

The Corporation of the
CITY OF WHITE ROCK



Regular Council Meeting
AGENDA

Monday, July 25, 2022, 7:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

***Live Streaming/Telecast:** Please note that all Committees, Task Forces, Council Meetings, and Public Hearings held in the Council Chamber are being recorded and broadcasted as well included on the City's website at: www.whiterockcity.ca

T. Arthur, Director of Corporate Administration

Pages

1. CALL MEETING TO ORDER

1.1. FIRST NATIONS LAND ACKNOWLEDGEMENT

We would like to recognize that we are standing/working/meeting on the traditional unceded territory of the Semiahmoo First Nation, and also wish to acknowledge the broader territory of the Coast Salish Peoples.

2. ADOPTION OF AGENDA

RECOMMENDATION

THAT the Corporation of the City of White Rock Council adopt the agenda for its regular meeting scheduled for July 25, 2022 as circulated.

3. ADOPTION OF MINUTES

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RECOMMENDATION

THAT the Corporation of the City of White Rock Council adopt the following meeting minutes as circulated:

- Regular Council, July 11, 2022; and,
- Public Hearing/Public Meeting, July 18 for Bylaw 2434 (CD Zone 62 - 1453 Stayte Road), Bylaw 2435 (CD Zone 67 - 15704, 15724/28/38/48/58/70 North Bluff Road), and DVP 445 (15385 Semiahmoo Avenue).

3.1. SPECIAL PRESENTATION

3.1.a. OUTSTANDING CANADIANS ON THE PENINSULA 2022 AWARD RECIPIENTS

The 2022 Outstanding Canadians on the Peninsula Legacy Award recipients to attend to be recognized for their contributions to to the City of White Rock:

- Ramona Kaptyn
- Marie Sabine
- Upkar Singh Tatley

4. QUESTION AND ANSWER PERIOD

Question and Answer Period will be taking place both in person at the meeting, as well as electronically through email.

If you wish to have your question submitted electronically you may forward questions and comments to Mayor and Council by emailing ClerksOffice@whiterockcity.ca with Question and Answer Period noted in the subject line.

As of 8:30 a.m., July 20, 2022 there were no Question and Answer period submissions received.

Note: there are to be no questions or comments on a matter that will be the subject of a public hearing (time between the public hearing and final consideration of the bylaw).

RECOMMENDATION

THAT Council receive for information the correspondence submitted for Question and Answer Period by 8:30 a.m. July 25 2022, **including “On-Table”** information provided with staff responses that are available at the time.

4.1. CHAIRPERSON CALLS FOR SPEAKERS TO QUESTION AND ANSWER PERIOD

5. DELEGATIONS AND PETITIONS

5.1. DELEGATIONS

None

5.2. PETITIONS

None

6. PRESENTATIONS AND CORPORATE REPORTS

6.1. PRESENTATIONS

6.1.a. WHITE ROCK RCMP 2nd QUARTER PRESENTATION

Staff Sergeant Kale Pauls to provide a presentation regarding the RCMP April - June 2022 Quarterly report.

6.2. CORPORATE REPORTS

6.2.a. CITY OF WHITE ROCK'S 2021 ANNUAL REPORT

39

Corporate report dated July 25, 2022 from the Director of Corporate Administration titled "City of White Rock's 2021 Annual Report".

Note: City of White Rock's 2021 Annual Report can be viewed by Separate Cover

An opportunity for public participation will be available at this time.

Those wishing to submit questions/comments are encouraged to do so by:

- attending the meeting in person; or
- emailing **ClerksOffice@whiterockcity.ca** or mailing/dropping off their submissions to Corporate Administration - 15322 Buena Vista Avenue (deadline for written submissions is 12:00 p.m. [noon] on Monday, July 25, 2022).

Notice regarding the 2021 Annual Report was published in the July 14 and 21 editions of the Peace Arch News.

As of 8:30 a.m. on Wednesday, July 20, 2022 no submissions were received. Further submissions received up to the deadline will be presented to Council "On-Table" at the Council meeting.

***Mayor to call for speakers to the 2021 Annual Report at this time.**

RECOMMENDATION

THAT Council receive the written and verbal submissions regarding the 2021 Annual Report.

RECOMMENDATION

THAT Council endorse the City of White Rock 2021 Annual Report as circulated.

6.2.b. 2021 STATEMENT OF FINANCIAL INFORMATION

42

Corporate report dated July 25, 2022 from the Director of Financial Services titled "2021 Statement of Financial Information".

RECOMMENDATION

THAT Council:

1. Receive for information the July 25, 2022 corporate report from the Director of Financial Services and the Manager of Budgets & Accounting, titled "2021 Statement of Financial Information (SOFI)";
2. Approve the 2021 Statement of Financial Information attached as Appendix A; and
3. Authorize the Mayor to sign the document as approved on behalf of City Council.

6.2.c. INTERGRATED TRANSPORTATION INFRASTRUCTURE MASTER PLAN

86

Corporate report dated July 25, 2022 from the Director of Engineering and Municipal Operations titled "Integrated Transportation Infrastructure Master Plan".

Note: Brian Patterson, Urban Systems, to attend virtually to provide a presentation on this topic.

Note: White Rock Integrated Transportation Infrastructure Master Plan can be viewed by separate cover.

RECOMMENDATION

THAT Council:

1. Receive the report dated July 25, 2022, from the Director of Engineering and Municipal Operations, titled "Integrated Transportation infrastructure Master Plan" (ITIMP) and the July 2022 Integrated Transportation and Infrastructure Master Plan prepared and being presented by Urban Systems Ltd.
2. Endorse the final ITIMP report subject to Council feedback on specific details at the September 19, 2022, Regular Council Meeting.
3. Approve submission of the ITIMP report to the British Columbia Ministry of Transportation and Infrastructure as the fulfilment of the terms of the \$50K grant from the Ministry's Active Transportation Fund.

6.2.d. INTERPRETIVE SIGNAGE

90

Corporate report from the Directors of Engineering and Municipal Operations, Recreation and Culture and the Manager of Communications and Government Relations titled "Interpretive Signage Design Concept for the Pier".

RECOMMENDATION

THAT Council approve the recommended signage concept for an interpretive sign series to be installed on the Pier.

6.2.e.	PROGRESS UPDATE ON MULTI-FAMILY AND INSTITUTIONAL, COMMERCIAL AND INDUSTRIAL SOLID WASTE TRANSITION	95
	<p>Corporate report dated July 25, 2022 from the Director of Engineering and Municipal Operations titled "Progress Update on Multi-Family and Institutional, Commercial and Industrial Solid Waste Transition".</p>	
	<p><u>RECOMMENDATION</u> THAT Council:</p>	
	<ol style="list-style-type: none"> 1. Receive for information the corporate report dated July 25, 2022, from the Director of Engineering and Municipal Operations, titled "Progress Update on MF & ICI Solid Waste Transition; 2. Direct staff continue preparing the RFP documents; 3. Endorse the project schedule outlined in this Corporate Report including recommendation for awarding of the contract at Council's regular meeting on October 3, 2022. 	
6.2.f.	2023 - 2027 NOT FOR PROFIT PERMISSIVE TAX EXEMPTIONS BYLAW 2022, NO. 2436	100
	<p>Corporate report dated July 25, 2022 from the Director of Financial Services titled "2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436".</p>	
	<p><u>Note:</u> Bylaw 2436 is on the agenda for consideration of first, second and third readings under Item 8.1.a.</p>	
	<p><u>RECOMMENDATION</u> THAT Council receive the July 25, 2022, corporate report from the Director of Financial Services, titled "2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436".</p>	
6.2.g.	2023-2025 PERMISSIVE TAX EXEMPTIONS CENTRE FOR ACTIVE LIVING BYLAW, 2022, NO. 2437	111
	<p>Corporate report dated July 25, 2022 from the Director of Financial Services titled "2023-2025 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437".</p>	
	<p><u>Note:</u> Bylaw 2437 is on the agenda for consideration of first, second and third readings under Item 8.1.b.</p>	
	<p><u>RECOMMENDATION</u> THAT Council receive the July 25, 2022, corporate report from the Director of Financial Services, titled "2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437".</p>	

6.2.h.	BIA REQUEST FOR BEER GARDENS AT TD CONCERTS AT THE PIER	121
Corporate report dated July 25, 2022 from the Director of Recreation and Culture titled "BIA request for Beer Gardens at the TD Concerts at the Pier".		
<u>RECOMMENDATION</u>		
<u>THAT</u> Council approve the request from the White Rock Business Improvement Association (BIA) to hold “beer and wine” gardens hosted by the White Rock Beach Beer Company at two TD Concerts at the Pier performances on:		
<ul style="list-style-type: none"> • Thursday, August 18, 2022 from 6:00 p.m. – 10:00 p.m. at Five Corners (Johnston Road and Pacific Avenue), and • Thursday, August 25, 2022 from 6:00 p.m. – 10:00 p.m. at the East Beach parking lot east of Grand Chief Bernard Robert Charles Memorial Plaza). 		
6.2.i.	PROMENADE SCULPTURE COMPETITION UPDATE	127
Corporate report dated July 25, 2022 from the Director of Recreation and Culture titled "Promenade Sculpture Competition Update".		
<u>RECOMMENDATION</u>		
<u>THAT</u> Council:		
1.Receive for information the corporate report dated July 25, 2022, from the Director of Recreation and Culture, titled “Promenade Sculpture Competition Update”; and		
2. Waive the earlier requirement that White Rock Events Society secure commitments for 75% of the Society’s \$75,000 fundraising target (\$56,250) prior to any pedestals or sculptures being installed at the waterfront.		
6.2.j.	HEALTHY COMMUNITIES AGE FRIENDLY GRANT APPLICATION 2022 RESOLUTION	132
Corporate report dated July 25, 2022 from the Director of Recreation and Culture titled "Healthy Communities Age Friendly Grant Application 2022 Resolution".		
<u>RECOMMENDATION</u>		
<u>THAT</u> Council endorse the City’s BC Healthy Communities Age Friendly 2022 Grant Application and indicate support for city staff to provide overall grant management.		

- 6.2.k. **WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD-68-14937 THRIFT AVENUE AND 1441, 1443-45, AND 1465 VIDAL STREET) BYLAW, 2022, NO. 2439** 143

Corporate report from the Director of Planning and Development Services titled "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 - 14937 Thrift Avenue and 1441, 1443-45, and 1465 Vidal Street) Bylaw, 2022, No. 2439".

Note: Bylaw 2439 is on the agenda for consideration of first and second readings under Item 8.1.c.

RECOMMENDATION

THAT Council receive for information the corporate report from the Director of Planning and Development Services titled "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439".

- 6.2.l. **CONSIDERATION OF FIRST AND SECOND READING OF "WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (RT-1 - 1091 STAYTE ROAD) BYLAW, 2022, NO. 2438"** 356

Corporate report dated July 25, 2022 from the Director of Planning and Development Services titled "Consideration of First and Second Reading of "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (R-T -1 - 1091 Stayte Road) Bylaw, 2022, No. 2438".

Note: This item would usually go the Land Use and Planning Committee then onto the Regular Council meeting agenda. The applicant's revised submission and staff review were completed just short of the advertising deadline to notify a Land Use and Planning Committee meeting has been scheduled. Due to timing of the Council term, it was requested to place the item on the Regular Council meeting. Council may wish to consider this application directly or you may defer discussion / consideration until the next scheduled meeting date in September.

Note: If Council considers this item at the Regular Meeting, Bylaw 2438 is on the agenda for consideration of first and second reading under Item 8.1.d.

RECOMMENDATION

THAT Council receive for information the corporate report from the Director of Planning and Development Services titled "Consideration of 1st and 2nd Reading of White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438".

6.2.m. STATUS UPDATE OF COUNCIL'S 2022 TO PRIORITIES

Council's 2022 Top Priorities with new activity comments provided for information:

- **Solid Waste Pickup for Multi-Family:** Staff plan to present a report to Council on July 25th outlining the RFP posting and subsequent schedule for recommendation to Council on October 3rd. It is proposed that the implementation will be done in stages as properties become available; however, there are still concerns to be addressed with long term contracts.
- **Housing Needs / Affordable Housing:** On April 25, 2022 the draft Housing Strategy presented to Council from the Committee was removed by Council for consideration - later in the agenda some committee recommendations were received by Council and some were not approved / not endorsed. Council has directed the Housing Advisory Committee to focus on partnering with non-profit organizations and utilizing the funding and grants as outlined at the Affordable Housing workshop, held in April 2022, in furtherance of affordable seniors housing. Work on this is ongoing.
- **Community Amenity Contribution "Shovel-in-the-Ground" Projects:**
 - **Emerson Park Playground Upgrade:** Project is completed. Opening ceremony was held on July 4th. Staff to report to Council on additional options for playground equipment suitable for older children.
 - **Maccaud Park Upgrade:** Asphalt pathways are constructed and trees planted. Benches on site ready for installation. Landscaping to be completed in time for opening in September.
 - **Helen Fathers Centre Street Hillside Walkway:** The petition to the Court by three properties for an injunction to prohibit work until legal issues related to encroachments are resolved was denied. The hearings on these petitions are tentatively scheduled for August and September. The City Solicitor will update Council in Closed on July 25th.

- Review Options for Upgrading Multiple Hillside Walkways (Road Ends) to Waterfront: A funding application was submitted before the March 28th deadline, no further update at this time

- **The City's Relationship with the Semiahmoo First Nation (SFN):**
 - At the direction of Council, in response to the suggestion from Chief Harley Chappell, staff is researching the feasibility of permanently flying the SFN flag at City Hall. Staff will report back.
 - Manager, Communications & Government Relations has established a monthly 1:1 meeting with Councillor Jennine Cook who is now overseeing SFN communications.
 - City Events staff is working with SFN on enhanced programming for Semiahmoo Days for this year's Sea Festival/Semiahmoo Days joint event on the August long weekend.
 - Staff are working with SFN Council to see when they will be ready to meet again to discuss the draft Communications Memorandum of Understanding.

7. MINUTES AND RECOMMENDATIONS OF COMMITTEES

7.1. STANDING AND SELECT COMMITTEE MINUTES 415

RECOMMENDATION

THAT Council receive for information the following standing and select committee meeting minutes as circulated:

- Public Art Advisory Committee, July 12, 2022;
- Housing Advisory Committee, July 13, 2022; and,
- Arts and Cultural Advisory Committee, July 14, 2022.

7.2. STANDING AND SELECT COMMITTEE RECOMMENDATIONS

7.2.a. HOUSING ADVISORY COMMITTEE (COUNCILLOR MANNING, CHAIRPERSON)

7.2.a.a. HOUSING STRATEGY ACTION PLAN 429

Note: Committee report dated July 13, 2022, to the Housing Advisory Committee from the Manager of Planning, titled "Affordable Housing Framework" is attached for reference.

RECOMMENDATION

THAT Council support the integration of the following five strategies into the existing development process to create a long-term framework for affordable housing that creates the capacity to provide affordable housing in multiple ways:

1. Use the City's regulatory tools to encourage a diverse mix of housing types
2. Maximize use of City resources and financial tools
3. Build capacity with non-profit housing and service providers
4. Facilitate and strengthen partnership opportunities
5. Increase advocacy, awareness, and education roles

7.2.b. ARTS AND CULTURAL ADVISORY COMMITTEE (COUNCILLOR MANNING, CHAIRPERSON)

7.2.b.a. MULTI-COMMITTEE PLACEMAKING WORKSHOP FOLLOW-UP

RECOMMENDATION

THAT Council approve the scheduling of additional multi-committee roundtable placemaking discussion workshops in late July / early August 2022.

8. BYLAWS AND PERMITS

8.1. BYLAWS

8.1.a. BYLAW 2436 - 2023 - 2027 NOT FOR PROFIT PERMISSIVE TAX EXEMPTIONS BYLAW 2022, NO. 2436 480

Bylaw 2436 - A bylaw to provide an exemption from municipal property taxes under section 224 of the *Community Charter*.

Note: Bylaw 2436 was the subject of a Corporate Report under item 6.2.f.

RECOMMENDATION

THAT Council give first, second and third readings to the "2023-2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436".

8.1.b. BYLAW 2437 - 2023-2025 PERMISSIVE TAX EXEMPTIONS CENTRE FOR ACTIVE LIVING BYLAW 2022, NO. 2437 482

Bylaw 2437 - A bylaw to provide an exemption from municipal property taxes under section 224 of the *Community Charter*.

Note: Bylaw 2437 was the subject of a Corporate Report under Item 6.2.g.

RECOMMENDATION

THAT Council give first, second and third readings to the "2023 Permissive Tax Exemptions Centre for Active Living Bylaw, 2022, No. 2437".

8.1.c.

BYLAW 2439: WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD 68 - 14937 THRIFT AVENUE AND 1441, 1443-45 AND 1465 VIDAL STREET) BYLAW, 2022, NO. 2439

485

Bylaw 2439 - A bylaw to rezone four properties from the "RS-1 One Unit Residential Zone" to the "Comprehensive Development Zone (CD) 68". If approved, it would enable the proposed multi-building residential project that consists of 70 purpose built rental apartments ranging from 1 to 3-bedroom units to be built.

Note: This bylaw was the subject of a Corporate Report under Item 6.2.k.

RECOMMENDATION #1

THAT Council give first and second readings to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439" as presented.

RECOMMENDATION #2

That Council direct staff to schedule the public hearing for "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68- 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439".

RECOMMENDATION #3

That Council direct staff to resolve the following issues before bringing "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68- 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439" back for consideration of final adoption:

- a. **Ensure that all engineering requirements and issues, including road dedication and the execution of a Works and Servicing Agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;**
- b. **Ensure that all matters about tree protection and retention are addressed to the satisfaction of the Director of Planning and Development Services;**
- c. **Confirm that a tree protection covenant, if and as required, is registered on title to ensure the recommendations of the final Arborist Report, approved by the Director of Planning and Development Services and, more specifically, the City's Arboricultural Technician, are implemented and maintained through future demolition and construction activities;**
- d. **Complete the demolition of the existing buildings to the satisfaction of the Director of Planning and Development Services; and**
- e. **A statutory-right-of-way be registered on title regarding the community urban park space.**

Bylaw 2438 - A bylaw to amend the "White Rock Zoning Bylaw, 2012, No. 2000 to rezone the property from the "RS-1 One-Unit Residential Zone" to the "RT-1 Two-Unit (duplex) Residential Zone" at 1091 Stayte Road. If approved, it would allow for the construction of a duplex on the property.

Note: This bylaw was the subject of a Corporate Report under Item 6.2.i

RECOMMENDATION #1

THAT Council give first and second readings to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438".

RECOMMENDATION

THAT Council direct staff to schedule the public hearing for "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438".

RECOMMENDATION

THAT Council direct staff to address the following conditions prior to bringing "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438" back for consideration of final adoption:

- a. Ensure that all engineering requirements and issues, including road dedication and the execution of a Works and Servicing Agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;
- b. Ensure that the east pedestrian walkway is revised, and updated drawings are submitted and approved by the Director of Planning and Development Services;
- c. Registration of a Section No. 219 Covenant to restrict basement suites;
- d. Ensure that all matters pertaining to tree protection and retention are addressed to the satisfaction of the Director of Planning and Development Services.
- e. Confirm and ensure the recommendations of the final arborist report, approved by the Director of Planning and Development Services and, more specifically the city's Arboricultural Technician, are implemented and maintained through future demolition and construction activities; and
- f. Complete the demolition of the existing dwelling to the satisfaction of the Director of Planning and Development Services.

- 8.1.e. **BYLAW 2435: WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD ZONE 67 - 15704, 15724/28/38/48/58/70 North Bluff Road) Bylaw, 2022, No. 2435"** 506

Bylaw 2435: A Bylaw to amend the "White Rock Zoning Bylaw, 2012, No. 2000" to allow for a multi-building development at 15074, 15724/28/38/48/58/70 North Bluff Road. Bylaw 2435 received first and second reading on June 27th, and went to a Public Hearing on July 18th. It is on the agenda for consideration of third reading.

RECOMMENDATION #1

THAT Council give third reading to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD Zone 67 – 15704, 15724/28/38/48/58/70 North Bluff Road) Bylaw, 2022, No. 2435" as presented.

- 8.1.f. **BYLAW 2434- WHITE ROCK ZONING BYLAW 2012, NO. 2000, AMENDMENT (CD 62-1453 STAYTE ROAD) BYLAW, 2019, NO. 2434** 515

Bylaw 2434 - A bylaw to amend the "*White Rock Zoning Bylaw, 2012, No. 2000*" to enable the development of a 20 unit townhouse project at 1453 Stayte Road. This Bylaw was given first and second reading on June 13 and went to a Public Hearing held July 18, 2022. It is on the agenda for consideration of third reading.

RECOMMENDATION #1

THAT Council give third reading to "*White Rock Zoning Bylaw 2012, No. 2000, Amendment (CD Zone 62 - 1453 Stayte Road) Bylaw, 2022, No. 2434*".

- 8.1.g. **BYLAW 2433 - ROAD EXCHANGE BYLAW (14977 BEACHVIEW AVENUE), 2022, NO. 2433** 525

Bylaw 2433 - A bylaw to close and remove the highway dedication and to dispose of a portion of Beachview Avenue in exchange for road dedication on a portion of the property known as 14977 Beachview Avenue. This bylaw received three (3) readings on July 11, 2022 and is on the agenda for consideration of final reading.

RECOMMENDATION

THAT Council give final reading to "*Road Exchange Bylaw (14977 Beachview Avenue), 2022, No. 2433*".

- 8.2. **PERMITS**

None

9. **CORRESPONDENCE**

9.1. CORRESPONDENCE - RECEIVED FOR INFORMATION

Note: Further action on the following correspondence items may be considered. Council may request that any item be brought forward for discussion, and may propose a motion of action on the matter.

RECOMMENDATION

THAT Council receive correspondence Items 9.1.a - 9.1.b as circulated.

9.1.a. **#1792 Project - 230th ANNIVERSARY OF 15 SHIPS TO SIERRA LEONE: BLACK LOYALIST EXODUS** 529

Correspondence dated July 7, 2022 providing information on the 230th Anniversary of Black Loyalist exodus from Halifax to Freetown (Sierra Leone). The #1792Project is looking to raise awareness of this piece of history throughout Canada.

Note: Council Policy No. 109 notes that the City of White Rock does not make official proclamations. Items 9.1.a has been included under correspondence for public information purposes only.

9.1.b. **GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT DEVELOPMENT COST CHARGE AMENDING BYLAW** 533

Correspondence dated July 8, 2022 from Metro Vancouver to inform of their adoption of the GVS&DD Development Cost Charge Rate Amending Bylaw No. 353, 2022 as circulated.

Note: Development Cost Charges (DCC's): municipalities and regional districts levy DCC's on new development to pay for new or expanded infrastructure such as sewer, water, drainage, parks and roads necessary to adequately service the demands of that new development. DCC's are established by bylaw with the approval of the Inspector of Municipalities.

10. MAYOR AND COUNCILLOR REPORTS

10.1. MAYOR'S REPORT

10.2. COUNCILLORS REPORTS

11. MOTIONS AND NOTICES OF MOTION

11.1. MOTIONS

Councillor Chesney provided the following Notice of Motion at the July 11, 2022 Regular Council meeting for consideration at this time:

THAT Council direct staff to prepare a report on the feasibility of changing the City's Zoning/Building Bylaws that would state the following:

All future homes built in White Rock must have 40% of their land maintained on a permeable surface; and further this motion would also pertain to driveways which would be constructed with interlocking bricks and earth openings to diminish water runoff.

11.2. NOTICES OF MOTION

12. RELEASE OF ITEMS FROM CLOSED COUNCIL MEETINGS

13. OTHER BUSINESS

14. CONCLUSION OF THE JULY 25 REGULAR COUNCIL MEETING

Regular Council Meeting of White Rock City Council

Minutes



July 11, 2022, 7:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

PRESENT: Mayor Walker
Councillor Chesney
Councillor Johanson
Councillor Kristjanson
Councillor Manning

ABSENT: Councillor Trevelyan

STAFF: Jim Gordon, Acting Chief Administrative Officer
Tracey Arthur, Director of Corporate Administration
Anne Berry, Director of Planning and Development Services
Candice Gartry, Director of Financial Services
Eric Stepura, Director of Recreation and Culture (via electronic means)
Debbie Johnstone, Deputy Corporate Officer

PUBLIC: 38 (approximately)

1. **CALL MEETING TO ORDER**

The meeting was called to order at 7:00 p.m.

1.1 **FIRST NATIONS LAND ACKNOWLEDGEMENT**

We would like to recognize that we are standing/working/meeting on the traditional unceded territory of the Semiahmoo First Nation, and also wish to acknowledge the broader territory of the Coast Salish Peoples.

2. **ADOPTION OF AGENDA**

Motion Number: 2022-297 It was MOVED and SECONDED

THAT the Corporation of the City of White Rock Council adopt the agenda for its regular meeting scheduled for July 11, 2022 as amended to add on On-Table submission Item 4 Question and Answer Period.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

3. **ADOPTION OF MINUTES**

Motion Number: 2022-298 It was MOVED and SECONDED

THAT the Corporation of the City of White Rock Council adopt the June 27, 2022 Regular Council meeting minutes as amended to include the word "herbicide" within motion 2022-295 ".

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

4. **QUESTION AND ANSWER PERIOD**

Question and Answer Period was held both in person at the meeting, as well as electronically through email.

As of 8:30 a.m., July 6, 2022 there was **one (1)** Question and Answer period submission received:

- D. Reeve, White Rock - Question on why there were no fireworks this year for Canada Day with response from staff included.

Note: there are to be no questions or comments on a matter that will be the subject of a public hearing (time between the public hearing and final consideration of the bylaw).

4.1 CHAIRPERSON CALLS FOR SPEAKERS TO QUESTION AND ANSWER PERIOD

- On-Table Item from G. Gumley, White Rock, commenting in regard to the timing of when the City's 2021 Annual Report and the Statement of Financial Information would be provided in relation to the public comment opportunity.

Staff confirmed both reports will be included on the July 25, 2022 Regular Council meeting agenda. Public comments will be received on both topics at that same meeting.

- K. Jones, White Rock, noted concern regarding need for maintenance at Maccaud Park (walkway has blackberry bushes interfering with it, grass is high and brambles are along the fence line) making walking difficult and a possible fire hazard, further, how many replacement trees would be required for the 105 cm diameter tree (in Maccaud Park).

Staff noted the Parks Crews are scheduled to do maintenance at Maccaud Park later this week.

Staff will review the Tree Bylaw and get back regarding the replacement tree requirement(s) for Maccaud Park.

5. DELEGATIONS AND PETITIONS

5.1 DELEGATIONS

None

5.2 PETITIONS

None

6. PRESENTATIONS AND CORPORATE REPORTS

6.1 PRESENTATIONS

6.1.a KENT STREET ACTIVITY CENTRE - ENHANCEMENT AND EXPANSION PROPOSAL COMMITTEE

Bob Zoltok and Clive Griffiths, Kent Street Centre Expansion Proposal Committee, attended to discuss the impact of COVID and the physical constraints of the current size of the Kent Street Activity Centre and the limitations this has on the delivery of seniors' programs and services.

The Kent Street Seniors Society (KSSS) asked Council to approve their request for a Needs Assessment Study to review expanding the Kent Street Activity Centre. KSSS (the fundraising arm of the Kent Street Activity Centre) is prepared to assist in sharing some of the cost of the study.

Motion Number: 2022-299 It was MOVED and SECONDED

THAT Council:

1. **Approve a Needs Assessment Study for the Kent Street Activity Centre be conducted; and**
2. **Direct this study be completed under a cost-sharing formula with the Kent Street Seniors Society at an estimated cost of \$25,000.**

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

6.2 CORPORATE REPORTS

6.2.a RCMP AND BYLAW OFFICER BICYCLE USE ON THE WATERFRONT PROMENADE

Corporate report dated July 11, 2022 from the Director of Planning and Development Services titled "RCMP and Bylaw Officer Use on the Waterfront Promenade".

Motion Number: 2022-300 It was MOVED and SECONDED

THAT Council authorize RCMP Officers, Community Policing volunteers, and City of White Rock Bylaw Officers to patrol the City's waterfront promenade, and on other City sidewalks, trails, or property on bicycles, despite the restrictions in Street and Traffic Bylaw 1999, No. 1529.

Note: Prior to the vote it was clarified the Pier is meant to be included in the motion.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

6.2.b ROAD EXCHANGE BYLAW (14977 BEACHVIEW AVENUE), 2022, NO. 2433

Corporate report dated July 11, 2022 from the Director of Planning and Development Services titled "Road Exchange Bylaw (14977 Beachview Avenue), 2022, No. 2433".

Note: Bylaw 2433 is on the agenda for consideration under Item 8.1.a.

An inquiry was made if the proposal impacted the maximum height for the lot.

Staff confirmed that the Zoning would regulate the building height and prior to the Bylaw being voted on further confirmed that moving the property line in this circumstance does not increase the maximum height allowed for the lot.

