
1513 JOHNSTON ROAD,
WHITE ROCK, BC**

FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
**NORTH (PARK)
ELEVATION**

OPTION 1: CITY HALL

DATE: JULY 29, 2020	SHEET NO:
SCALE: 1/16" = 1'-0"	A-14
DESIGN: FAA	
DRAWN: NW	
PROJECT NO: 2003	



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PROJECT TITLE:
MIXED USE DEVELOPMENT
1513 JOHNSTON ROAD,
WHITE ROCK, BC

FOR:
PRIME JOHNSTON HOLDINGS LTS.
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
**EAST (JOHNSTON ROAD)
& WEST (LANE) ELEVS**

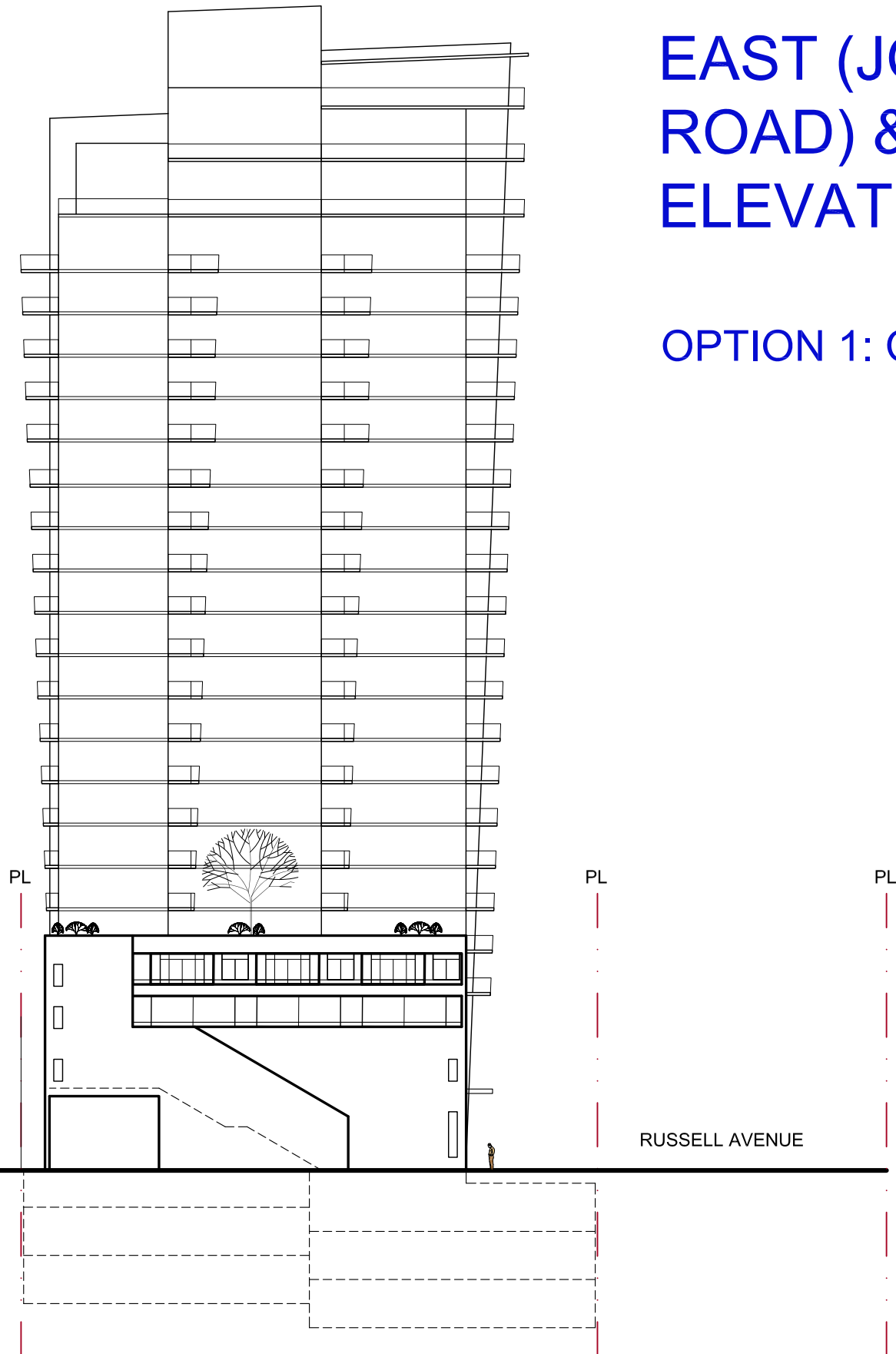
OPTION 1: CITY HALL

DATE:	JULY 29, 2020	SHEET NO:	
SCALE:	1/16" = 1'-0"		
DESIGN:	FAA		
DRAWN:	NW		
PROJECT NO:	2003		

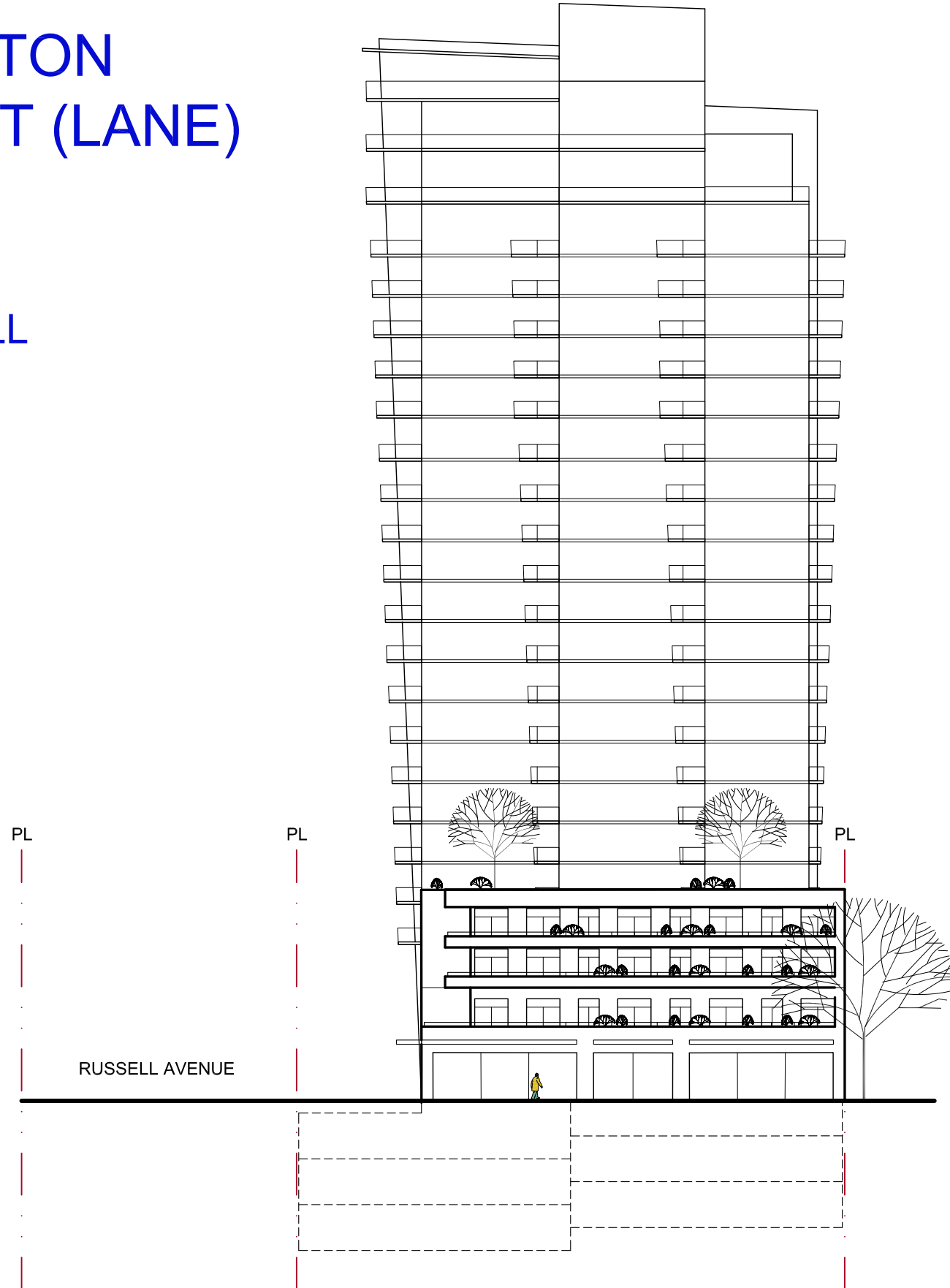
A-15

EAST (JOHNSTON ROAD) & WEST (LANE) ELEVATIONS

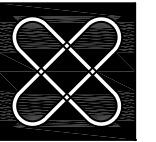
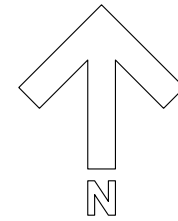
OPTION 1: CITY HALL



WEST (LANE)



EAST (JOHNSTON RD)



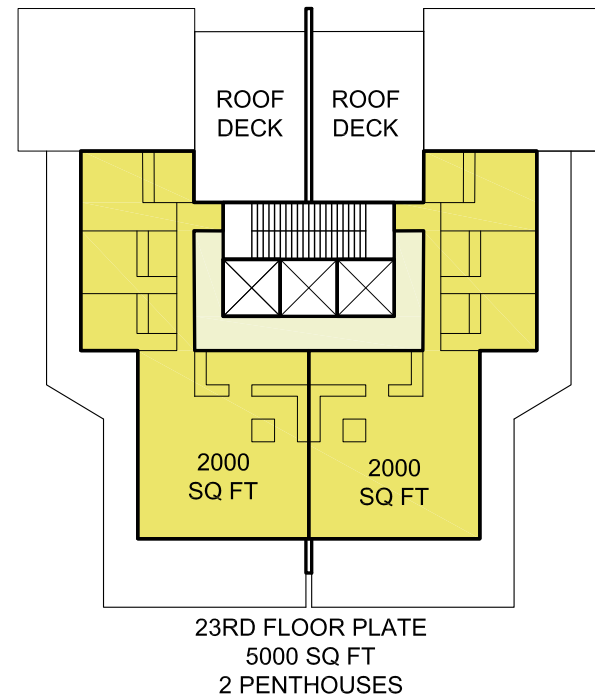
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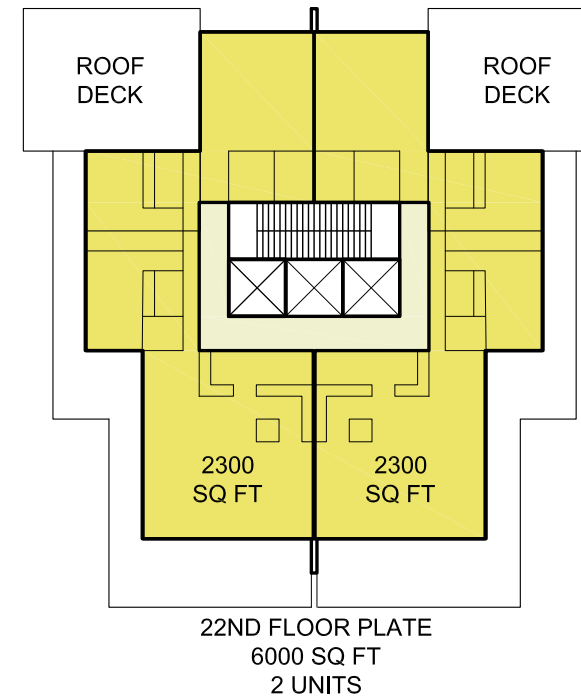
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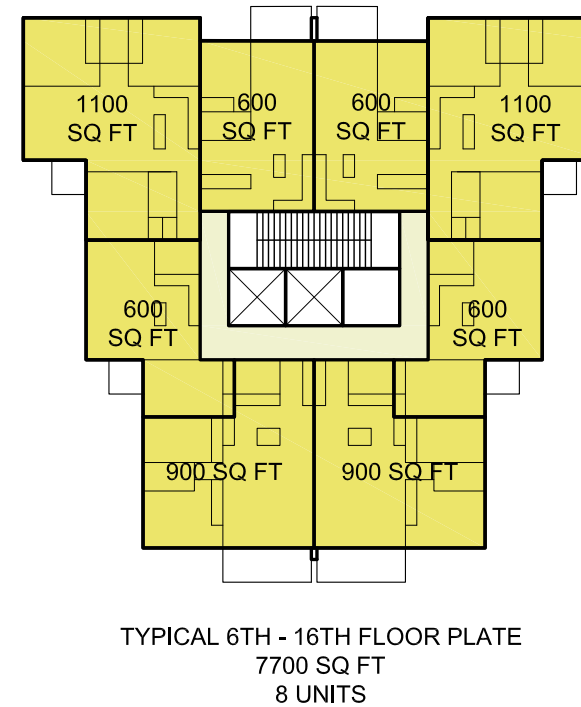
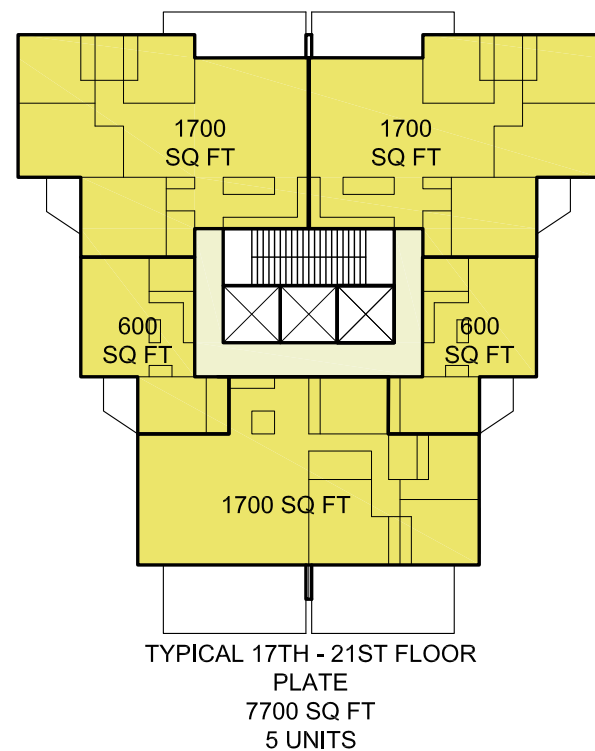
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TYPICAL TOWER FLOOR PLATES



OPTION 1: CITY HALL



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1513 JOHNSTON ROAD,
WHITE ROCK, BC**

FOR:
**PRIME JOHNSTON
HOLDINGS LTS.**
1325-1500 WEST GEORGIA ST.
VANCOUVER, BC V6G 2Z6

DRAWING TITLE:
**TYPICAL TOWER
FLOOR PLATES**
OPTION 1: CITY HALL

DATE:	JULY 29, 2020	SHEET NO:	
SCALE:	1/16" = 1'-0"		
DESIGN:	FAA		
DRAWN:	NW		
PROJECT NO:	2003		

A-16

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: October 19, 2020
TO: Land Use and Planning Committee
FROM: Carl Isaak, Director, Planning and Development Services
SUBJECT: Consideration of Active Rezoning Applications

RECOMMENDATIONS

THAT the Land Use and Planning Committee recommends that Council:

1. Resolve that the zoning amendment application at 1464 Vidal Street proceed to the next stage in the application review process or be denied;
 2. Resolve that the zoning amendment application at 15963 Marine Drive proceed to the next stage in the application review process or be denied;
 3. Resolve that the zoning amendment application at 1441 Vidal Street proceed to the next stage in the application review process or be denied;
 4. Resolve that the zoning amendment application at 15704 North Bluff Road proceed to the next stage in the application review process or be denied;
 5. Resolve that the zoning amendment application at 14401 Sunset Drive proceed to the next stage in the application review process or be denied;
 6. Resolve that the zoning amendment application at 14068 North Bluff Road proceed to the next stage in the application review process or be denied;
 7. Resolve that the zoning amendment application at 1091 Stayte Road proceed to the next stage in the application review process or be denied;
 8. Resolve that the zoning amendment application at 15109 Elm Street proceed to the next stage in the application review process or be denied;
 9. Resolve that the zoning amendment application at 1361 Finlay Street proceed to the next stage in the application review process or be denied; and
 10. Resolve that the zoning amendment application at 15053 Marine Drive Street proceed to the next stage in the application review process or be denied.
-

EXECUTIVE SUMMARY

On September 28, 2020, Council gave final reading to a bylaw, which amended City of White Rock Planning Procedures Bylaw, 2017, No. 2234, to enable an early review of zoning bylaw amendment (“rezoning”) applications. The early review includes the presentation of preliminary information related to a rezoning application to the City’s Land Use and Planning Committee (LUPC), prior to the application moving to a Public Information Meeting (PIM) and Advisory Design Panel (ADP) review, if applicable. During the September meeting, Council directed staff to bring forward current rezoning applications that have not had a PIM and/or ADP review to the

LUPC to decide whether each application should proceed to the next stage of review or be denied by Council. This corporate report brings forward the basic information about each of the rezoning applications noted in the September 28, 2020 corporate report, including site plan and building elevation architectural drawings as well as project statistics.