Motion Number: 2022-301 It was MOVED and SECONDED

THAT Council receive for information the July 11, 2022, corporate report from the Director of Planning and Development Services, titled "Road Exchange Bylaw (14977 Beachview Avenue), 2022, No. 2433".

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

6.2.c STATUS UPDATE OF COUNCIL'S 2022 TOP PRIORITIES

Council's 2022 Top Priorities with new activity comments provided for information:

- **Solid Waste Pickup for Multi-Family:** Staff continue to work with the consultant in developing the RFP with the intent to award the contract at Council on October 3rd. Implementation is proposed to be staged starting in January 2023. Contract is proposed to be for five years with possibility for extension. Cart and bins to be owned by the contractor and amortized over the five-year contract.
- **Housing Needs / Affordable Housing:** On April 25, 2022 the draft Housing Strategy presented to Council from the Committee was removed by Council for consideration - later in the agenda some committee recommendations were

received by Council and some were not approved / not endorsed. Council has directed the Housing Advisory Committee to focus on partnering with non-profit organizations and utilizing the funding and grants as outlined at the Affordable Housing workshop, held in April 2022, in furtherance of affordable seniors housing. Work on this is ongoing.

- **Community Amenity Contribution "Shovel-in-the-Ground" Projects:**

- **Emerson Park Playground Upgrade:** Project is completed. Opening ceremony was held on July 4th. Staff to report to Council on additional options for playground equipment suitable for older children.

- **Maccaud Park Upgrade:** Trees are planted, base preparation completed for pathways and topsoil work underway. Paving of the pathways is scheduled within the next two weeks and project completion is estimated at mid to late Summer.

- **Helen Fathers Centre Street Hillside Walkway:** The petition to the Court by three properties for an injunction to prohibit work until legal issues related to encroachments are resolved was denied. The hearings on these petitions are tentatively scheduled for August and September. The City Solicitor will update Council in a closed meeting on July 25th.

- **Review Options for Upgrading Multiple Hillside Walkways (Road Ends) to Waterfront:** A funding application was submitted before the March 28th deadline, no further update at this time.

- **The City's Relationship with the Semiahmoo First Nation (SFN):**

- At the direction of Council, in response to the suggestion from Chief Harley Chappell, staff is researching the feasibility of permanently flying the SFN flag at City Hall. Staff will report back.

- Manager, Communications & Government Relations has established a monthly 1:1 meeting with Councillor Jennine Cook who is now overseeing SFN communications.
- City Events staff is working with SFN on enhanced programming for Semiahmoo Days for this year's Sea Festival/Semiahmoo Days joint event on the August long weekend.
- Staff are working with SFN Council to see when they will be ready to meet again to discuss the draft Communications Memorandum of Understanding.

The following discussion points were noted:

- City staff are working with the Semiahmoo First Nation to bring a combined event of Sea Festival and Semiahmoo Days (rather than two (2) separate events)
- It was requested that once the RFP is ready for the Solid Waste Project that staff will let Council know

7. MINUTES AND RECOMMENDATIONS OF COMMITTEES

7.1 STANDING AND SELECT COMMITTEE MINUTES

Motion Number: 2022-302 It was MOVED and SECONDED

THAT Council receive for information the following standing and select committee meeting minutes as circulated:

- **Finance and Audit Committee, June 27, 2022;**
- **Land Use and Planning Committee, June 27, 2022;**
- **Grants-in-Aid Sub-Committee, June 29, 2022; and,**
- **Economic Development Advisory Committee, June 15, 2022.**

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

7.2 STANDING AND SELECT COMMITTEE RECOMMENDATIONS

7.2.a GRANTS-IN-AID SUB-COMMITTEE MEETING (COUNCILLOR CHESNEY, CHAIRPERSON)

7.2.a.a 2022 LATE SUBMISSIONS GRANTS-IN-AID ARTS AND CULTURE

Staff Note: In accordance with Grants-in-Aid Policy 302, funds are to be used for the year in which they are granted (2022). If the White Rock Orchestra is unable to provide confirmation of a future event (in 2022) by September, the funds will then be allocated to the Surrey Youth Theatre Company (SYTCO).

Grants-in-Aid Policy 302 attached for information.

Note: The White Rock City Orchestra applied for the grant following their event which is not in compliance with the City's Grants-in-Aid policy.

Motion Number: 2022-303

It was MOVED and SECONDED

THAT Council allocate the 2022 Late Submission Grants-in-Aid - Arts and Culture funding to:

- **Arts of Course - South Rock Art Tour in the amount of \$2,500; and,**
- **The White Rock City Orchestra in the amount of \$2,500 (contingent that the grant be used for a future event/ performance);**

AND THAT should the White Rock City Orchestra be unable to use the funds for a future event, the funding then be allocated to SYTCO's 25th Season event in the amount of \$2,500.

Voted in the negative (3): Councillor Johanson, Councillor Kristjanson, and Councillor Manning

Absent (1): Councillor Trevelyan

Motion DEFEATED (2 to 3)

Motion Number: 2022-304

It was MOVED and SECONDED

THAT Council allocate the 2022 Late Submission Grants-in-Aid - Arts and Culture funding to:

- **Arts of Course - South Rock Art Tour in the amount of \$2,500; and**
- **Surrey Youth Theatre Company's (SYTCO) 25th Season event in the amount of \$2,500; and**
- **Encourage the White Rock City Orchestra to apply for the City's Grant Program early next year.**

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

**7.2.b ECONOMIC DEVELOPMENT ADVISORY COMMITTEE
(COUNCILLOR TREVELYAN, CHAIRPERSON)**

7.2.b.a SISTER CITY AND FRIENDSHIP CITY

**7.2.b.a.a Recommendation #1: Sister City Agreement and
Friendship City Memorandum of Understanding**

Motion Number: 2022-305

It was MOVED and SECONDED

THAT Council direct staff to engage in discussions with La Conner Washington to renew White Rock's Sister City Agreement based on the draft Agreement prepared by the EDAC Working Group with the assistance of the Economic Development Officer; and that staff also engage in discussions with Blaine Washington to establish a Memorandum of Understanding (MOU) regarding a Friendship City relationship based on the draft MOU prepared by the EDAC Working Group with the assistance of the Economic Development Officer.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

7.2.b.a.b Recommendation #2: Sister City

Motion Number: 2022-306

It was MOVED and SECONDED

THAT Council direct staff to (1) investigate what Sister City signage is currently in place and explore options to install a sign in a more visible location, (2) arrange for Council representation to attend one Council meeting in the Town of La Conner each year, (3) provide a link to La Conner's website on the City's website, and (4) promote La Conner's popular tourist activities and events, after the Sister City Agreement is signed; and that Council further direct staff to discuss that reciprocal actions be taken by La Conner when discussing the formal Sister City Agreement.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

7.2.b.a.c Recommendation #3: Invitations to Future Events

Motion Number: 2022-307

It was MOVED and SECONDED

THAT Council invite both the Sister City La Conner and the Friendship City Blaine to a minimum of two major events per year, ideally Canada Day and the Bright Walk in White Rock Festival, and that as hosts of these events Council provide support or suggestions (dependent upon / as noted in the final agreements between the the City and La Conner and the City of Blaine) for transportation and accommodations inside the City and meals.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

8. BYLAWS AND PERMITS

8.1 BYLAWS

8.1.a BYLAW 2433 - ROAD EXCHANGE BYLAW (14977 BEACHVIEW AVENUE), 2022, NO. 2433

Bylaw 2433 - A bylaw to close and remove the highway dedication and to dispose of a portion of Beachview Avenue in exchange for road dedication on a portion of the property known as 14977 Beachview Avenue.

Note: This Bylaw was the subject of a Corporate Report under Item 6.2.b.

Motion Number: 2022-308 It was MOVED and SECONDED

THAT Council give first, second and third readings to "*Road Exchange Bylaw (14977 Beachview Avenue), 2022, No. 2433*".

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

8.2 PERMITS

None

9. CORRESPONDENCE

9.1 CORRESPONDENCE - RECEIVED FOR INFORMATION

Note: Further action on the following correspondence items may be considered. Council may request that any item be brought forward for discussion, and may propose a motion of action on the matter.

Motion Number: 2022-309 It was MOVED and SECONDED

THAT Council receive correspondence Items 9.1.a - 9.1.b as circulated.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

Motion Number: 2022-310 It was MOVED and SECONDED

THAT Council direct Fraser Health be contacted and asked to facilitate some mobile care, as outlined in the June 20, 2022 correspondence from M. Andrews within the community.

Absent (1): Councillor Trevelyan

Motion CARRIED (5 to 0)

9.1.a METRO VANCOUVER BOARD IN BRIEF

Metro Vancouver Board in Brief from meetings of June 24, 2022.

9.1.b M. ANDREWS - PROPOSAL FOR ADDITIONAL ASSISTANCE IN THE CITY FOR THOSE EXPERIENCING HOMELESSNESS

Correspondence dated June 20, 2022 from M. Andrews with suggestions for the City to arrange for mobile access to medical care, particularly foot care and physical exams, for vulnerable populations.

10. MAYOR AND COUNCILLOR REPORTS

10.1 MAYOR'S REPORT

Mayor Walker noted the following information:

- June 28, Annual New Mobility Forum hosted by TransLink "Electrification Session"
- June 28, Council Meeting Re-cap with Councillor Trevelyan
- June 28, South Surrey & White Rock Chamber of Commerce Annual General Meeting
- June 29, Rotary Installation Dinner for the new Board
- June 30, TransLink Mayors' Council on Regional Transportation
- July 1, Canada Day by the Bay event
- July 3, Henri Lorieau Tournament
- July 4, Emerson Park Opening
- July 5, White Rock Rotary Club
- July 7, Metro Vancouver Joint Board meeting
- July 7, Metro Vancouver Housing Committee meeting
- July 7, TD Concerts at the Pier event, the second concert with the act "The Paperboys"
- July 9, "Dancing at the Pier"
- July 10, Rotary's cross Canada Tour: Rotary International President, Jennifer Jones

Upcoming:

- Friday, July 15, "Pride Flag Raising" and "Ride for Pride" events starting at 5:30 p.m.

10.2 COUNCILLORS REPORTS

Councillor Manning noted the following information:

- June 28, South Surrey & White Rock Chamber of Commerce Annual General Meeting
- July 1, Canada Day by the Bay event
- July 7, South Surrey & White Rock Community Action Team
- July 9, Pop Up Town Gallery "Paris Salon"
- July 10, Rotary's Cross Canada Tour: Rotary International President, Jennifer Jones

Councillor Johanson noted the following information:

- June 29, Grants in Aid Committee meeting
- July 1, Canada Day by the Bay event
- July 4, Emerson Park Opening

Councillor Chesney noted the following information:

- July 7, TD Concerts at the Pier event, the second concert with the act "The Paperboys"
- July 9, "Dancing at the Pier"
- July 9, Pop Up Town Gallery "Paris Salon"

Coming Up:

- July 12, Council Meeting Re-cap with Mayor Walker
- July 13, Pickleball Court Opening - Centennial Park

Councillor Kristjanson noted the following information:

- Noted there are many summer community events planned for all to enjoy

11. MOTIONS AND NOTICES OF MOTION

11.1 MOTIONS

11.2 NOTICES OF MOTION

Councillor Chesney provided the following Notice of Motion for consideration at the July 25, 2022 Regular Council meeting:

THAT Council direct staff to prepare a report on the feasibility of changing the City's Zoning/Building Bylaws that would state the following:

All future homes built in White Rock must have 40% of their land maintained on a permeable surface; and further this motion would also pertain to driveways which would be constructed with interlocking bricks and earth openings to diminish water runoff.

12. RELEASE OF ITEMS FROM CLOSED COUNCIL MEETINGS

12.1 RELEASE OF ITEMS FROM CLOSED COUNCIL MEETING ON JUNE 27, 2022

HELEN FATHERS CENTRE STREET WALKWAY - CONTRACT AWARD FOLLOW-UP WITH GRANT APPLICATION UPDATES

Motion Number: 2022-IC-061 It was MOVED and SECONDED

THAT Mayor and Council:

- 1. Receive for information the corporate report dated June 27, 2022, from the Director of Engineering and Municipal Operations, titled "Helen Fathers Centre Street Walkway Contract Award Follow-Up with Grant Application Updates;"**
- 2. Direct staff to defer the project for retendering in January 2023; and**
- 3. Direct staff to release this item from Closed Council on July 11, 2022.**

Motion CARRIED (6 to 0)

13. **OTHER BUSINESS**

None

14. **CONCLUSION OF THE JULY 11, 2022 REGULAR COUNCIL MEETING**

The meeting was concluded at 8:23 p.m.

Mayor Walker



Tracey Arthur, Director of Corporate
Administration

Unapproved

**Regular Council Meeting for the purpose of Public Hearing/
Meeting of White Rock City Council**

Minutes



July 18, 2022, 5:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

PRESENT: Mayor Walker
Councillor Chesney
Councillor Johanson
Councillor Kristjanson (arrived at 5:08 p.m.)
Councillor Trevelyan

ABSENT: Councillor Manning

STAFF: Guillermo Ferrero, Chief Administrative Officer
Tracey Arthur, Director of Corporate Administration
Anne Berry, Director of Planning and Development Services
Debbie Johnstone, Deputy Corporate Officer

PUBLIC: 13 (approx.)

1. CALL HEARING/ MEETING TO ORDER

The Public Hearing was called to order at 5:00 p.m.

**2. DIRECTOR OF CORPORATE ADMINISTRATION READ A STATEMENT
REGARDING THE PROCEDURE TO BE FOLLOWED FOR THE PUBLIC
HEARINGS AND PUBLIC MEETING FOR THE EVENING**

3. **PUBLIC HEARING #1 - BYLAW 2434: WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD ZONE 62 - 1453 STAYTE ROAD) BYLAW, 2022, NO. 2434**

CIVIC ADDRESS: 1453 Stayte Road

PURPOSE: Bylaw 2434 proposes to rezone the subject property from the "RS-1 One Unit Residential Zone" to the "Comprehensive Development Zone (CD) 62". If approved, it would enable the development of the proposed townhouse project in the form of 20 units which are 3 storey ground-oriented units with a roof top deck (ranging from 288 sq.ft. to 808 sq.ft.).

4. **DIRECTOR OF CORPORATE ADMINISTRATION TO ADVISE HOW THIS PUBLIC HEARING HAS BEEN PUBLICIZED**

- Notice was published in the July 7 and 14 editions of the Peace Arch news.
- 154 notices were mailed to owners and occupants within 100 meters of the subject property.
- A copy of the notice was placed on the public notice posting board on July 5, 2022.

5. **THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO PRESENT THE PROPOSED BYLAW**

Note: Public information package can be viewed online [HERE](#).

Councillor Kristjanson arrived at the meeting at 5:08 p.m.

6. **THE CHAIRPERSON WILL REQUEST THE DIRECTOR OF CORPORATE ADMINISTRATION TO ADVISE OF ANY CORRESPONDENCE OR SUBMISSIONS RECEIVED**

As of 8:30 a.m. on July 13, 2022, there have been two (2) submissions.

Note: Submissions received between 8:30 a.m. July 13, 2022, and 12:00 p.m. (noon), July 18, 2022 will be presented "On-Table" at the Public Hearing.

Author	Date Received	City of Residence	Status	Item #
P. Kealy	July 7, 2022	White Rock	Opposed	C-1
C. McGurrin	July 13, 2022	White Rock	Opposed	C-2

Summary of On-Table submissions:

- On-Table submissions were received up until 12:00 p.m. (noon) Monday, July 18th.
- There have been eleven (11) submissions On-Table, ten (10) in opposition and one (1) in support.

7. **THE CHAIRPERSON INVITES THOSE IN ATTENDANCE TO PRESENT THEIR COMMENTS**

- J. Mah, White Rock, not in support of the application stating concern with density, impact on climate and tree canopy, traffic (volume, speed, congestion and pollution), the traffic pattern (left turn difficult to make resulting in those using Thrift Avenue), parking and creating parking issues on nearby residential streets and with the proposed height (solid shade and privacy issues caused by the roof decks).
- C. North (and D. King), White Rock, not in support of the application stating concern with density on the size of lot (out of place in the neighbourhood), height - four storey (rooftop deck adds the extra story), traffic and exit onto Stayte Road.
- K. Jones, White Rock, not in support of the application, stating concern with density, height (rooftop decks add an extra storey), traffic, parking; and further for the lot no more than ten (10) units, the proposal is not compatible with the residential neighbourhood.

Requested Council not approve any project for construction without a completed Construction Parking Plan.

- The Project Architect answered the following question from a Member of Council: How will the additional traffic be addressed? In consideration of this the project did move the ingress/ egress further north, each unit has two parking stalls, and the development has included four (4) visitor parking stalls.

At 5:25 p.m. information was noted and displayed on how to call in for anyone that did not register ahead of time.

There were no callers and no further speakers regarding Bylaw 2434.

8. **IF REQUIRED, THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO SUMMARIZE THE PROPOSED BYLAW**

N/A

9. **CONCLUSION OF PUBLIC HEARING #1 - BYLAW 2434: WHITE ROCK ZONING BYLAW 2012, NO. 2000, AMENDMENT (CD ZONE 62 - 1453 STAYTE ROAD) BYLAW, 2022, NO. 2434**

The Public Hearing for Bylaw 2434 was concluded at 5:30 p.m.

10. **PUBLIC HEARING #2 - BYLAW 2435: WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD ZONE 67 - 15704, 15724/28/38/48/58/70 NORTH BLUFF ROAD) BYLAW, 2022, NO. 2435**

CIVIC ADDRESS: 15704, 15724/28/38/48/58/70 NORTH BLUFF ROAD

PURPOSE: Bylaw 2435 proposes to rezone the subject properties from "RS-1 One Unit Residential Zone" to "Comprehensive Development Zone (CD) 67". If approved, it would enable the proposed multi-building residential project that consists of 87 units a mixture of townhomes and apartments ranging from studios to 3-bedroom units to be built.

The Public Hearing was called to order at 5:31 p.m.

11. **DIRECTOR OF CORPORATE ADMINISTRATION ADVISED HOW THIS PUBLIC HEARING HAS BEEN PUBLICIZED**

- Notice was published in the July 7 and 14 editions of the Peace Arch news.
- 90 notices were mailed to owners and occupants within 100 meters of the subject property.
- A copy of the notice was placed on the public notice posting board on July 5, 2022

12. **THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO PRESENT THE PROPOSED BYLAW**

Note: Public information package can be viewed online [HERE](#).

13. **THE CHAIRPERSON WILL REQUEST THE DIRECTOR OF CORPORATE ADMINISTRATION TO ADVISE OF ANY CORRESPONDENCE OR SUBMISSIONS RECEIVED**

As of 8:30 a.m. on July 13, 2022, there have been two (2) submissions.

Note: Submissions received between 8:30 a.m. July 13, 2022, and 12:00 p.m. (noon), July 18, 2022 will be presented "On-Table" at the Public Hearing.

Author	Date Received	City of Residence	Status	Item #
P. Kealy	July 7, 2022	White Rock	Opposed	C-1
M. Stewart	July 11, 2022	White Rock	Opposed	C-2

Summary of On-Table submissions:

- On-Table submissions were received up until 12:00 p.m. (noon) today, Monday, July 18th.
- There have been four (4) submissions received On-Table. Three (3) in opposition, and one (1) submission was combined correspondence from 15 individuals, all noting their support for the project.

14. THE CHAIRPERSON INVITES THOSE IN ATTENDANCE TO PRESENT THEIR COMMENTS

- K. Jones, White Rock, not in support of the application noting concern with the density, close proximity to single family residential homes, height (overshadow nearby homes), resulting impact for access to Maccaud Park and the proximity of the building(s) will block sunlight, parking, difficult access onto North Bluff Road. Would like to see for the site lower density, with a step back from Maccaud Park and more trees on the site.

Requested Council not approve any project for construction without a completed Construction Parking Plan.

At 5:42 p.m. information was noted and displayed on how to call in for anyone that did not register ahead of time.

- S. Craig, Urban Architecture, called into the meeting and provided a summary project overview and it was noted to address construction parking they planned to use prefab construction resulting in shorter on-site construction, however a plan can be put in place to address concerns of nearby residents.
- Speaking a second time K. Jones, White Rock, inquired if the powerlines on North Bluff Road would be brought underground.

D. Wade, on behalf of the Developer stated they would need to confirm this noting on the North side of the road there are transmission lines and they

cannot be moved underground. They would need to ensure that this is note the case on the South side.

Staff confirmed the Works and Servicing Agreement would be a condition of approval; and a Construction Parking Plan would be determined at the building permit stage.

There were no further callers and no further speakers regarding Bylaw 2435.

15. **IF REQUIRED, THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO SUMMARIZE THE PROPOSED BYLAW**

N/A

16. **CONCLUSION OF PUBLIC HEARING #2 - BYLAW 2435: WHITE ROCK ZONING BYLAW, 2012, NO. 2000, AMENDMENT (CD ZONE 67 - 15704, 15724/28/38/48/58/70 NORTH BLUFF ROAD**

The Public Hearing for Bylaw 2435 was concluded at 5:55 p.m.

17. **PUBLIC MEETING #3 - DEVELOPMENT VARIANCE PERMIT NO. 445 - 15385 SEMIAHMOO AVENUE**

CIVIC ADDRESS: 15385 SEMIAHMOO AVENUE

PURPOSE: The proposed Development Variance Permit (DVP) allows for the installation of an oversized fascia sign and a combined fascia sign (fascia sign and electronic message board sign) at the residential care facility and Peninsula United Church development (currently under construction).

The Public Meeting was called to order at 5:56 p.m.

18. **DIRECTOR OF CORPORATE ADMINISTRATION ADVISES HOW THIS PUBLIC HEARING HAS BEEN PUBLICIZED**

- Notice was published in the July 7 and 14 editions of the Peace Arch news.
- 224 notices were mailed to owners and occupants within 100 meters of the subject property.
- A copy of the notice was place on the public notice posting board on July 5, 2022

19. THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO PRESENT THE PROPOSED APPLICATION

Note: Public information package can be viewed online [HERE](#).

20. THE CHAIRPERSON WILL REQUEST THE DIRECTOR OF CORPORATE ADMINISTRATION TO ADVISE OF ANY CORRESPONDENCE OR SUBMISSIONS RECEIVED

Note: Submissions received between 8:30 a.m. July 13, 2022, and 12:00 p.m. (noon), July 18, 2022, will be presented "On Table" at the Public Hearing.

There have been no submissions for this Public Meeting to date (none published in the agenda package, and none received on table).

21. THE CHAIRPERSON INVITES THOSE IN ATTENDANCE TO PRESENT THEIR COMMENTS

- J. MacDonald, Surrey, on behalf of the Church, answered an inquiry by a Member of Council in regard to the brightness of the light on the sign at night and the impact to nearby residents.

The sign is dimmable, if any concerns are noted they are willing to work with the neighbours to address them, and further noted the purpose of the screen is there to announce activities and after a certain time it may not need to be on.

At 6:03 p.m. information was noted and displayed on how to call in for anyone that did not register ahead of time.

- C. King, White Rock, called into the meeting and stated concern with the oversized sign and electronic board and the impact on the residential area south of the site, the sign is not consistent with the nearby homes, inquired if it is necessary the sign be oversized and be erected on the south side of the building.
- Speaking a second time K. Jones, White Rock, asked for Council support for the proposal and inquired if changing of the location of the sign (facing west instead) was an option to be taken into consideration.
- P. Heslop, on behalf of the Church, noted the location was selected to not confuse between the church and residential entrances. Willing to not have the sign on in the evenings due to there not being a lot of traffic in the area to see it.

There were no further callers and no further speakers regarding Development Variance Permit 445.

22. **IF REQUIRED, THE CHAIRPERSON INVITES THE DIRECTOR OF PLANNING AND DEVELOPMENT SERVICES TO SUMMARIZE THE PROPOSED APPLICATION**

N/A

23. **CONCLUSION OF PUBLIC MEETING #3 - DEVELOPMENT VARIANCE PERMIT NO. 445 - 15385 SEMIAHMOO AVENUE**

The Public Meeting for Development Variance Permit 445 was concluded at 6:12 p.m.

24. **CONCLUSION OF THE JULY 18, 2022 PUBLIC HEARING/MEETING**

The meeting for the evening was concluded at 6:12 p.m.



Mayor Walker

Tracey Arthur, Director of Corporate Administration

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Tracey Arthur, Director, Corporate Administration

SUBJECT: City of White Rock 2021 Annual Report

RECOMMENDATION

THAT Council endorse the City of White Rock's 2021 Annual Report as circulated.

EXECUTIVE SUMMARY

The 2021 Annual Report (Appendix A) includes statistical information, departmental overviews, information on civic grants- in-aid and capital projects. The last portion of the report contains detailed financial statements for the city, year ended December 31, 2021.

The document also includes a summary of 2021 milestones, a progress report on Council's Strategic Priorities and statements from the Mayor and Chief Administrative Officer.

Once completed, the annual report must be made available for public inspection 14 days prior to being presented to Council to hear public submissions /questions. The City's Annual report was made available from Friday, July 8, 2022.

In accordance with the *Community Charter* the municipality is to have the Annual Report completed by June 30 however due to some unavoidable circumstances regarding staffing the information was not ready for the report until early July. It is understood this is after the deadline for this reporting. Staff have been in contact with the Ministry to inform of the situation and discuss the matter. Staff informed the Ministry they anticipated the 2021 Financial Statements, and the Annual Report would be late however the items would be published as soon as possible following the deadline with assurance it would be prior to the fall 2022.

INTRODUCTION/BACKGROUND

The 2021 Annual Report was shared in a variety of ways, including the placement of hard copies at White Rock City Hall, the White Rock Library and the White Rock Museum and Archives.

The city issued a media release that was posted on the city's website and sent to local media. A link to the 2021 Annual Report was posted on the city's social media channels.

The 2021 Annual Report can be found on the city's website at:
www.whiterockcity.ca/annualreport.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

The City of White Rock's 2021 Annual Report was available more than 14 days to review and provide submissions and comments or ask questions in time for the City Council meeting scheduled July 25, 2022.

Promotion of the Annual Report included:

- Advertising in the Peace Arch News (PAN);
- News item posted to website homepage;
- News item sent to RSS feed mailing list;
- News item included in e newsletter;
- Social media that includes Twitter, Facebook and Instagram;
- Website information at www.whiterockcity.ca/annualreport;
- Hard copies at the White Rock Library branch of Fraser Library;
- Hard copies at the White Rock Museum and Archives;
- Hard copies at City Hall; and
- Link and information included in CAO weekly staff Activity Update.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The Annual Report was prepared by the Communications and Government Relations and the Financial Services Departments of the City of White Rock in collaboration with the city's Senior Leadership Team.

All City of White Rock departments contributed updates to the Annual Report, based on milestones and progress in 2021.

ALIGNMENT WITH STRATEGIC PRIORITIES

The 2021 Annual Report aligns with Council's 2021-2022 Strategic Priorities updated from the 2018-2022 document due to revised needs created by the COVID-19 pandemic that occurred mid-way through the Council term.

Details and a dashboard showing the city's progress are online at:

[2021-2022-Council-Strategic-Priorities-Update---Top-Priority-2022-05-12 \(whiterockcity.ca\)](http://2021-2022-Council-Strategic-Priorities-Update---Top-Priority-2022-05-12(whiterockcity.ca))

OPTIONS / RISKS / ALTERNATIVES

The following options are available for Council's consideration:

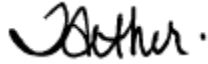
1. Approve White Rock's 2021 Annual Report as provided in Appendix A of this corporate report; and submit the Annual Report to the Province of British Columbia.
2. Approve White Rock's 2021 Annual Report with changes as indicated by Council and/or collected by City staff; and submit the Annual Report to the Province of British Columbia.

CONCLUSION

White Rock's 2021 Annual Report is presented for endorsement by Council. There is an

opportunity for the public to comment or to ask questions on the report at the July 25, 2022, regular Council meeting.

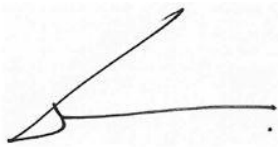
Respectfully submitted,

A handwritten signature in black ink, appearing to read "Arthur".

Tracey Arthur
Director, Corporate Administration

Comments from the Chief Administrative Officer

I concur with the recommendation of this corporate report.

A handwritten signature in black ink, appearing to read "Guillermo Ferrero".