If the LUPC and Council consider that an application may have the potential to be approved, or that further information would be required prior to making a decision on the application, it would be appropriate to direct that the application move forward to the next stage in the application process (i.e., interdepartmental review, Public Information Meeting, and/or Advisory Design Panel review). This direction to proceed does not fetter Council’s final decision regarding the project and is not a guarantee that the project would ultimately be approved.

If LUPC and Council are certain that the application has fundamental issues which means they would not be approved without changing the nature of the proposal, Council may direct that the application be denied and identify their concerns with the applicant. An applicant would be refunded a portion of their application fees and may consider making a subsequent application that is responsive to Council’s concerns, continuing the existing land use on the property, or they may choose to develop within their current zoning provisions.

PREVIOUS COUNCIL DIRECTION

Resolution # and Date	Resolution Details
September 28, 2020 2020-459	<p>THAT Council, pending adoption of “City of White Rock Planning Procedures Bylaw, 2017, No. 2234, Amendment (Initial Information Reports for Zoning Amendments) Bylaw, 2020, No. 2357,” endorse the approach to bringing forward active zoning amendment applications as recommended in this corporate report, being [...]</p> <p>Zoning amendment applications which have not had a Public Information Meeting, or have had a Public Information Meeting but no review by the Advisory Design Panel when applicable, will be brought forward with basic architectural drawings (i.e., site plan and building elevations) and project statistics (e.g., number of units, height, setbacks, etc.) to the Land Use and Planning Committee at a future meeting, for the Committee and subsequently Council to decide whether the application should proceed or be denied.</p>
September 28, 2020 2020-473	<p>THAT Council give final reading to "City Of White Rock Planning Procedures Bylaw, 2017, No. 2234, Amendment (Initial Information Reports For Zoning Amendments) Bylaw, 2020, No. 2357"</p>

INTRODUCTION/BACKGROUND

The following section introduces basic information for each of the ten (10) rezoning applications included in Category 2 of the corporate report dated September 28, 2020. The applications at 14990 North Bluff Road and 1164 Elm Street, which were included in the noted corporate report, have been subsequently withdrawn by the applicants and are therefore excluded in this report.

As each of these applications do not require an Official Community Plan (OCP) amendment, it has already been confirmed that the proposed land use, density, and other applicable development related policies (e.g. family friendly housing, rental replacement, etc.) in the OCP are met by the application. Council is not obligated to approve a rezoning application that is

consistent with the OCP; Council would not be able to adopt a zoning amendment bylaw if it conflicts with the OCP, per section 478(2) of the *Local Government Act*.

Active Rezoning Applications

1464 Vidal Street – 13-Unit, Four-Storey Residential Building

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix A.



	Existing Zoning (RT-1)	Proposed Zoning (CD)
Use	Duplex Residential	Multi-Unit Residential
Height	Four (4) storeys / ~15.4 metres	Four (4) storeys / ~15.4 metres
Density	13 strata apartment units (60.7 units per acre) / 1.5 FAR	13 strata apartment units (60.7 units per acre) / 1.5 FAR
Parking Spaces	21 spaces (exceeds minimum requirement by 1 space)	21 spaces (exceeds minimum requirement by 1 space)

OCP Land Use Designation	Urban Neighbourhood – allows multi-unit residential uses in townhouses and low-rise buildings, with a density of up to 1.5 FAR (gross floor area ratio) in buildings up to four storeys in height.
--------------------------	--

15963 Marine Drive - 5 Rental Units, Four-Storey Residential Building

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix B.



	Existing Zoning (RS-1) Standards	Proposed Zoning (CD)
Use	Detached (One-Unit) Residential	Multi-Unit Residential (Rental Tenure)
Height	7.7 metres maximum	Four (4) storeys / ~11.1 metres
Density	1 detached residential dwelling	5 rental apartment units (52.9 units per acre) / 1.82 FAR
Parking Spaces	2 spaces minimum	5 spaces (below minimum requirement by 3 spaces; consideration for reduction based on rental tenure and proximity to transit services)

OCP Land Use Designation	Waterfront Village – allows multi-unit residential uses and mixed-uses (commercial/residential) in low-rise buildings, with a density of up to 2.0 FAR (gross floor area ratio) in buildings up to four storeys in height.
--------------------------	--

1441 Vidal Street (et al – four lots) - 129 Rental Units, Six-Storey Residential Building

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix C. A Public Information Meeting for this project was held (virtually) on August 20, 2020, and is scheduled to be reviewed by the Advisory Design Panel on October 20, 2020 (tomorrow).



	Existing Zoning (RS-1/CD-32/RT-1) Standards	Proposed Zoning (CD)
Use	Detached (One-Unit) Residential Duplex Residential Townhouse Residential	Multi-Unit Residential (Rental Tenure)
Height	7.7 metres – 11.5 metres maximum	Six (6) storeys / ~25.7 metres
Density	Varies depending on lot	129 rental apartment units (134.6 units per acre) / 2.10 FAR
Parking Spaces	Varies depending on lot	180 spaces (below minimum requirement by 14 spaces; consideration for reduction based on rental tenure and proximity to transit services, etc.)
OCP Land Use Designation	Town Centre Transition – allows multi-unit residential uses in building types ranging from low-rise to high-rise, with a density of up to 2.1 FAR (gross floor area ratio) based on the properties location on Figure 9 of the OCP and incorporating a 40% density increase allowed for provision of rental housing, above the base density of 1.5 FAR. Height guidance from Figure 10 indicated a conceptual height between 4-12 storeys.	

15704 North Bluff Rd (et al – seven lots) - 147 Rental Units, Three Six-Storey Buildings

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix D. A Public Information Meeting for this project was held (virtually) on September 9, 2020, and is scheduled to be reviewed by the Advisory Design Panel on October 20, 2020 (tomorrow). The applicant is the same proponent of a similar six-storey application on the opposite side of Lee Street (‘Beachway 1’), which was considered by LUPC on July 27, 2020 and recommended for denial; the recommendation is to be ratified on tonight’s Council agenda.



	Existing Zoning (RS-1) Standards	Proposed Zoning (CD)
Use	Detached (One-Unit) Residential	Multi-Unit Residential (Rental Tenure)
Height	7.7 metres	Six (6) storeys / ~21.2 metres
Density	One detached home and one secondary suite per lot	147 rental apartment units, 46 of which would be affordable (below market) rental (110.9 units per acre) / 2.5 FAR
Parking Spaces	Two per detached home and one per secondary suite	144 spaces, including 16 car share spaces (below minimum requirement by 76 spaces; consideration for reduction based on rental tenure and below market rental units, proximity to transit services, car share etc.)
OCP Land Use Designation	East Side Large Lot Infill Area – allows multi-unit residential uses in townhouses and low-rise apartments on North Bluff Road, with a density of up to 2.5 FAR (gross floor area ratio) and six storeys based on the OCP housing policy 11.2.1(c) density bonus for affordable rental developments, above the base density of 1.5 FAR and three storeys.	

14401 Sunset Drive – Rezoning to Allow a Two-Lot Subdivision

The table below the project rendering includes the basic development statistics. A location map (illustrating the location and context of the property) and additional development drawings are included as Appendix E. A Public Information Meeting for this project was held (virtually) on October 15, 2020.



	Existing Zoning (RS-1) Standards	Proposed Zoning (CD) – for two lots
Use	Detached (One-Unit) Residential	
Minimum Lot Requirements	Lot Width: 15.0m (49.2ft) Lot Area: 464.0m ² (4,994.6ft ²) Lot Depth: 27.4m (89.8ft)	Lot Width: 12.5m (41.0 ft) (per lot) Lot Area: 447.4m ² (4816 ft ²) (per lot) Lot Depth: 24.61m (80.7ft) (east lot) Lot Depth: 27.01m (88.5ft) (west lot)
Lot Coverage	40%	30% (per lot)
Height	7.7 metres	
Density (RGFA)	0.5 times the lot area	0.47 times the lot area (per lot)
Setbacks	Front: 7.5 m (24.61 ft) Rear: 7.5 m (24.61 ft) Interior Side: 1.5 m (4.95 ft) Exterior Side: 3.8 m (12.47 ft)	Front: 7.61 m (east lot) / 7.64 m (west lot) Rear: 7.56 m / 7.56 m Interior Side: 1.5 m / 1.5 m Exterior Side: 2.41 m (east lot line) / 3.83 m (west lot line)
Parking Spaces	Two per detached home and one per secondary suite	
OCP Land Use Designation	Mature Neighbourhood – allows single family homes (including secondary suites), duplexes, and triplexes, with density and height allowed per the applicable provisions in the Zoning Bylaw.	

14068 North Bluff Road – Proposed Triplex

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix F. A Public Information Meeting for this project was held (virtually) on September 23, 2020.



	Existing Zoning (RS-1) Standards	Proposed Zoning (RT-2)
Use	Detached (One-Unit) Residential	Attached (Three-Unit) Residential
Minimum Lot Requirements	Lot Width: 15.0m (49.2ft) Lot Depth: 27.4m (89.8ft) Lot Area: 464.0m ² (4,994.6ft ²)	Lot Width: 18.0m (59.0 ft) Lot Depth: 30.5 m (100.4 ft) Lot Area: 742.0m ² (7,986.8 ft ²)
Lot Coverage	40%	45%
Height	7.7 metres	
Density (RGFA)	0.5 times the lot area	0.6 times the lot area
Setbacks	Front: 7.5 m (24.61 ft) Rear: 7.5 m (24.61 ft) Interior Side: 1.5 m (4.95 ft)	
Parking Spaces	Two per detached home and one per secondary suite	Two per unit (six total)
OCP Land Use Designation	Mature Neighbourhood – allows single family homes (including secondary suites), duplexes, and triplexes, with density and height allowed per the applicable provisions in the Zoning Bylaw.	

1091 Stayte Road – Proposed Duplex

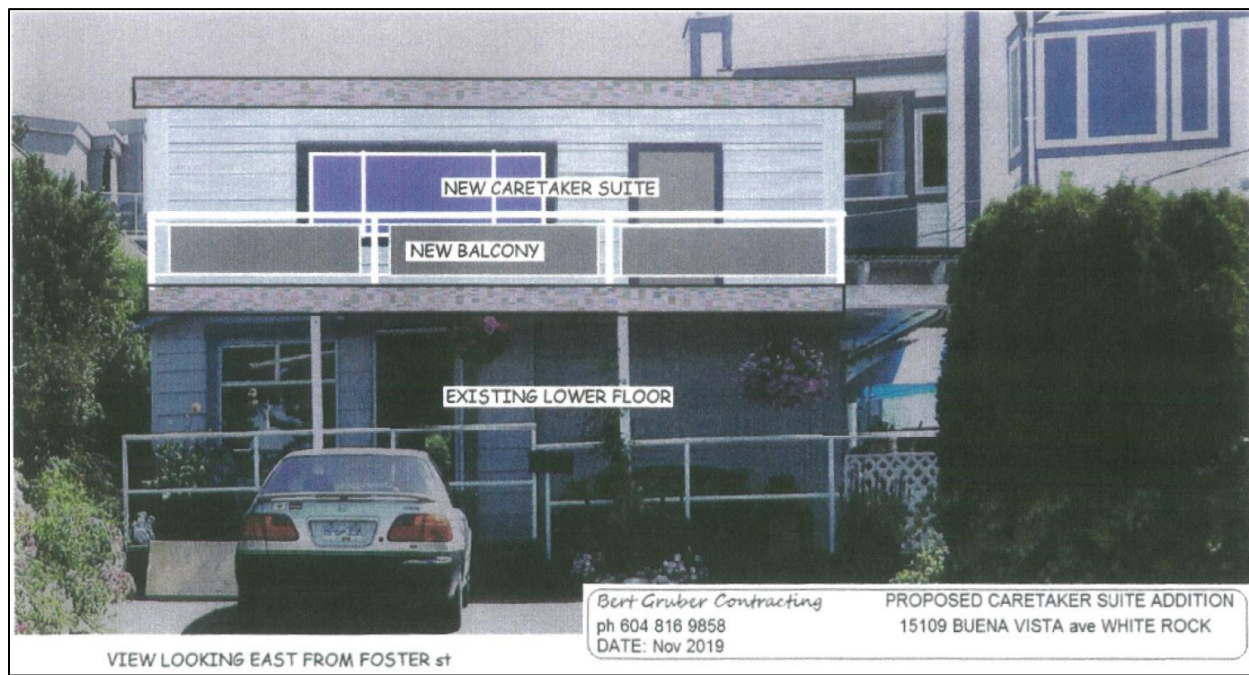
The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix G. A Public Information Meeting for this project was held (virtually) on October 8, 2020.



	Existing Zoning (RS-1) Standards	Proposed Zoning (RT-1)
Use	Detached (One-Unit) Residential	Attached (Two-Unit) Residential
Minimum Lot Requirements	Lot Width: 15.0m (49.2ft) Lot Depth: 27.4m (89.8ft) Lot Area: 464.0m ² (4,994.6ft ²)	Lot Width: 18.0m (59.0 ft) Lot Depth: 30.5 m (100.4 ft) Lot Area: 742.0m ² (7,986.8 ft ²)
Lot Coverage	40%	45%
Height	7.7 metres	
Density (RGFA)	0.5 times the lot area	0.5 times the lot area
Setbacks	Front: 7.5 m (24.61 ft) Rear: 7.5 m (24.61 ft) Interior Side: 1.5 m (4.95 ft)	
Parking Spaces	Two per detached home and one per secondary suite	Two per unit (four total)
OCP Land Use Designation	Mature Neighbourhood – allows single family homes (including secondary suites), duplexes, and triplexes, with density and height allowed per the applicable provisions in the Zoning Bylaw.	

15109 Buena Vista Avenue – Addition to allow Caretaker’s Suite

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix H. A Public Information Meeting for this project is tentatively scheduled (virtually) for November 12, 2020.