Guillermo Ferrero
Chief Administrative Officer

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Candice Gartry, Director, Financial Services
Shannon Johnston, Manager, Budgets & Accounting

SUBJECT: 2021 Statement of Financial Information (SOFI)

RECOMMENDATIONS

THAT Council:

1. Receive for information the July 25, 2022 corporate report from the Director of Financial Services and the Manager of Budgets & Accounting, titled “2021 Statement of Financial Information (SOFI)”;
 2. Approve the 2021 Statement of Financial Information attached as Appendix A; and
 3. Authorize the Mayor to sign the document as approved on behalf of City Council.
-

EXECUTIVE SUMMARY

This corporate report presents the 2021 Statement of Financial Information (SOFI). The SOFI is prepared each year in accordance with the *Financial Information Act*. The *Financial Information Act* requires that the SOFI be approved by Council and the Financial Officer. The complete SOFI is included in Appendix A.

INTRODUCTION/BACKGROUND

The Statement of Financial Information meets the requirements of the B.C. *Financial Information Act*. Once approved by Council, the document will be finalized and submitted.

The attached 2021 Statement of Financial Information is segregated into two sections: Part I includes the City’s audited 2021 Financial Statements and Part II includes other supplementary information such as schedules of employee remuneration and payments to suppliers.

CONCLUSION

It is recommended that the 2021 Statement of Financial Information be approved by Council and

that the Mayor be authorized to sign the document as approved on behalf of Council.

Respectfully submitted,



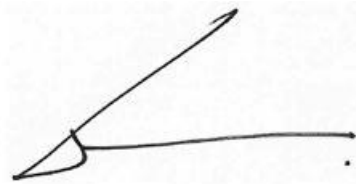
Candice Gartry
Director, Financial Services



Shannon Johnston
Manager, Budgets & Accounting

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

Appendix A: 2021 Statement of Financial Information

PRICE: \$5.00

**CITY OF WHITE ROCK
2021
STATEMENT OF FINANCIAL INFORMATION**

In accordance with the Financial Information Act (RSBC 1996 Chapter 140)

PARTS I & II

PENDING CITY COUNCIL APPROVAL

THE CORPORATION OF THE CITY OF WHITE ROCK
STATEMENT OF FINANCIAL INFORMATION APPROVAL
FOR THE YEAR ENDED DECEMBER 31, 2021

The undersigned, represents the Council of the Corporation of the City of White Rock and approves all the statements and schedules included in this Statement of Financial Information, prepared in accordance with the Financial Information Act.

Darryl Walker
Mayor
July 25, 2022

The undersigned, as authorized by the Financial Information Regulation, Schedule 1, subsection 9(2), approves all the statements and schedules included in this Statement of Financial Information, prepared in accordance with the Financial Information Act.



Candice Gartry, CPA, CGA
Director of Financial Services
July 18, 2022

Prepared in accordance with the Financial Information Regulation, Schedule 1, section 9

THE CORPORATION OF THE CITY OF WHITE ROCK

STATEMENT OF FINANCIAL INFORMATION PARTS I & II (in accordance with the Financial Information Act - (RSBC 1996 Chapter 140))

For the Year Ended December 31, 2021

Table of Contents

<u>Required Statement</u>	<u>Page Reference</u>
<u>PART I - Audited Requirements</u>	
Auditor's Report	Financial Information Regulation Schedule 1, Section 1,2, and 3
(a) Statement of Assets and Liabilities	This information can be found in the statements and schedules that make up the Audited Financial Statements
(b) Operational statement	

PART II - Other Requirements

<u>Required Statements</u>	<u>Page Reference</u>
(c) Schedule of Debts	Part II Page 1
(d) Schedule of Guarantee and Indemnity Agreements	Part II Page 2
(e) Schedule of Employee Remuneration and Expenses	Part II Page 3
(f) Schedule of Payments to Suppliers of Goods and Services	Part II Page 6
(g) Management Report	Part II Page 9

Financial Statements of

**THE CORPORATION OF THE
CITY OF WHITE ROCK**

And Independent Auditors' Report thereon

Year ended December 31, 2021

MAYOR AND COUNCIL

It is my pleasure to present The Corporation of the City of White Rock's Financial Statements for the fiscal year ended December 31, 2021 pursuant to Section 167 of the *Community Charter*. The statements have been prepared in accordance with the Canadian Public Sector Accounting Standards as recommended by the Public Sector Accounting Board of the Chartered Professional Accountants of Canada.

The preparation and presentation of the financial statements and related information are the responsibility of management of The Corporation of the City of White Rock. This involves the use of management's best estimates and careful judgement. The City maintains a system of internal accounting controls designed to provide reasonable assurances for the safeguarding of assets and the reliability of financial records.

The City's auditors, KPMG LLP, have given the City an unqualified audit opinion on the City's financial statements, stating in their opinion, that the financial statements present fairly, in all material respects, the financial position of the City as at December 31, 2021, the results of its operations, changes in net financial assets and its cash flows for the year then ended.


Financial Summary

Operating activities during the year contributed a surplus of \$11,166,768. This is mainly comprised of an increase in the City's investment in tangible capital assets and of planned contributions to reserves. The total accumulated surplus was \$229,662,974 as of December 31, 2021. Like most local governments, the largest component of accumulated surplus is the City's investment in tangible capital assets, which amounted to \$134,703,982 at the end of 2021. The remainder of accumulated surplus includes reserves and other surplus funds.

Net financial assets increased by \$10,360,151 to \$71,361,326 as of December 31, 2021. This is a positive indicator of the City's financial position.

Debt, net of sinking funds, outstanding as of December 31, 2021 was \$21,900,837, compared to \$22,626,177 on December 31, 2020. It is comprised of long-term debenture debt mainly attributed to the purchase of property and assets of the local water utility and subsequent water system infrastructure improvements.

The City ended the year 2021 in a sound financial position.



Shannon Johnston, CPA, CGA
Acting Director of Financial Services



KPMG LLP
PO Box 10426 777 Dunsmuir Street
Vancouver BC V7Y 1K3
Canada
Telephone (604) 691-3000
Fax (604) 691-3031

INDEPENDENT AUDITORS' REPORT

To the Mayor and Council of The Corporation of the City of White Rock

Opinion

We have audited the financial statements of The Corporation of the City of White Rock (the "City"), which comprise:

- the statement of financial position as at December 31, 2021
- the statement of operations for the year then ended
- the statement of changes in net financial assets for the year then ended
- the statement of cash flows for the year then ended
- and notes to the financial statements, including a summary of significant accounting policies

(hereinafter referred to as the "financial statements").

In our opinion, the accompanying financial statements present fairly, in all material respects, the financial position of the City as at December 31, 2021, and its results of operations, its changes in net financial assets and its cash flows for the year then ended in accordance with Canadian public sector accounting standards.

Basis for Opinion

We conducted our audit in accordance with Canadian generally accepted auditing standards. Our responsibilities under those standards are further described in the "***Auditors' Responsibilities for the Audit of the Financial Statements***" section of our auditors' report.

We are independent of the City in accordance with the ethical requirements that are relevant to our audit of the financial statements in Canada and we have fulfilled our other ethical responsibilities in accordance with these requirements.

We believe that the audit evidence we have obtained is sufficient and appropriate to provide a basis for our opinion.



Responsibilities of Management and Those Charged with Governance for the Financial Statements

Management is responsible for the preparation and fair presentation of the financial statements in accordance with Canadian public sector accounting standards and for such internal control as management determines is necessary to enable the preparation of financial statements that are free from material misstatement, whether due to fraud or error.

In preparing the financial statements, management is responsible for assessing the City's ability to continue as a going concern, disclosing as applicable, matters related to going concern and using the going concern basis of accounting unless management either intends to liquidate the City or to cease operations, or has no realistic alternative but to do so.

Those charged with governance are responsible for overseeing the City's financial reporting process.

Auditors' Responsibilities for the Audit of the Financial Statements

Our objectives are to obtain reasonable assurance about whether the financial statements as a whole are free from material misstatement, whether due to fraud or error, and to issue an auditors' report that includes our opinion.

Reasonable assurance is a high level of assurance, but is not a guarantee that an audit conducted in accordance with Canadian generally accepted auditing standards will always detect a material misstatement when it exists.

Misstatements can arise from fraud or error and are considered material if, individually or in the aggregate, they could reasonably be expected to influence the economic decisions of users taken on the basis of the financial statements.

As part of an audit in accordance with Canadian generally accepted auditing standards, we exercise professional judgment and maintain professional skepticism throughout the audit.

We also:

- Identify and assess the risks of material misstatement of the financial statements, whether due to fraud or error, design and perform audit procedures responsive to those risks, and obtain audit evidence that is sufficient and appropriate to provide a basis for our opinion.

The risk of not detecting a material misstatement resulting from fraud is higher than for one resulting from error, as fraud may involve collusion, forgery, intentional omissions, misrepresentations, or the override of internal control.

- Obtain an understanding of internal control relevant to the audit in order to design audit procedures that are appropriate in the circumstances, but not for the purpose of expressing an opinion on the effectiveness of the City's internal control.



- Evaluate the appropriateness of accounting policies used and the reasonableness of accounting estimates and related disclosures made by management.
- Conclude on the appropriateness of management's use of the going concern basis of accounting and, based on the audit evidence obtained, whether a material uncertainty exists related to events or conditions that may cast significant doubt on City's ability to continue as a going concern. If we conclude that a material uncertainty exists, we are required to draw attention in our auditors' report to the related disclosures in the financial statements or, if such disclosures are inadequate, to modify our opinion. Our conclusions are based on the audit evidence obtained up to the date of our auditors' report. However, future events or conditions may cause City to cease to continue as a going concern.
- Evaluate the overall presentation, structure and content of the financial statements, including the disclosures, and whether the financial statements represent the underlying transactions and events in a manner that achieves fair presentation.
- Communicate with those charged with governance regarding, among other matters, the planned scope and timing of the audit and significant audit findings, including any significant deficiencies in internal control that we identify during our audit.

KPMG LLP

Chartered Professional Accountants

Vancouver, Canada
June 28, 2022

THE CORPORATION OF THE CITY OF WHITE ROCK

Statement of Financial Position

December 31, 2021, with comparative information for 2020

	2021	2020
Financial Assets		
Cash and cash equivalents (note 2)	\$ 131,642,172	\$ 127,772,639
Temporary investments (note 2)	13,855,996	-
Accounts receivable (note 3)	5,979,185	8,946,234
	151,477,353	136,718,873
Liabilities		
Accounts payable and accrued liabilities (note 4)	13,063,304	9,498,604
Post-employment benefits (note 5)	1,700,250	1,540,864
Refundable performance deposits (note 2(b))	13,413,939	12,785,980
Capital lease obligations (note 6)	132,180	164,718
Debt (note 7)	21,900,837	22,626,177
Deferred revenue (note 8)	29,905,517	29,101,355
	80,116,027	75,717,698
Net Financial Assets	71,361,326	61,001,175
Non-Financial Assets		
Tangible capital assets (note 9)	157,122,145	156,425,967
Prepaid expenses	1,006,934	884,797
Inventories of supplies	172,569	184,267
	158,301,648	157,495,031
Accumulated surplus (note 10)	\$ 229,662,974	\$ 218,496,206

Commitments and contingencies (note 13)

See accompanying notes to financial statements.



Shannon Johnston, CPA, CGA
Acting Director of Financial Services

THE CORPORATION OF THE CITY OF WHITE ROCK

Statement of Operations

Year ended December 31, 2021, with comparative information for 2020

	2021 Budget (note 1(b))	2021	2020
Revenue:			
Property taxes:			
General purposes	\$ 25,132,500	\$ 25,053,083	\$ 23,631,761
Regional library levy	990,100	986,310	975,183
Business improvement levy	343,000	318,952	336,779
	26,465,600	26,358,345	24,943,723
Receipts in lieu of taxes	22,600	22,590	22,356
Development cost charges (note 8(a))	937,100	45,059	61,005
Sanitary sewer fees and parcel tax	3,535,400	3,656,853	3,562,975
Drainage user fees	2,777,400	2,775,458	2,658,487
Water user fees	5,413,900	5,704,188	5,139,529
Sales of services and other revenue (note 16)	17,167,600	16,149,009	14,926,479
Government grants (note 17)	16,620,900	570,727	5,459,271
Developers' contributions of tangible capital assets (note 9(b))	-	633,263	663,861
	72,940,500	55,915,492	57,437,686
Expenses:			
General government	8,825,276	7,339,240	7,401,407
Protection services	12,647,936	12,442,628	12,395,323
Transportation, engineering and operations	7,875,812	7,275,906	6,782,642
Parks, recreation and cultural services	10,405,277	9,042,693	9,177,189
Solid waste services	1,201,300	1,005,667	1,057,036
Sanitary sewer system	3,444,600	2,759,416	2,655,801
Drainage system	1,156,900	832,913	857,446
Water system	4,537,800	4,050,261	3,910,691
	50,094,901	44,748,724	44,237,535
Annual surplus	22,845,599	11,166,768	13,200,151
Accumulated surplus, beginning of year	218,496,206	218,496,206	205,296,055
Accumulated surplus, end of year	\$ 241,341,805	\$ 229,662,974	\$ 218,496,206

See accompanying notes to financial statements.

THE CORPORATION OF THE CITY OF WHITE ROCK

Statement of Changes in Net Financial Assets

Year ended December 31, 2021, with comparative information for 2020

	2021 Budget (note 1(b))	2021	2020
Annual surplus	\$ 22,845,599	\$ 11,166,768	\$ 13,200,151
Acquisition of tangible capital assets	(57,114,000)	(6,636,960)	(5,853,630)
Developers' contributions of tangible capital assets	-	(633,263)	(663,861)
Amortization of tangible capital assets	7,305,000	6,370,411	6,404,914
Loss (gain) on disposal of tangible capital assets	-	31,399	(239,580)
Proceeds on disposal of tangible capital assets	-	172,235	367,067
	(26,963,401)	10,470,590	13,215,061
Acquisition of prepaid expenses	-	(1,006,934)	(884,797)
Acquisition of inventories of supplies	-	(172,569)	(184,267)
Use of prepaid expenses	-	884,797	742,851
Consumption of inventories of supplies	-	184,267	232,598
Change in net financial assets	(26,963,401)	10,360,151	13,121,446
Net financial assets, beginning of year	61,001,175	61,001,175	47,879,729
Net financial assets, end of year	\$ 34,037,774	\$ 71,361,326	\$ 61,001,175

See accompanying notes to financial statements.

THE CORPORATION OF THE CITY OF WHITE ROCK

Statement of Cash Flows

Year ended December 31, 2021, with comparative information for 2020

	2021	2020
Cash provided by (used in):		
Operating:		
Annual surplus	\$ 11,166,768	\$ 13,200,151
Items not involving cash:		
Amortization	6,370,411	6,404,914
Loss (gain) on disposal of tangible capital assets	31,399	(239,580)
Developers' contributions of tangible capital assets	(633,263)	(663,861)
Changes in non-cash operating working capital:		
Accounts receivable	2,967,049	3,237,568
Prepaid expenses	(122,137)	(141,946)
Inventories of supplies	11,698	48,331
Accounts payable and accrued liabilities	3,564,700	(253,090)
Post-employment benefits	159,386	(64,639)
Refundable performance deposits	627,959	201,383
Deferred revenue	804,162	667,263
Net change in cash provided by operating activities	24,948,132	22,396,494
Capital activities:		
Cash used to acquire tangible capital assets	(6,636,960)	(5,605,331)
Proceeds on disposal of tangible capital assets	172,235	367,067
Net change in cash used in capital activities	(6,464,725)	(5,238,264)
Financing activities:		
Principal payments on debt	(725,340)	(702,441)
Principal payments on capital lease obligations	(32,538)	(83,581)
Net change in cash used in financing activities	(757,878)	(786,022)
Investing activities:		
Change in temporary investments	(13,855,996)	-
Net change in cash and cash equivalents	3,869,533	16,372,208
Cash and cash equivalents, beginning of year	127,772,639	111,400,431
Cash and cash equivalents, end of year	\$ 131,642,172	\$ 127,772,639
Non-cash transactions:		
Acquisition of tangible capital assets under capital lease	\$ -	\$ 248,299

See accompanying notes to financial statements.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements

Year ended December 31, 2021

The Corporation of the City of White Rock (the "City") is incorporated under the Local Government Act of British Columbia. The City's principal activities include the provision of local government services to residents, businesses and visitors of the incorporated area. These include legislative, operational and administrative support, protective, infrastructure, transportation, engineering and municipal operations, parks, recreation and cultural, solid waste, water system, drainage system, and sanitary sewer services.

1. Significant accounting policies:

The City follows Canadian Public Sector Accounting Standards as prescribed by the Public Sector Accounting Board ("PSAB") of the Chartered Professional Accountants of Canada, including the following significant policies:

(a) Basis of presentation:

These financial statements present the resources and operations including all accounts and funds of the City. All inter-fund transactions, assets and liabilities have been eliminated.

(b) Budget reporting:

The budget figures reported in the statement of operations and statement of changes in net financial assets represent the 2021 component of White Rock Financial Plan (2021-2025) Bylaw, 2021, No. 2377, adopted by the City Council on April 26, 2021.

(c) Cash and cash equivalents:

Cash and cash equivalents include short-term highly liquid investments with maturity dates within three months of acquisition that are readily convertible to known amounts of cash and that are subject to an insignificant risk of changes in value.

(d) Temporary investments:

Temporary investments are recorded at cost which approximates market value and are comprised of term deposits and bankers' acceptances.

(e) Revenue:

Revenue is recorded on the accrual basis and is recognized when it is earned and measurable. Revenue relating to future periods, including prepaid property taxes, government grants and certain facility upgrade contributions, are reported as deferred revenue and recognized as revenue when earned.

(f) Development cost charges ("DCCs"):

DCCs collected under the City's Development Cost Charge Bylaw ("DCC Bylaw"), plus interest earned thereon, are recorded as deferred revenue. DCCs are recognized as revenue when related costs are incurred.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

1. Significant accounting policies (continued):

(g) Deferred revenue:

Deferred revenues represent the receipt of funds for which the provision of services occurs in the future. These revenues may only be used in the conduct of certain services, in the completion of specific work, or for the purchase of tangible capital assets. These amounts will be recognized as revenues in the year in which the expenditures are incurred.

(h) Refundable performance deposits:

Refundable performance deposits represent cash collected by the City as security against work performed by property developers. The security is returned to the property developer upon satisfactory completion of the work, or recognized into revenue if the work is not performed in accordance with applicable bylaws.

(i) Capital lease obligation:

Capital lease obligations are recorded at the present value of the minimum lease payments excluding executor costs e.g., insurance, maintenance costs etc. The discount rate used to determine the present value of the lease payments is the City's interest rate implicit in the lease.

(j) Expenses:

Expenses are recorded on the accrual basis and are recognized as they are incurred. This is upon the receipt of goods or services and/or the creation of a legal obligation. Interest expense on debt and capital lease obligations is accrued to the end of the reporting period.

(k) Government transfers:

Restricted transfers from governments are deferred and recognized as revenue as the related expenditures are incurred or the stipulations in the related agreements are met. Unrestricted transfers are recognized as revenue when received or if the amount to be received can be reasonably estimated and collection is reasonably assured.

(l) Debt:

Debt is recorded net of repayments and actuarial adjustments.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

1. Significant accounting policies (continued):

(m) Non-financial assets:

Non-financial assets are not available to discharge existing liabilities and are held-for-use in the provision of services. They have useful lives extending beyond the current year and are not intended for sale in the ordinary course of operations.

(i) Tangible capital assets:

Tangible capital assets are initially recorded at cost which includes amounts that are directly attributable to acquisition, construction, development or betterment of the asset. The cost, less residual value, of the tangible capital assets, excluding land, is amortized on a straight-line basis over their estimated useful lives as follows:

Asset	Useful Life - Years
Land improvements	3 to 100
Buildings and facilities	2 to 50
Machinery and equipment	2 to 25
Roads and sidewalks	10 to 50
Sanitary sewer infrastructure	10 to 80
Storm sewer infrastructure	5 to 80
Water system infrastructure	4 to 80

Leasehold improvements are amortized using the lesser of the remaining term of the lease, including all renewal terms at the option of the City, or the life of the asset.

Annual amortization is charged commencing when the asset is available for use. Assets under construction are not amortized until the asset is put into productive use.

(ii) Contributions of tangible capital assets:

Tangible capital assets received as contributions are recorded at their fair value at the date of receipt and are also recorded as revenue.

(iii) Works of art and cultural and historic assets:

Works of art and cultural and historic assets are not recorded as assets in these financial statements.

(iv) Interest capitalization:

The City does not capitalize interest costs associated with the acquisition or construction of tangible capital assets.

(v) Leased tangible capital assets:

Leases which transfer substantially all of the benefits and risks incidental to ownership of property are accounted for as leased tangible capital assets. All other leases are accounted for as operating leases and the related payments are charged to expenses as incurred.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

1. Significant accounting policies (continued):

(m) Non-financial assets (continued):

(vi) Inventories of supplies:

Inventories of supplies held for consumption are recorded at the lower of cost and replacement cost.

(n) Employee future benefits:

(i) The City and its employees make contributions to the Municipal Pension Plan. The Municipal Pension Plan is a multi-employer defined benefit pension plan. The City's contributions are expensed as incurred.

(ii) Sick leave and post-employment benefits also accrue to the City's employees. The liability relating to these benefits is actuarially determined based on service and best estimates of retirement ages and expected future salary and wage increases. The liabilities under these benefit plans are accrued based on projected benefits prorated as employees render services necessary to earn the future benefits.

(o) Use of accounting estimates:

The preparation of financial statements requires management to make estimates and assumptions that affect the reported amounts of assets and liabilities and disclosure of contingent assets and liabilities at the date of these financial statements and the reported amounts of revenue and expenses during the reporting period. Actual results could differ from those estimates. Areas requiring the use of management estimates relate to the determination of the value of contributed assets, useful lives of tangible capital assets for amortization, collectibility of receivables, accrued sick and other post-employment benefits and provision for contingencies. Adjustments, if any, will be reflected in the financial statements in the period of settlement or change in the amount of the estimate.

(p) Segment disclosures:

A segment is defined as a distinguishable activity or group of activities of government for which it is appropriate to separately report financial information. The City has provided definitions of segments used by the City, as well as, presented financial information in segmented format (note 20).

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

1. Significant accounting policies (continued):

(q) Contaminated sites:

Contaminated sites are a result of contamination being introduced into air, soil, water, or sediment of a chemical, organic or radioactive material, or a live organism that exceeds an environmental standard. Liabilities are recorded net of any expected recoveries.

A liability for remediation of contaminated sites is recognized when a site is not in productive use and the following criteria are met:

- (i) An environmental standard exists;
- (ii) Contamination exceeds the environmental standards;
- (iii) The City is directly responsible or accepts responsibility;
- (iv) It is expected that future economic benefits will be given up; and
- (v) A reasonable estimate of the amount can be made.

The liability is recognized as management's estimate of the cost of post-remediation including operation, maintenance and monitoring that are an integral part of the remediation strategy for a contaminated site.

2. Cash and cash equivalents and temporary investments:

	2021	2020
Cash balances	\$ 127,391,177	\$ 123,528,131
Cash equivalents:		
Municipal Finance Authority Money Market Fund	4,250,995	4,244,508
Total cash and cash equivalents (a) (b)	131,642,172	127,772,639
Temporary investments consist of term deposits maturing within 365-days of inception	13,855,996	-
Total cash and cash equivalents and temporary investments	\$ 145,498,168	\$ 127,772,639

As at December 31, 2021, cash equivalents and temporary investments including the Municipal Finance Authority Money Market Fund, have annual yields that range from 0.15% to 1.05% (2020 - 0.89%).

- (a) Included in cash and cash equivalents is an amount of \$15,907,883 (2020 - \$12,532,028) that can only be used for the acquisition of tangible capital assets as provided for by the DCC Bylaw.
- (b) Included in cash and cash equivalents is an amount of \$13,413,939 (2020 - \$12,785,980) which consists of refundable performance deposits.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

3. Accounts receivable:

	2021	2020
Property taxes	\$ 1,287,510	\$ 1,757,625
Water user fees	1,534,190	1,419,170
Government grants	21,593	1,071,503
Amounts receivable secured by letters of credit (a)	582,322	2,415,733
Loan receivable	52,200	7,000
Other (note 7)	2,501,370	2,275,203
	<u>\$ 5,979,185</u>	<u>\$ 8,946,234</u>

(a) Amounts receivable secured by letters of credit of \$582,322 (2020 - \$2,415,733) can only be used for the acquisition of tangible capital assets as provided for by the DCC Bylaw (note 8(a)).

4. Accounts payable and accrued liabilities:

	2021	2020
Trade accounts payable and other	\$ 9,001,010	\$ 5,581,833
Due to governments and agencies	3,410,428	3,080,164
Other payroll liabilities	651,866	836,607
	<u>\$ 13,063,304</u>	<u>\$ 9,498,604</u>

5. Post-employment benefits:

The City provides certain post-employment and sick leave benefits to its employees. These benefits include accumulated non-vested sick leave, post-employment gratuity and time-in-lieu benefits, and certain vacation benefits.

	2021	2020
Accrued benefit obligation:		
Balance, beginning of year	\$ 1,857,606	\$ 1,755,977
Current service cost	159,661	145,837
Interest cost	39,180	47,236
Benefits paid	(74,402)	(274,537)
Actuarial (gain) loss	(112,420)	183,093
Balance, end of year	<u>\$ 1,869,625</u>	<u>\$ 1,857,606</u>

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

5. Post-employment benefits (continued):

	2021	2020
Accrued benefit obligation, end of year	\$ 1,869,625	\$ 1,857,606
Unamortized net actuarial loss	(169,375)	(316,742)
Accrued benefit liability, end of year	\$ 1,700,250	\$ 1,540,864

Actuarial gains and losses are amortized over the expected average remaining service period of the related employee group, commencing the year after the gain or loss arises. In 2021, the expected average remaining service period of the related employee group is 11 years (2020 - 11 years).

The significant actuarial assumptions used to determine the City's accrued benefit obligation are as follows:

	2021	2020
Discount rate	2.50%	2.00%
Expected future inflation rate	2.50%	2.50%
Expected wage and salary increases	2.58% to 5.90%	2.58% to 5.90%

6. Capital lease obligation:

The City has financed certain equipment by entering into three capital leasing arrangements. Two of the arrangements expire on June 30, 2025, and the third arrangement expires on June 30, 2027. The leases are repayable in monthly blended installments of \$3,075 including principal and interest. The amount of interest incurred on the leases in the current year was \$4,321 (2020 - \$5,272). Minimum lease payments due under the capital leases are:

Year ending December 31:

2022	\$ 36,897
2023	36,897
2024	36,897
2025	21,215
2026	5,532
Thereafter	2,767
Minimum capital lease payments	140,205
Less amounts representing interest (2.85%)	8,025
	\$ 132,180

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

7. Debt:

	2021	2020
Debentures, net of repayments and actuarial adjustments	\$ 21,900,837	\$ 22,626,177

The City issues debt instruments through the Municipal Finance Authority ("MFA") to finance certain capital acquisitions. Repayments and actuarial adjustments are netted against related long-term debts. Details are as follows:

Bylaw number	Purpose	Maturity date	Interest rate	Refinancing year *	Authorized amount	Sinking fund credits	2021	2020
1667	Sanitary Sewers-Local Improvement	2023	5.95%	n/a	\$ 224,580	\$ 191,073	\$ 33,507	\$ 49,074
2098	Water Utility Acquisition Advance Payment	2046	2.60%	2026	14,250,000	1,480,261	12,769,739	13,086,502
2163	Water System Infrastructure Construction	2027	2.80%	n/a	440,000	160,573	279,427	321,367
2163	Water System Infrastructure Construction	2047	2.80%	2027	5,779,000	508,186	5,270,814	5,403,548
2178	Water System Infrastructure Construction	2027	3.15%	n/a	1,662,000	606,530	1,055,470	1,213,890
2178	Water System Infrastructure Construction	2047	3.15%	2027	400,000	35,176	364,824	374,012
2179	Water Treatment Facilities Construction	2048	3.15%	2028	2,274,850	147,794	2,127,056	2,177,784
						\$ 25,030,430	\$ 3,129,593	\$ 21,900,837
								\$ 22,626,177

* During the refinancing year, the City will have the option to retire part or all of the debt early or refinance the borrowing at a new interest rate.

Total interest on the debt for the year ended December 31, 2021 was \$694,606 (2020 - \$694,606).

As a condition of these borrowings, a portion of the debt proceeds is withheld by the MFA in a debt reserve fund. If at any time the City does not have sufficient funds to meet payments due on its obligations, the payments shall be made from the debt reserve fund. Amounts withheld for this purpose are as follows:

Bylaw number	Purpose	Debt reserve fund
1667	Sanitary Sewers - Local Improvement	\$ 2,246
2098	Water Utility Acquisition Advance Payment	142,500
2163	Water System Infrastructure Construction	62,190
2178	Water System Infrastructure Construction	20,620
2179	Water Treatment Facilities Construction	22,749
		\$ 250,305

These cash deposits are included as part of accounts receivable in the Statement of Financial Position (note 3).