	Existing Zoning (CD-24)	Proposed Zoning (CD-24, as amended)
Use	Personal Care Use	Personal Care Use and Accessory Caretakers Suite
Height	7.7 metres (4.0 metres for accessory building)	7.7 metres (6.3 metres for accessory building)
Density	12 sleeping units for personal care use	12 sleeping units for personal care use plus caretakers suite
Setbacks (excerpt)	For an accessory building: 6.9 metres from an exterior side lot line	For an accessory building: 5.95 metres from an exterior side lot line
OCP Land Use Designation	Mature Neighbourhood – allows single family homes (including secondary suites), duplexes, and triplexes, with density and height allowed per the applicable provisions in the Zoning Bylaw. No change of land use from personal care is proposed in this application, only the addition of an accessory use (caretakers suite).	

1361 Finlay St – Proposed Duplex

The table below the project rendering includes the basic development statistics. An orthophoto (illustrating the location and context of the property) and additional development drawings are included as Appendix I. A Public Information Meeting for this project is tentatively scheduled (virtually) for November 26, 2020.



	Existing Zoning (RS-1) Standards	Proposed Zoning (RT-1)
Use	Detached (One-Unit) Residential	Attached (Two-Unit) Residential
Minimum Lot Requirements	Lot Width: 15.0m (49.2ft) Lot Depth: 27.4m (89.8ft) Lot Area: 464.0m ² (4,994.6ft ²)	Lot Width: 18.0m (59.0 ft) Lot Depth: 30.5 m (100.4 ft) Lot Area: 742.0m ² (7,986.8 ft ²)
Lot Coverage	40%	45%
Height	7.7 metres	
Density (RGFA)	0.5 times the lot area	0.5 times the lot area
Setbacks	Front: 7.5 m (24.61 ft) Rear: 7.5 m (24.61 ft) Interior Side: 1.5 m (4.95 ft)	
Parking Spaces	Two per detached home and one per secondary suite	Two per unit (four total)
OCP Land Use Designation	Mature Neighbourhood – allows single family homes (including secondary suites), duplexes, and triplexes, with density and height allowed per the applicable provisions in the Zoning Bylaw.	

15053 – Proposed Cannabis Store (through Temporary Use Permit)

The table below the proposed building elevation includes the basic proposal; no changes to the building are proposed but the proposed amendment to the Zoning Bylaw would allow for the applicant to also obtain a Temporary Use Permit for a cannabis store use in the existing building. An orthophoto (illustrating the location and context of the property) is included as Appendix J. A Public Information Meeting for this project has not yet been scheduled.



	Existing Zoning (CR-3)	Proposed Zoning (CR-3)
Use	Commercial and Multi-Unit Residential	Commercial (cannabis retail); proposal is to amend the eligible area for Temporary Use Permits for cannabis stores to include the subject property (and subsequently apply for the Temporary Use Permit).
Height	11.3 metres	
Density	1.75 FAR (gross floor area ratio)	
OCP Land Use Designation	Waterfront Village – allows multi-unit residential uses and mixed-uses (commercial/residential) in low-rise buildings, with a density of up to 2.0 FAR (gross floor area ratio) in buildings up to four storeys in height.	

FINANCIAL IMPLICATIONS

Application fees for zoning amendment applications that are refused by Council would, per the recent amendments to the City's Planning Procedures Bylaw, be subject to a refund minus 30% of the original fees, intended to cover administrative costs; where a Public Information Meeting has not yet been held a refund, minus 20% for administrative costs, would be processed. Council's denial of any of the above-listed zoning amendment applications would therefore result in a loss of revenue; however, the time and resources otherwise dedicated to advancing the review of the applications would be allocated to other tasks.

LEGAL IMPLICATIONS

Per section 479 of the *Local Government Act*, a City's zoning bylaw may regulate the use of land and the density of the uses of land, among other regulations that apply to buildings.

Section 460 of the *Local Government Act* establishes that where a local government has adopted a zoning bylaw it must define procedures under which an owner of land may apply for an amendment to the bylaw and must consider every application for an amendment.

The City's Planning Procedures Bylaw provides these application procedures, and by changing the steps for considering a zoning amendment application in the Planning Procedures Bylaw, Council would be able to provide earlier input to applicants who may be pursuing a project which Council would not support. If the recommended amendment to the Planning Procedures Bylaw is adopted, and a rezoning application is moved forward in the process, this does not fetter Council's final decision regarding the project and is not a guarantee that the project would be approved.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

If Council does deny some of the active rezoning applications, it would reduce the number of Public Information Meetings held related to development applications, but may help to avoid some of the concerns raised and experienced by members of the public when they feel they must strenuously oppose a proposal which Council ultimately would not approve.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

Not applicable.

CLIMATE CHANGE IMPLICATIONS

Not applicable.

ALIGNMENT WITH STRATEGIC PRIORITIES

Not applicable.

OPTIONS / RISKS / ALTERNATIVES

Alternatives, or supplemental resolutions to the staff recommendation at the start of this report include:

1. LUPC may direct that staff obtain additional information as specified by LUPC for certain applications be made available prior to deciding whether to advance or deny the application;
or

2. LUPC may advance applications and request a particular aspect of the application be given additional focus or scrutiny during the review process.

CONCLUSION

Council has adopted an amendment to the Planning Procedures Bylaw, which allows for early/initial consideration of rezoning applications prior to these applications proceeding to Public Information Meeting and Advisory Design Panel. This corporate report identifies ten (10) zoning amendment applications, which are currently active and, per direction on the approach for existing applications provided at the September 28, 2020 Council meeting, LUPC and Council have an opportunity to decide on whether each application should proceed to the next step or be denied.


Respectfully submitted,



Carl Isaak, MCIP, RPP
Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendation of this report.

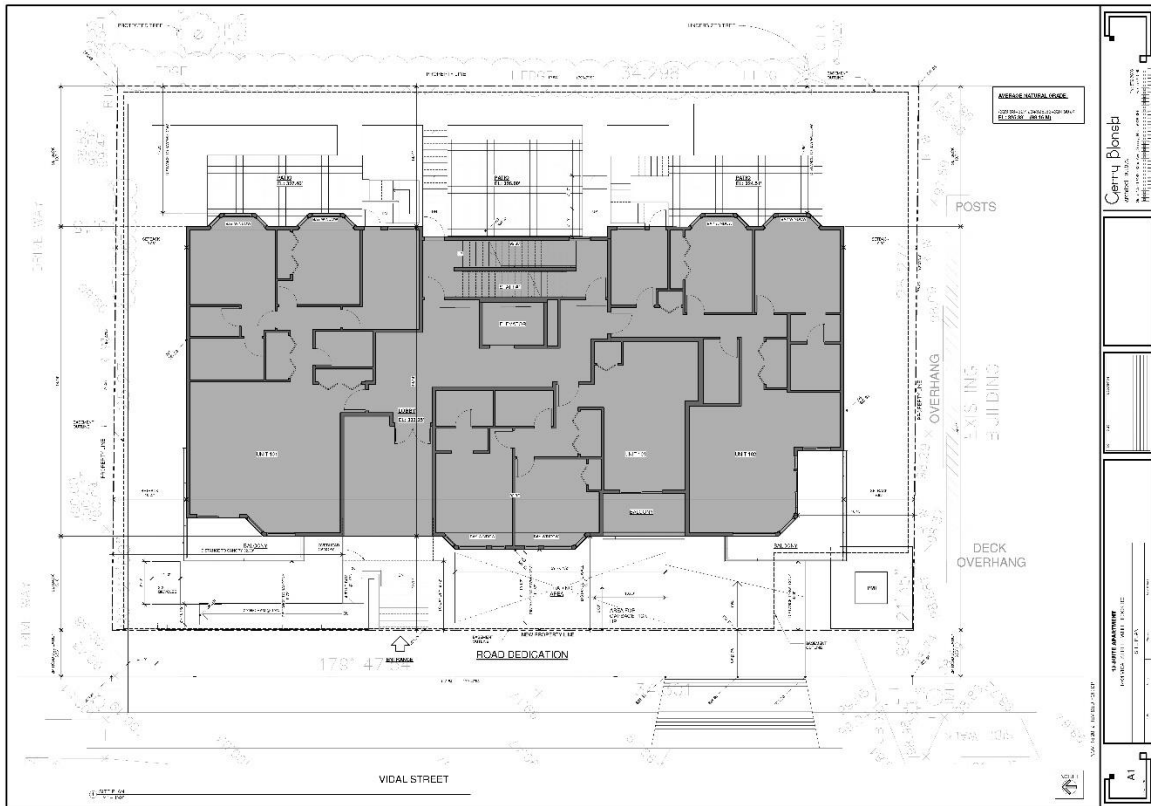


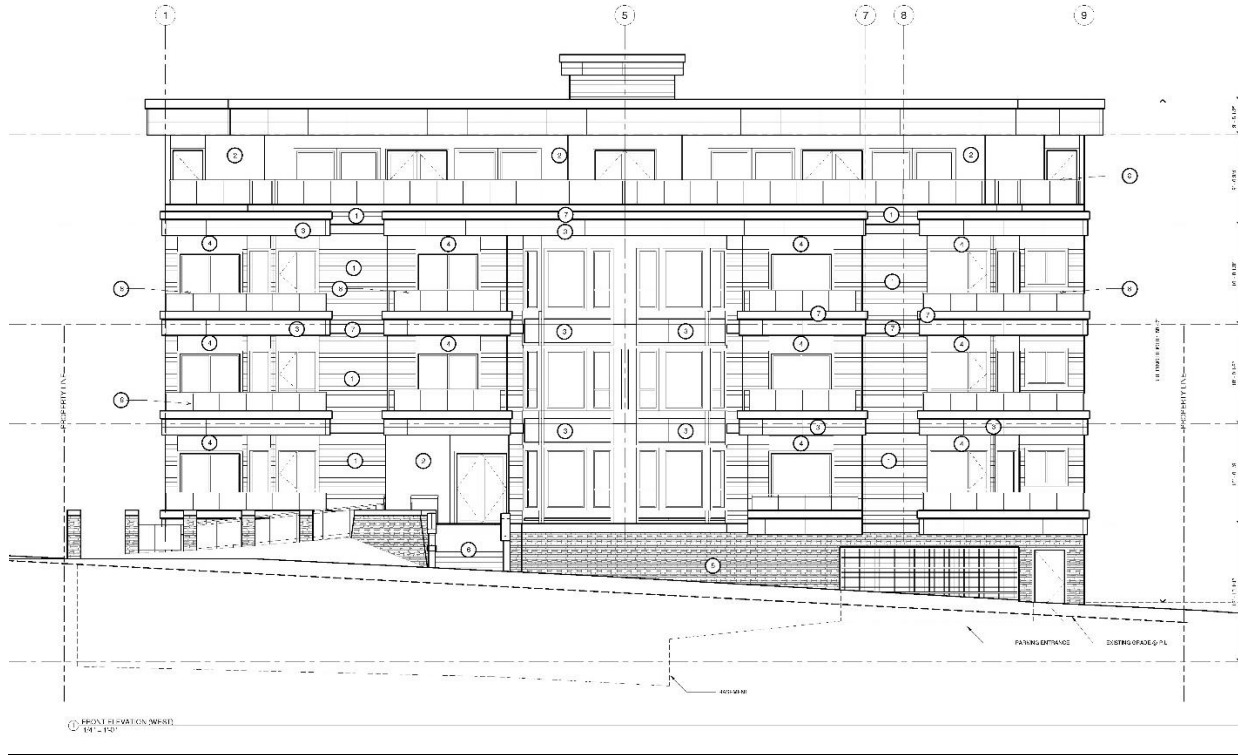
Guillermo Ferrero
Chief Administrative Officer

- Appendix A: Orthophoto and Development Drawings - 1464 Vidal Street
- Appendix B: Orthophoto and Development Drawings - 15963 Marine Drive
- Appendix C: Orthophoto and Development Drawings - 1441 Vidal Street (et al)
- Appendix D: Orthophoto and Development Drawings - 15704 North Bluff Road (et al)
- Appendix E: Location Map and Development Drawings - 14401 Sunset Drive
- Appendix F: Location Map and Development Drawings - 14068 North Bluff Road
- Appendix G: Location Map and Development Drawings - 1091 Stayte Road
- Appendix H: Orthophoto and Development Drawings - 15109 Buena Vista Avenue
- Appendix I: Orthophoto and Development Drawings - 1361 Finlay Street
- Appendix J: Orthophoto - 15053 Marine Drive

APPENDIX A

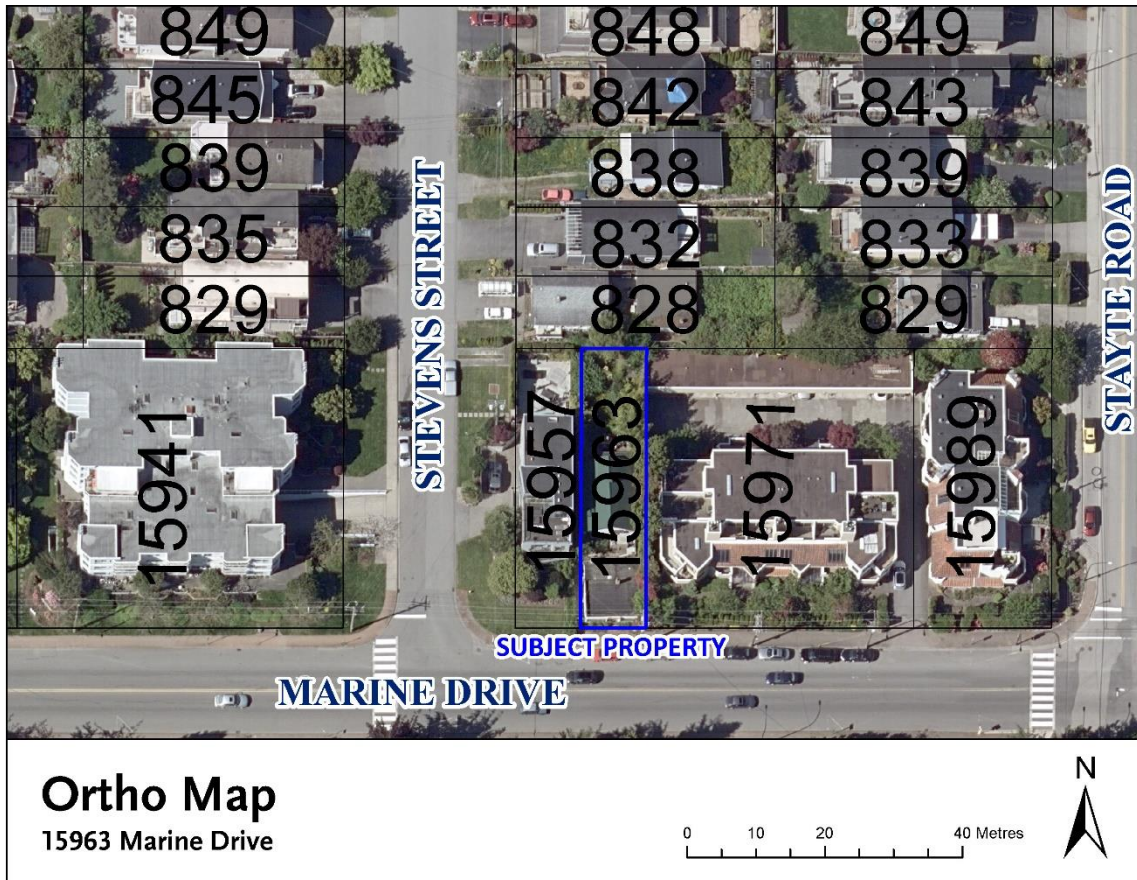
Orthophoto and Development Drawings - 1464 Vidal Street





APPENDIX B

Orthophoto and Development Drawings - 15963 Marine Drive



North Elevation
1/8" = 1'-0"

Key	Material
1	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR CHOROSOL
2	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR TOPEY
3	SMOKE ALUMINUM GLASS CURTAIN WALL
4	STEEL WINDOW FRAME - COLOUR CHOROSOL
5	STEEL F.F.L. GLASS PHOTO SLATS CURTAIN WALL - FRAME COLOUR CHOROSOL

East Elevation
1/8" = 1'-0"

Key	Material
1	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR CHOROSOL
2	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR TOPEY
3	SMOKE ALUMINUM GLASS CURTAIN WALL
4	STEEL WINDOW FRAME - COLOUR CHOROSOL
5	STEEL F.F.L. GLASS PHOTO SLATS CURTAIN WALL - FRAME COLOUR CHOROSOL
6	ALUMINUM AND GLASS SLIP
7	SPITTED FINE SAND-BLACK CONCRETE TEXTURED WALL
8	ALUMINUM SLAT CURTAIN WALL WITH SLIDING FRAME COLOUR CHOROSOL

ARCHITECTS
ANKENMAN MARCHAND

1963 Marine Dr
West Vancouver, BC V8V 2G7
Tel: (604) 273-2222
Fax: (604) 273-2223
www.ankenmanmarchand.com

Project: **1963 Marine Dr**
Owner: **1963 Marine Drive**
Drawing: **ELEVATIONS & MATERIALS**

Project Status: **DP**

SUBMISSION	
Date	Description

REVISION

No.	Date	Description

All Drawings of this set to be read in conjunction with the project description and approved by the appropriate authorities. The Architect is not responsible for the accuracy of the information provided by the client. The Architect is not responsible for the accuracy of the information provided by the client. The Architect is not responsible for the accuracy of the information provided by the client.