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

7. Debt (continued):

Principal payments and estimated actuarial adjustments on the outstanding debt over the following five years and thereafter are as follows:

2022	\$ 748,996
2023	773,431
2024	780,652
2025	805,830
2026	831,820
Thereafter	17,960,108
	\$ 21,900,837

8. Deferred revenue:

	2021	2020
Property taxes	\$ 3,299,003	\$ 3,218,594
Contributions for future capital works	3,630,013	3,731,110
Development cost charges (a)	16,490,205	14,947,761
Utility service connections	1,563,750	1,151,750
Deferred revenue - facility upgrades (note 11)	385,146	428,121
Deferred revenue - building permits	2,614,950	4,386,140
Other	1,922,450	1,237,879
	\$ 29,905,517	\$ 29,101,355

(a) Development cost charges:

	Highways	Drainage	Parkland	Sanitary	Water	Total
Balance, beginning of year	\$ 3,505,452	\$ 3,085,749	\$ 6,184,906	\$ 881,842	\$ 1,289,812	\$ 14,947,761
Add:						
Amounts received	299,648	242,636	674,395	100,104	143,335	1,460,118
Investment income	29,669	26,324	53,187	7,354	10,851	127,385
Deduct:						
Acquisition of tangible capital assets	-	25,059	-	20,000	-	45,059
Balance, end of year	\$ 3,834,769	\$ 3,329,650	\$ 6,912,488	\$ 969,300	\$ 1,443,998	\$ 16,490,205

The balance as of December 31, 2021 includes \$582,322 (2020 - \$2,415,733) in amounts receivable secured by letters of credit (note 3(a)).

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

9. Tangible capital assets:

	Land	Land improvements	Buildings and facilities	Machinery and equipment	Roads and sidewalks	Sanitary sewer infrastructure	Storm sewer infrastructure	Water system infrastructure	Assets under construction (a)	2021 Total
Cost:										
Balance, beginning of year	\$ 17,310,385	\$ 24,698,389	\$ 42,856,241	\$ 14,069,320	\$ 46,482,120	\$ 21,497,841	\$ 23,840,136	\$ 37,077,251	\$ 4,179,781	\$ 232,011,464
Additions	-	639,944	287,163	1,154,595	854,741	553,830	460,373	782,935	2,536,642	7,270,223
Transfers	-	1,568,227	66,572	32,624	-	28,520	28,516	700,517	(2,424,976)	-
Disposals	-	(148,702)	(160,000)	(504,934)	(468,489)	(122,247)	(85,896)	(32,155)	-	(1,522,423)
Balance, end of year	\$ 17,310,385	\$ 26,757,858	\$ 43,049,976	\$ 14,751,605	\$ 46,868,372	\$ 21,957,944	\$ 24,243,129	\$ 38,528,548	\$ 4,291,447	\$ 237,759,264
Accumulated amortization:										
Balance, beginning of year	\$ -	\$ 7,852,933	\$ 17,332,361	\$ 7,196,237	\$ 22,416,546	\$ 7,499,126	\$ 9,880,305	\$ 3,407,989	\$ -	\$ 75,585,497
Amortization expense	-	1,159,523	1,401,127	1,023,112	1,057,716	326,820	310,756	1,091,357	-	6,370,411
Disposals	-	(148,702)	(121,981)	(492,832)	(407,217)	(52,130)	(74,611)	(21,316)	-	(1,318,789)
Balance, end of year	\$ -	\$ 8,863,754	\$ 18,611,507	\$ 7,726,517	\$ 23,067,045	\$ 7,773,816	\$ 10,116,450	\$ 4,478,030	\$ -	\$ 80,637,119
Net book value, end of year	\$ 17,310,385	\$ 17,894,104	\$ 24,438,469	\$ 7,025,088	\$ 23,801,327	\$ 14,184,128	\$ 14,126,679	\$ 34,050,518	\$ 4,291,447	\$ 157,122,145
Net book value, beginning of year	\$ 17,310,385	\$ 16,845,456	\$ 25,523,880	\$ 6,873,083	\$ 24,065,574	\$ 13,998,715	\$ 13,959,831	\$ 33,669,262	\$ 4,179,781	\$ 156,425,967

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

9. Tangible capital assets (continued):

(a) Assets under construction:

Assets under construction have not been amortized. Amortization of these assets will commence when the asset is put into productive use.

(b) Contributed tangible capital assets:

Contributed tangible capital assets have been recognized at fair value at the date of contribution. The value of developers' contributions of tangible capital assets received during the year ended December 31 was comprised of:

	2021	2020
Roads and sidewalks	\$ 295,344	\$ 464,171
Sanitary sewers	222,773	50,569
Storm sewers	-	100,356
Water system	115,146	48,765
Developers' contributions of tangible capital assets	\$ 633,263	\$ 663,861

(c) Tangible capital assets disclosed at nominal values:

Where an estimate of fair value could not be made, the tangible capital asset was recognized at a nominal value.

(d) Works of art and historical treasures:

The City manages and controls various works of art and non-operational historical cultural assets including totem poles, sculptures and paintings located at City sites and public display areas. These assets are not reflected in the financial statements.

(e) Write-down of tangible capital assets:

There were no write-downs of tangible capital assets during the year. The City disposed of assets as part of planned replacement capital projects in the year.

(f) Leased tangible capital assets:

The City has leased radio equipment for RCMP and Fire operations. Total cost of the leased assets is \$248,299. As at December 31, 2021, \$122,140 (2020 - \$89,213) of accumulated amortization has been recorded, with \$32,927 (2020 - \$19,283) of amortization related to the current fiscal year.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

10. Accumulated surplus:

	Reserves established by bylaw (note 12(a))	Other reserves (note 12(b))	Other surplus funds	Investment in tangible capital assets (note 11)	Total
General Fund	\$ 31,428,393	\$ 22,201,808	\$ 6,236,425	\$ 92,915,654	\$ 152,782,280
Sanitary Sewer Fund	-	14,823,480	1,107,116	14,202,858	30,133,454
Drainage Fund	-	11,257,191	784,117	15,047,085	27,088,393
Water Fund	-	5,872,462	1,248,000	12,538,385	19,658,847
Total for 2021	\$ 31,428,393	\$ 54,154,941	\$ 9,375,658	\$ 134,703,982	\$ 229,662,974
Total for 2020	\$ 26,632,385	\$ 49,880,870	\$ 8,771,058	\$ 133,211,893	\$ 218,496,206

Included in the water fund investment in tangible capital assets is an adjustment in the amount of nil (2020 - \$4,942) for unspent debt proceeds which have been earmarked for use in future years (note 11).

11. Investment in tangible capital assets:

	2021	2020
Tangible capital assets (note 9)	\$ 157,122,145	\$ 156,425,967
Deduct:		
Capital lease obligations (note 6)	(132,180)	(164,718)
Debt (note 7)	(21,900,837)	(22,626,177)
Deferred revenue - facility upgrades (note 8)	(385,146)	(428,121)
Add:		
Unspent debt proceeds (note 10)	-	4,942
	\$ 134,703,982	\$ 133,211,893

12. Reserves:

(a) The following reserves were established, by bylaw, in accordance with BC municipal legislation:

	2021	2020
General Fund:		
Land sale reserve	\$ 416,665	\$ 413,013
Equipment replacement reserve	5,470,192	5,339,079
Capital works, machinery and equipment reserve	7,615,295	6,547,194
Local improvement reserve	33,963	33,665
Community amenity reserve	13,892,278	14,299,434
Affordable Housing Reserve	4,000,000	-
	\$ 31,428,393	\$ 26,632,385

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

12. Reserves (continued):

(b) The following additional reserve amounts are set aside for specific purposes:

	2021	2020
General Fund:		
Infrastructure reserve	\$ 4,982,797	\$ 4,656,217
Roadworks reserve	5,030,102	4,407,146
Secondary suite service fee reserve	1,556,289	1,189,107
Density bonus/amenity contribution reserve	196,829	214,634
Incomplete asset improvement projects	3,846,100	3,272,500
COVID-19 Safe Restart Reserve	1,142,616	2,586,388
Other reserves	5,447,075	4,629,243
	<u>22,201,808</u>	<u>20,955,235</u>
Sanitary Sewer Fund:		
Infrastructure reserve	14,068,546	13,055,437
Terry Road local improvement reserve	25,634	39,096
Incomplete asset improvement projects	729,300	1,069,700
	<u>14,823,480</u>	<u>14,164,233</u>
Drainage Fund:		
Infrastructure reserve	8,760,991	7,069,996
Operating reserve	25,000	25,000
Incomplete asset improvement projects	2,471,200	2,878,500
	<u>11,257,191</u>	<u>9,973,496</u>
Water Fund:		
Infrastructure reserve	4,890,175	4,026,448
Debt retirement reserve	28,687	24,058
Operating reserve	953,600	737,400
	<u>5,872,462</u>	<u>4,787,906</u>
	<u>\$ 54,154,941</u>	<u>\$ 49,880,870</u>

13. Commitments and contingencies:

(a) Borrowing liability:

The City has a contingent liability with respect to debentures of the Metro Vancouver Sewerage and Drainage District and the Metro Vancouver Regional District, to the extent provided for in their respective Enabling Acts, Acts of Incorporation and Amending Acts. Management does not consider payment under this contingency to be likely and therefore no amounts have been accrued.

(b) Third party claims:

There are various lawsuits and claims pending by and against the City. The outcome of these claims is undeterminable, and it is the opinion of management that final determination of these claims will not materially affect the financial statements of the City.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

13. Commitments and contingencies (continued):

(c) Lease commitments:

The City initially entered into a 5-year agreement ending July 31, 2008 to lease certain parcels of real property from The Burlington Northern and Santa Fe Railway Company. Under the terms of this agreement, the City is committed to annual rent payments as well as the costs of all taxes, utilities, insurance, repairs and maintenance of the leased premises. This is accounted for as an operating lease. This agreement provides for renewal options consisting of 5 additional 5-year terms. In April 2018, the City exercised its third option to renew this lease for the 5-year term ending July 31, 2023. During this period, the City is committed to annual base rent payments of \$472,500.

The City has also entered into various leases for office and other operating equipment.

Total annual commitments for the next five years, net of applicable taxes are approximately as follows:

2022	\$ 484,464
2023	504,638
2024	524,538
2025	519,084
2026	510,294
Thereafter	3,542,077
	<hr/>
	\$ 6,085,095

(d) Agreements and contractual commitments:

In addition to the leases described in note 13(c), the City has entered into various agreements and contracts for services and construction with periods ranging from one to 5-years, including the following:

	Total contractual commitment	Total contractual commitment remaining at year end
Ruth Johnston Park ravine restoration	\$ 1,463,264	\$ 445,592
2021 paving program	689,819	8,698
Solid waste recycling trucks	838,569	838,569
Solid waste residential collection vehicles	1,131,801	1,131,801
Janitorial services	477,007	238,081

The City records the capital costs incurred to the end of the year on incomplete projects as tangible capital assets under construction. The City's 5-year financial plan is amended as necessary to reflect the carryover of the required expenditure budgets and the financing of these obligations to the following year.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

13. Commitments and contingencies (continued):

(e) Debt agreement with the MFA:

The City issues debt instruments through the MFA. As a condition of these borrowings the City is required to execute demand notes in connection with each debenture whereby the City may be required to loan certain amounts to the MFA. The debt agreement with the MFA provides that if at any time the scheduled payments provided for in the agreement are not sufficient to meet the MFA's obligations in respect to such borrowing, the resulting deficiency becomes the joint and several liability of the City and all other participants to the agreement through the MFA. The City is similarly liable on a contingent basis for the debt of other municipalities secured through the MFA. Details of the contingent demand notes are as follows:

Bylaw number	Purpose	2021	2020
1667	Sanitary Sewers - Local Improvement	\$ 7,364	\$ 7,364
2098	Water Utility Acquisition Advance Payment	180,771	180,771
2163	Water System Infrastructure Construction	104,802	104,802
2178	Water System Infrastructure Construction	88,549	88,549
2179	Water Treatment Facilities Construction	36,988	36,988
		\$ 418,474	\$ 418,474

These contingent demand loans are not recorded in the City's financial statements as they are not likely to be paid.

(f) E-Comm Emergency Communications for British Columbia Incorporated:

The City is a shareholder of E-Comm Emergency Communications for British Columbia Incorporated ("E-Comm"). The City receives services for the regional 9-1-1 call centre for Metro Vancouver Regional District and the Wide Area Radio network from E-Comm. The City has two Class A shares (of a total 36 Class A and 19 Class B shares issued and outstanding as at December 31, 2021).

As a Class A shareholder, the City is committed to paying levies for services received under a cost-sharing formula to fund operating and capital costs of the E-Comm operations. In addition, the City is contingently liable to cover its proportionate share of such costs should any member be unable to fulfill its funding obligations. Annual levy amounts fluctuate based on various factors under the cost sharing formula.

(g) Municipal Insurance Association of British Columbia (the "Association"):

The City is a participant in the Association. Should the Association pay out claims in excess of premiums received, it is possible that the City, along with the other participants, would be required to contribute towards the deficit. Management does not consider payment under this contingency to be likely and therefore no amounts have been accrued.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

14. Pension plan:

The City and its employees contribute to the Municipal Pension Plan (the "Plan"), a jointly trustee pension plan. The Board of Trustees, representing plan members and employers, is responsible for administering the Plan, including investment of the assets and administration of benefits. The Plan is a multi-employer defined benefit pension plan. Basic pension benefits provided are based on a formula. The Plan has about 220,000 active members and approximately 112,000 retired members. Active members include approximately 42,000 contributors from local government and 150 contributors from the City.

Every three years, an actuarial valuation is performed to assess the financial position of the Plan and adequacy of Plan funding. The actuary determines an appropriate combined employer and member contribution rate to fund the Plan. The actuary's calculated contribution rate is based on the entry-age normal cost method, which produces the long-term rate of member and employer contributions sufficient to provide benefits for average future entrants to the Plan. This rate may be adjusted for the amortization of any actuarial funding surplus and will be adjusted for the amortization of any unfunded actuarial liability.

The most recent actuarial valuation as at December 31, 2018 indicated a \$2,866 million funding surplus for basic pension benefits on a going concern basis. The next valuation will be at December 31, 2021 with results available in 2022. Employers participating in the Plan record their pension expense as the amount of employer contributions made during the fiscal year (defined contribution pension plan accounting). This is because the Plan records accrued liabilities and accrued assets for the Plan in aggregate, resulting in no consistent and reliable basis for allocating the obligation, assets and cost to the individual employers participating in the Plan.

The City paid \$1,273,000 (2020 - \$1,269,600) for employer contributions to the Plan in fiscal 2021.

15. Collections for other governments:

The City collects certain taxation revenue on behalf of other government bodies. These funds are excluded from the City's financial statements as they are not revenue of the City. Such taxes collected and remitted to other government bodies during the year are as follows:

	2021	2020
Province of British Columbia - school tax	\$ 13,303,585	\$ 11,671,297
South Coast BC Transportation Authority - TransLink tax	2,786,379	2,612,455
Other regional bodies	1,035,496	963,477
	<u>\$ 17,125,460</u>	<u>\$ 15,247,229</u>

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

16. Sales of services and other revenue:

	2021	2020
Parking	\$ 2,842,810	\$ 2,206,698
Parks, recreation and cultural services	785,451	745,032
Licences and permits	3,254,455	2,737,882
Solid waste services	1,573,085	1,569,079
Utility service connection fees	378,067	647,672
Community amenity contributions	3,600,000	3,386,000
Interest and penalties	1,065,203	1,433,641
Contributions toward tangible capital asset improvements	461,393	450,882
Other	2,188,545	1,749,593
	<u>\$ 16,149,009</u>	<u>\$ 14,926,479</u>

17. Government grants:

Government grant revenue is comprised of the following government transfers:

	2021	2020
Federal	\$ 254,216	\$ 157,061
Provincial	316,511	5,302,210
	<u>\$ 570,727</u>	<u>\$ 5,459,271</u>

18. Contractual rights:

The City's contractual rights arise from rights to receive payments under lease, license, rental, grant and other agreements. The City has contractual rights to receive the following amounts in the next 5 years and thereafter:

2022	\$ 359,682
2023	272,366
2024	281,051
2025	13,048
2026	8,440
Thereafter	3,600
	<u>\$ 938,187</u>

The City is entitled to receive revenue from certain other grants and agreements which is difficult to quantify. The contractual rights from these agreements have not been included in the amounts noted above.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

19. Comparative information:

Certain comparative information has been reclassified to conform to the financial statement presentations adopted for the current year.

20. Segmented information:

The City is a diversified municipal government institution that provides a wide range of services to its citizens, visitors and many others. In compliance with *PS2700, Segment Disclosures*, certain financial information is required to be reported for major activities involving these services. Each of these major activities is comprised of various City departments and/or divisions as noted below.

General Government - Mayor and Council, Chief Administrative Officer, Corporate Administration, Communications, Financial Services, Human Resources, Planning and Development, and Information Services. These departments are responsible for many legislative, operational and administrative support services including but not limited to City Council, bylaw and procedural matters, levying and collecting property taxes, hiring City staff, supporting the City's information technology infrastructure, preparing land use plans and approving new development in the City.

Protection Services - Police, Fire, Building and Bylaw Enforcement (non-parking related). These departments are responsible for ensuring public safety and security, preventing crimes as well as enforcing various laws.

Transportation, Engineering and Municipal Operations - Public Works, Engineering and Parking. These areas are responsible for providing and maintaining the systems that enable the community to use transportation facilities such as roads, sidewalks and parking lots. This category also includes management and administrative services of the City's Engineering and Municipal Operations Department.

Parks, Recreation and Cultural Services - These areas are responsible for providing, facilitating the development of, and maintaining high quality parks, recreation facilities and cultural services such as the public library.

Solid Waste Services - These services include the City's garbage collection, green waste collection and recycling programs.

Sanitary Sewer System - These services include the provision and maintenance of all systems related to the distribution and disposal of sanitary sewage.

Drainage System - These services include the provision and maintenance of all systems involving the distribution of storm water run-off in the City.

Water System - These services include the provision and treatment of water supply, as well as, the provision and maintenance of all other systems involving the distribution of water.

THE CORPORATION OF THE CITY OF WHITE ROCK

Notes to Financial Statements (continued)

Year ended December 31, 2021

20. Segmented information (continued):

	General Government	Protection Services	Transportation, Engineering and Operations Services	Parks, Recreation and Cultural Services	Solid Waste Services	Sanitary Sewer System	Drainage System	Water System	Adjustments	2021	2020
Revenue:											
Property taxes:											
General purposes	\$ 25,053,083	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 25,053,083	\$ 23,631,761
Regional library levy	-	-	-	986,310	-	-	-	-	-	986,310	975,183
Business improvement levy	318,952	-	-	-	-	-	-	-	-	318,952	336,779
	25,372,035	-	-	986,310	-	-	-	-	-	26,358,345	24,943,723
Receipts in lieu of taxes	22,590	-	-	-	-	-	-	-	-	22,590	22,356
Development cost charges	-	-	-	-	-	-	-	-	-	-	61,005
Sanitary sewer fees and parcel tax	-	-	-	-	-	20,000	25,059	-	-	45,059	-
Drainage user fees	-	-	-	-	-	3,684,931	-	-	(28,078)	3,656,853	3,562,975
Water user fees	-	-	-	-	-	-	2,775,458	-	-	2,775,458	2,658,487
Sales of services and other revenue:	-	-	-	-	-	-	-	5,913,201	(209,013)	5,704,188	5,139,529
Parking	-	-	2,842,810	-	-	-	-	-	-	2,842,810	2,206,698
Parks, recreation, and cultural services	-	-	-	785,451	-	-	-	-	-	785,451	745,032
Licenses and permits	11,640	3,242,815	-	-	-	-	-	-	-	3,254,455	2,737,882
Solid waste services	-	-	-	-	1,573,085	-	-	-	-	1,573,085	1,569,079
Utility service connection fees	-	-	-	-	-	106,975	113,850	157,242	-	378,067	647,672
Community amenity contributions	3,600,000	-	-	-	-	-	-	-	-	3,600,000	3,386,000
Interest and penalties	988,113	-	-	-	-	31,486	12,000	33,604	-	1,065,203	1,433,641
Contributions toward tangible capital asset improvements	-	-	-	-	-	-	-	-	-	-	450,882
Other	3,288,465	335,802	441,884	19,509	-	-	-	-	-	461,393	1,749,593
Government grants	262,185	292,639	507,519	42,975	36,169	8,775	-	164,290	(2,195,450)	2,188,545	5,459,271
Developers' contributions of tangible capital assets	-	-	2,211	13,692	-	-	-	-	-	570,727	-
	33,545,028	3,871,256	295,344	-	-	222,773	-	115,146	-	633,263	663,861
	-	-	4,089,768	1,847,937	1,609,254	4,074,940	2,926,367	6,383,483	(2,432,541)	55,915,492	57,437,686
Expenses:											
Salaries, wages and benefits	4,694,062	5,995,593	2,727,250	2,742,890	670,679	362,762	432,607	1,280,433	(1,225,042)	17,681,234	17,410,695
Contracted services	1,233,579	5,768,050	2,033,415	3,374,905	660,285	2,364,465	541,291	995,836	(549,193)	16,422,633	16,814,727
Supplies and other	1,178,040	336,171	733,835	962,958	230,416	175,820	133,073	483,475	(658,306)	3,575,482	2,896,547
Amortization	273,428	352,025	1,799,898	2,163,271	25,899	326,820	310,756	1,113,314	-	6,370,411	6,404,914
Interest	4,358	-	-	-	-	13,363	-	681,243	-	696,964	710,652
	7,383,467	12,451,839	7,294,398	9,244,024	1,587,279	3,243,230	1,417,727	4,559,301	(2,432,541)	44,748,724	44,237,535
Annual surplus (deficit)	\$ 26,161,561	\$ (8,580,583)	\$ (3,204,630)	\$ (7,396,087)	\$ 21,975	\$ 831,710	\$ 1,508,640	\$ 1,824,182	\$ -	\$ 11,166,768	\$ 13,200,151

THE CORPORATION OF THE CITY OF WHITE ROCK

Unaudited Statement of the COVID-19 Safe Restart Grant

Year ended December 31, 2021

The COVID-19 Safe Restart Grant was received November 2020 from the Province of British Columbia. A requirement of the COVID-19 Safe Restart Grant is to include a schedule to the financial statements presenting the amount of funding received, use of funds and year-end balance of unused funds. A schedule will continue to be reported annually until funds are fully drawn down.

	2021
COVID-19 Safe Restart Grant received	\$ 3,769,000
COVID-19 Safe Restart Grant funds used 2020	(1,182,612)
COVID-19 Safe Restart Grant funds used 2021:	
Decreased revenues:	
Parking services	(712,789)
Recreation and culture	(864,162)
Miscellaneous	(37,945)
	(1,614,896)
Decreased expenses:	
Parking services	17,410
Recreation and culture	624,545
General government	23,846
Miscellaneous	83,678
	749,479
Increased expenses:	
Parks and facilities	(286,645)
Roads	(291,710)
	(578,355)
Total COVID-19 Safe Restart Grant funds used 2021	(1,443,772)
Year end COVID-19 Safe Restart Grant funds available as at December 31, 2021	\$ 1,142,616

**PART II - CITY OF WHITE ROCK 2021
STATEMENT OF FINANCIAL INFORMATION**

**SUPPLEMENTARY INFORMATION
(SCHEDULE OF REMUNERATION AND EXPENSES,
SCHEDULE OF PAYMENTS TO SUPPLIERS OF
GOODS AND SERVICES, ETC.)**

THE CORPORATION OF THE CITY OF WHITE ROCK
FOR THE YEAR ENDED DECEMBER 31, 2021
SCHEDULE OF DEBTS

For information on the long-term debt and all other debt owed by the City of White Rock as of December 31, 2021, refer to the 2021 Audited Financial Statements Note 7, "Debt", included in Part 1 of the Statement of Financial Information.

THE CORPORATION OF THE CITY OF WHITE ROCK
FOR THE YEAR ENDED DECEMBER 31, 2021
SCHEDULE OF GUARANTEE AND INDEMNITY AGREEMENTS

This organization has not given any guarantees or indemnities under the Guarantees and Indemnities Regulation.

THE CORPORATION OF THE CITY OF WHITE ROCK

**SCHEDULE SHOWING THE REMUNERATION AND EXPENSES PAID TO OR ON BEHALF OF EACH EMPLOYEE
YEAR ENDED DECEMBER 31, 2021**

1. Elected Officials

Name	Position	Remuneration	Expenses
Chesney, David	Councillor	\$ 45,470	\$ -
Fathers, Helen	Councillor	45,470	-
Johanson, Ramona	Councillor	45,470	1,385
Kristjanson, Scott	Councillor	45,470	1,025
Manning, Anthony	Councillor	45,470	-
Trevelyan, Christopher	Councillor	45,470	-
Walker, Darryl	Mayor	104,930	8,030
Totals		\$ 377,750	\$ 10,440

The above expense figures are based on the criteria defined in the Financial Information Act.

2. Employees

The schedule below shows:

- In respect to each employee earning more than \$75,000, the total remuneration paid to the employee and the total amount paid for his/her expenses and
- A consolidated total of all remuneration paid to all other employees.

Remuneration includes gross salaries (including taxable benefits, vehicle allowances, retroactive pay, overtime, vacation and other banked time paid out, etc). The amounts noted with an " * " below include unused vacation and other banked time paid out.

Expenses include travel, memberships, training, registration fees and other similar perquisites and amounts.

Name	Position	Remuneration	Expenses
Abt, Dustin William	Manager, Public Works	\$ 126,818	\$ 880
Agustin, Arturo	Water Operator I	89,562 *	807
Almeida, Paulo	Public Works Foreman	110,130 *	1,689
Armstrong, Michael	Firefighter	106,159	63
Arthur, Tracey	Director, Corporate Administration	163,014 *	1,242
Bancroft, Michael	Firefighter	113,951 *	-
Bennett, Spencer	Firefighter	77,239	362
Berg, Wayne	Manager, Building & Bylaw Enforcement	105,543	2,420
Bindra, Raghbir	Firefighter	121,094 *	258
Booth, Scott	Firefighter	113,236 *	-
Brierley-Green, Janene	Manager, Revenue Services	130,369	2,656
Brown, Dean	Water Operator II	86,526 *	235
Cheung, Alan	Business Systems Analyst	88,582 *	2,273
Choy, Rosaline	Manager Engineering	125,468 *	1,976
Claffey, Alanna	Arborist	80,880 *	738
Cram, Andrew	Firefighter	105,102 *	(15)
Dadivas, Joseph	Network Administrator	99,098 *	2,930
Dai, Weibo	Financial Analyst	87,329 *	1,461
Davison, James	Firefighter, Captain	139,564	314
continued on following page			

Name	Position	Remuneration	Expenses
continued from previous page			
Dhillon, Sandeep	Senior Bylaw Enforcement Officer	\$ 82,983 *	\$ 493
Diamantopoulos, Michael	Firefighter	109,149 *	63
Farrant, Paul	Firefighter	100,869	220
Ferrero, Guillermo	Chief Administrative Officer	232,616	855
Fulton, Cordell	Firefighter, Captain	142,547 *	63
Gareau, Guy	Building Official I	98,159 *	819
Gilchrist, Sherry	Building Official III	84,962 *	1,155
Gill, Harmail	Bylaw Enforcement Officer II	77,652 *	283
Gordon, James	Director Engineering & Municipal Operations	191,048 *	2,203
Gorst, Donald	Tandem Truck Driver	87,633	1,240
Goulet, Thomas	Firefighter	113,559	305
Hamilton, Scott	Firefighter, Captain	139,207 *	811
Hasan, Syed Ahmed	Foreman, Water Treatment	117,035 *	702
He, Li Hua	Project Engineer	76,155	670
Hockemeyer, Heiko	Water Operator I	86,167 *	872
Isaak, Carl	Director Planning & Development Services	136,668 *	779
Johnston, Shannon	Manager, Budgets & Accounting	150,221 *	3,409
Johnstone, Debra	Deputy Corporate Officer	82,296	1,658
Johnstone, Jacqueline	Director, Human Resources	154,587 *	1,280
Kameka, Eric	Firefighter	107,724	449
Kell, Donna-Lynn	Manager, Communications & Government Relations	90,588 *	1,189
Keurvorst, Helen	Manager, Cultural Development	102,867	1,089
Lin, Ying	GIS Applications Developer	103,468 *	204
Lo, Hiep	Engineering Technologist	102,652 *	578
Macleod, Norman	Deputy Fire Chief	140,118	2,438
Madsen, Birk	Engineering Technologist	98,754	634
Magnus, Christina	Executive Assistant to Mayor & CAO	82,658	350
Mann, Richard	Firefighter	126,609 *	1,030
McKinnon, Cory	Firefighter	105,967	-
McMahon, Connor	Firefighter	103,518	63
Meneghin, Matthew	Firefighter	104,602	404
Miki, James	Foreman, Trades Mechanic	96,472 *	641
Mollan, Kenneth	Deputy Fire Chief	158,866 *	1,393
Mossop, David	Firefighter	97,790 *	-
Newman, Gregory	Manager, Planning	127,179 *	1,515
Nono, Tiburcio	Water Operator I	91,362 *	2,065
Overton, Kenneth	Manager, Property & Risk Management	89,416	1,660
Passas, Michael	Firefighter	105,450 *	1,568
Paterson, Danielle	HR Advisor	95,005	1,364
Pither, Simon	Water Foreman	113,886	735
Ponzini, Mary Colleen	Director, Financial Services	126,439	2,347
Reimer, T. Lee	Site Supervisor - Arena & Special Events	79,416 *	778
Roffe, Candice	Health & Safety Advisor	95,577 *	2,292
Ryan, Patrick	Firefighter	109,029 *	169
Sauve, Tara	Manager, RCMP Support Services	87,774 *	1,931
Smith, Douglas	Firefighter, Captain	129,939	1,914
Stepura, Eric	Director, Recreation and Culture	170,209 *	2,046
Thomas, Michael	Electrician	97,720	1,416
Volkens, Ralph	Manager, Parking Services	87,322	2,703
Waddington, Jenine	Capital Asset Analyst	81,113 *	1,334
Webb, Catherine	Administrative Assistant - Fire	75,349 *	-
Wolfe, Edward	Fire Chief	174,476 *	1,530
Yee, Sylvia	Manager, Community Recreation	121,662	2,249
Zota, Chris	Manager, Information Services	144,623 *	984
Consolidated total of all remuneration paid to other employees		5,771,832	70,239
Total Employees		\$ 13,930,608	\$ 149,470

3. Reconciliation

Total remuneration - elected officials	\$ 377,750
Total remuneration - employees	13,930,608
Subtotal	14,308,358
Differences:	
There are differences due to inconsistent reporting formats and accounting treatments required by Canadian Public Sector Accounting Standards for the Financial Statements versus the Financial Information Act for the Statement of Financial Information. For example:	
a) This schedule is prepared on a cash basis of accounting versus the Salaries, Wages and Benefits from Note 20 in the Financial Statements which is prepared on an accrual accounting basis.	93,234
b) This schedule does not include the employer portion of contributions to Work Safe BC, registered pension plans, employer health tax, extended health, and dental plans and other employee benefits that are included in "Salaries, Wages and Benefits" in the Financial Statements Note 20.	2,579,803
c) This schedule does not include the employer portion of Employment Insurance and Canada Pension Plan remittances paid to the Canada Revenue Agency that are included in "Salaries, Wages and Benefits" in the Financial Statement Note 20.	699,839
Total per Financial Statements Note 20, Salaries, Wages & Benefits	\$ 17,681,234
Variance	\$ -

4. Statement of Severance Agreements

There were no severance agreements in accordance with the Financial Information Act made between the City of White Rock and its non-unionized employees during fiscal year 2021.