Scale: **1/8" = 1'-0"** DWG. NO.: **SD200**

North Elevation
1/8" = 1'-0"

Key	Material
1	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR CHOROSOL
2	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR TOPEY
3	SMOKE ALUMINUM GLASS CURTAIN WALL
4	STEEL WINDOW FRAME - COLOUR CHOROSOL
5	STEEL F.F.L. GLASS PHOTO SLATS CURTAIN WALL - FRAME COLOUR CHOROSOL

West Elevation
1/8" = 1'-0"

Key	Material
1	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR CHOROSOL
2	SMOOTH FIBER CEMENT PANEL PANEL SYSTEM - COLOUR TOPEY
3	SMOKE ALUMINUM GLASS CURTAIN WALL
4	STEEL WINDOW FRAME - COLOUR CHOROSOL
5	STEEL F.F.L. GLASS PHOTO SLATS CURTAIN WALL - FRAME COLOUR CHOROSOL
6	ALUMINUM AND GLASS SLIP
7	SPITTED FINE SAND-BLACK CONCRETE TEXTURED WALL
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Scale: **1/8" = 1'-0"** DWG. NO.: **SD205**

APPENDIX C

Orthophoto and Development Drawings - 1441 Vidal Street (et al)





1 SOUTH ELEVATION
Scale: 1:500



2 WEST ELEVATION
Scale: 1:500



3 EAST ELEVATION
Scale: 1:500



4 NORTH ELEVATION
Scale: 1:500



2 EAST ELEVATION BUILDING #1 UNPROTECTED OPENINGS DIAGRAM
Scale: 1:500

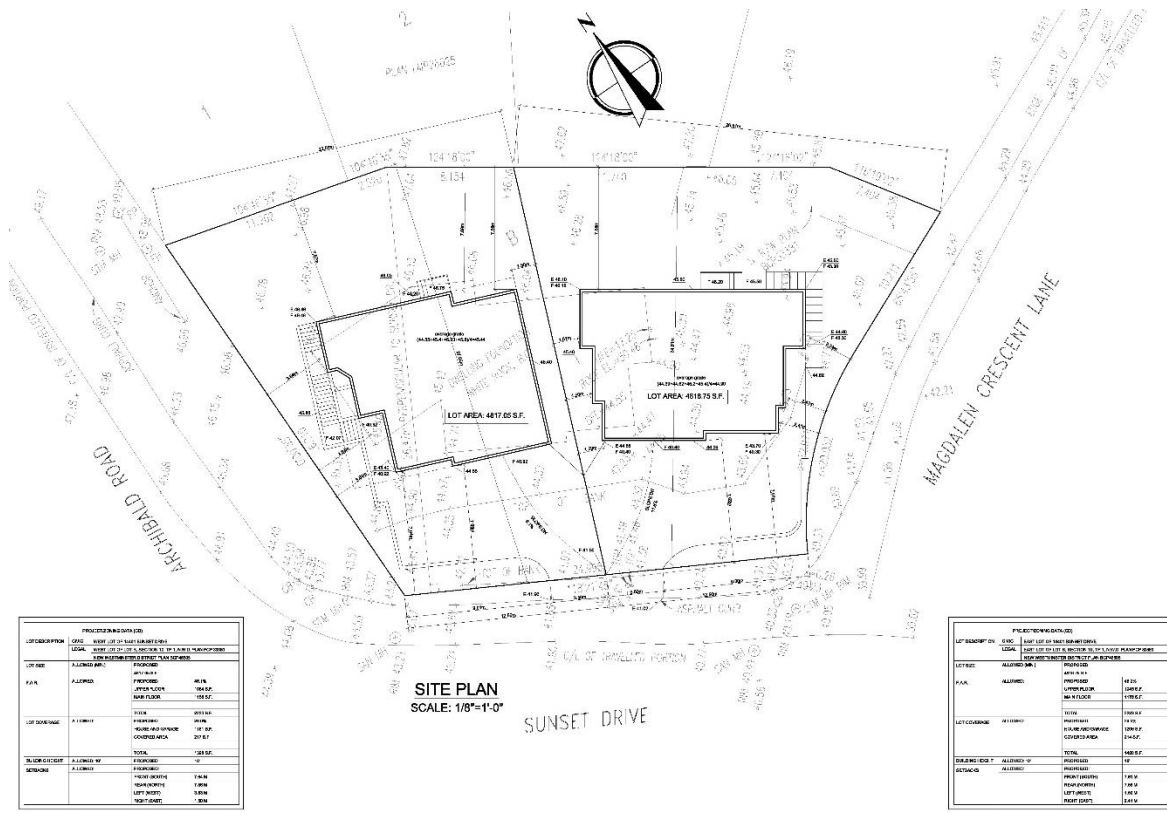
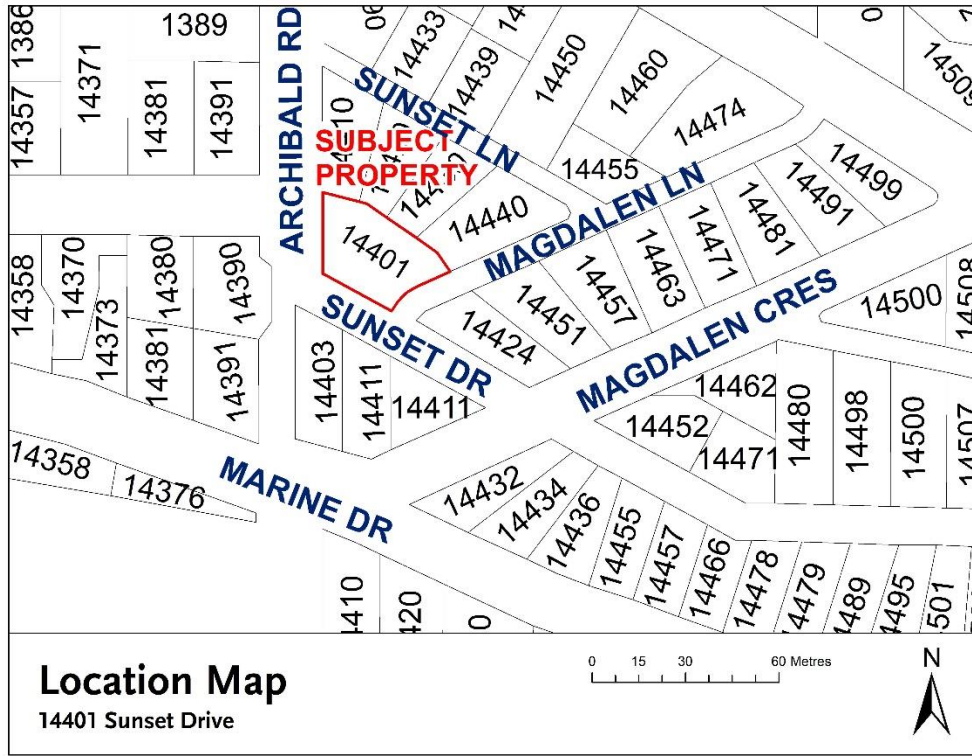


2 WEST ELEVATION BUILDING #2 UNPROTECTED OPENINGS DIAGRAM
Scale: 1:500

NOTES:
 1. UNPROTECTED OPENINGS ARE SHOWN IN RED.
 2. UNPROTECTED OPENINGS ARE SHOWN IN RED.
 3. UNPROTECTED OPENINGS ARE SHOWN IN RED.

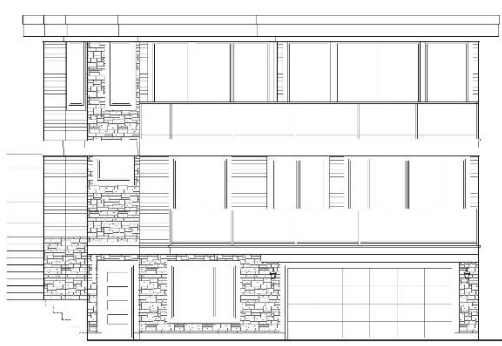
APPENDIX E

Location Map and Development Drawings - 14401 Sunset Drive

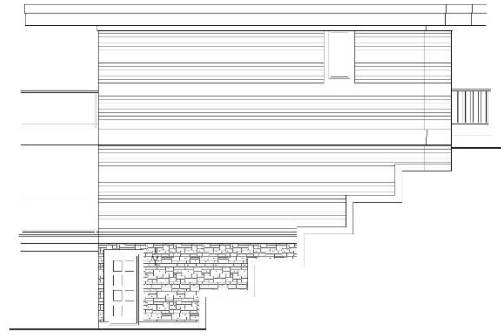
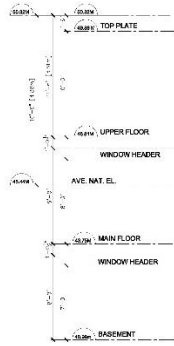


PROPOSED/ALLOWED		EXIST. LOT-TO-LOT DIMENSIONS	
LOT NO.	AREA (S.F.)	PROPOSED	EXIST.
14401	4817.05	4817.05	4817.05
14402	4518.75	4518.75	4518.75
TOTAL	9335.80	9335.80	9335.80

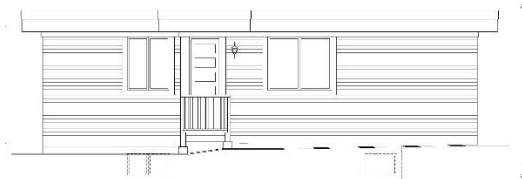
PROPOSED/ALLOWED		EXIST. LOT-TO-LOT DIMENSIONS	
LOT NO.	AREA (S.F.)	PROPOSED	EXIST.
14401	4817.05	4817.05	4817.05
14402	4518.75	4518.75	4518.75
TOTAL	9335.80	9335.80	9335.80



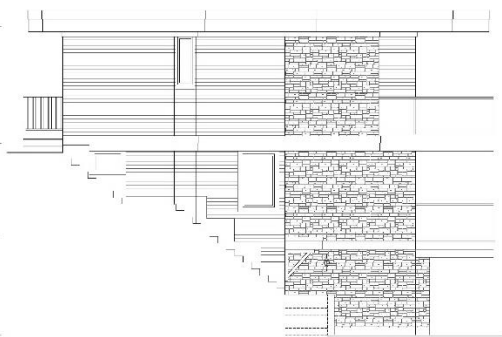
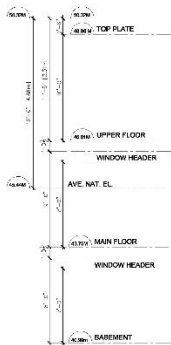
SOUTH ELEVATION
 1/4" = 1'-0"



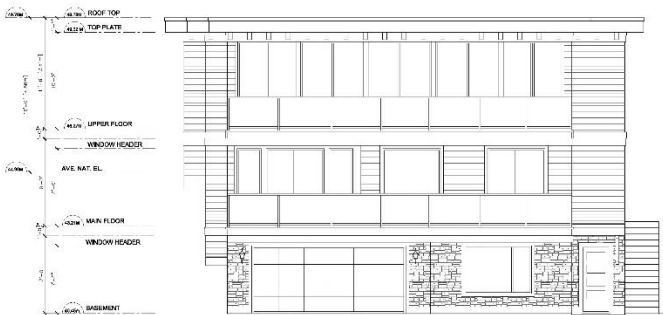
EAST ELEVATION
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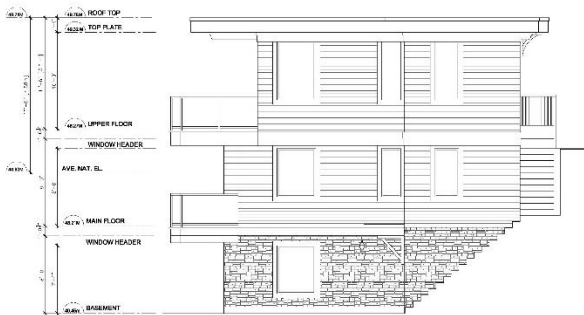
NORTH ELEVATION
 1/4" = 1'-0"



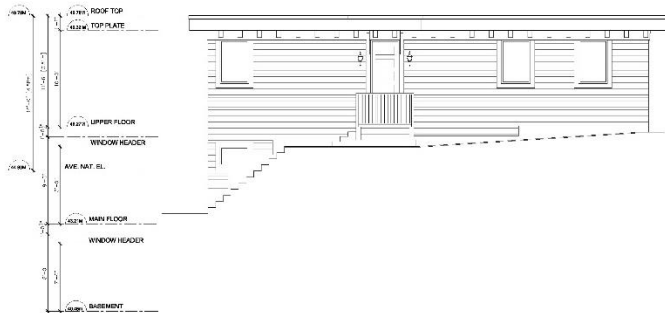
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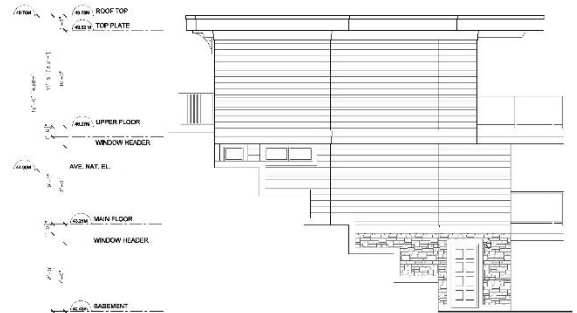
SOUTH ELEVATION
 1/4" = 1'-0"



EAST ELEVATION
 1/4" = 1'-0"



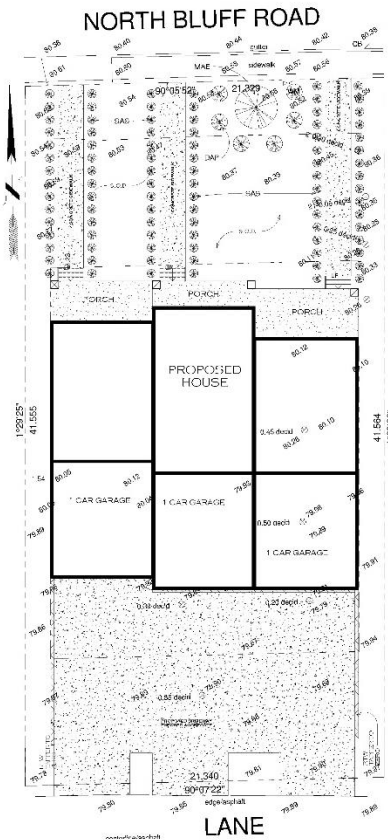
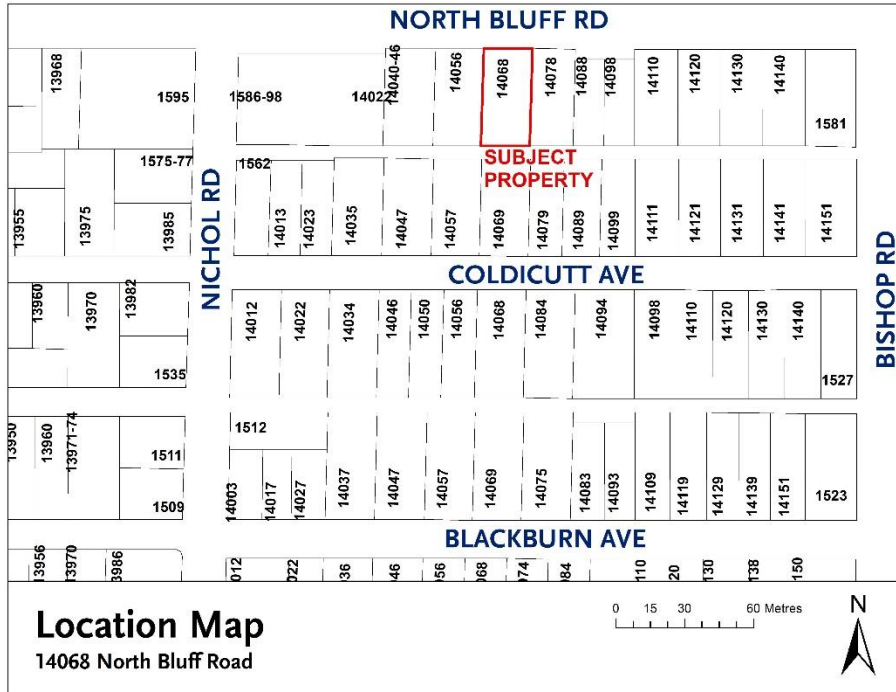
NORTH ELEVATION
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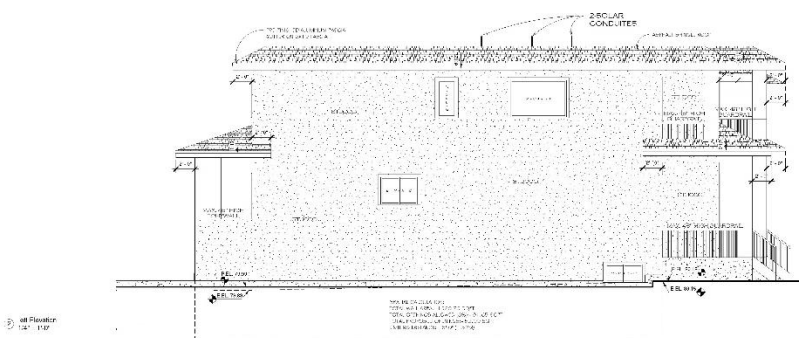
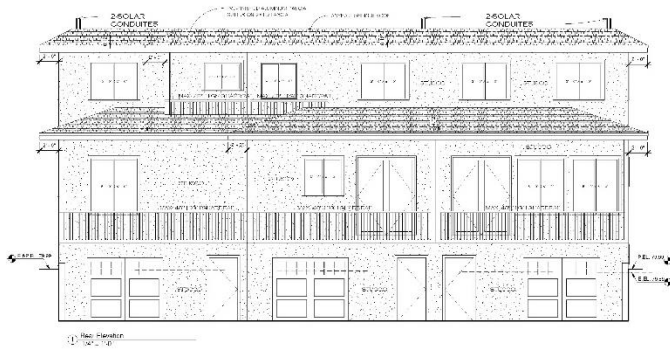
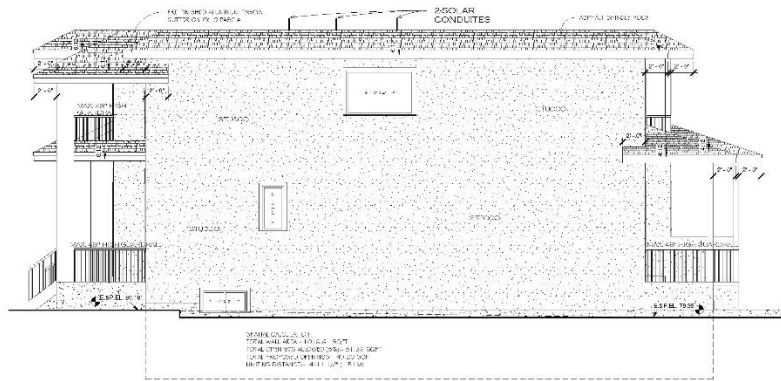
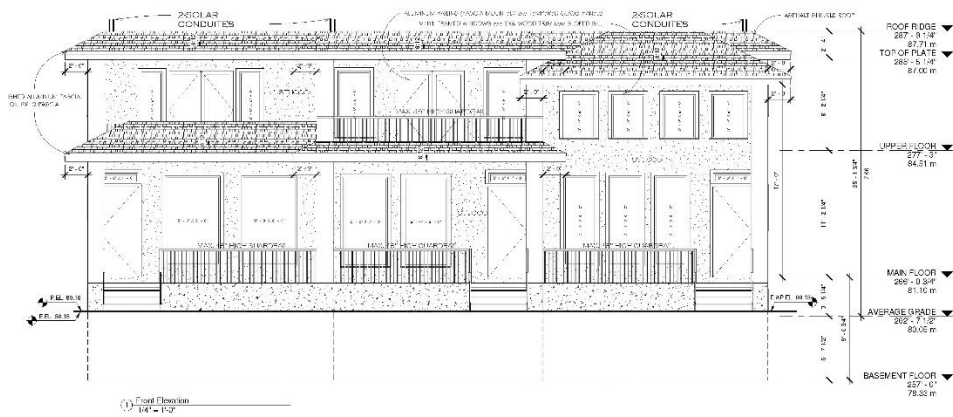


WEST ELEVATION
 1/4" = 1'-0"

APPENDIX F

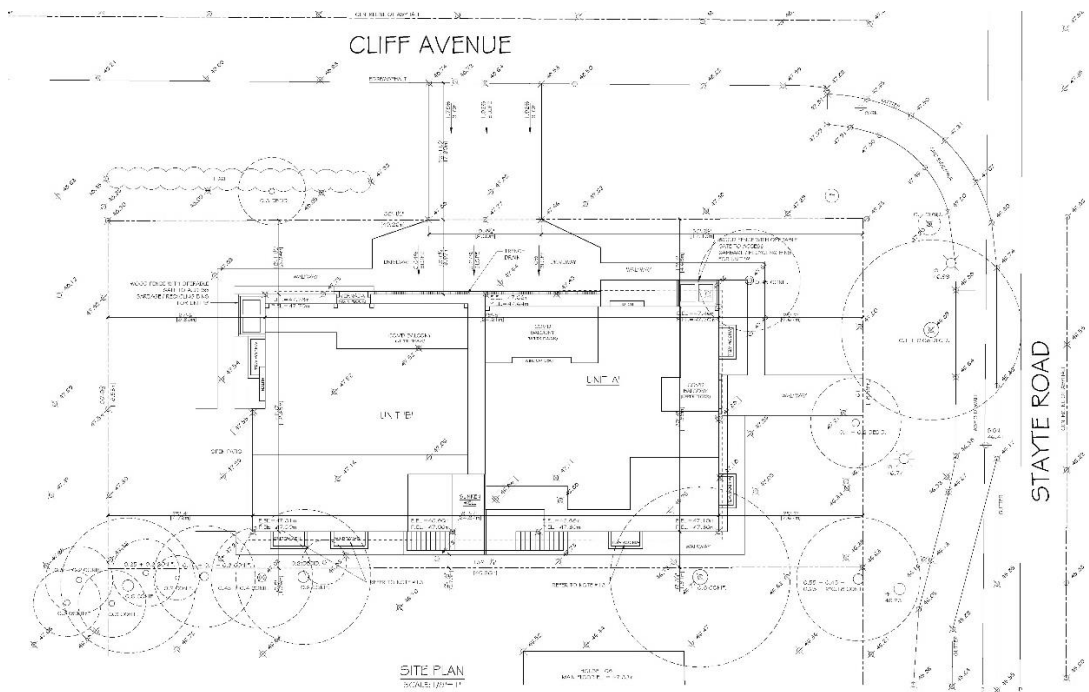
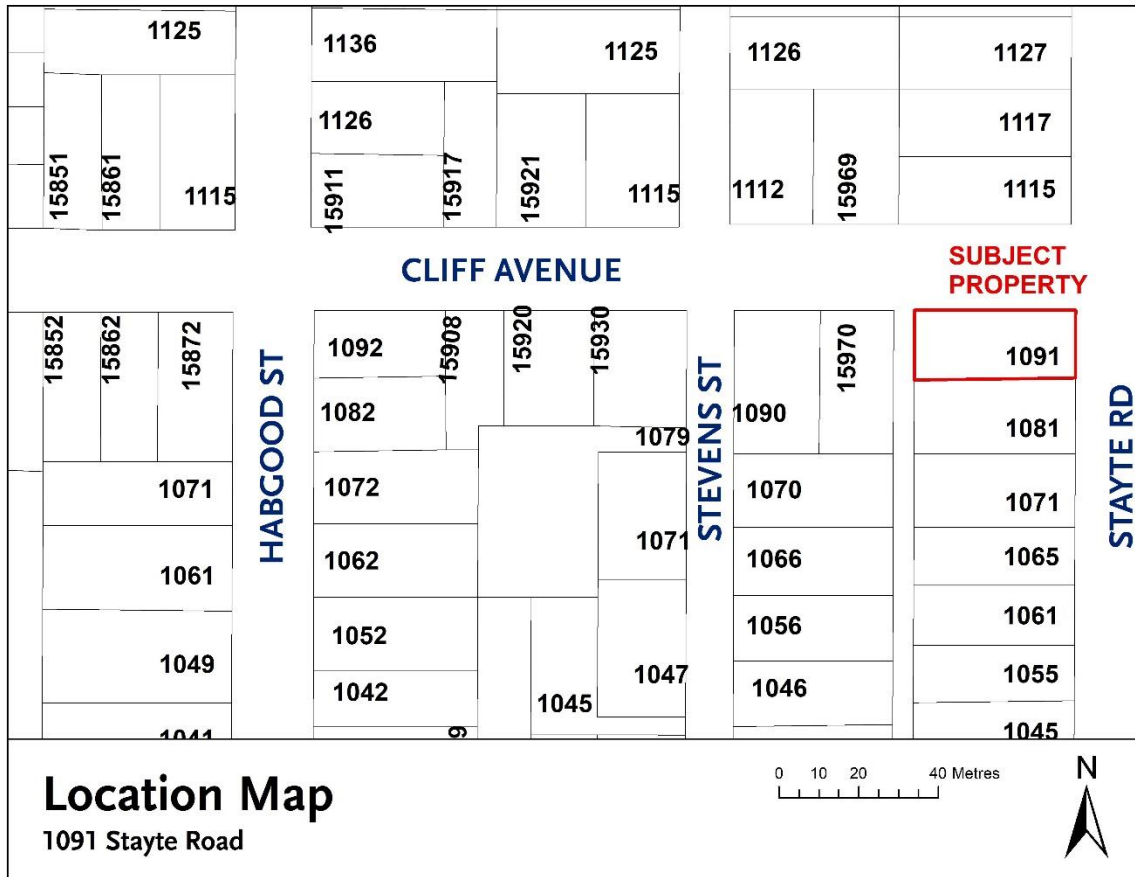
Location Map and Development Drawings - 14068 North Bluff Road

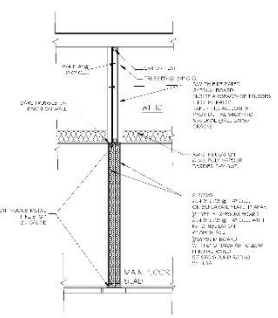
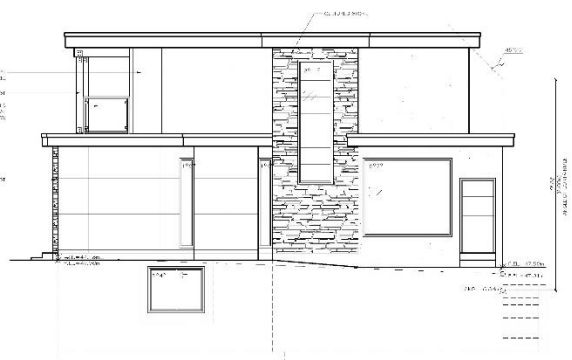
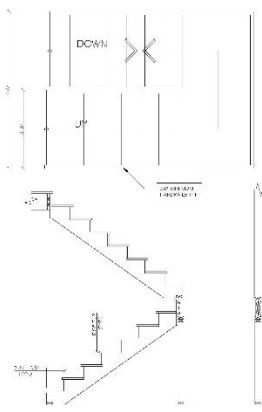
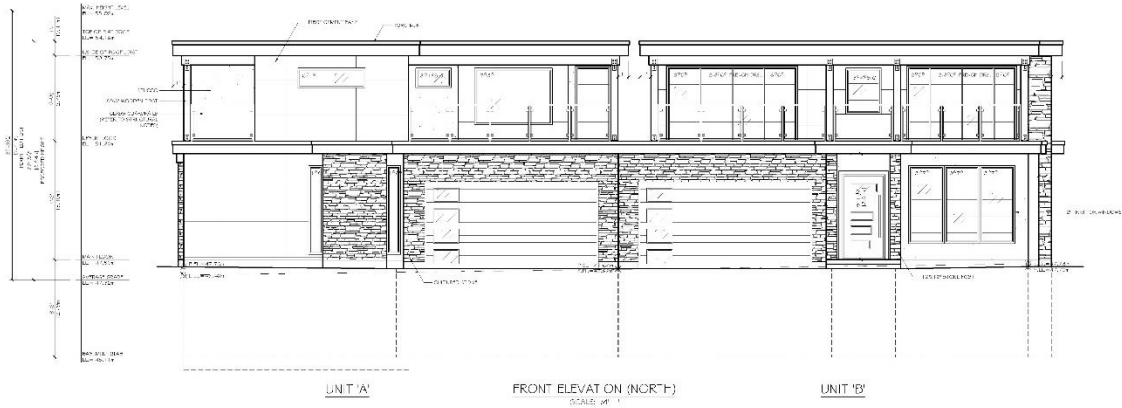