THE CORPORATION OF THE CITY OF WHITE ROCK

**SCHEDULE SHOWING PAYMENTS MADE FOR THE PROVISION OF GOODS OR SERVICES
FOR THE YEAR ENDED DECEMBER 31, 2021**

Schedule showing the aggregate of amounts paid to all suppliers of goods and services who received more than \$25,000 during the year ended December 31, 2021. This schedule also includes payments for the purposes of grants or contributions.

1. Suppliers > \$25,000

SUPPLIER NAME	AMOUNT PAID
A & A TESTING LTD.	\$ 137,677
AJM DISPOSAL SERVICES LTD.	163,141
ALPHA BRAVO CHARLIE MAINTENANCE LTD DBC: ABC	299,207
AON REED STENHOUSE	457,864
ASSOCIATED FIRE SAFETY EQUIPMENT	87,929
BC HYDRO	810,625
BC PLANT HEALTH CARE INC.	69,086
BLACK PRESS GROUP LTD.	117,154
BLACKRETE PAVING LTD.	234,508
BNSF RAILWAY COMPANY	472,581
BRIGHOUSE CIVIL CONTRACTING DTM LTD.	490,773
CANADA POST CORPORATION	27,062
CANADA REVENUE AGENCY	699,839
CELCO CONTROLS LTD.	60,725
CENTRALSQUARE CANADA SOFTWARE INC.	195,749
CITY OF SURREY	522,834
CITY SPACES	30,685
CLEARTECH INDUSTRIES INC.	44,910
CLEARVIEW HORTICULTURAL PRODUCTS INC.	28,409
CRESCENT ELECTRICAL CONTRACTORS LTD.	152,560
CROWN CONTRACTING LIMITED	172,537
DAMS FORD LINCOLN SALES LTD.	203,209
DEKRA-LITE INDUSTRIES INC.	65,136
DELL CANADA INC.	96,020
DOUGNESS HOLDING LTD.	38,036
E-COMM EMERGENCY COMMUNICATIONS FOR BC INC.	74,651
ELEMENT MATERIALS TECHNOLOGY CANADA INC.	43,525
EMCO CORPORATION	117,137
ESRI CANADA LTD.	36,602
FLOCOR INC.	27,854
FORTIS BC	89,173
FRASER VALLEY FIRE PROTECTION LTD.	45,031
FRASER VALLEY REFRIGERATION LTD.	46,480
FRASER VALLEY REGIONAL LIBRARY	990,050
FRED SURRIDGE LTD.	140,034
GEOWEST ENGINEERING LTD.	145,367
GFL ENVIRONMENTAL INC.	220,427
GOODBYE GRAFFITI SURREY	53,923
GPM CIVIL CONTRACTING INC.	222,385
GREATER VANCOUVER REGIONAL SEWERAGE & DRAINAGE DISTRICT	1,830,138
GREGG DISTRIBUTORS (B.C.) LTD.	75,034
GREYSTONE DESIGN MANAGEMENT CONSTRUCTION	1,927,958
GUILLEVIN INTERNATIONAL CO.	69,170
HACH SALES AND SERVICE CANADA LP	44,078
HALLMARK FORD SALES LTD.	130,626
HATFIELD CONSULTANTS LLP	60,063
IDRS	28,145
IMPARK	87,847
continued on following page	

SUPPLIER NAME	AMOUNT PAID
continued from previous page	
INFINITE ROAD MARKING LTD.	\$ 44,568
JW RECYCLE-IT INC DBA RECYCLE IT CANADA	28,826
KIKHOSROWKIAN, FEREIDON	30,645
KONICA MINOLTA BUSINESS SOLUTIONS (CANADA) LTD.	39,608
KPMG LLP	37,065
LANDMARK PREMIERE PROPERTIES (CENTRAL PLAZA) LTD.	39,200
LANGLEY ROOFING CO LTD.	210,734
LEE'S TREES	28,943
LIDSTONE & COMPANY BARRISTERS & SOLICITORS	162,398
LOCK-BLOCK LTD.	78,093
LONG VIEW SYSTEMS CORPORATION	45,255
MAD PROPS MARINE LTD.	44,541
MAINLAND CONSTRUCTION MATERIALS ULC	645,995
MCRAES ENVIRONMENTAL SERVICES LTD	26,346
MEN IN KILTS - VANCOUVER	34,033
METRO VANCOUVER	142,205
MICROSOFT LICENSING GP	74,286
MINISTER OF FINANCE	431,105
MINISTRY OF PUBLIC SAFETY AND SOLICITOR GENERAL	137,635
MPOWERED VENTURES LTD.	55,160
MUNICIPAL INSURANCE ASSOCIATION OF BC	225,590
MUNICIPAL PENSION PLAN	1,273,834
NEXGEN TECHNOLOGIES INC.	43,960
NORTON ROSE FULBRIGHT CANADA LLP	31,835
NUTECH FACILITY SERVICES LTD.	627,309
ODGERS BERNDTSON CANADA INC.	87,502
ORKIN CANADA CORPORATION	41,975
PACIFIC BLUE CROSS	493,421
PACIFIC BREEZE HEATING & COOLING INC.	73,511
PAYBYPHONE TECHNOLOGIES INC.	65,258
POSTAGE BY PHONE - PITNEY BOWES	45,433
PRECISION SERVICE & PUMPS INC.	49,798
PW TRENCHLESS CONSTRUCTION INC.	926,108
R F BINNIE & ASSOCIATES LTD.	533,867
RECEIVER GENERAL FOR CANADA	4,725,376
ROCKY MOUNTAIN PHOENIX	27,341
ROLLINS MACHINERY LTD.	61,924
RTR TERRA CONTRACTING LTD.	26,088
S2 ARCHITECTURE	57,245
SANDPIPER CONTRACTING LTD.	106,087
STAPLES ADVANTAGE	33,110
STAPLES PROFESSIONAL	65,173
STRATA PLAN BCS 3236	73,566
SUDDEN TECHNOLOGIES	73,835
SUNCOR ENERGY PRODUCTS PARTNERSHIP	214,258
SWR DRAIN SERVICE LTD.	51,204
T & S POWER DIGGING	60,590
TELUS	80,019
TELUS MOBILITY	69,366
TK ELEVATOR (CANADA) LIMITED	35,515
TOURISM WHITE ROCK ASSOCIATION	115,500
TYBO CONSTRUCTORS LTD.	64,951
URBAN GROVE TREE CARE & CONSULTING	91,529
URBAN SYSTEMS LTD.	108,004
VALLEY TRAFFIC SYSTEMS INC.	197,553
VIMAR EQUIPMENT	441,059
WASTE CONNECTIONS OF CANADA INC.	92,548
WESCO DISTRIBUTION CANADA LP	34,035
WESTLAND INSURANCE GROUP LTD.	119,703
WESTMAR ADVISORS INC.	127,668
continued on following page	

SUPPLIER NAME	AMOUNT PAID
continued from previous page	
WFR WHOLESALE FIRE & RESCUE	\$ 30,490
WORK SAFE BC	381,444
WORK TRUCK WEST A DIVISION OF WEST COAST	136,094
YMCA OF GREATER VANCOUVER	32,341
TOTAL AMOUNT	\$ 26,773,612

2. Other Suppliers

CONSOLIDATED TOTAL PAID TO SUPPLIERS WHO RECEIVED AGGREGATE PAYMENTS OF \$25,000 OR LESS	\$ 2,466,333
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3. Payments of Grants and Contributions

SUPPLIER NAME	AMOUNT PAID
WHITE ROCK BUSINESS IMPROVEMENT ASSOCIATION	\$ 343,425
WHITE ROCK MUSEUM & ARCHIVES SOCIETY (EXCLUDES GRANTS IN AID)	150,700
AGGREGATE GRANT AND CONTRIBUTION PAYMENTS OF \$25,000 OR LESS: GRANTS IN AID	29,800
TOTAL GRANTS AND CONTRIBUTIONS	\$ 523,925

4. Reconciliation

Total of payments exceeding \$25,000 paid to suppliers	\$ 26,773,612
Total of payments of \$25,000 or less paid to suppliers	2,466,333
Total of grants and contributions	523,925
Subtotal	\$ 29,763,870
Differences:	
There are differences due to inconsistent reporting formats and accounting treatments required by Canadian Public Sector Accounting Standards for the Financial Statements versus the Financial Information Act for the Statement of Financial Information. For example:	
a) This schedule includes the employer portion of contributions to Work Safe BC, registered pension plans, extended health, and dental plans and other employee benefits that are not included in "Contracted Services, and Supplies and Other" in the Financial Statements Note 20.	(2,579,803)
b) This schedule includes the employer portion of Employment Insurance and Canada Pension Plan remittances paid to the Canada Revenue Agency that are not included in "Contracted Services, and Supplies and Other" in the Financial Statements Note 20.	(699,839)
c) This schedule does not include the items categorized as employee expenses and shown on the Statement of Remuneration. These items are included in "Contracted Services, and Supplies and Other" in the Financial Statements Note 20.	159,910
d) This schedule does not include the gain on disposal of tangible capital assets that is included in "Contracted Service, and Supplies and Other" in the Financial Statements.	31,399
e) This schedule is prepared on a cash basis versus the "Contracted Services, and Supplies and Other" amount from Note 20 in the Financial Statements which is prepared on an accrual accounting basis. This schedule also includes payments for the acquisition of tangible capital assets that are not included in "Contracted Services, and Supplies and Other" in the Financial Statements Note 20. There are also different accounting treatments for other items such as GST.	(6,677,422)
Total per Financial Statements Note 20, Contracted Services, and Supplies and Other	\$ 19,998,115
Variance	\$ -

Prepared in accordance with the Financial Information Regulation, Schedule 1, section 7 and the Financial Information Act, section 2.

THE CORPORATION OF THE CITY OF WHITE ROCK
STATEMENT OF FINANCIAL INFORMATION – FINANCIAL MANAGEMENT
REPORT
YEAR ENDED DECEMBER 31, 2021

The consolidated financial statements contained in this Statement of Financial Information under the *Financial Information Act* have been prepared by management in accordance with generally accepted accounting principles. The integrity and objectivity of the consolidated financial statements is management's responsibility. Management is also responsible for all the schedules prepared for the Statement of Financial Information, and for ensuring that the schedules are consistent, where appropriate, with the information contained in the consolidated financial statements.

Management is also responsible for implementing and maintaining a system of internal controls to provide reasonable assurance that reliable financial information is produced.

Council is responsible for ensuring that management fulfills its responsibilities for financial reporting and internal control.

The external auditors, KPMG LLP, conducted an independent examination, in accordance with generally accepted auditing standards, and expressed their opinion on the financial statements. Their examination did not relate to the other schedules and statements required by the Act. Their examination included a review and evaluation of the corporation's system of internal control and appropriate tests and procedures to provide reasonable assurance that the financial statements are presented fairly.



Candice Gartry, CPA, CGA
Director of Financial Services

July 18, 2021

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Jim Gordon P.Eng., Director, Engineering and Municipal Operations

SUBJECT: Integrated Transportation Infrastructure Master Plan

RECOMMENDATIONS

THAT Council:

1. Receive the report dated July 25, 2022, from the Director of Engineering and Municipal Operations, titled "Integrated Transportation infrastructure Master Plan" (ITIMP) and the July 2022 Integrated Transportation and Infrastructure Master Plan prepared and being presented by Urban Systems Ltd.
 2. Endorse the final ITIMP report subject to Council feedback on specific details at the September 19, 2022, Regular Council Meeting.
 3. Approve submission of the ITIMP report to the British Columbia Ministry of Transportation and Infrastructure as the fulfilment of the terms of the \$50K grant from the Ministry's Active Transportation Fund.
-

EXECUTIVE SUMMARY

This Corporate Report discusses the "Integrated Transportation & Infrastructure Master Plan (ITIMP)". The final ITIMP, dated July 2022 and attached as Appendix A, was prepared for the City of White Rock by Urban Systems and is the culmination of public consultation, collected data, resident feedback, staff consultation and technical evaluation. A short presentation to Council by Urban Systems outlining the process and final ITIMP will be made on July 25, 2022.

The ITIMP is a comprehensive multi-modal transportation master plan intended to guide the City's transportation investments, municipal infrastructure improvements, capital expenditures, and decision making over the next 25 years and beyond. Council's Strategic Plan provides the foundation for the plan which was further developed through consultation, resident feedback and best practices. The ITIMP is aligned with Metro Vancouver and TransLink long range plans and consistent with environmental and healthy living principles in reducing reliance on single occupancy vehicle trips.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
2020-050 February 10, 2020	<p><i>That Council:</i></p> <ol style="list-style-type: none"> 1. Receives for information the corporate report dated February 10, 2020 from the Director of Engineering and Municipal Operations Department titled “BC Active Transportation Network and Infrastructure Grants Application”; and 2. Approved grant application to the BC Active Transportation Infrastructure Grants Program for the Integrated Transportation Master Plan, the Royal Avenue and Johnston Road Improvements project and the in-ground lighting improvements project for the pedestrian Crosswalk on Johnston Road between North Bluff and Russell. <p style="text-align: right;"><u>Carried</u></p>

INTRODUCTION/BACKGROUND

In January 2020, the City engaged Urban System to develop the ITIMP. The goal was to provide a long range engineering planning document as a tool for future budgets and programs based on Council Strategic Priorities, resident feedback, best practices, and alignment with the plans of neighbouring jurisdictions. The ITIMP is a living document and intended to be flexible enough to change in concert with potential changes in the priorities of Council or residents.

The BC Active Transportation Fund approved a grant of \$50K for the ITIMP. In accordance with the agreement with BC Ministry of Transportation and Infrastructure, the original ITIMP completion date in order to comply with funding was no later than March 31, 2021. Due to the COVID-19 and staff changes, an extension has been approved to September 2022.

The ITIMP was developed over a five-phase process between 2020-2022. The ITIMP is consistent with environmental and healthy living principles and is intended to respond to how we live today and how we want to live in the future.

ANALYSIS/DISCUSSION

The ITIMP is composed of several interrelated layers that form the foundation for the plan’s long-term recommendations. It is designed to align with White Rock’s City Council’s Strategic Priorities. The plan outlines a vision, guiding principles, and a series of goals and objectives that guide all aspects of the City’s transportation network, emphasizing the safety, health, and well-being of all road users.

The ITIMP encourages walking as the first choice for short trips, cycling, and transit use for access to schools, local businesses, recreation facilities, and local and regional employment centers while also accommodating growth in vehicle traffic as required for economic growth and community development patterns. In addition, the ITIMP will ensure that transportation improvements coincide with other municipal infrastructure improvement projects including sewers, drainage, and water.

The ITIMP identifies six big moves that will make the greatest impact to achieving the vision and goals of the plan, as well as 13 strategies and 36 actions that will help achieve the vision, goals and objectives of the ITIMP.

The strategies and actions developed as part of the ITIMP are intended to guide White Rock's capital, operations maintenance, policy, and programming decisions as well as ongoing resource requirements over the next 25 years and beyond. While the plan has been developed as a long-term plan, it will require financial investment, staff resources, and an implementation strategy to prioritize improvements over the short, medium, and long term. An implementation plan was developed based on both technical evaluation results as the results of public input.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

In the development of the ITIMP, two phases of public engagement took place. The first phase of engagement in late 2020 and early 2021 focused on identifying issues, opportunities, and challenges for transportation in White Rock. The second phase of engagement focused on obtaining input on the draft long-term plan.

The second phase of engagement took place between September and November 2021 and included an online survey and three public open houses. Engagement materials were posted on the talkwhiterock.ca project website, including the draft plan, a project video, and summary infographics. The City also regularly promoted engagement through its social media channels. The results of the online survey and public open houses are used to refine and finalize the ITIMP.

ALIGNMENT WITH STRATEGIC PRIORITIES

The ITIMP is designed to align with City Council's strategic priorities.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The ITIMP along with Council Strategic Priorities will form the basis of long term capital plans developed by staff in Engineering and Municipal Operations and Financial Services.

CLIMATE CHANGE IMPLICATIONS

The development of infrastructure and programs to encourage walking, cycling and transit transportation modes reduces the climate change impacts of single occupancy vehicle usage – especially for short trips.

OPTIONS / RISKS / ALTERNATIVES

The following option is available for Council's consideration:

1. Not adopt the ITIMP; however, this would be a missed opportunity to set the framework for a more sustainable transportation direction.

CONCLUSION

This corporate report presents and transmits to Council the ITIMP prepared by Urban Systems Ltd. in July 2022. This ITIMP sets a new sustainable direction for transportation in White Rock by encouraging walking, cycling and transit as transportation modes. The plan builds upon Council's Strategic Priorities and resident feedback and will be used to inform the development of future transportation capital budgets. Staff seeks Council endorsement of the final ITIMP report, subject to Council feedback on specific details, at the September 19, 2022 Regular Council Meeting, and approval of the submission of the ITIMP report to the British Columbia

Ministry of Transportation and Infrastructure as the fulfilment of the terms of the \$50K grant from the Ministry's Active Transportation Fund.

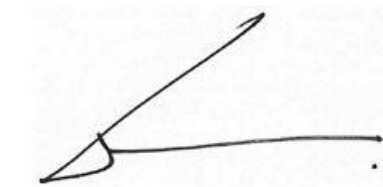
Respectfully submitted,

A handwritten signature in black ink, appearing to read 'Jim Gordon', written over a light blue grid background.

Jim Gordon
Director, Engineering and Municipal Operations

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, appearing to read 'Guillermo Ferrero', written over a light blue grid background.

Guillermo Ferrero
Chief Administrative Officer

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Jim Gordon, Director, Engineering & Municipal Operations
Eric Stepura, Director, Recreation & Culture
Amanda Silvers, Manager, Communications & Government Relations

SUBJECT: Interpretive Signage Design Concept for the Pier

RECOMMENDATION

THAT Council approve the recommended signage concept for an interpretive sign series to be installed on the Pier.

EXECUTIVE SUMMARY

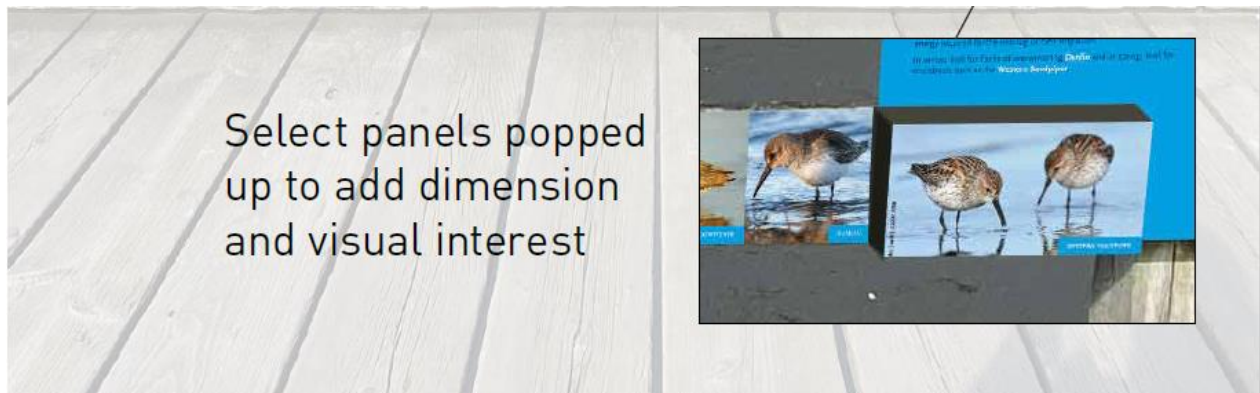
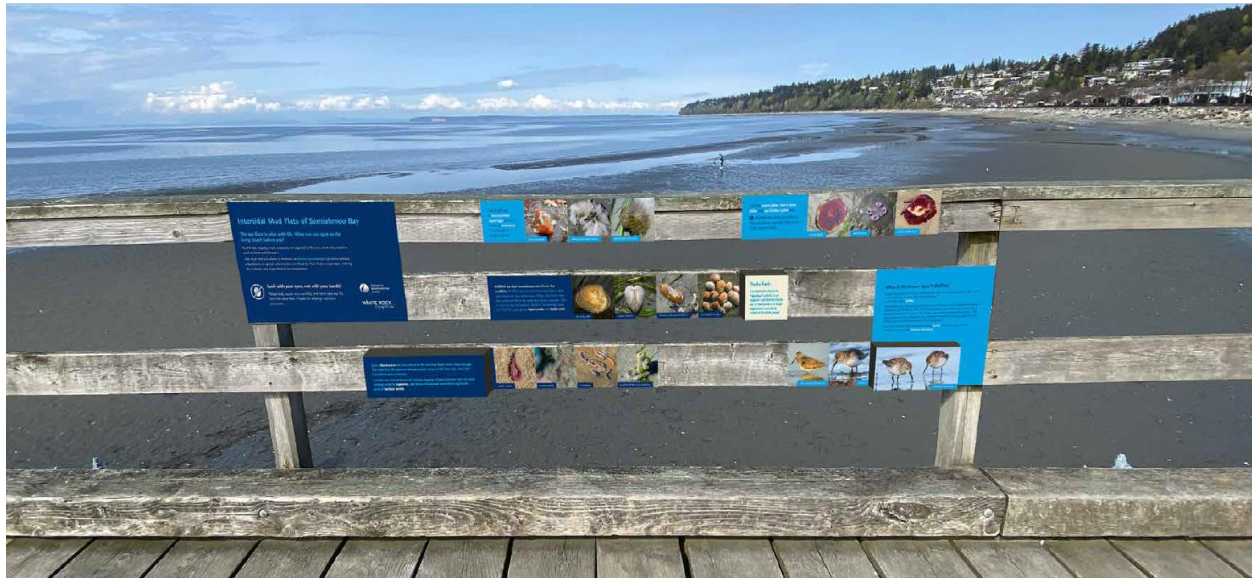
The city has been working with the Friends of Semiahmoo Bay Society on a design concept to produce a series of environmental interpretive signs for installation on the Pier (or along the Promenade). This sign series will be a redevelopment of the signs developed in 2012 by the Friends of the Semiahmoo Bay Society. The cost of design, production and installation of the signs will be covered by existing budget remaining in the Pier Restoration project.

The design mock-up included in this corporate report has been developed based on content from Communications staff working with the Friends of Semiahmoo Bay Society. The Friends of Semiahmoo Bay Society are satisfied with the proposed concept and will continue to work with Communications staff to develop the series should Council approve it. The intent of the mock-up is to present the visual concept only – the sign content is subject to change as the series is developed and finalized.

Recommended design concept: Pier Railing Signs

The following Pier Railing Sign design concept meets several priority design criteria:

- Cost-effective/affordable construction.
- Does not impede Pier views.
- Vibrant design with “popped up” panels for visual interest complements the Pier environment.
- Sign height is accessible for children and those in wheelchairs in addition to adults.
- Unobtrusive design is less inviting to graffiti/vandalism.
- Modular design means that damaged panels are easy and relatively inexpensive to replace on an as-needed basis.



PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
2019-359 September 13, 2019	THAT Council requests staff report on the feasibility of installing the existing eight interpretive signs at either the White Rock, the Pier or the future floating facility southwest of the Pier.

INTRODUCTION/BACKGROUND

The Pier is a beloved landmark and arguably the “crown jewel” of White Rock. Any signage installed on the Pier therefore should be designed mindfully with careful consideration of ensuring that the signs do not impede views or detract from the natural beauty of the Pier. In an ideal scenario, the design of the signs would serve to enhance the overall experience.

In 2012, Friends of Semiahmoo Bay Society, in partnership with the City of White Rock, designed a series of eight (8) metal interpretive signs featuring the flora and fauna of the ocean and shore environment. The intent of the signs was to educate residents and visitors about the plants and creatures that depend on the area for survival. This was meant both to peak readers’ interest and inspire them to be respectful of the sensitive environment. The signs were installed at the top of the stairs above Memorial Plaza near the clock.



Example of 2012 Sign Series

When Memorial Park was renovated in 2016 the signs were removed with assurance to Friends of Semiahmoo Bay Society that they would be re-installed when the renovation was complete. Following the renovation, City staff decided that the sign series would benefit from a visual refresh, and Council directed that the signs should be installed on the Pier rather than at the top of the stairs so as not to impede the views of patrons from the adjacent restaurants.

This spring, the Communications, Recreation & Culture, and Engineering Departments have worked with Friends of Semiahmoo Bay Society to develop a refreshed signage concept for the series to be installed on the Pier.

FINANCIAL IMPLICATIONS

The cost of producing the signs will be approximately \$700 per sign installation. The cost of this project is covered by budget from the Pier Restoration project.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

These interpretive signs will enhance the visitor experience by providing opportunity for greater understanding and engagement with the Pier environment. Visitors will be able to learn about, and ideally develop a deeper appreciation of the ocean, shore and surrounding environment and the importance of protecting it.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The Communications Department will oversee the design and production work, and work with Engineering & Municipal Operations staff for installation.

CLIMATE CHANGE IMPLICATIONS

The environmental content of this sign series will educate visitors of how to protect our oceans and shores.

ALIGNMENT WITH STRATEGIC PRIORITIES

This project directly supports Council's "Environment" pillar which seeks to improve our environmental stewardship to preserve and protect the natural environment for future generations.