APPENDIX G

Location Map and Development Drawings - 1091 Stayte Road

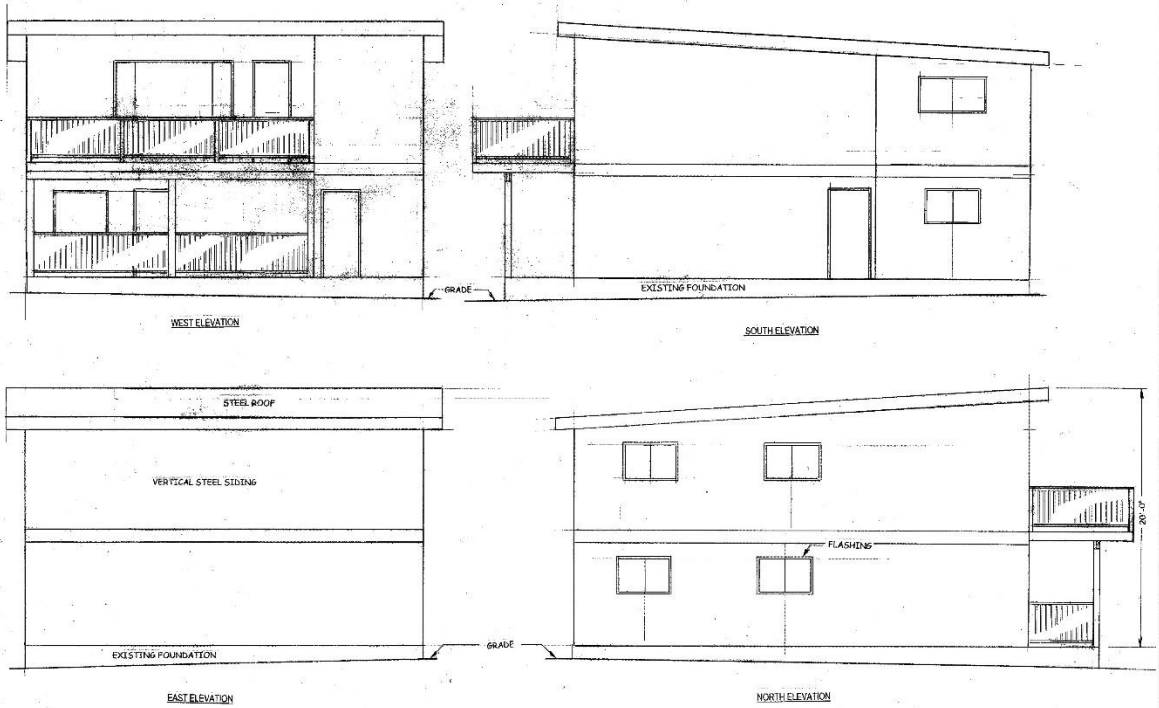
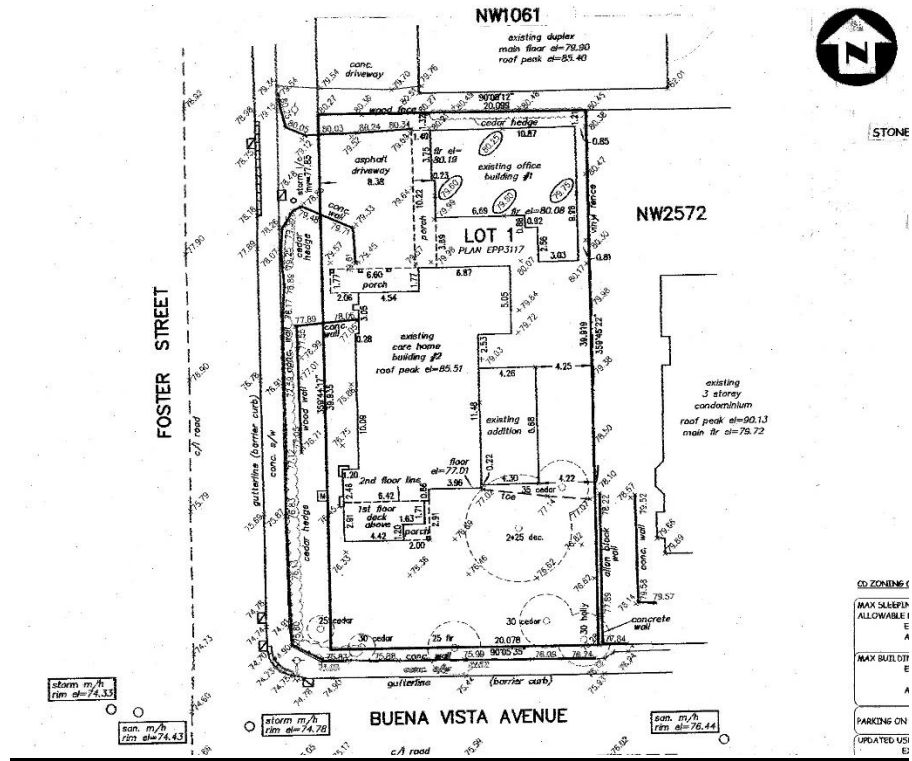




APPENDIX H

Orthophoto and Development Drawings - 15109 Buena Vista Avenue





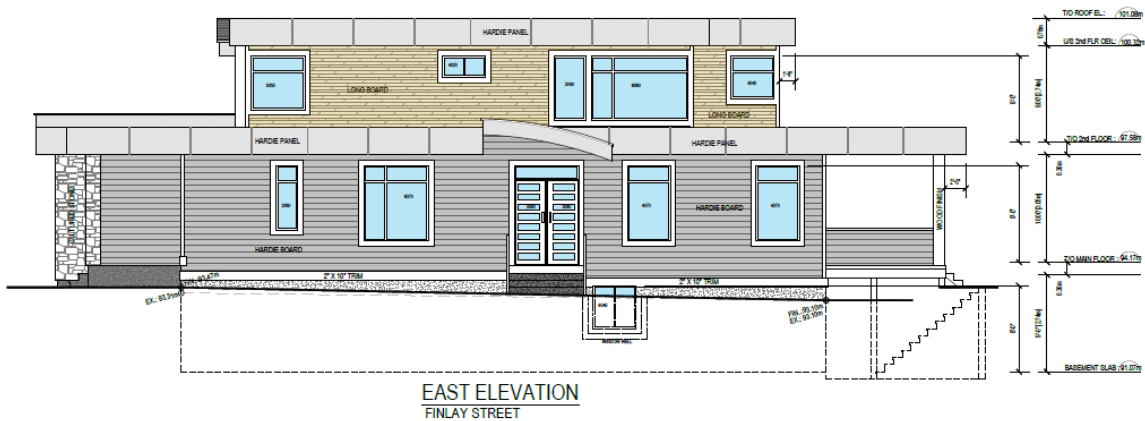
Bert Gruber Contracting
 614615188
 DATE: April 2020

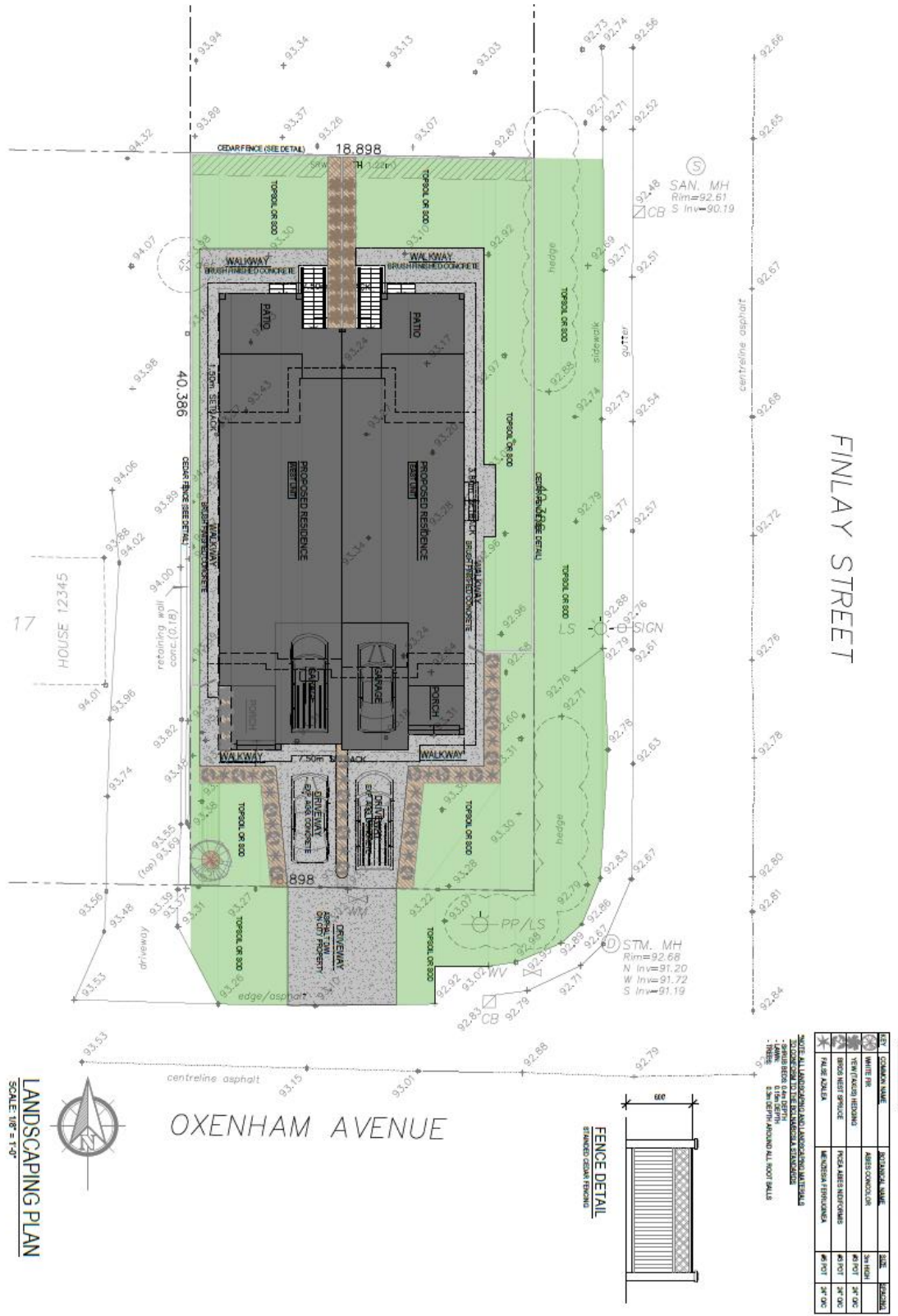
PROPOSED RENOVATIONS/CARETAKER SUITE
 1508 BUENA VISTA AVE WHITE ROCK

SCALE 1/4"=1'-0"
 ELEVATIONS
A3

APPENDIX I

Orthophoto and Development Drawings – 1361 Finlay Street





APPENDIX J

Orthophoto - 15053 Marine Drive



THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: **October 19, 2020**

TO: **Land Use and Planning Committee**

FROM: **Carl Isaak, Director, Planning and Development Services**

SUBJECT: **Early Review of Rezoning Applications (1363 Johnston Road & 1453 Stayte Road)**

RECOMMENDATIONS

THAT the Land Use and Planning Committee recommends that Council:

1. Resolve that the zoning amendment application at 1363 Johnston Road proceed to the next stage in the application review process or be denied; and
2. Resolve that the zoning amendment application at 1453 Stayte Road proceed to the next stage in the application review process or be denied.

EXECUTIVE SUMMARY

Recent amendments to White Rock Planning Procedures Bylaw, 2017, No. 2234 establish a process for the early review of rezoning applications by the City’s Land Use and Planning Committee (LUPC). This report presents two recent rezoning applications for which the proponents are seeking early input and direction from the Committee/Council. Staff are prepared to advance the review of the application on the basis of the recommendation of the Committee, subject to ratification by Council.

PREVIOUS COUNCIL DIRECTION

Resolution # and Date	Resolution Details
September 14, 2020 2020-443	THAT Council gives first, second, and third reading to "City of White Rock Planning Procedures Bylaw, 2017, No. 2234, Amendment (Initial Information Reports for Zoning Amendments) Bylaw, 2020, No. 2357".
September 28, 2020 2020-473	THAT Council give final reading to "City Of White Rock Planning Procedures Bylaw, 2017, No. 2234, Amendment (Initial Information Reports For Zoning Amendments) Bylaw, 2020, No. 2357"

INTRODUCTION/BACKGROUND

The City of White Rock has recently received two (2) zoning bylaw amendment applications associated with 1363 Johnston Road and 1453 Stayte Road. Each application is outlined below.

1363 Johnston Road – 10 Unit, Four-Storey, Apartment Building with Commercial at Grade

The following table provides an overview of the proposed development at 1363 Johnston Road. An ortho photo (illustrating the location and context of the property) and additional development drawings are included as Appendix A.

	Existing	Proposed
OCP Land Use Designation	Lower Town Centre <i>OCP policies recognize multiunit residential uses and mixed (commercial / residential) uses in low-to-mid rise buildings. Density of up to 3.5 FAR in buildings up to 6 storeys in height.</i>	
Zone	CR-2 Lower Town Centre Area Commercial / Residential Zone	CD Comprehensive Development Zone
Use	Multi-unit residential use and retail (commercial) uses	Four-storey, 10 unit residential building with commercial use at grade
Max. Height	10.7m (35.1ft)	16.06m (52.67ft) (four storeys)
Density	Maximum gross floor area (GFA) is 1.75 times the lot area	2.27 times the area of the lot
Parking Spaces	Varies depending on mix of uses: per 4.14.1 of the Zoning Bylaw	15 spaces (12 for residents + 3 for visitors)



1453 Stayte Road – 23 Unit (Three-Storey) Townhouse Development

The following table provides an overview of the proposed development at 1453 Stayte Road. An ortho photo (illustrating the location and context of the property) and additional development drawings are included as Appendix B. The subject property was the site of a proposed four-storey, 49 unit, apartment development which was denied by Council. The applicant has provided a letter to accompany the revised proposal (see Appendix C).

	Existing	Proposed
OCP Land Use Designation	Urban Neighbourhood: <i>OCP policies recognize multifamily residential uses in townhouse and low-rise buildings. Maximum density of up to 1.5 FAR in buildings up to 4 storeys in height.</i>	
Zone	RS-1 One Unit Residential Zone	CD Comprehensive Development Zone
Use	One-Unit Residential Use (secondary suite permitted)	23 three-storey townhouse units
Max. Height	7.7m (25.26ft)	12.04m (top of stairwell) | 10.06m (top of parapet wall) | 3 storeys of residential floor area
Density	Residential GFA limited to 0.5 times the lot area [3,164m ² max residential GFA = 1,582m ²]	0.962
Parking Spaces	2 per dwelling unit plus 1 for secondary suite, as applicable	46 residential spaces (2 per unit) plus 3 visitor parking spaces



FINANCIAL IMPLICATIONS

Not applicable.

LEGAL IMPLICATIONS

Not applicable.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

The two (2) applications presented in this corporate report have not yet been formally introduced to the public by way of a notice of application or any on-site signage. Should either or both of the

applications proceed, notice would be provided and a public information meeting (PIM) would be held to address early comments and interest from the public.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The early review of rezoning applications bring such applications before the Land Use and Planning Committee (LUPC) prior to referral to internal City departments, and several external agencies (e.g., School District, RCMP, etc.).

CLIMATE CHANGE IMPLICATIONS

Not applicable.

ALIGNMENT WITH STRATEGIC PRIORITIES

Not applicable.

OPTIONS / RISKS / ALTERNATIVES

Alternatives to the staff recommendation at the start of this report include:

1. LUPC may direct staff to obtain additional project-specific information prior to deciding whether to advance or deny the application; or
2. LUPC may advance applications and request a particular aspect of the application be given additional focus or scrutiny during the review process.

CONCLUSION

Council has adopted amendments to the Planning Procedures Bylaw, which enable an early review of rezoning applications. This corporate report presents two (2) rezoning applications, one (1) tied to the property at 1363 Johnston Road and one (1) at 1453 Stayte Road. Staff will advance the review of each application based on the direction provided by the LUPC and any subsequent decision of Council.

Respectfully submitted,



Carl Isaak, MCIP, RPP.
Director of Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.