OPTIONS / RISKS / ALTERNATIVES

Provided for information purposes are two additional design concepts that were developed and are not recommended based on the following:

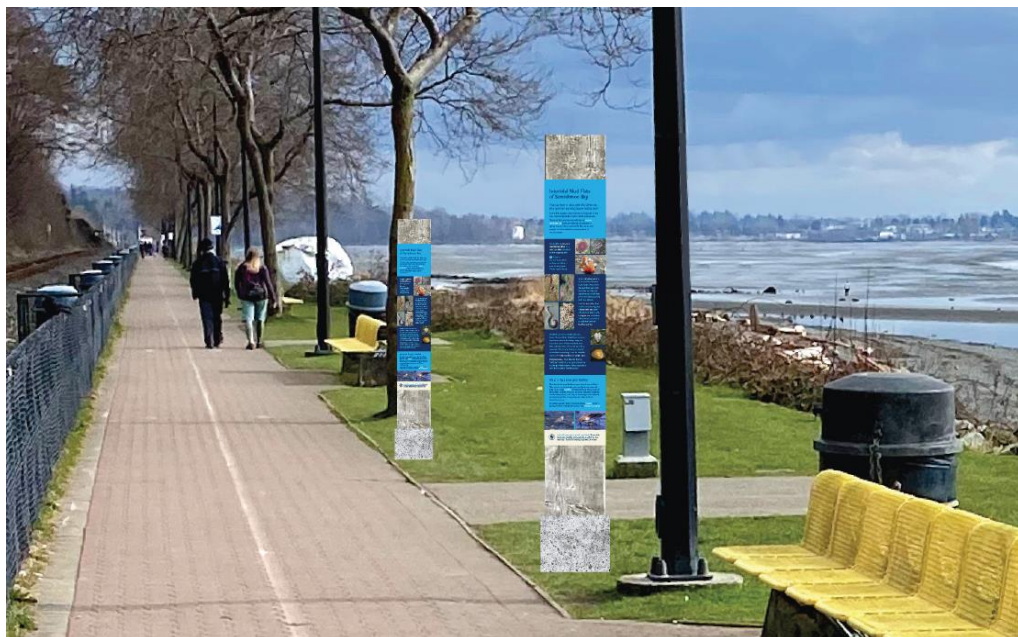
Design Alternative #1: Kiosks

- Expensive construction including the addition of concrete pads to avoid rotting.
- Vandalism replacement will be costly.
- Location along Promenade grass may sometimes be in the way of event activation.



Design Alternative #2: Vertical Planks

- Obtrusive design and location invite vandalism.
- Location along Promenade grass may impede event activation in these areas.



CONCLUSION

Visitors to the Pier will benefit from the added engagement value and education of mindfully designed interpretive signs. Staff seek Council's approval of the recommended signage design concept for the Pier railings as shown for the described interpretive sign series to be installed on the Pier.

Respectfully submitted,



Jim Gordon, Director
Engineering & Municipal
Operations



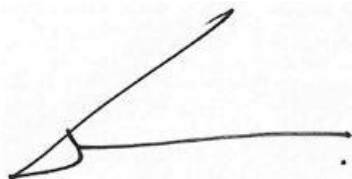
Eric Stepura, Director
Recreation & Culture



Amanda Silvers, Manager
Communications &
Government Relations

Comments from the Chief Administrative Officer

I concur with the recommendation of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Jim Gordon, P. Eng., Director, Engineering and Municipal Operations

SUBJECT: Progress Update on MF & ICI Solid Waste Transition

RECOMMENDATIONS

THAT Council:

1. Receive for information the corporate report dated July 25, 2022, from the Director of Engineering and Municipal Operations, titled "Progress Update on MF & ICI Solid Waste Transition;
 2. Direct staff continue preparing the RFP documents;
 3. Endorse the project schedule outlined in this Corporate Report including recommendation for awarding of the contract at Council's regular meeting on October 3, 2022.
-

EXECUTIVE SUMMARY

In response to direction from Council at the Regular Council meeting on October 4, 2021, staff has been working on the transition of individual MF & ICI properties into City-managed solid waste collection services. This report provides Council with a progress update on the RFP preparation and other transition details.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
2021-440 November 8, 2021	THAT Council give final reading "White Rock Financial Plan (2021-2025) Bylaw, 2021, No. 2377, Amendment No. 2, 2021, No. 2403". Bylaw 2403 – A bylaw to amend the Financial Plan for 2021 to 2025 to include: \$111K to the 2021 Financial Plan for a new regular full time Solid Waste Coordinator position starting in December 2021, \$8K to 2021 and \$103K to 2022, funded from Reserves;

	<p>\$50K to the 2021 Financial Plan for a consultant to assist with the Request for Proposal for a Solid Waste Contractor funded from Reserves; and</p> <p>Add \$650K for the Centre Street Walkway Project funded from CAC's.</p> <p style="text-align: right;">CARRIED</p>
2021-408 October 25, 2021	<p>THAT Council give final reading to "Collection, Removal, Disposal and Recycling of Solid Waste, 2015, Bylaw No. 2084, Amendment Bylaw, 2021, No. 2402".</p> <p style="text-align: right;">CARRIED</p>
2021-381 October 4, 2021	<p>THAT Council give first, second and third reading to "Collection, Removal, Disposal and Recycling of Solid Waste, 2015, Bylaw No. 2084, Amendment Bylaw, 2021, No 2402".</p>
2021-360 October 4, 2021	<p>THAT Council:</p> <ol style="list-style-type: none"> 1. Receive the October 4, 2021, corporate report from the Director of Engineering and Municipal Operations, titled "Solid Waste Bylaw Update;" 2. Approve the Regular full time Solid Waste Coordinator position in advance of adopting the 2022 Financial Plan to enable recruitment for this position in Fall 2021; 3. Direct staff to procure a contractor to provide solid waste collection services for multifamily (MF) and institutional, commercial, and industrial (ICI) properties, and user-pay bulky waste collection services for single family (SF), MF and (ICI); and 4. Consider approving the Collection, Removal, Disposal and Recycling of Solid Waste Bylaw which stipulates that MF and ICI properties can opt into the City managed collection services beginning January 30, 2023, and must transition by March 27, 2023, or the termination date of any private solid waste service agreement that was entered into prior to the Bylaw adoption date. <p style="text-align: right;">CARRIED</p>
2021-084 February 22, 2021	<p>THAT Council:</p> <ol style="list-style-type: none"> 1. Direct staff to conduct a financial analysis and review funding models that would enable the City to provide solid waste collection for Multi- Family (MF) and Institutional, Commercial and Industrial (ICI) buildings by a City managed contractor; and 2. Direct staff to obtain a legal opinion to determine if the City through bylaw can request private property owners to terminate contracts with private solid waste haulers. <p style="text-align: right;">CARRIED</p>

2020-490 October 5, 2020	<p>THAT Council:</p> <ol style="list-style-type: none"> 1. Direct staff to prepare a 2020 Financial Plan Amendment that would include a \$985K adjustment for the purchase of the three solid waste vehicles discussed in this report; and 2. Approve the purchase of the three solid waste collection vehicles in the amount of \$1.131M (excluding GST) upon adoption of the 2020 Financial Plan Amendment Bylaw. <p style="text-align: right;">CARRIED</p>
2020-394 July 27, 2020	<p>THAT Council:</p> <ol style="list-style-type: none"> 1. Approves the purchase of two (2) recycling replacement vehicles from the 2020 Financial Plan in the amount of \$820K (excluding GST) from Rollins Machinery Ltd; and 2. Approves an increase of \$304K for the purchase of the two recommended vehicles that would be reallocated from funding that was to have been for the purchase of the three (3) residential collection vehicles. <p style="text-align: right;">CARRIED</p>
2018-395 November 19, 2018	<p>THAT Council receives for information the corporate report dated November 19, 2018 from the Director of Engineering and Municipal Operations titled "Solid Waste Management: Current Status of the Program".</p> <p style="text-align: right;">CARRIED</p>
2015-114 April 10, 2015	<p>THAT Council endorses option three (3) as outlined in April 10, 2015 corporate report titled "Multi-Family Solid Waste Collection Additional Information" endorsing the City "Provide technical assistance to transition to private collection for multi-family properties."</p> <p style="text-align: right;">CARRIED</p>

INTRODUCTION

The updated Solid Waste Bylaw approved by Council on October 4, 2021, provides the legal basis for the transition of multifamily (MF) & institutional, commercial and industrial (ICI) solid waste collection services into city managed services starting January 31, 2023. This transition to city managed contract services is a significant paradigm shift for MF and ICI properties.

The collection of detailed data from the 350 MF and ICI properties has been a large and fundamentally important project. Individual contract end dates, numbers and configurations of collection containers, locations etc. form the base for the RFP and resultant contract. The dataset is the foundation for defining the scope of services, pricing base for the contractor, and billing base for the city.

The critical work of the transition is to retain a contractor to take over the individual collection services. The procurement of solid waste services through a public competitive process is complex as it requires comprehensive knowledge of industry practices and expertise in solid waste management to detail the scope of work and to structure the billing mechanism. Dillon

Consulting was retained as a consultant in February 2022 to assist the City in preparing an RFP to procure the solid waste collection services.

To date, the pre-transition work has been completed including the following:

- Communication to MF & ICI property owners on the transition and the timeline
- Engaging MF & ICI property owners in data survey/input
- Preliminary data collection for 331 properties of MF and ICI
- Research and recommendation
- Industry expert consultation and hauler consultation and feedback
- Ownership of collection containers
- MF recycle stream composition - mixed single stream or separated multi-stream
- Discussion with Recycle BC on MF recycling program incentives
- Identifying the property number of six units and less currently serviced by the city staff for the transition into city managed contracted services

Right now, staff and the consultant are focusing on RFP preparation and finalization including:

- Investigating and validating dataset
- Formatting RFP
- Drafting Service Agreement
- Defining and detailing Scope of Work including MF, ICI, Recycle BC, and bulky waste collection
- Formatting proponent's technical proposal
- Formatting proponent's financial proposal

The RFP timeline is proposed below:

1. RFP to be issued on BC bid August 12, 2022
2. RFP Closure September 9, 2022
3. Evaluation and Selection September 29, 2022
4. Council Award of Contract October 3, 2022

ANALYSIS/DISCUSSION

The updated Solid Waste Bylaw (Part 9) stipulates that MF & ICI properties transition into city managed collection service after January 30, 2023 and pay the city the solid waste collection service fee upon commencing using the city's collection services or after January 1, 2024. The preliminary data collection found some properties, approximately 10% of the total MF & ICI properties, having a service contract end beyond January 1, 2024. During the communication with properties on the transitions, this portion of property owners expressed concerns about double payments: paying the contractor as per the service agreement and paying the city as per the bylaw requirements.

There are options for city Council consideration to resolve the potential double billing situation. These include:

- The contractor may release the property from the contract as a sign of corporate goodwill;
- The contractor may be selected as the best value provider for the city during the RFP procurement process, awarded the contract, and as such releases the property from the contract;

- The city may initiate Business License or other bylaw changes ending the contractor's operation in the city; and
- City Council may agree to grandfather the properties with longer-term contracts (this may be possible if there are not too many of these situations and provided that the contracts were entered into before bylaw adoption last Fall).

Because of the complexity of the transition and the issue with potential double billing, it is proposed that the rollout of the transition will be done in stages.

ALIGNMENT WITH STRATEGIC PRIORITIES

The MF & ICI solid waste transition is consistent with Council's Strategic Priorities.

CONCLUSION

Staff and the consultant will continue RFP preparation and aim for the release on BC Bid on August 12, 2022. The Council award contract to the successful proponent is scheduled for October 3, 2022. Staff seek Council's support to continue with the process in preparing the RFP documents and endorse the project schedule outline as described, including the recommendation for awarding the contract at Council's regular meeting on October 3, 2022.

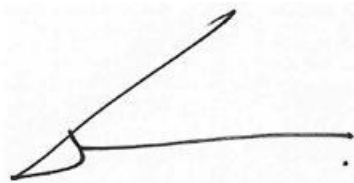
Respectfully submitted,



Jim Gordon
Director, Engineering and Municipal Operations

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Candice Gartry, Director, Financial Services

SUBJECT: 2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436

RECOMMENDATION

THAT Council receive the July 25, 2022, corporate report from the Director of Financial Services, titled "2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436," endorse the proposed five (5) year bylaw, and grant applications for exemption, to be given three readings and after the public notice requirements have been met, for adoption.

EXECUTIVE SUMMARY

This corporate report introduces the "2023 – 2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436" to Council for approval and adoption.

INTRODUCTION/BACKGROUND

The City has the authority to grant permissive property tax exemptions for non-profit organizations under conditions identified in the *Community Charter*, Section 224. Council has provided further direction for eligibility requirements in the White Rock City Council Policy No. 317 – Municipal Property Tax Exemptions (Appendix A).

Eligible Properties

Council Policy No. 317 – Municipal Property Tax Exemptions provides for consideration of permissive tax exemption applications for Not-for-Profit Organizations under the following section:

Section II: Types of Properties Exempted

- f) Property owned by organizations whose principal purpose is to directly support Peace Arch Hospital's provision of health and wellness services to the citizens of White Rock;*
- g) Property owned by charitable, philanthropic or other not-for-profit organizations whose principal purpose is delivery of social services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community; and*
- h) Property owned by not-for-profit organizations whose principal purpose is delivery of cultural services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community.*

The city received the following applications for exemption for 2023 that are eligible under this section, all of which were granted the exemption in prior years:

- Peace Arch Hospital Auxiliary Society;
- Sources Community Resources Society;
- White Rock Players' Club;
- Peace Arch Hospital and Community Health Foundation; and
- Options Community Services Society.

Previously the city has granted annual permissive tax exemptions on these properties. The same properties have been exempted for many years (except for 2018 when Options missed the deadline for application). The Community Charter and Council Policy No. 317 allow permissive tax exemptions to be granted for up to ten (10) years. Staff recommend that a five (5) year permissive tax exemption be granted for these organizations, (2023 – 2027), to reduce administration of the permissive exemption process for both the City and these organizations.

Annually, each year before the June 30, permissive tax exemption application deadline, staff will request confirmation that the use of these properties have not changed. If required, amendments to the five year bylaw will be presented to Council before the October 31 deadline for permissive tax exemption bylaw adoption.

FINANCIAL IMPLICATIONS

The eligible applicants received municipal tax exemptions totaling approximately \$85K in 2022. The extension of these permissive tax exemption has been included in the 2022 – 2026 Financial Plan and is expected to be extended into 2027.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

In advance of the bylaw being adopted, public notice must be given in accordance with Sections 227 and 94 of the *Community Charter* regardless of whether Council endorses the proposed five (5) year bylaw, 2023 – 2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436 or, the alternate one (1) year bylaw, 2023 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436. Communication to the public will be done through the assistance of the Communications Department.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

Communications Department staff assistance will be needed to provide public notice of the bylaw in advance of it being adopted.

ALIGNMENT WITH STRATEGIC PRIORITIES

Endorsement of the proposed five (5) year bylaw, 2023 – 2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436 aligns with the Community strategic objective number two (2) *Manage the delivery of City services efficiently and effectively.*

OPTIONS / RISKS / ALTERNATIVES

Staff have prepared two versions of the Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436, one is for the recommended five (5) years, the second is a one (1) year bylaw, which is

what council has approved in the past. The following options are available for Council's consideration:

1. Endorse the proposed five (5) year bylaw, 2023 – 2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436 or,
2. Endorse the alternate one (1) year bylaw, 2023 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436 or;
3. Not grant any permissive exemptions for the Not for Profit organizations.

CONCLUSION

In order to be effective for the 2023 taxation year, permissive tax exemption bylaws must be adopted by City Council before October 31, 2022. Before these bylaws can be adopted, public notice must be given in accordance with sections 227 and 94 of the *Community Charter*.

It is recommended that Council endorse the proposed five year bylaw, the "2023 – 2027 Not for Profit Permissive Tax Exemptions Bylaw, 2022, No. 2436" to be given three readings and, after the public notice requirements have been met, be adopted.

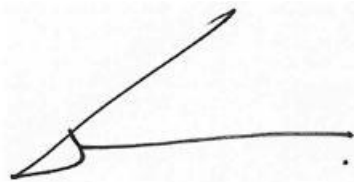
Respectfully submitted,



Candice Gartry
Director, Financial Services

Comments from the Chief Administrative Officer

I concur with the recommendation of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

Appendix A: Council Policy No. 317 – Municipal Property Tax Exemptions

THE CORPORATION OF THE
CITY OF WHITE ROCK
15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



POLICY TITLE: MUNICIPAL PROPERTY TAX EXEMPTIONS

POLICY NUMBER: FINANCE - 317

<i>Date of Council Adoption: September 28, 2015</i>	<i>Date of Last Amendment: June 24, 2019</i>
<i>Council Resolution Number: 2007-410; 2013-346, 2015-336, 2019-233, 2019-264</i>	
<i>Originating Department: Finance</i>	<i>Date last reviewed by Governance and Legislation: June 10, 2019</i>

Policy:

I: General

Section 224 of the *Community Charter* authorizes municipal Councils to provide a permissive tax exemption for properties and facilities owned by non-profit organizations providing services that Council considers to be a benefit to the community and directly related to the purposes of the organization. A municipal property tax exemption is a means for Council to support community organizations that further Council's objectives of enhancing quality of life (social, economic, cultural) and delivering services economically. Section 224 also authorizes tax exemptions for other properties, such as land and improvements owned or held by a municipality and certain land or improvements surrounding places of worship.

In this Policy, Council recognizes the presence of non-profit organizations in White Rock that are providing a beneficial service to the community. To assist these organizations, it is reasonable that they be eligible to apply for a permissive exemption of municipal property taxes for a specified period of time. Permissive tax exemptions do not apply to sanitary sewer, drainage, solid waste and other user utility fees, which will still be payable by the organization.

II: Types of Property Exempted

In order for an organization's property to be eligible for consideration of a Permissive Tax Exemption, it must align with one of the following categories:

- (a) The land surrounding the buildings of places of worship which have been statutorily exempt from paying property taxes, in accordance with Section 220 of the *Community Charter*

- (b) Halls that Council considers necessary to buildings of places of worship which have been exempt from property taxes in accordance with Section 220 of the *Community Charter*
- (c) Land or improvements used or occupied by a religious organization, as a tenant or licensee, for the purpose of public worship or for the purposes of a hall that Council considers is necessary to land or improvements so used or occupied
- (d) BNSF City Leased property
- (e) City properties leased to not-for-profit organizations that
 - (i) are providing a community service not currently available through the City; and
 - (ii) have not previously paid property taxes on the City property in question.
- (f) Property owned by organizations whose principal purpose is to directly support Peace Arch Hospital's provision of health and wellness services to citizens of White Rock
- (g) Property owned by a charitable, philanthropic or other not-for-profit organizations whose principal purpose is delivery of social services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community
- (h) Property owned by not-for-profit organizations whose principal purpose is delivery of cultural services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community.

Criteria for Ineligibility

Organizations that fall into the following categories shall not be eligible for a permissive tax exemption:

- (a) Organizations providing, or associated with, housing
- (b) Service clubs

III: Process

Council will consider applications for permissive tax exemptions annually. The opportunity to apply will be advertised two times in the local newspaper and letters to this effect will be mailed to recipients designated in the preceding tax year.

Applications must be submitted to the Director of Financial Services, using a prescribed application form, before June 30 of each year. The Director will review the applications for completeness and arrange contact with applicants for additional information as necessary.

Application submissions must include:

- Copy of audited financial statements for the last 3 years. If not available, consideration will be given to applicants that submit financial statements which have

been reviewed by a certified accountant along with Canada Revenue Agency information returns containing relevant financial information.

- Copy of state of title certificate or lease agreement, as applicable
- Description of programs/services/benefits delivered from the subject lands/improvements including participant numbers, volunteer hours, groups benefited, fees charged for participation
- Description of any third-party use of the subject land/improvements including user group names, fees charged, and conditions of use
- Demonstrated legal status as a registered charity or other not-for-profit organization
- Written confirmation that any and all revenue generated from any of the properties being exempted, even if only on a temporary basis, is being dedicated to the programs and/or service delivery of the not-for-profit organization.

The Director of Financial Services will present a summary report of the applications, relative to the eligibility criteria, to Council and arrange for delegations to Council by applicants as necessary.

IV: Duration of Exemption

Eligible organizations may be considered for tax exemptions exceeding one year where it is demonstrated that the services/benefits they offer to the community are of a duration exceeding one year. Council will establish the term of the exemption in the bylaw authorizing the tax exemption. No exemption shall exceed a period of 10 consecutive years.

V: Conditions

Under Section 224 of the *Community Charter*, Council may impose conditions on land/improvements that are exempted under this Policy. These conditions will be specified in each bylaw authorizing the exemption. The conditions may include but are not limited to:

- Registration of a restrictive covenant on the property
- An agreement committing the organization to continue a specific service or program
- An agreement committing the organization to immediately disclose any substantial increase in the organization's revenue or anticipated revenue

Section 224 of the *Community Charter* stipulates that a tax exemption bylaw under this section ceases to apply when the use or ownership of the property in question no longer conforms to the conditions necessary to qualify for exemption. After this, the property will be liable to taxation.

Penalties

Council may impose penalties on an exempted organization for knowingly breaching conditions of exemption, including but not limited to:

- (a) Revoking exemption with notice
- (b) Disqualifying any future application for exemption for specific time period
- (c) Requiring repayment of monies equal to the foregone tax revenue

THE CORPORATION OF THE CITY OF WHITE ROCK BYLAW 2436



A Bylaw to provide an exemption from municipal property taxes
under section 224 of the *Community Charter*.

Council may, by bylaw, exempt properties from taxation for a fixed period of time pursuant to section 224 of the *Community Charter*, S.B.C. 2003. c. 26.

The following social service organizations have made application for permissive tax exemption grants for said lands with improvements listed below in the City of White Rock, the said lands being legally described as:

Options Community Services Society	Parcel Identifier: 000-452-505 Lot 1, Block 21, Section 11, Township 1, New Westminster Land District, Plan NWP11178
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 005-113-521 Lot 12, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-941 Lot 13, Block N 1/2 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 009-218-173 Lot 33, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP24976
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 001-829-653 Lot 16, Block 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-967 Lot 17, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital Auxiliary Society	Parcel Identifier: 012-364-096 Lot 7, Block 5, Part NE ¼, Section 10, Township 1, New Westminster Land District, Plan NWP1864

Sources Community Resources Society	Parcel Identifier: 011-232-099 Lot “B”, Block 7, Part E ½, Section 11, Township 1, New Westminster Land District, Plan NWP7459
White Rock Players’ Club	Parcel Identifier: 011-306-599 Lot 3 Except: West 7 Feet, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP8437

(the “Lands”)

Pursuant to section 224(2)(a) of the *Community Charter*, the City of White Rock wishes to grant a five (5) year municipal property tax exemption in respect of the lands with improvements.

The CITY COUNCIL of the Corporation of the City of White Rock, in open meeting assembled,
ENACTS as follows:

1. The Lands and improvements thereon are hereby exempt from municipal taxation for the calendar years 2023 to 2027, providing that the use or ownership of the property does not change.
2. Annually before June 30th, the above not-for-profit organizations, noted as the Lands, must inform the City of White Rock in writing of any changes to use or ownership of the property. The City of White Rock will require repayment of monies equal to the foregone tax revenue if the above social service organizations, noted as the Lands, fail to disclose changes in use or ownership, before June 30th, that breach the conditions of this exemption.
3. This bylaw may be cited for all purposes as the “2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436”.

RECEIVED FIRST READING on the day of
RECEIVED SECOND READING on the day of
RECEIVED THIRD READING on the day of
ADOPTED on the day of

MAYOR

CITY CLERK

THE CORPORATION OF THE CITY OF WHITE ROCK BYLAW 2436



A Bylaw to provide an exemption from municipal property taxes
under section 224 of the *Community Charter*.

Council may, by bylaw, exempt properties from taxation for a fixed period of time pursuant to section 224 of the *Community Charter*, S.B.C. 2003. c. 26.

The following social service organizations have made application for permissive tax exemption grants for said lands with improvements listed below in the City of White Rock, the said lands being legally described as:

Options Community Services Society	Parcel Identifier: 000-452-505 Lot 1, Block 21, Section 11, Township 1, New Westminster Land District, Plan NWP11178
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 005-113-521 Lot 12, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-941 Lot 13, Block N 1/2 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 009-218-173 Lot 33, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP24976
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 001-829-653 Lot 16, Block 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-967 Lot 17, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital Auxiliary Society	Parcel Identifier: 012-364-096 Lot 7, Block 5, Part NE ¼, Section 10, Township 1, New Westminster Land District, Plan NWP1864

Sources Community Resources Society	Parcel Identifier: 011-232-099 Lot “B”, Block 7, Part E ½, Section 11, Township 1, New Westminster Land District, Plan NWP7459
White Rock Players’ Club	Parcel Identifier: 011-306-599 Lot 3 Except: West 7 Feet, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP8437

(the “Lands”)

Pursuant to section 224(2)(a) of the *Community Charter*, the City of White Rock wishes to grant a one (1) year municipal property tax exemption in respect of the lands with improvements.

The CITY COUNCIL of the Corporation of the City of White Rock, in open meeting assembled, ENACTS as follows:

1. The Lands and improvements thereon are hereby exempt from municipal taxation for the calendar year 2023.
2. This bylaw may be cited for all purposes as the “*2023 Annual Permissive Tax Exemptions Bylaw 2022, No. 2436*”.

RECEIVED FIRST READING on the	day of
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RECEIVED THIRD READING on the	day of
ADOPTED on the	day of

MAYOR

CITY CLERK

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Candice Gartry, Director, Financial Services

SUBJECT: 2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437

RECOMMENDATION

THAT Council receive the July 25, 2022, corporate report from the Director of Financial Services, titled “2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437” for approval and subsequent three readings, and following public notice requirements being met, adoption.

EXECUTIVE SUMMARY

This corporate report introduces the 2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437 to Council for approval and adoption.

INTRODUCTION/BACKGROUND

The city has the authority to grant permissive property tax exemptions for not-for-profit organizations under conditions identified in the *Community Charter* Section 224. Council has provided further direction for eligibility requirements in the White Rock City Council Policy No. 317 – Municipal Property Tax Exemptions (Appendix A).

Eligible Properties:

Council Policy No. 317 – Municipal Property Tax Exemptions provides for consideration of permissive tax exemption applications for Not-for-Profit Organizations under the following section:

Section II: Types of Properties Exempted

- e) City properties leased to not-for-profit organizations that
 - (i) are providing a community service not currently available through the City; and
 - (ii) have not previously paid property taxes on the City property in question.

The city has previously leased a portion of the Horst and Emmy Werner Centre for Active Living (“CAL”), located at 1475 Anderson Street, to the White Rock/South Surrey Division of Family Practice Society, the Alzheimer Society of BC and the White Rock South Surrey Stroke Recovery Association.

The White Rock/South Surrey Division of Family Practice Society lease will expire in September 2022. The city is in negotiations to renew this lease. The lease for the White Rock South Surrey Stroke Recovery Association expires on December 31, 2025. The lease for the Alzheimer Society of BC expired in July of 2022 and they will not be renewing their lease. However, the city is in negotiations with the Fraser Health Authority and the White Rock/South Surrey Division of Family Practice Society to lease this space. It is expected that one of these parties will enter into a lease for the space previously occupied by the Alzheimer Society of BC.

Previously the city has granted permissive tax exemptions on this property. The *Community Charter* and Council Policy No. 317 allow permissive tax exemptions to be granted for up to ten (10) years. Staff recommend that a one (1) year permissive tax exemption be granted for 2023. The permissive exemptions for White Rock/South Surrey Division of Family Practice Society and the Fraser Health Authority are contingent upon successful negotiation of these leases.

FINANCIAL IMPLICATIONS

The three organization's municipal tax exemptions totaled approximately \$2.1K in 2022. The extension of these permissive tax exemption has been included in the 2022 – 2026 Financial Plan.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

In advance of the bylaw being adopted, public notice must be given in accordance with Sections 227 and 94 of the *Community Charter*. Communication to the public will be done through the assistance of the Communications Department.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

Communications Department assistance will be needed to provide public notice of the bylaw in advance of it being adopted.

CONCLUSION

In order to be effective for the 2023 taxation year, permissive tax exemption bylaws must be adopted by City Council before October 31, 2022. Before these bylaws can be adopted, public notice must be given in accordance with Sections 227 and 94 of the *Community Charter*.

It is recommended that the 2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437 be given three readings and, after the public notice requirements have been met, be adopted.

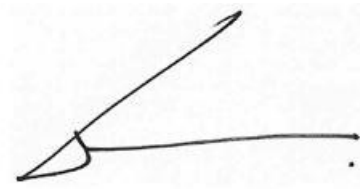
Respectfully submitted,



Candice Gartry
Director, Financial Services

Comments from the Chief Administrative Officer

I concur with the recommendation of this corporate report.

A handwritten signature in black ink, consisting of a stylized 'G' followed by a horizontal line and a small dot at the end.

Guillermo Ferrero
Chief Administrative Officer

Appendix A: Council Policy No. 317 – Municipal Property Tax Exemptions

THE CORPORATION OF THE
CITY OF WHITE ROCK
15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



POLICY TITLE: MUNICIPAL PROPERTY TAX EXEMPTIONS

POLICY NUMBER: FINANCE - 317

<i>Date of Council Adoption: September 28, 2015</i>	<i>Date of Last Amendment: June 24, 2019</i>
<i>Council Resolution Number: 2007-410; 2013-346, 2015-336, 2019-233, 2019-264</i>	
<i>Originating Department: Finance</i>	<i>Date last reviewed by Governance and Legislation: June 10, 2019</i>

Policy:

I: General

Section 224 of the *Community Charter* authorizes municipal Councils to provide a permissive tax exemption for properties and facilities owned by non-profit organizations providing services that Council considers to be a benefit to the community and directly related to the purposes of the organization. A municipal property tax exemption is a means for Council to support community organizations that further Council's objectives of enhancing quality of life (social, economic, cultural) and delivering services economically. Section 224 also authorizes tax exemptions for other properties, such as land and improvements owned or held by a municipality and certain land or improvements surrounding places of worship.