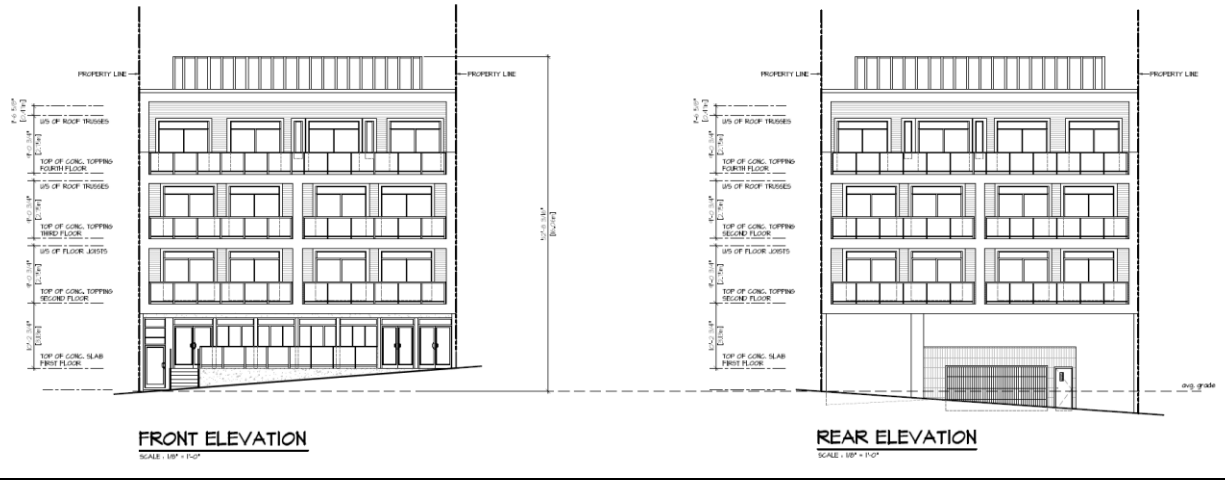
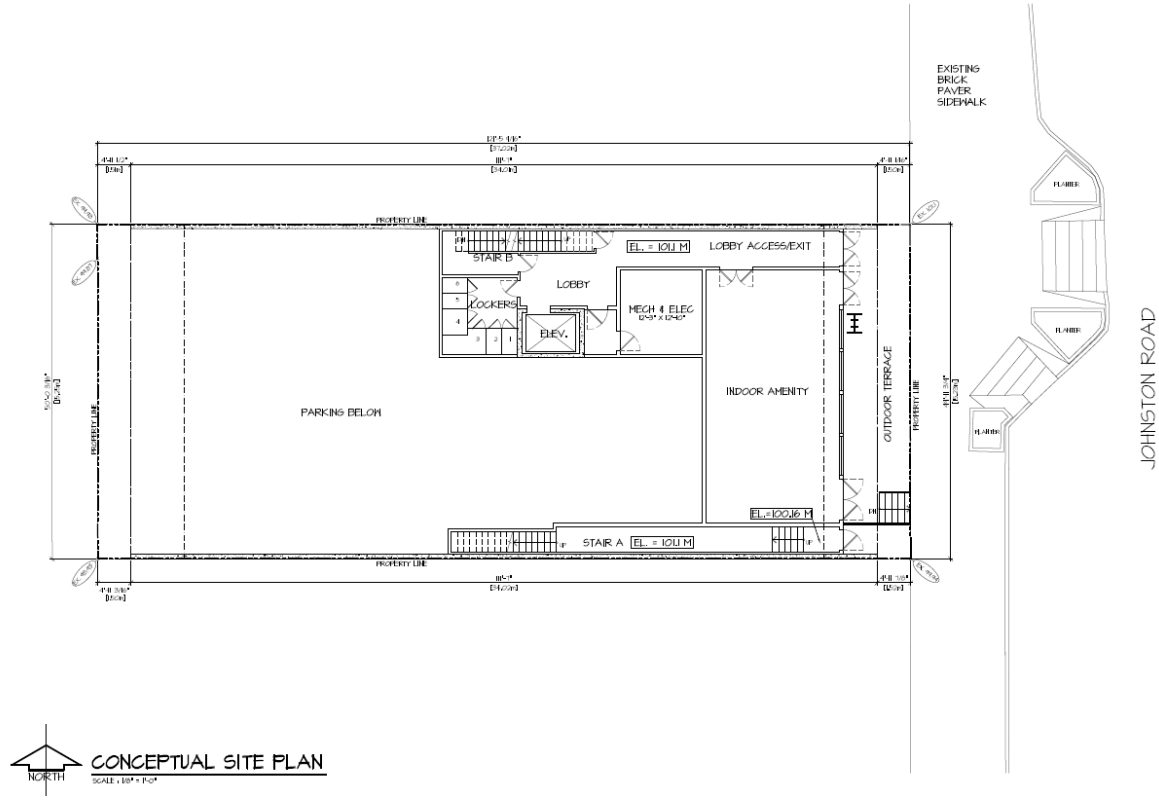
Guillermo Ferrero
Chief Administrative Officer

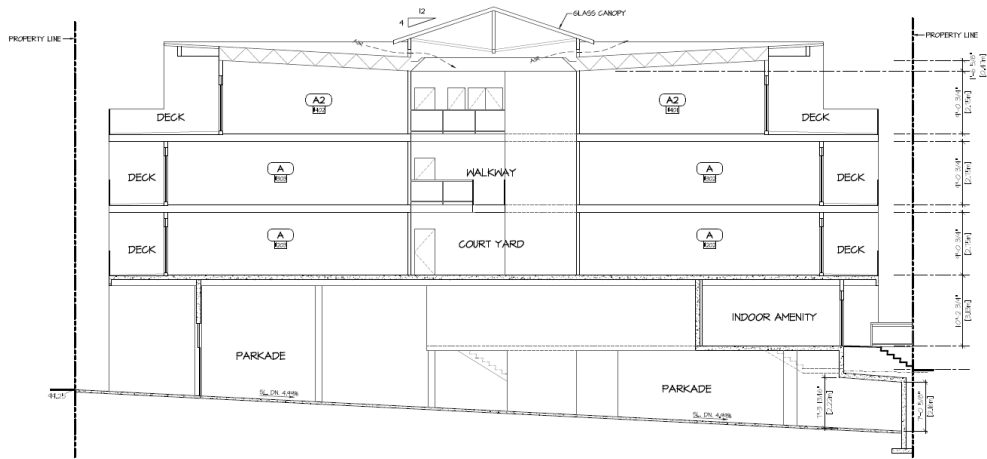
- Appendix A: Ortho Photo and Development Drawings – 1363 Johnston Road
- Appendix B: Ortho Photo and Development Drawings – 1453 Stayte Road
- Appendix C: Written Submission from Applicant - 1453 Stayte Road

APPENDIX A

Ortho Photo and Development Drawings – 1363 Johnston Road





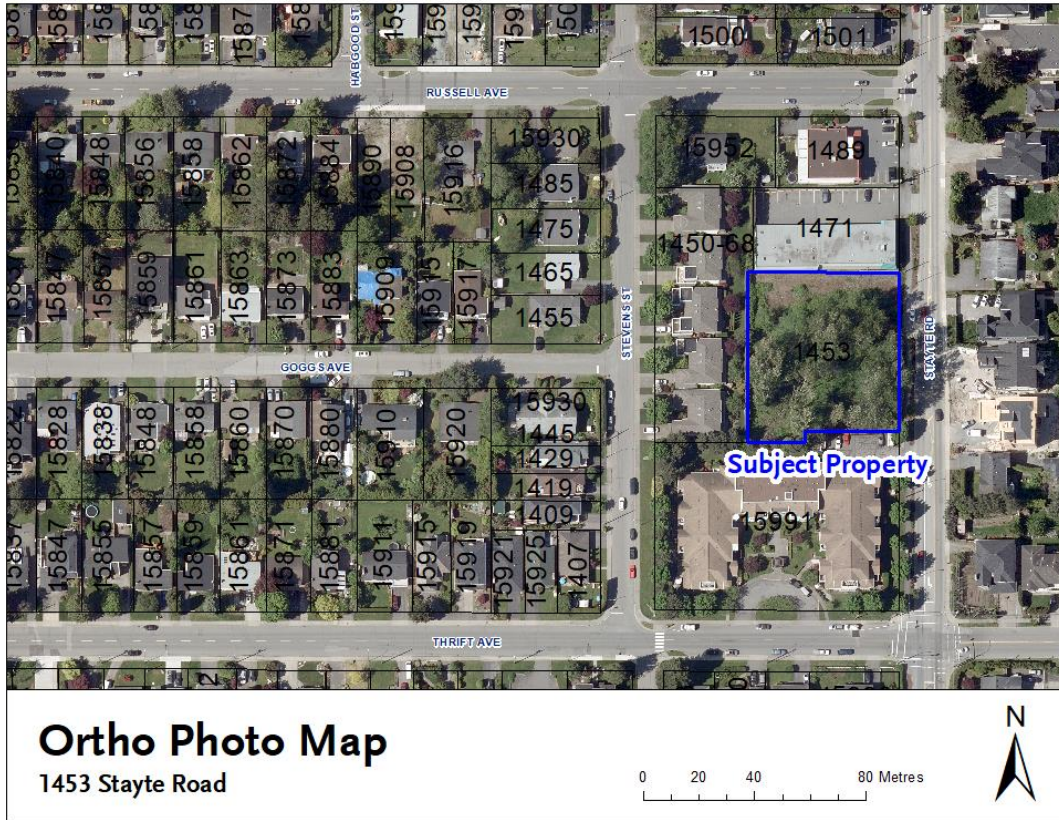


SECTION A
SCALE: 1/8" = 1'-0"



APPENDIX B

Ortho Photo and Development Drawings – 1453 Stayte Road





APPENDIX C

Written Submission from Applicant - 1453 Stayte Road



Oct 5, 2020

Planning Department
City of White Rock
15322 Buena Vista Avenue,
White Rock, BC V4B 1Y6

Dear Planning Department,

Re: Rezoning Application 1453 Stayte Rd, White Rock

We hereby respectfully submit a rezoning/development permit application of a 23-unit townhouse residential development. The application completes a long three years process of designing and re-designing a project for this infill site. It was unfortunate and disappointing to us that our previous application was turned down on the third reading; however, we fully respect the council's decision and understand their position and acting on the public's best interest.

Before we initiated this new application, we carefully reviewed comments from the previous public hearing and Councillor's discussion. The major concern from the neighbourhood and councillors was the proposed density of the development, especially from the neighbours who lives in the adjacent apartment. We also noted that a large group of the neighbours along with some members from the Council suggested a smaller scale project such as three-storey apartment or ideally a townhouse development would be more suitable on this lot.

Although we still consider that a four-storey apartment with 49 units is not a large development and is the most cost efficient and economically feasible project for this lot, we also understand the necessity of compromise to the public interest in order to create a harmonious neighbourhood. On the positive side, everyone seems agree that the lot should be developed if we can address the issues raised with the previous development application.

After numerous discussions with our design team, we both agree the best approach to balance the interest of the public and us would be a completely new design. The new design would take planning from neighbouring Surrey townhouse developments, for example: South on 16th. While in discussion, we conclude that the potential development must meet the following criteria:

1. Lower density
2. Lower unit count while keep the units as much affordable as we can
3. Mitigate neighbour's concerns we collected through previous PIM and Public Hearing
4. Reduce the building cost to keep the project feasible
5. Keep the family friendly oriented design
6. Abide with the current OCP



Based on the above criteria, our design team and we decided that a townhouse development is so far the best option for us, which also minimize the impact to the neighbours. The current application substantially reduced the density and floor space ratio and creates a much smaller project. Below is a table to illustrate the differences between our previous and new application.

	Previous Application	New Application	Difference
Form	Apartment	Townhouse	
Building Height	4 Storey	3 Storey	-1
Floor space ratio	1.41	0.96	-0.45
Gross floor Area	47854 SQFT	32760 SQFT	-15094 SQFT
Density	62.7/Acre	29.4/Acre	-33.3/Acre
Unit Count	49 Units	23 Units	-26 Units

The new application poses a significant financial risk to us as the developer due to the substantially reduction of the floor area while building material costs are on the rise. But we believe the project is more favourable to the neighbours and presents great opportunities to potential buyers for finding an ideal affordable home in the City of White Rock. Moreover, we notice that newly townhome development is insufficient in White Rock area. Although It is not the perfect option for us, we want the community to know that we have heard their voices and we are determined to work with them to build a better neighbourhood.

It has been a long way for both the City of White Rock and us to arrive this point. We sincerely hope the council will favourably consider this application and looking forward to building this community together!

Sincerely,

A handwritten signature in black ink, appearing to read 'David Yang', with a long horizontal flourish extending to the right.

David Yang
Director
Vanhome Properties Inc.
Vanhome Ventures 1 LP
Phone: 604-370-7333
Email: david@vanho.ca

ITEM 4.5 ON TABLE SUBMISSIONS:

1464 Vidal Street G. Blonski, Aibc

1441 Vidal Street K.Maust, Weststone Group

14401 Sunset Drive M. and L. Carlson

15053 Marine Drive C. Grzywacz, Seed & Stone

15704 North Buff
Road South Surrey & White Rock
Chamber of Commerce



Gerry Blonski

architect a.i.b.c.

Suite 1A - 12468 - 82nd Ave., Surrey, B.C. V3W 3E9 Bus.: (604) 572-3608 Fax: (604) 572-3760 Email: gerry@gerryblonskiarchitect.com

October 19, 2020

ON TABLE
LUPC - OCT 19, 2020
ITEM 4.5

13 Suite Condominium
1464 Vidal Street
White Rock, B.C.

Council Initial Information Report White Rock File No. 16-010 – 1464 Vidal Street.

1. Property Upgrade

The existing building is the anomaly on Vidal Street. It was constructed as a duplex and is the last remaining low density building in the block. Its immediate neighbors on the East side are 3 storeys. Across the street on the West side 1501 Vidal St. there is the 12 storey, 88 suite Beverley condominium and a 6-storey apartment building with 129 residential rental units at 1441, 1443-1445, and 1465 Vidal Street that is in the Development Permit stage.

The proposed project at 1464 Vidal Street will be commensurate with the height and density of the existing neighborhood and the trend for new developments.

2. 1464 Vidal Street improvements

This project will improve Vidal Street by dedicating 2 M of frontage to widen and improve Vidal Street. New paving, sidewalk, curb and gutter, street lighting and boulevard landscaping to City standards will be part of the project.

3. Public Information Meeting held August 27, 2020

The majority of questions were concerned with features of the building, compliance with City standards, the concerns for traffic and on-street parking on Vidal St and Thrift Avenue.

The building will comply with ALL the zoning and Building Code requirements regarding, siting, size, parking, loading, construction, fire, life, safety and disabled requirements. NO relaxations are requested.

There was no opposition from the immediate neighbours on Vidal Street or on Blackwood Street. Immediately East of this project.

4. We have worked with City staff to review all the ADP comments of a previous application by a previous owner and incorporated current staff comments for this application.

Gerry Blonski, aibc

PROJECT RATIONALE

14937 THRIFT AVENUE, 1441, 1443-1445, AND 1465 VIDAL STREET

WS VIDAL PROPERTIES LP

October 15, 2020

DELIVERING ON THE OFFICIAL COMMUNITY PLAN

WS Vidal Properties LP, in collaboration with Keystone Architecture, has submitted an application to the City of White Rock to redevelop the properties at 14937 Thrift Avenue, 1441, 1443-1445, and 1465 Vidal Street. The proposal seeks to deliver on the priorities identified in the *City of White Rock's Official Community Plan*, including the growing and well documented need for *secured rental housing* with a particular focus on *enhancing housing choices for families and seniors*.