In this Policy, Council recognizes the presence of non-profit organizations in White Rock that are providing a beneficial service to the community. To assist these organizations, it is reasonable that they be eligible to apply for a permissive exemption of municipal property taxes for a specified period of time. Permissive tax exemptions do not apply to sanitary sewer, drainage, solid waste and other user utility fees, which will still be payable by the organization.

II: Types of Property Exempted

In order for an organization's property to be eligible for consideration of a Permissive Tax Exemption, it must align with one of the following categories:

- (a) The land surrounding the buildings of places of worship which have been statutorily exempt from paying property taxes, in accordance with Section 220 of the *Community Charter*

- (b) Halls that Council considers necessary to buildings of places of worship which have been exempt from property taxes in accordance with Section 220 of the *Community Charter*
- (c) Land or improvements used or occupied by a religious organization, as a tenant or licensee, for the purpose of public worship or for the purposes of a hall that Council considers is necessary to land or improvements so used or occupied
- (d) BNSF City Leased property
- (e) City properties leased to not-for-profit organizations that
 - (i) are providing a community service not currently available through the City; and
 - (ii) have not previously paid property taxes on the City property in question.
- (f) Property owned by organizations whose principal purpose is to directly support Peace Arch Hospital's provision of health and wellness services to citizens of White Rock
- (g) Property owned by a charitable, philanthropic or other not-for-profit organizations whose principal purpose is delivery of social services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community
- (h) Property owned by not-for-profit organizations whose principal purpose is delivery of cultural services to citizens of White Rock, provided that the property is being used for that purpose and it provides a beneficial service to the Community.

Criteria for Ineligibility

Organizations that fall into the following categories shall not be eligible for a permissive tax exemption:

- (a) Organizations providing, or associated with, housing
- (b) Service clubs

III: Process

Council will consider applications for permissive tax exemptions annually. The opportunity to apply will be advertised two times in the local newspaper and letters to this effect will be mailed to recipients designated in the preceding tax year.

Applications must be submitted to the Director of Financial Services, using a prescribed application form, before June 30 of each year. The Director will review the applications for completeness and arrange contact with applicants for additional information as necessary.

Application submissions must include:

- Copy of audited financial statements for the last 3 years. If not available, consideration will be given to applicants that submit financial statements which have

been reviewed by a certified accountant along with Canada Revenue Agency information returns containing relevant financial information.

- Copy of state of title certificate or lease agreement, as applicable
- Description of programs/services/benefits delivered from the subject lands/improvements including participant numbers, volunteer hours, groups benefited, fees charged for participation
- Description of any third-party use of the subject land/improvements including user group names, fees charged, and conditions of use
- Demonstrated legal status as a registered charity or other not-for-profit organization
- Written confirmation that any and all revenue generated from any of the properties being exempted, even if only on a temporary basis, is being dedicated to the programs and/or service delivery of the not-for-profit organization.

The Director of Financial Services will present a summary report of the applications, relative to the eligibility criteria, to Council and arrange for delegations to Council by applicants as necessary.

IV: Duration of Exemption

Eligible organizations may be considered for tax exemptions exceeding one year where it is demonstrated that the services/benefits they offer to the community are of a duration exceeding one year. Council will establish the term of the exemption in the bylaw authorizing the tax exemption. No exemption shall exceed a period of 10 consecutive years.

V: Conditions

Under Section 224 of the *Community Charter*, Council may impose conditions on land/improvements that are exempted under this Policy. These conditions will be specified in each bylaw authorizing the exemption. The conditions may include but are not limited to:

- Registration of a restrictive covenant on the property
- An agreement committing the organization to continue a specific service or program
- An agreement committing the organization to immediately disclose any substantial increase in the organization's revenue or anticipated revenue

Section 224 of the *Community Charter* stipulates that a tax exemption bylaw under this section ceases to apply when the use or ownership of the property in question no longer conforms to the conditions necessary to qualify for exemption. After this, the property will be liable to taxation.

Penalties

Council may impose penalties on an exempted organization for knowingly breaching conditions of exemption, including but not limited to:

- (a) Revoking exemption with notice
- (b) Disqualifying any future application for exemption for specific time period
- (c) Requiring repayment of monies equal to the foregone tax revenue

**THE CORPORATION OF THE
CITY OF WHITE ROCK
BYLAW 2437**



A Bylaw to provide an exemption from municipal property taxes
under section 224 of the *Community Charter*.

Council may, by bylaw, exempt properties from taxation for a fixed period of time pursuant to section 224 of the *Community Charter*, S.B.C. 2003. c. 26.

Various social service agencies have leased certain parts of the facility known as the Horst and Emmy Werner Centre for Active Living (“CAL”), located at 1475 Anderson St, from the Corporation of the City of White Rock (“the City”). The CAL is legally described as:

Parcel Identifier: 008-523-444
Lot 97, Section 10, Township 1
New Westminster District, Plan 38518

(the “Lands”)

Pursuant to section 224 of the *Community Charter*, the City of White Rock wishes to grant a one (1) year municipal property tax exemption in respect of the leased property with improvements.

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled, ENACTS as follows:

1. Each of the Premises described in the leases as particularized in section 3 are hereby exempt from municipal taxation for the calendar year 2023.
2. The Conditions imposed on each tenant are as outlined in each of their leases of their respective Premises and are conditions precedent to the municipal tax exemption granted by this bylaw.
3. The tenants and Premises that are exempt from taxation are:
 - (a) White Rock South Surrey Stroke Recovery Association (Premises shown in green on Schedule A);
 - (b) Fraser Health Authority or the White Rock/South Surrey Division of Family Practice Society (Premises shown in blue on Schedule A), depending on which organization leases this space; and
 - (c) White Rock/South Surrey Division of Family Practice Society (Premises shown in yellow on Schedule A).

Schedule A is attached to and forms part of this bylaw.

The White Rock South Surrey Stroke Recovery Association has a lease until 2025, the City is in the process of negotiating the White Rock/South Surrey Division of Family Practice Society lease for its existing space. The City anticipates leasing the space previously occupied by the Alzheimer Society of BC (Premises shown in blue on Schedule A) to the

Fraser Health Authority or the White Rock/South Surrey Division of Family Practice Society. The permissive exemption for these organizations is contingent upon successful negotiation of the leases.

4. This bylaw may be cited for all purposes as the “*2023 Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437*”.

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RECEIVED THIRD READING on the	day of
ADOPTED on the	day of

MAYOR

CITY CLERK

Schedule A



THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Eric Stepura, Director, Recreation and Culture

SUBJECT: BIA Request for Beer Gardens at TD Concerts at the Pier

RECOMMENDATIONS

THAT Council approve the request from the White Rock Business Improvement Association (BIA) to hold “beer and wine” gardens hosted by the White Rock Beach Beer Company at two TD Concerts at the Pier performances on:

- Thursday, August 18, 2022 from 6:00 p.m. – 10:00 p.m. at Five Corners (Johnston Road and Pacific Avenue), and
- Thursday, August 25, 2022 from 6:00 p.m. – 10:00 p.m. at the East Beach parking lot east of Grand Chief Bernard Robert Charles Memorial Plaza).

EXECUTIVE SUMMARY

In 2018, Council approved beer and wine gardens being held at the TD Concerts at the Pier performances at Five Corners and East Beach. The beer gardens were operated by the Rotary Club of White Rock as a fundraiser for their club. Part of the proceeds went to offsetting the cost of the TD Concerts at the Pier expenses.

There were no TD Concerts at the Pier held in 2020 or 2021 due to the Provincial Health Order restrictions on the size of group gatherings in response to the COVID-19 pandemic.

For 2022, the BIA Executive Director approached the Rotary Club of White Rock to organize beer and wine gardens at TD Concerts at the Pier again this year, but the club was not interested.

After a search for another beer garden operator, White Rock Beach Beer Company agreed to organize beer and wine gardens at two TD Concerts at the Pier performances on August 18 and 25, 2022.

This corporate report will outline the City policy (#704 attached as Appendix A) regarding beer gardens, and the conditions that must be met for beer and wine gardens to be held on City owned or leased property.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
Recreation and Culture Policy-704 July 27, 2015	Beer Garden/Public Liquor Licences Recreation and Culture Policy 704 outlines the guidelines for beer gardens held on City owned or leased property.

INTRODUCTION/BACKGROUND

On July 15, 2022, staff received a letter from BIA Executive Director, Alex Nixon (Appendix B) requesting permission to hold beer and wine gardens hosted by the White Rock Beach Beer Company at two TD Concerts at the Pier performance on Thursday, August 18, 2022 from 6:00 p.m. – 10:00 p.m. at Five Corners (Johnston Road and Pacific Avenue) and at the TD Concerts at the Pier performance on Thursday, August 25, 2022 from 6:00 p.m. – 10:00 p.m. at East Beach (parking lot east of Grand Chief Bernard Robert Charles Memorial Plaza).

In previous years, beer and wine gardens have added a festive atmosphere, and attract more spectators to the concerts.

Staff discussed event details with BIA Executive Director, Alex Nixon and he agreed to meet the following conditions:

- The beer/wine garden area is limited to 200 guests plus up to ten (10) servers and volunteers and security personnel.
- The August 18, 2022 beer/wine garden at Five Corners will be located on the section of Johnston Road between Pacific Avenue and Beachview Avenue, and the adjacent roads will be closed for the events.
- The August 25, 2022 beer/wine garden will be held at the west end of the parking lot at East Beach which is located just east of Grand Chief Bernard Robert Charles Memorial Plaza.
- The beer/wine garden area will be fenced off to create an enclosed festive décor complimented by tents, lighting, beverage serving stations, and tables and seating for patrons.
- Setup of the hospitality areas will start at 1:00 p.m., and the beer and wine garden serving hours will be from 6:00 p.m. – 10:00 p.m., and cleanup will be completed by 11:00 p.m.

Staff recommend approval of the two beer and wine gardens, subject to the following conditions being met prior to the events taking place:

- Provide the city with a copy of a certificate of insurance for the two events, which includes Comprehensive General Liability Insurance naming the City of White Rock as an additional insured for the entire duration of both events (from setup through takedown) with a minimum limit of \$5M coverage. Proof of insurance must be received ten (10) days in advance of the August 18, 2022 event;
- Obtain an application for the two Special Occasions Licenses from the BC Liquor Control and Licensing Branch, submit it to the White Rock Detachment of the RCMP for approval at least ten (10) days in advance of the August 18, 2022 event. Following RCMP approval, the Special Occasions License may be issued by a BC Liquor Store Manager; and
- Provide the city with a sketch showing the festival and beer garden site layout, including the dimensions of all tents. The White Rock Fire Chief will review permit and safety issues related to tent size, structure and crowd capacity.
- The BIA must arrange for certified security personnel to provide security of the beer and wine garden area at both events.

- The BIA must provide an adequate number of additional portable washrooms to be located in the beer and wine garden area at both events.

In addition, city staff will send letters to adjacent neighbors to notify them of these events.

FINANCIAL IMPLICATIONS

There are no additional costs to the city as all costs for the operation of these beer/wine gardens will be borne by the White Rock BIA and the White Rock Beach Beer Company.

LEGAL IMPLICATIONS

Approvals of both the White Rock RCMP and the BC Liquor Control and Licencing Branch are required.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

White Rock RCMP will review the beer/wine garden special occasion application to ensure compliance with Provincial liquor laws.

The White Rock Fire Chief will review permit and safety issues related to tent size, structure and crowd capacity.

ALIGNMENT WITH STRATEGIC PRIORITIES

Council desires to make the best possible community decisions in collaboration with residents and stakeholders, providing an excellent quality of life.

OPTIONS / RISKS / ALTERNATIVES

The following alternative is available for Council's consideration:

1. Decline the BIA's request to hold beer and wine gardens at the TD Concerts at the Pier on August 18, 2022 and August 25, 2022.

CONCLUSION

Staff have reviewed event details with representatives of the BIA and the White Rock RCMP. Staff recommend Council approve the two Beer/Wine Garden Special Occasions Liquor Licenses to be hosted by the White Rock Beach Beer Company at the TD Concerts at the Pier performance on Thursday, August 18, 2022 from 6:00 p.m. – 10:00 p.m. at Five Corners (Johnston Road and Pacific Avenue) and at the TD Concerts at the Pier performance on Thursday August 25, 2022 from 6:00 p.m. – 10:00 p.m. at East Beach, subject to the conditions outlined in this corporate report.

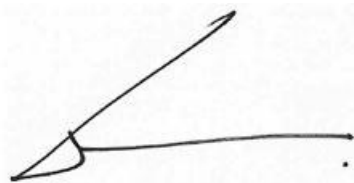
Respectfully submitted,



Eric Stepura
Director, Recreation and Culture

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.



Guillermo Ferrero
Chief Administrative Officer

Appendix A: Appendix B: Beer Garden/Public Function Liquor Licences - Recreation and Culture Policy #704

Appendix B: Letter from White Rock BIA

THE CORPORATION OF THE
CITY OF WHITE ROCK
 15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



POLICY TITLE: BEER GARDEN / PUBLIC FUNCTION LIQUOR LICENCES

POLICY NUMBER: RECREATION AND CULTURE - 704

<i>Date of Council Adoption:</i> April 29, 2013	<i>Date of Last Amendment:</i> July 27, 2015
<i>Council Resolution Number:</i> 2013-134, 2015-285	
<i>Originating Department:</i> Recreation and Culture	<i>Date last reviewed by the Governance and Legislation Committee:</i> July 13, 2015

Policy:

General Policy Guidelines:

1. The process for Council approval of a beer garden is as follows:
 - a) Application must be made to the Director of Recreation and Culture at least 45 days in advance of the event in order to provide sufficient time for processing.
 - b) The Director along with a representative of the White Rock Detachment of the RCMP will review the request and report to Council.
 - c) If the Council approves the beer garden, then the applicant must next obtain the permission of the NCO in charge of White Rock Detachment of the RCMP, the application can then be taken to the local Liquor Control and Licensing Branch authority for final approval and a Special Occasion Licence.
2. Licences for outdoor public functions will only be approved by the R.C.M.P. after the applicant has first received approval from the City Council.
3. Following approval by the RCMP, the licence may be issued by the Liquor Control and Licensing Branch.



July 15 2022

To: Mayor & Council,
City of White Rock

Re: Beer Gardens at the 2022 TD Concerts at the Pier

Dear Mayor Walker & Councillors Chesney, Johanson, Kristjanson, Manning, and Trevelyan:

I hope that this letter finds you well!

I'm writing you regarding the 2022 TD Concerts at the Pier. As you may be aware, the TD Concerts at the Pier typically holds beer gardens at the East Beach and Five Corners locations. Beer gardens are a very popular request from our audience members, and we are able to provide them at those two locations without negatively impacting businesses (East Beach and Five Corners provide more take-out options, while West Beach is renowned for its patios).

The beer gardens also provide revenue for the TD Concerts at the Pier, allowing us to offset costs- increasingly important as costs significantly rise.

While in the past Rotary Clubs have organized and staffed the beer gardens, they are unable to provide their services this year. Thankfully, White Rock Beach Beer Co has stepped forward on very short notice and offered to organize and staff the beer gardens. This would allow us to partner with a local business, provide a desired service for concert attendees, and receive much needed revenue to offset rising costs.

We are requesting Council's approval for two beer gardens at the 2022 TD Concerts at the Pier: August 18 at Five Corners and August 25 at East Beach.

On behalf of the organizers of the TD Concerts at the Pier, I want to thank you for considering our request. Please contact me if you have any questions.

Sincerely,

Alex Nixon
Executive Director,
White Rock BIA

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Eric Stepura, Director, Recreation and Culture

SUBJECT: Promenade Sculpture Competition Update

RECOMMENDATIONS

THAT Council:

1. Receive for information the corporate report dated July 25, 2022, from the Director of Recreation and Culture, titled "Promenade Sculpture Competition Update"; and
 2. Waive the earlier requirement that White Rock Events Society secure commitments for 75% of the Society's \$75,000 fundraising target (\$56,250) prior to any pedestals or sculptures being installed at the waterfront.
-

EXECUTIVE SUMMARY

At its regular meeting of February 28, 2022, Council asked staff to review an event application from the White Rock Events Society (WRES) requesting permission to host a yearlong Promenade Sculpture Competition exhibit at the White Rock waterfront.

As the 2022 Promenade Sculpture Competition is not a "special event" as defined in the city's Special Event Policy, but rather an outdoor art display, a further review of this proposal was carried out by staff from both the Recreation and Culture Department and the Engineering and Municipal Operations Department. Additionally, input was sought from the City of White Rock's Arts and Culture and Public Art Advisory Committees.

At the March 28, 2022 Regular Council Meeting, Council approved funding support of \$15,000 in cash plus an estimated \$10,000 in services-in-kind from the city. Also, Council recently approved a \$5,000 grant-in-aid for this proposed event. The source of \$25,000 in funding will come from the 2022 operating contingency budget, and the \$5,000 from the city's grants-in-aid budget.

At the June 27, 2022 Regular Council meeting, the WRES members did a presentation to Council to provide an update on the Promenade Sculpture Competition. Council directed staff to help the WRES to get the event kicked off in September 2022 as planned.

This corporate report provides an update on a recent event planning meeting staff held with members of the White Rock Events Society, and makes recommendations to Council on city support that will be provided to the WRES so their event will happen in White Rock starting September 14, 2022.

PREVIOUS COUNCIL DIRECTION

At the June 27, 2022 Regular Council Meeting, Council made the following motion:

Motion # & Meeting Date	Motion Details
2022-278 June 27, 2022	THAT Council direct staff to look into helping the White Rock Events Society to help get the event kicked off in September as planned.

INTRODUCTION/BACKGROUND

WRES members are working hard to deliver a sculpture competition display along the White Rock waterfront promenade from September 14, 2022 to approximately September 2023. Their plan is to select ten (10) sculptures for display along the waterfront between P'Quals (white rock) and the entrance to the Pier. To date, the Society's selection panel have received eighteen (18) entrants for the sculpture competition.

The Society members have also received quotes for insurance, engineering services, production and installation of the bases and steel pedestals that support the sculptures, and other required expenses.

At the Regular Council Meeting held March 28, 2022, Council approved the following funding support for the WRES Sculpture Competition:

- \$15,000 in cash.
- An estimated \$10,000 in services-in-kind from the city.

The source of \$25,000 in funding would come from the 2022 operating contingency budget.

In addition, Council has approved a grant-in-aid of \$5,000 for this proposed event.

The event approval and City funding support was conditional on WRES meeting the following conditions:

1. That the White Rock Events Society secure commitments for 75% of the Society's \$75,000 fundraising target (\$56,250) prior to any pedestals or sculptures being installed at the waterfront.
2. That a Memorandum of Understanding (MOU) between the WRES and the City be developed outlining the responsibilities of each party, and the conditions that must be met by the WRES prior to the installation of the pedestals or sculptures. The MOU would also include the scope of the project, schedule, and resources required. A final debriefing report, based on the MOU metrics, would measure success, including sustainability of the event for future years.
3. That the display be limited to the grass area east of the Pier between the flagpole and P'Quals (white rock).
4. That event sponsorship and other signage be pre-approved by city staff.
5. That the project aligns with the City of White Rock's Public Art and Placemaking Policy including that the selection of artists, and art be coordinated with the Public Art Advisory Committee, and that artists are compensated fairly.
6. That before beginning the installation, the WRES obtain liability insurance for the entirety of the time the display is on city owned or leased property. The insurance must cover injuries and/or death to organizers, volunteers, contractors, and visitors, as well as potential damage to

streetlights and other infrastructure. The insurance policy must be a minimum \$5M coverage, naming the City of White Rock as added insured, include a cross liability clause and a no cancellation clause stating that the policy cannot be cancelled without first providing the City with 30 days written notice of cancellation.

On July 15, 2022, city staff met with members of the WRES to discuss what progress the event organizers have made, and to clarify what funding and services-in-kind the City will be providing in support of the event.

City Funding

The city will provide \$20,000 cash to WRES.

City Services-In-Kind

Recreation and Culture support includes: staff assistance with event planning, coordination, promotion and site meetings; and provision of event related materials, equipment and supplies. This includes the cost of design, production and installation of a Pier head banner, event posters and promotion on the city's web site (event calendar) and social media posts.

Engineering and Municipal Operations support includes: staff assistance with sculpture site selection meetings, transportation of bases and pedestals to and from the site, review of engineering drawings, daily cleanup, maintenance of sculptures as needed and project planning assistance.

The meeting on July 15, 2022 clarified many questions that WRES event organizers had about city support of the sculpture competition. It also provided staff with greater assurance that WRES will meet its revenue targets and deliver a safe, attractive and well organized sculpture competition that meets the city's conditions for event approval as outlined above. Staff therefore recommend, that Council waive condition # 1 above so that WRES can proceed with the installation of the pedestals and sculptures at the designated location on the waterfront.

FINANCIAL IMPLICATIONS

Council support to the WRES Promenade Sculpture Competition is estimated as follows:

Cash requested from the City	\$15,000
Council's Grant-in-Aid as recommended by the City's Grants-in-Aid Sub-Committee	\$ 5,000
Recreation and Culture services-in-kind	\$ 5,000
Engineering and Municipal Operations services in- kind	\$ 5,000
Total Estimated Funds Required from the City of White Rock	\$30,000

As the 2022-2026 Financial Plan does not include funding for this event, the \$25,000 portion would have to be funded from the city's 2022 operating contingency, and \$5,000 from the city's grants-in-aid budget.

LEGAL IMPLICATIONS

There is some risk of injury to visitors to the waterfront if the sculptures are climbed on. Having a city staff representative on the panel that selects the art pieces will help reduce this risk.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

Staff will promote the competition on the city's web site and on social media channels.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

Recreation and Culture support includes: staff assistance with event planning, coordination, promotion and site meetings; and provision of event related materials, equipment and supplies. This includes the cost of design, production and installation of a Pier head banner, event posters and promotion on the city's web site (event calendar) and social media posts.

Engineering and Municipal Operations support includes: staff assistance with sculpture site selection meetings, transportation of bases and pedestals to and from the site, review of engineering drawings, daily cleanup, maintenance of sculptures as needed and project planning assistance.

The city's Communication Department will assist with promoting the event.

ALIGNMENT WITH STRATEGIC PRIORITIES

Council desires to make the best possible community decisions in collaboration with residents and stakeholders, providing an excellent quality of life.

OPTIONS / RISKS / ALTERNATIVES

The following alternate option is available for Council's consideration:

1. Decline waiving the earlier requirement for White Rock Events Society to secure commitments for 75% of the Society's \$75,000 fundraising target (\$56,250) prior to any pedestals or sculptures being installed at the waterfront.

CONCLUSION

The proposal from the WRES is exciting and ambitious. If successful, the event could attract more visitors to White Rock, and continue to build on the Council's goal identified in the Cultural Strategic Plan to integrate arts and culture into everyday life of White Rock residents.

The city must provide oversight of all events taking place on its owned or leased facilities as it has a duty to monitor public safety, public health, and manage its fiduciary responsibilities.

Staff have recently met with WRES organizers of the Promenade Sculpture Competition to review their event planning and fundraising progress and are supportive of the event proceeding to the next stage, which is the installation of the bases, pedestals and sculptures on city leased property at the White Rock waterfront.

The WRES has requested \$15,000 in cash plus an estimated \$10,000 in services-in-kind from the city and Council recently approved a \$5,000 grant-in-aid for this proposed event. The source of \$25,000 in funding would come from the 2022 operating contingency budget, and \$5,000 from the city's grants-in aid budget.

The White Rock Promenade Sculpture Competition has the potential to be a welcome new addition to the City's cultural event calendar if organizers can secure the necessary financial support from sponsors.

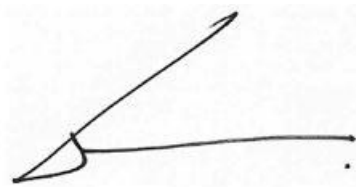
Respectfully submitted,

A handwritten signature in black ink, appearing to read "Eric Stepura".

Eric Stepura
Director, Recreation and Culture

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, appearing to read "Guillermo Ferrero".

Guillermo Ferrero
Chief Administrative Officer

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Eric Stepura, Director, Recreation and Culture

SUBJECT: Healthy Communities Age Friendly Grant Application 2022 Resolution

RECOMMENDATION

THAT Council endorse the City's BC Healthy Communities Age Friendly 2022 Grant Application and indicate support for city staff to provide overall grant management.

EXECUTIVE SUMMARY

To seek a Council resolution indicating support for the City's BC Healthy Communities Age Friendly 2022 Grant Application, as required by BC Health Communities Society.

INTRODUCTION/BACKGROUND

The proposed project, 'Poetry in Motion', is an inclusive and accessible community engagement walk that provides community members an opportunity to engage in culturally diverse poetry while participating in a social physical activity. Participants will be provided community maps and the opportunity to join free group walking tours where they will be able to share in unique cultural poetry on permanent podiums throughout their community. Participants will receive a coffee or tea voucher at a local business.

The purpose of the 'Poetry in Motion' project is to reduce social isolation by increasing social connectedness and providing opportunities for physical activity.

Intended outcomes of the 'Poetry in Motion' project are to create opportunities for 55+ community members to form new relationships, promote physical activity and mental wellness, spend more time in nature, encourage participants in learning of different cultures, aging in place, and support local businesses. The project also aims to enhance opportunities for intergenerational engagement and bring greater awareness.

Fraser Health, Brella Community Services, Semiahmoo Seniors Planning Table, DIVERSEcity, Canadian Association for Retired Persons (C.A.R.P.), and Sources Community Resource Society are all partners in the project.

ANALYSIS

The Poetry in Motion project is designed with the direct input from older adult community members to ensure the project is tailored to their needs and asks. By partnering with age-friendly groups and organizations and enhancing networks, community members will be able to find social supports and feel a greater sense of connectedness. The project will also enhance physical

activity in the community, support local businesses, and will add to the city's cultural assets. Poetry content will be diverse as to highlight the different cultural groups and perspectives in the community.

FINANCIAL IMPLICATIONS

The grant application is seeking \$15,000 in funding with an anticipated start date of May 2023. If successful in the grant application, the city would be responsible to manage the funds and report expenditures to BC Healthy Communities.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The city's Parks Department and Operations Department Staff will be involved in the installation of the Poetry markers.

ALIGNMENT WITH STRATEGIC PRIORITIES

The Healthy Community Partnership Committee function aligns with the Council's Strategic Priorities under "Our Community" and in providing an excellent quality of life.

OPTIONS / RISKS / ALTERNATIVES

The following alternate option is available for Council's consideration:

1. Not endorse the city's BC Healthy Communities Age Friendly 2022 Grant application signifying an unwillingness to provide overall grant management.

CONCLUSION

This report seeks a Council resolution for the endorsement of the City's BC Healthy Communities Age Friendly 2022 Grant application and confirming that Council is willing for city staff to provide overall grant management, as required by the BC Healthy Communities grant application program.

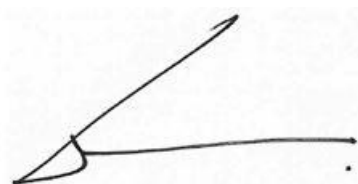
Respectfully submitted,

A handwritten signature in black ink, appearing to read "Eric Stepura".

Eric Stepura
Director, Recreation and Culture

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, appearing to read "Guillermo Ferrero".

Guillermo Ferrero
Chief Administrative Officer

Appendix "A"

BC Healthy Communities Age Friendly 2022 Grant application 'Poetry in Motion'

APPENDIX A

Age-friendly Communities 2022 Grant Application

2. Applicant Information

1. Stream: This application is for (select one stream):

Stream 2 Projects Grant (project implementation). (up to \$15,000)

2. Please indicate the name of the Indigenous government or local government applying.

City of White Rock

3. Please indicate the type of government applying.

Municipality

3. Applicant contact details

4. Applicant mailing address and contact information Primary Contact full name

Dianne Sawicki

Primary contact position or title

Recreation Programmer

Street Address

1475 Kent Street

Apt/Suite/Office

City, District, Village or Town

White

Rock

Provinc

e BC

Postal Code

V4B 5A2

Email address

DSawicki@whiterockcity.ca

5. Primary contact role in the project:

Project Manager

6. Only primary contacts will receive communication regarding this application. What is your preferred method of communication?

Email

4. Health authorities and past AFC funding or BCHC capacity building support

7. Has your organization received AFC funding or support in the past (i.e., AFC funding from the Union of BC Municipalities (UBCM), and/or Age-friendly Capacity Building (AFCB) support from BCHC?*

Yes

8. If yes, please indicate the plan/project name, year, and whether it was funding from UBCM or Age-friendly Capacity Building support from BC Healthy Communities.