ENHANCING HIGH QUALITY, SECURE HOUSING CHOICES

WS Vidal Properties LP is proposing to bring *129 secured market rental homes* to the community, while also respecting the existing neighbourhood *character and context*. The *West Coast modern style*, ground oriented building form, landscape treatment, stone, wood and glass material elements have been designed to blend in with the existing neighbourhood character. Through this application, WS Vidal Properties LP is committed to developing a *high quality* housing project and is exploring the use of *innovative building techniques* and *noncombustible materials* to deliver an attractive building that will serve the community for the *long term*.

THOUGHTFUL BUILDING DESIGN

While the *Official Community Plan* allows building heights up to 12 storeys on the subject site, WS Vidal Properties LP are proposing a 6-storey building (4-storeys at the northern end, adjacent to The Beverley, due to the site's significant slope). The decision regarding the building *massing and height* has been carefully considered to *mitigate potential impacts* on neighbouring properties, in particular, The Beverley. To *preserve existing view corridors*, the building form is stepped back along Thrift to follow the sloped grading of the site and *minimize the impact of the building* on the street, as well as our neighbours to the north with a reduced elevation of 4-storeys. It is recognized that no project comes without impact; however, the building has *been carefully designed* to be *sensitive* to its surroundings, while also *expanding housing choices* for existing and future residents.



Above: West elevation showing the proposed building in context with the streetscape. While the overall building height is 6-storeys, due to the contour of the site, the height presents as 4-storeys south of The Beverley building.

VEHICLE PARKING & ELECTRIC CHARGING STATIONS

The proposal features 194 vehicle parking spaces. The application that has been submitted to the City of White Rock includes 19 parking spaces for electric vehicles, however, in response to community input, WS Vidal Properties LP has committed to equipping *100% of the vehicle parking spaces* in this development with *electric vehicle charging infrastructure*. The number of parking spaces proposed is a result of careful design provisions to the building form to *maximize tree retention* on the site. As a result, a slight parking variance (1.10% reduction) is being sought in order to preserve the existing trees along the site's boundary.

ENCOURAGING NEW & AFFORDABLE ALTERNATIVES TO VEHICLE OWNERSHIP

The proposed parking supply is expected to be sufficient in accommodating the parking demand based on the Institute of Transportation Engineers parking generation rate, given the proposed tenure. Recognizing that existing vehicle traffic is a concern for local residents, the project has been designed to encourage the use of *multi-modal transportation* options, including existing transit routes, regional cycling routes and ride sharing services. Additionally, WS Vidal Properties LP is exploring opportunities to offer *car share* as part of this application. As *car share services* are not currently offered in White Rock, this project presents an opportunity to provide new and more affordable *transportation alternatives* to vehicle ownership.

A COMMITMENT TO COMMUNITY ENGAGEMENT

For the past 26 years, WS Vidal Properties LP has developed a reputation for building *high quality* projects that have delivered *positive contributions* to communities throughout the *South Fraser Region*. With this project, WS Vidal Properties LP is committed to building *lasting relationships* through an open and transparent *consultation process*. WS Vidal Properties LP has formally reached out to our neighbours at The Beverley and remains committed to consulting and engaging with residents of The Beverley, other neighbours and local stakeholders throughout the process to listen, answer questions, and evolve and improve the proposal design in response to community input, where possible. If permitted to proceed, the project will support the City of White Rock to achieve their *housing goals* through the creation of new *secured market rental* housing supply, while also ensuring that White Rock remains an *inclusive community* with a *diverse housing mix* to suit the needs of the local residents.



Kim Maust

Vice President – Real Estate Development

Weststone Group

Marlon and Linda Carlson
14401 Sunset Drive
White Rock, BC V4B 2V6
604-374-0101
Email: marlon.carlson@mac.com

October 7, 2020

City of White Rock
Attention: Athena von Hausen
Re: Summary of CD Rezone Application for 14401 Sunset Drive

Dear Athena,

This summary is intended to provide a quick and easy overview of the rationale for our application. Further detail is available in the accompanying presentation.

We are the owners and live at 14401 Sunset Drive. Our home was built in the early 1970's and we are only the second owners. We have lived here for almost 14 years and continue to thoroughly enjoy every day of our life in White Rock. Over the years much has changed in our lives as well as the City of White Rock. Our desire is to stay in this location while building to meet our needs and the increasing needs of the City.

We propose subdividing our existing single lot into two and building two new homes. Using the OCP we have designed homes that will allow us to grow older in our neighborhood, will preserve views for our neighbors and will fit in with the community character. Also, this development will add to the existing housing base while respecting and preserving the area that we know and love.

We meet almost all of the requirements for subdividing our property in our RS-1 zoned neighborhood. In the areas that we are short of the requirements, we believe we have compelling circumstances that support our proposal. While we are modestly short on the required square footage and depth of the lots, we are an irregular shaped lot and also have no direct neighbors beside us.

Perhaps most significantly, we have registered restrictive covenants on the property that were put in place by the original owner more than 45 years ago. While some covenants are out dated and moot, the most significant one is that we are restricted to building heights on the property. This height restriction on the covenant is substantially lower than what the City of White Rock allows. The homes in our proposal have been designed by fully respecting each of these covenants. We have good relationships with our neighbors and we have openly shared our application with them and shown that we are indeed properly complying with all of the covenants. As such the homes in our proposal actually calculate out to be a bit lower in height than the existing home. Additionally, we have designed the homes to conform to all the setbacks of the current RS-1 requirements.

Further depth can be found in the accompanying presentation. We ask for your support in what we believe will be a solid addition to our neighborhood and the City of White Rock. Thank you and we trust that this summary is helpful.

Yours truly,

Marlon and Linda Carlson

10/16/2020

TO: The Honorable Mayor and Council of White Rock
FR: Vikram Sachdeva,

I, Vikram Sachdeva, on behalf of Seed & Stone, have entered an application to operate a nonmedical cannabis retail store at 15053 Marine Drive, White Rock, and is proud to have earned the support of the property owner to proceed with the rezoning application.

Seed & Stone wants to be part of the revitalization of retail row along Marine Drive, that has unfortunately experienced a steady decline in recent years. Our community-oriented approach to our business practices is rooted in a forward-thinking attitude. We plan to enhance the popular community space with our stylish, modern, elegant retail design. Seed & Stone is about integrating our business into the community with harmony and making sure we remain a comfortable fit for everyone today – and *tomorrow*.

Local business will benefit from an increase of shoppers who walk and drive to visit Seed & Stone. Regional government studies across North American consistently show an increased boost in business to stores located near cannabis retail. Not only that, but it also shows a direct link to lower crime in the area. Much of that is due to the installation of a state-of-the-art security envelop that wraps around the entire store – in addition to increased uniformed security patrols at night.

This new growth in economic activity is creating jobs, providing high paying salaries, and elevating other local industries like transportation, food, tourism, banking, real estate, and construction, just to name a few.

I have personally reached out to community members and local businesses and have received many signatures of support (*see supporting document*). Through this process, I have also received feedback from the community on retail cannabis

Based on community feedback, Seed & Stone will:

- **Add commercial ozone filters to negate any smell, no products will be opened or consumed on site.**
- **Keep frosted windows in place although no longer a requirement.**
- **Remove the word cannabis from all White Rock signage to keep youth away from cannabis**
- **Hire through a community job fair**
- **Pay above living wage and offer medical and dental benefits to all employees**
- **Donate \$10,000 to community initiatives (enhancing the waterfront, strengthening transit and greening the City)**
- **Prevent any loitering and consumption in and around the retail location**

Seed & Stone has taken the communities safety concerns to heart and will bring an unheard level of security to our store, and as a result, the entire neighborhood.

Once our rezoning application is granted, we will perform an additional risk assessment tied to the geographical area we will be operating in. We will then apply the RCMP-backed **Crime Prevention Through Environment Design (CPTED)** model to the location.

In addition, Seed & Stone's security plan includes:

- **A dedicated monitoring station that is maintained 24/7 - 365**
- **Every inch of our stores interior and exterior will be monitored by cameras, alarms, and state of the art security sensors and locks.**
- **Uniformed, on-site security officers checking 2 pieces of ID upon entering.**
- **Up to 10, after hour visits from a security patrol team for site inspections.**
This will provide added deterrence to criminals who may target other businesses in the community.

With only a single licensed cannabis retail location in White Rock, residents are often forced to travel to neighbouring municipalities. It means folks are spending money that could've been spent in White Rock. It also means the community of White Rock is missing out on huge revenue gains. Our Chilliwack location paid over \$100,000 in PST over their first 6 months in business. Not only that, but lack of access also helps fuel un-regulated grey-market dispensaries.

For this reason, we request our application for cannabis retail be moved to the public hearing phase.

Thank you for your time and I appreciate all feedback
Vikram Sachdeva
CEO, Seed & Stone

Community engagement petition for a Government Licensed & Regulated Cannabis Retail Location at 15053 Marine drive, White Rock, BC

By signing this petition, you declare that you are 19 years and older date 1 Oct, 2020

In case of queries, please contact Vikram Sachdeva by email at vikrams@seedandstone.com

S. No.	Name	Address	Consent (Yes/ No)	Signature
	Chandro Indan Aishwarya Asha Pandan	14945 Marine Dr	Yes	
	Gagan Marble Slab Creamery	14967 Marine Drive	Yes	
	Dier Gift Store	14969 Marine DRive	yes	
	Choco flower	14973 Marine Dr	Yes	
	Alkush Chat House.	14981 Marine Drive	Yes	
	David Preddy	435-15850 26 Ave., Surrey, Bc	YES	
	Jamnet Wait Jon's on The Beach.	14989 Marine White Drive Rocks	Yes	
	Ocean Beach	14995 MARINE WHITE ROCK	Yes	
	Webber Zhuang	15019 Marine white rock drive	Yes	

Community engagement petition for a Government Licensed & Regulated Cannabis Retail Location at 15053 Marine drive, White Rock, BC

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S. No.	Name	Address	Consent (Yes/ No)	Signature
	Chantelle Pasco	1087 Finlay St White Rock	Yes	
	Nicole Murray	A-15440 VICTORIA AVE WATEROCK, BC	Yes	
	PATRICK MURRAY	A-15440 VICTORIA AVE WATEROCK, BC	Yes	
	Shrey Mehta	217-1952 152A Street, Surrey BC V4A 4M6	Yes	
	Tobias Tan	16363 36A Ave Surrey BC V3Z0M1 V3Z0M1	Yes	
	Vaughn Hodson	3502 150 ASt South Surrey, BC	Yes	
	Ramesh RASINDER SIKHA	13660 Marine Dr White Rock BC V4B 1A4	yes	
	RAKHI BHATNAGAR	2528 -156 ST South SURREY, BC V4P 0E5	yes	
	Abdulrahman Wazeer	15123 Marine Dr.	Yes Pis	

Community engagement petition for a Government Licensed & Regulated Cannabis Retail Location at 15053 Marine drive, White Rock, BC

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S. No.	Name	Address	Consent (Yes/ No)	Signature
	Tyger Blue	15061 Marine Dr.	(Yes)	TB
	BALVI BHULLAR	15061 Marine Dr. Dock's Dive Fish & Chips	Yes	BH
	John Brown Threke	15065 Marine Dr. LaSesquett	Yes.	JB
	Mayas Aimee Webb	15045 White Rock Marine Dr.	Yes -	AW
	Thi Affeir Polled Celato Naelia Brillante	150 Museum (White Rock)	Yes	Thi Polled
	MONTY SIKKA	13660 MARINE DR WHITE ROCK, BC	YES	MS
	Heather Cat Poppa	13588 16th Ave	Yes	HC
	Mark Catoppa	2165 123 St	Yes	MC
	Raj Sikka	13660 Marine Dr. White Rock, BC	Yes	RS

Dear Mayor Walker and White Rock City Council,

We are writing an urgent letter to you in advance of this evenings' White Rock Council meeting. As we understand it, one month ago, Council changed the procedure bylaw in approving development applications to have an early look at them before they go into the full process. Our understanding, however, is that applications that had already been started and are well underway are now being subjected to this new process. Also, the criteria for the decision making is not clear for today's meeting.

This raises a significant concern for all businesses that are looking to invest in White Rock. It does not give confidence to a business if they think rules and processes can be changed midstream. As you can appreciate, there is a significant amount of money, time, and resources that a business has to invest to start and continue to engage in a development application review. If a business has been following all the established rules and guidelines, it does not send a good signal to change it midstream and expect them to follow through when they have already been doing so under good faith.

It is fair that any new applications received after the new rules are established to be expected to follow through on new guidelines. We do think it is a good idea that Council would receive preliminary proposals to review before a formal application process begins and significant investment does occur by any business or development wishing to invest in White Rock.

We are submitting this letter on behalf of all business and chamber members that wish to invest in White Rock to know that procedures and rules are not changed midstream. We wish that all businesses know that they are being treated fairly once they begin any approval process in the City.

This issue came to our attention from one of our Chamber members, **Waterstock Properties**. Waterstock Properties has a development proposal being reviewed this evening. The project offers purpose-built below-market, moderate-income rental housing units located at 15704-15770 North Bluff Road. This project provides gentle density, with a mix of apartments and townhouses, in-line with the current Official Community Plan. The proposed 46 moderate-income rental homes are needed. The property is close to important amenities like the Peace Arch Hospital and Earl Marriott Secondary that employ teachers, first responders, care givers, other frontline workers and local professionals who are looking for affordable, reasonable housing close to where they work. This project is not just about building residential units, it's about the kind of livable and affordable community we want to create now and into the future.

Another consideration is that Waterstock Properties was scheduled to undergo its Design Panel Review tomorrow, Tuesday, October 20th. If the Design Panel Review had occurred last week, they would have been exempt from tonight's review. It's unfair for them to get prematurely cut tonight when they have prepared for the Design Panel Review to get feedback to incorporate into the project.

We want White Rock to be perceived as a favourable place to invest and business. We urge you to keep this particular project under consideration but also offer the same consideration of the other applications under review this evening.

Most Sincerely,

Patricia Lapena

President, Board of Directors

Ritu Khanna
Executive Director

South Surrey & White Rock Chamber of Commerce

1480 Foster Street, Unit 22, White Rock, BC V4B 3X7: Office: 604-536-6844

Join the network: www.sswrchamber.ca | [Facebook](#) | [Instagram](#) | [Twitter](#) | [LinkedIn](#)



Oct 5, 2020

Planning Department
City of White Rock
15322 Buena Vista Avenue,
White Rock, BC V4B 1Y6

Dear Planning Department,

Re: Rezoning Application 1453 Stayte Rd, White Rock

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Although we still consider that a four-storey apartment with 49 units is not a large development and is the most cost efficient and economically feasible project for this lot, we also understand the necessity of compromise to the public interest in order to create a harmonious neighbourhood. On the positive side, everyone seems agree that the lot should be developed if we can address the issues raised with the previous development application.

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Density	62.7/Acre	29.4/Acre	-33.3/Acre
Unit Count	49 Units	23 Units	-26 Units

The new application poses a significant financial risk to us as the developer due to the substantially reduction of the floor area while building material costs are on the rise. But we believe the project is more favourable to the neighbours and presents great opportunities to potential buyers for finding an ideal affordable home in the City of White Rock. Moreover, we notice that newly townhome development is insufficient in White Rock area. Although It is not the perfect option for us, we want the community to know that we have heard their voices and we are determined to work with them to build a better neighbourhood.

It has been a long way for both the City of White Rock and us to arrive this point. We sincerely hope the council will favourably consider this application and looking forward to building this community together!

Sincerely,

David Yang
Director
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