Plan/project name:

Seniors Housing and Support Initiative

Plan/project year:

2009

Please check all that apply

Age-friendly Communities grant funding from UBCM

9. Please indicate the health authority region in which your Indigenous government or local government is located. Please also indicate if your community is served by the First Nations Health Authority.

Fraser Health

10. Do you have an existing relationship with your health authority(ies)?

Yes

11. If you have an existing partnership with your health authority(ies), please provide key contact information for each.

Primary health authority contact name: : Adrianna Spyker

Primary health authority contact position: : Community Health Specialist

Primary health authority contact email: : adrianna.spyker@fraserhealth.ca

Primary health authority contact phone: : 604.751.5723

Secondary health authority contact name: : Hattaw Khalid

Secondary health authority contact position: : Active Living Community Health Specialist

Secondary health authority contact email: : hattaw.khalid@fraserhealth.ca

Secondary health authority contact phone: : 778.683.9797

12. Have you discussed this plan/project with your local health authority?

Yes

13. If yes, indicate what discussions have occurred, and if the local health authority is supportive of the initiative.

The local health authority is supportive of the initiative. Fraser Health approached the City of White Rock with the proposal and provided a letter of support.

5. Overview and Workplan

14. What is the name or proposed title of your plan/project?

Poetry in Motion

15. What are the Age-Friendly Community components that are the focus of your plan/project

Outdoor spaces and buildings

Social well-being and participation

Respect, social inclusion, and cultural safety

Communications and information

Community support and health and wellness services

16. Participation of older adults.

Please identify how this plan/project includes the participation of older adults. Please check all that apply and describe their direct participation in the space provided below.

Age-friendly Assessment

Inclusion of Age-friendly planning principles in the OCP/CCP/other Plans

Other - Write In: Direct feedback and input from Semiahmoo Seniors Planning Table and feedback from community members

17. Describe the direct participation by older adults in your proposed plan/project.

The proposed project, Poetry in Motion, is an inclusive and accessible community engagement walk that provides community members an opportunity to engage in culturally diverse poetry and existing community while participating in a social physical activity. Participants will be provided community maps and the opportunity join in group walking tours where they will be able to share in unique cultural poetry on permanent podiums throughout their community.

The guided walks and maps will be accessible in different languages and include other unique landmarks on the walk throughout the community. Participants on the guided walks will be provided a coffee/tea voucher for local cafes to continue their social interactions and support local businesses. The Poetry in Motion project builds upon existing group activities and utilizes spaces that are already frequented by the older adult population (i.e., parks, local businesses, community centres). Community feedback went directly into the design of the project of what seniors want to see in their community and builds off of existing walking routes.

The project will continue to work closely with the City and Fraser Health for senior safety plans for participation (i.e., safe walkways, snow removal, de-icing, lighting) and for safe and accessible community spaces as well as other community groups and organizations. The community maps will be provided throughout the City of White Rock for individual use and have information on how to join the group walks.

18. What is your target population(s) and why? Please describe any target population(s) within the overall older adult population. Please refer to the [grant application guide](#) for more information.

The target audience for the Poetry in Motion project is 55+ of multi-cultural and ethnic backgrounds and all-bodies and abilities. Based on the most recent Census data available (2022) the 55+ population accounts 45.5% of the City of White Rock's total population. The project is also inclusive towards any and all other community members with the opportunity for intergenerational engagement and as an activity for families, other groups, and/or individuals.

19. What is the purpose of the plan/project? (i.e., What do you intend to achieve?)

Based on the feedback from the community and the opportunities within the Age-Friendly Grant, the purpose of "Poetry in Motion" is to reduce social isolation by increasing social connectedness and providing opportunities for physical activity. This also aims to provide opportunities for intercultural engagement and learning.

20. What are the intended outcomes/What changes do you anticipate as a result of this plan/project?

The intended outcomes of the proposed project, "Poetry in Motion", are to create opportunities for 55+ community members to form new relationships, promote physical activity and mental wellness, spend more time in nature, encourage participation in learning of different cultures, aging in place, and support local businesses. The project also aims to enhance opportunities for intergenerational engagement and bring greater awareness to different older adult organizations and resources.

6. Overview and Workplan (continued)

21. How will this plan/project make your community more age-friendly?

Poetry in Motion is developed with the direct input from older adult community members to ensure to project is tailored to their needs and asks. By partnering with existing age-friendly groups and organizations and enhancing networks, community members are able to find social supports and feel a great sense of connectedness. The project will assist in identifying barriers for access and gaps in the community. Poetry in Motions provides opportunities for intergenerational engagement.

22. Please describe the activities that you plan to complete. Please be as specific as possible. Please refer to pages 13 - 14 of the [grant application guide](#) for a sample workplans.

*Attachment with application in more detail

1. Reach out to partners to start project development.
2. Host engagement sessions to collect further feedback from the community on project design and identification of potential barriers/challenges.
3. Contract a company to install podiums at identified locations.
4. Connect with local businesses to form partnerships for drink voucher portion of the project.

5. Design and development maps that highlight installations, facilities, benches and steps, and partner coffee shops.
6. Identify and select multi-cultural poetry and art work. Work with DIVERSEcity on translation and tactile signage.
7. Print site maps, posters, pamphlets, poetry displays, and start social media campaign.
8. Create a schedule for guided group walks through Kent Street Activity Centre.
9. Launch project in May 2023.
10. Collect on-going feedback from stakeholders and adapt to community wants and needs.
11. Collate feedback and survey responses from stakeholders and community organizations, and develop final report.

23. How will you apply an equity lens to your plan/project? Please refer to page 6 of the [grant application guide](#) for more information on using an equity lens in AFC planning.

- Resources will be available in different languages, as well as online or printed
- Project will involve the local First Nations community (Semiahmoo First Nations)
- Project will be promoted to newcomers for community engagement and integration opportunities
- Accessible for all abilities to participate. For example, maps will include facilities available and levels of difficulty, as well as noting routes with stairs and or pathways.
- FREE - no cost to participate.
- Guided walks will be offered for those who would prefer support
- FREE guided walks will also included a coffee/tea voucher for small local business
- Content throughout walk will be diverse as to highlight different cultural groups and perspectives in the community
- Promotion of the project will be throughout the community through different NGO's and community-based groups.

7. Overview and Work Plan (continued)

24. How will you know if your plan/project is successful?

We will assess the success of our project based on the numbers of participants in Groups Walks and number of participants using drink vouchers. We will also collect feedback from participants and participating organizations. We will also note online engagement with the walks.

25. How will you track progress?

1. Number of participants on Groups Walks
2. Number of drink vouchers used
3. Track numbers of maps taken/requested for refill from participating locations

26. Please share any ideas you have to sustain the benefits of this project beyond the grant period.

This project is project can be accessed and participated by all generations therefore has the potential to be supported by various community members and organizations.

This project is building off of existing activities and initiatives, but needs additional funding and support for organization and infrastructure.

The community was looking for a project that would incorporate physical activity and culture for the older adult population. Therefore, if this project is successful, it would become a permanent component in the community.

An intention of the project work is to create and/or reinforce networks within the community to sustain the activity. The work of the project will continue to work closely with the City and Fraser Health for any necessary adaptations.

8. Overview and Work Plan (continued)

27. Is the proposed plan/project building on previous work in your community?

Yes

28. If yes, please tell us the previous plan/project name and how the current proposed plan/project will build on this previous work.

"Walk and Talk" - This activity was established as result of the Covid-19 pandemic. Older adults in the community wanted to finds safe and inclusive ways to continue to socialize, participate in physical activity, and enjoy outdoor spaces.

The project also builds off of and aligns to the activities and goals of White Rock Recreation and Culture programming.

29. Please indicate if this project reflects one or more of the Health Promotion Initiatives for older adults. Please refer to page 9 of the [grant application guide](#). This question is not mandatory, but applicants are encouraged to respond if they are able.

Aging Well
Physical Activity Strategy

9. Collaboration and Key Partners/Stakeholders

30. Which partners, including health authorities and potential collaborators in other sectors, will be involved in your plan/project?

Using the table below, please list a) any partner organizations; b) a short description of how they will be involved and c) a specific contact person within each organization, if known.

Example categories include: Indigenous government and/or Indigenous organizations; Older adult-serving organizations (e.g., groups, clubs); Community organizations in general; Other local governments (e.g., municipalities, regional districts); Other partners (e.g., school districts, specific older adults, community businesses).

	Proposed Partner Organization	Proposed Roles and Responsibilities	Contact Name
1	Fraser Health	Project Coordination	Adrianna Spyker/Hattaw Khalid
2	City of White Rock - Recreation and Culture	Promotion, scheduling/staffing for guided tours	Dianne Sawicki
3	Brella Community Services Society	Promotion and planning input	Louise Tremblay/Janice Gunn

4	Semiahmoo Seniors Planning Table	Promotion	Heather Martin
5	DIVERSEcity	Language translation support, promotion, engagement with newcomers	Saleh Altaf
6	C.A.R.P.	Promotion	Ramona Kaptyn
7	SOURCES	Promotion	David Young
8	Rotary - South Surrey/White Rock	Promotion	TBD
9			
10			

10. Age-friendly Accomplishments to-Date & BC Age-friendly Community Recognition

Status

31. Please check any of the following that your community has completed:

Established an age-friendly advisory group or steering committee that includes the active participation of older adults. An existing committee can also take on this mandate.

Passed a local government council/board resolution or band council resolution to actively support, promote, and work towards becoming an age-friendly community. As an alternative, local governments may have chosen to commit to being age-friendly through specific goals, objectives, or policies in an official community plan or strategic plan.

Conducted an age-friendly assessment in consultation with older adults.

32. This AFC Grant Program aligns with the BC [Age-friendly BC Community Recognition Program](#) administered by the BC Ministry of Health. Does this Indigenous government or local government currently have Age-friendly BC Community Recognition Status?

Yes

11. In-kind Supports

33. The Age-friendly Communities Grants include a cash award as well as in-kind support from BC Healthy Communities staff. What support do you anticipate, if any?

Check all that apply:

Research support (e.g., environmental scans, sourcing literature or other resources) Document review & providing feedback

Development of data collection tools (e.g., surveys, interview guides)

Developing monitoring and evaluation frameworks including the development of indicators

12. Budget and Workplan

34. I would like to [Select one]:

upload the budget and workplan files.

35. When you are finished, upload the Excel document using the 'Browse...' button below. *Note: if you experience any technical issues with the file upload, please change your selection above to submit the document by email.*

[Work plan for WR Age-friendly grant - June 17 2022.xlsx](#)

13. Formal Council, Board or Band Support

36. Please provide your council/board resolution, band council resolution, or equivalent.

I would like to [select one]:

the resolution is on the agenda for the next meeting of council, band council, or board

37. The application must be submitted by July 5, but we will accept the council resolution by email for a short period after the deadline. Please submit within a week of the application deadline (i.e. by July 12) to ensure it is included in our adjudication process. The resolution can be sent to grants@bchealthycommunities.ca.

Please enter the date of the meeting which will consider the resolution below:

2022/07/11

15. Additional Files

38. Do you have anything else to add that we should know about your plan/project? (e.g., plan/project is coordinated with a sister city; measurables that will be applied to monitor the success of this planning process or project)?

n/a.

39. Do you have any additional files to include that support your application (e.g., presentations, diagrams, pictures, stories)? Please note that letters of support from plan/project partners are strongly encouraged. Up to three letters of support as evidence of partnership or collaboration can be included in this section.

Yes

40. I would like to [select one]:

upload the additional documents.

41. Please upload the documents using the 'Browse...' button below. Note: if you experience any technical issues with the file upload, please change your selection above to submit the document by email.

[Age-friendly grant application - WR - highlights from key documents.docx](#)
[Letter of support - age friendly grants - CARP 2022.pdf](#)
[White Rock - Fraser Health - Age Friendly Grant 2022.pdf](#)

17. Staying in Touch

42. Please tell us how you heard about this grant opportunity. BC Healthy Communities newsletter

Health authority

43. Would you like to subscribe to our BC Healthy Communities newsletters to hear about other funding opportunities, learning events, community stories and more? This includes the BCHC newsletter and one from another BCHC program, the PlanH newsletter.

Yes, please!

44. Great! Please provide the email address you would like to use:

adrianna.spyker@fraserhealth.ca

Enter Recommendation Here

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Anne Berry, Director, Planning and Development Services

SUBJECT: White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, and 1465 Vidal Street) Bylaw, 2022, No. 2439

RECOMMENDATIONS

THAT Council:

1. Give first and second readings to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439" as presented.
2. Direct staff to schedule the public hearing for "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68- 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439;" and
3. Direct staff to resolve the following issues before bringing "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68- 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439" back for consideration of final adoption:
 - a) Ensure that all engineering requirements and issues, including road dedication and the execution of a Works and Servicing Agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;
 - b) Ensure that all matters about tree protection and retention are addressed to the satisfaction of the Director of Planning and Development Services;
 - c) Confirm that a tree protection covenant, if and as required, is registered on title to ensure the recommendations of the final Arborist Report, approved by the Director of Planning and Development Services and, more specifically, the City's Arboricultural Technician, are implemented and maintained through future demolition and construction activities;
 - d) Complete the demolition of the existing buildings to the satisfaction of the Director of Planning and Development Services; and
 - e) A statutory-right-of-way be registered on title regarding the community urban park space.

EXECUTIVE SUMMARY

In July 2019, the city received applications for Zoning Bylaw Amendment and a Major Development Permit tied to the properties at 1441, 1443-45, 1465 Vidal Street, and 14937 Thrift

Avenue. The original proposal included a six-storey 129-unit apartment building intended to be rental in tenure.

The project has undergone a series of changes in response to city-initiated amendments to the Official Community Plan (OCP) and to address feedback from the Advisory Design Panel (ADP). The original proposal was presented to the Land Use and Planning Committee (LUPC) on October 19, 2020. At that time, the Committee was supportive of seeing the applications continue through the review process.

On November 22, 2021, Council passed a motion requesting the applicant to submit a new and revised proposal for the redevelopment of the subject property. This direction prompted the closure of the Major Development Permit Application (File No. 19-0111). At its meeting on January 10, 2022, Council resolved to reopen the file and bring it forward for full consideration, including a public hearing. At the February 7, 2022, meeting, an amending bylaw was presented for first and second reading and sought Council direction to schedule a public hearing. This motion was defeated.

The applicants have revised their proposal, which is being presented to Council for first reading and second reading, and staff is seeking Council direction to schedule a public hearing.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
2020-LU/P-036 October 19, 2020	THAT the Land Use and Planning Committee recommends that Council resolve that the zoning amendment application at 1441 Vidal Street proceed to the next stage in the application review process.
2021 – LU/P-089 November 22, 2021	1. Direct staff to advance the Zoning Bylaw Amendment Application at 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue, to the next stage in the application review process; and 2. Update the on-site development signage to reflect the revised development proposal as described in the corporate report titled "Initial Review (Revised Submission) 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue, Zoning Bylaw Amendment and Major Development Permit (File No. 19-011)." DEFEATED
2021-456 November 22, 2021	THAT Council direct staff to inform the proponent that Council are looking for a new project with less density and less massing on the site at 1441, 1443-45 and 1465 Vidal Street and 14937 Thrift Avenue. (This motion was communicated to the applicant and the file was closed.)
2022-IC-008 January 10, 2022	THAT Council authorize staff to reopen City File No. 19-011: Revised Submission by Weststone Group at 1441-1465 Vidal Street and 14937 Thrift Avenue and bring the application forward for consideration at the next available open Council meeting

2022-044 February 7, 2022	That Council give first and second readings to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-67 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2418" as presented. <div>DEFEATED</div>
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INTRODUCTION/BACKGROUND

The properties subject to these applications include 1441, 1443-45, 1465 Vidal Street, and 14937 Thrift Avenue ('Properties') (see Figure 1).



Figure 1- Subject Properties

The properties are situated within a block comprised mainly of multi-family dwellings. Immediately north of the site is the 12-storey "Beverley" building (1501 Vidal Street). To the east, there are several two and three-storey apartment buildings; to the west, there are several blocks of three-storey townhomes.

Official Community Plan

The Properties fall within the "Town Centre Transition" designation per the Official Community Plan (OCP). Within this designation, the plan permits multi-unit residential uses intended to support the commercial uses in the Town Centre. When the original proposal was made in 2019, the policies of the OCP enabled a maximum height of approximately 18 storeys along North Bluff Road, transitioning to four storeys along Thrift Avenue (see Figure 2). In the following section, staff will summarize the past proposals for the site.

2019 Proposal

The original proposal presented a six-storey building that stepped down as it approached Thrift Avenue (see Figure 2). The height of the project was compliant with the policies of the OCP; therefore, an OCP amendment was not required. The design had a floor area ratio (FAR) of 2.1, which was enabled by the policies of the plan. Specifically, the plan set a base density of 1.5 FAR, and policy 8.2.3 allowed for a 40 percent increase to 2.1 FAR, where at least half of the additional floor area was secured as rental units. The entire 129-unit project was proposed as rental in tenure and benefited from the density bonus.



Figure 2 - Original Proposal, July 2019 (File No. 19-011)

2021 Proposal

In 2021, the OCP was amended to limit height within the Town Centre Transition area. Regarding the properties, height was reduced to four storeys with the opportunity to support six storeys if an "affordable housing component" was included. Policy amendments were also made to lower the base density available within the area to 1.5 FAR, with the ability to increase this density to 2.5 FAR with an affordable housing component and 2.8 FAR with both an affordable housing component and replacement units. Although the amended OCP allows for consideration of a six-storey building, this iteration of the revised proposal was a four-storey building with 82 units, all of which would be rental in tenure (see Figure 3).



Figure 3- Revised Proposal, October 2021 (File No. 19-011)

This proposal included 17 three-bedroom units (20.7%), 17 two-bedroom units (20.7%), 38 one-bedroom units (46.4%) and 10 studio units (12.2%). The allocation of two and three-bedroom units support the City's "family-friendly housing" policy which seeks a minimum 10% supply.

Current Proposal



Figure 4- East perspective elevation

The current proposal is a 4-storey option with a rooftop amenity intended to soften the overall view from the Beverly and the Townhouses to the north. The design intends for residents living at higher levels to look at the trees and a landscaped rooftop amenity (see figure 7). In addition, the building has removed the tiered-down decks and have replaced them with a community green space to act as an urban park for all residents of White Rock (see figure 6). If approved staff will work with the applicant to put a statutory-right-of-way in place.



Figure 5- West perspective elevation



Figure 6 - Urban Park



Figure 7- Landscaped rooftop amenity

The total unit count for the building is 70, with a FAR of 1.5. This proposal included 20 three-bedroom units (28.6%), 25 two-bedroom units (35.7%), and 25 one-bedroom units (35.7%). The allocation of two and three-bedroom units supports the city's "family-friendly housing" policy which seeks a minimum 10% supply.

In the housing chapter of the OCP, the main goal is to ensure that the City of White Rock has a mix of housing choices that are appropriate and affordable for residents at various stages of their lives. The following objectives and policies apply to this project:

- Objective 11.1 - To expand housing choices for existing and future residents and increase the diversity of housing types for a variety of household sizes, incomes, tenures, needs, and preferences
- Policy 11.1.1 Family-Friendly Housing – Increase the attractiveness and affordability of housing in White Rock for families by:
 - Encouraging applicants to provide ground floor units with front door access to the street in all rezoning applications for residential developments greater than three storeys in height;
 - Providing a minimum of 10% of units with three bedrooms and a minimum of 35% with either two or three bedrooms in all rezoning applications for residential developments with more than 20 dwelling units
 - Establishing outdoor amenity space requirements for multi-unit residential developments
- Policy 11.1.3 Housing Choices Everywhere – Focus residential densities in the Town Centre but ensure that housing choices are distributed throughout the City in all neighbourhoods. Allow duplexes and triplexes throughout the Mature Neighbourhoods.
- Objective 11.2 - To support rental housing and a range of non-market housing options and needs along the housing spectrum.\

Purpose Built Rental

Low- and moderate-income families are challenged to afford the traditional single detached home in White Rock, given the disparity between median household incomes and benchmark sale prices. Many of these families are interested in ground-oriented rental and homeownership units, such as townhouses or apartments, but the availability of these units is limited. This project's design has several ground-oriented units along the street.

It is common for condos that may have enough bedrooms to accommodate all family members to be age-restricted, excluding families from accessing them. Engagement revealed that these circumstances are leading families to leave the city in search of options elsewhere. As a result, there is a need for two-, three- and four-bedroom units within multi-unit housing projects and for non-market rental, market rental and homeownership tenures. This development is proposing to include 20 three-bedroom units and 25 two-bedroom units.

White Rock currently has a mix of residential uses that characterize different areas of the city. For example, while single-family homes in the Mature Neighbourhood area account for the large majority of land area in White Rock, the most significant proportion of housing starts in the last decade have been apartments.

White Rock experiences housing affordability challenges along with the rest of the Lower Mainland, in which housing costs are well beyond a price-to-income ratio that justifies the price of housing. Regionally, 33.5% of households spent more than 30% of their income on housing in 2011. In White Rock, this value was slightly lower at 31.4%.

Supporting new Secure Market Rental (all rental buildings with rents at market rates) and Affordable Rental Housing (rent is subsidized) will help improve housing affordability in the city. Secure Market Rental units are designated for rental purposes only.

Secured rental housing is that portion of the rental stock that provides longer-term rental housing where tenants can reside without worry that their tenancy may be terminated by new owners who purchase the units. Rents are determined by market demand, subject to the *Residential Tenancy Act*.

Secured market rental housing means development or part of a development used only as market rental housing. A secure and robust stock of rental housing contributes to the city's social diversity, economic health, and community sustainability development. In addition, rental housing provides an option for those who cannot afford ownership housing and those who need flexible shorter-term housing options.

Also, particularly for households who are in the early stages of career development, renting provides the flexibility to respond to educational and employment opportunities that may require relocation. Finally, a good supply of rental housing includes housing options for the workforce, which is considered essential to attracting employers to locate in the city.

Strata Title Conversion

If this development is approved, these units will not be individually strata titled; they would be considered one titled strata unit. Therefore, they cannot be sold independently. The only way for this building to become strata is for the owner of the building to apply for a Strata Title Conversion (STC). With this proposed development, 100% of the residential floor space is unstratified rental housing.

When a multi-unit building is owned by one (or a group) of owners, and the owner(s) wish to create individual titles for each unit to allow for individual ownership, then a STC is required.

The *Condominium Act*, Zoning Bylaw, and Council policies control STCs in the City of White Rock. These Bylaws ensure that the proposed STC meets health, safety, and servicing requirements.

Strata Conversion is the creation of individual legal units in an existing multi-unit building with one (or a group) of owners. Strata conversion needs to be considered by Council when owners and/or tenants currently occupy the multi-unit building.

Examples of buildings that are converted from single legal units to strata-titled units include: duplexes, townhouses, apartments, multi-unit commercial and industrial buildings, and mixed-use residential/commercial buildings.

As outlined in Section 242 of the *Strata Property Act*, the province requires that a Local Government consider the following when a strata conversion request is submitted:

- (a) the priority of rental accommodation over privately owned housing in the area;
- (b) any proposals for the relocation of persons occupying a residential building;
- (c) the life expectancy of the building;
- (d) projected major increases in maintenance costs due to the condition of the building; and
- (e) any other matters that, in its opinion, are relevant.

Housing Need Report – Metro Vancouver – Community and Housing Profile (October 2020)

In terms of rental housing, the following subsection outlines information regarding the primary and secondary rental market in White Rock. Figure 8 shows the number of purpose-built rental units in the primary rental market in White Rock over time. This includes both purpose-built rental apartments and row housing (townhouses). In 2019, there were a total of 1,392 units in the primary rental market. From 2010 to 2019, the number of purpose-built rental units decreased slightly by 0.7% (10 units).

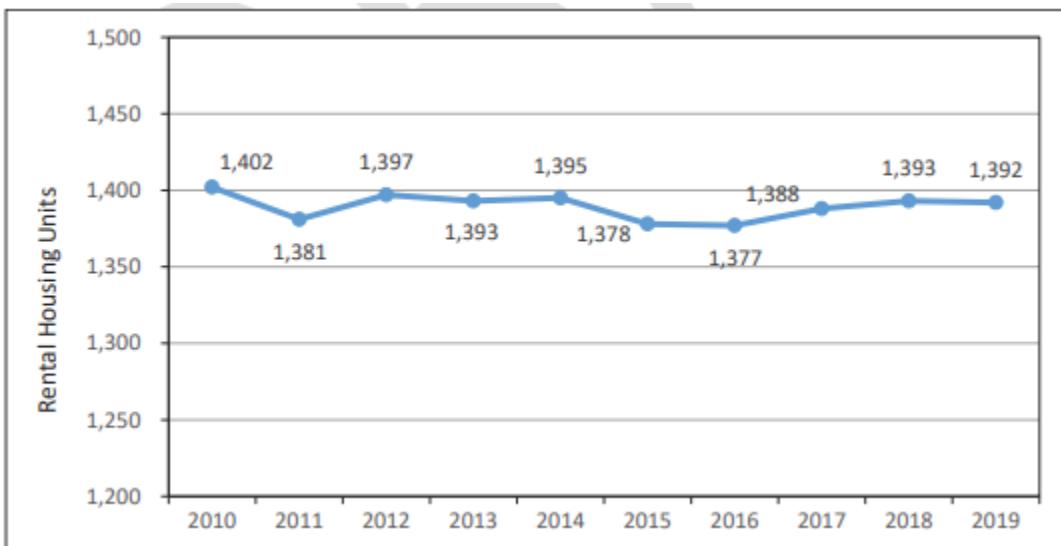


Figure 8- Total Number of Dwelling Units in the Primary Rental Market, White Rock (2010 to 2019)

Secondary suites and private condominium rentals represent a portion of the rental housing stock in the Metro Vancouver region. Data for private condominium rentals and secondary suites are challenging to obtain at the municipal level. According to the Metro Vancouver Housing Data

Book, there were an estimated 1,400-1,600 secondary suites in 2017 and an estimated 856 private rental condominium units in 2018 in the private rental market in White Rock.

Table 1 shows the rental vacancy rates in White Rock overall and by type of housing unit (i.e. number of bedrooms) since 2010. In 2019, the total vacancy rate in White Rock was at 1.7%, slightly higher than the 1.1% the previous year. The overall rental vacancy rate has varied significantly since 2010, reaching a high of 3.9% in 2013 and decreasing since then. In comparison, the overall vacancy rate in 2019 was 1.1% in Metro Vancouver and 1.5% in British Columbia.

Number of Bedrooms	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
0 bedrooms	0.0%	1.3%	2.6%	2.5%	4.1%	3.0%	0.0%	n/a	n/a	3.5%
1 bedroom	2.2%	0.7%	2.9%	4.4%	1.4%	0.8%	0.1%	0.3%	0.9%	2.2%
2 bedrooms	0.8%	0.3%	4.7%	2.8%	1.2%	0.3%	0.0%	0.6%	1.6%	0.3%
3+ bedrooms	10.6%	12.5%	0.0%	0.0%	n/a	0.0%	n/a	n/a	n/a	n/a
Total	1.8%	0.7%	3.3%	3.9%	1.5%	0.8%	0.1%	0.6%	1.1%	1.7%

Table 1- Vacancy Rate by Number of Bedrooms, White Rock (2010 to 2019)

Housing Needs Report (November 2021)

The purpose-built rental pool in White Rock has experienced stagnant growth over the past decade and has been surpassed by the growing number of rented condominiums. The secondary rental market, including secondary suites, are good options to offset the shortfall of purpose built rental housing; however, they are not secure, and tenants are subject to legal evictions for situations such as the owner wanting to move back into their unit. White Rock would benefit from securing more purpose-built rental units. In addition, special attention to design and configuration to allow for more accessible units for seniors, persons with disabilities, and family-sized units, would be appropriate.

Nearly one in three residents in White Rock are renters (32%). The rental vacancy rate has fluctuated in White Rock over the past decade, reaching a high of 3.9% in 2013. Since then, the rental vacancy rate has tightened and is currently less than 1% (see figure 9). The low vacancy creates pressure on the rental stock and can be considered an undersupply if sustained over a long period. In White Rock, there appears to be more pressure on two-bedroom rental units – which had a vacancy rate of 0.3% in 2020. Studio units had a vacancy rate of 1.3%. Although vacancy rates for three or more-bedroom units have not been reported for White Rock since 2015, the last three figures from 2012, 2013 and 2015 reported 0% vacancy for three-bedroom units in White Rock. Rental vacancy rates for studio units were 1.3%, and two-bedroom units were 0.3%. This data suggests that the demand and preference for larger rental units are on the rise in White Rock. This aligns with demographic data – studio units are typically too small and not appropriate for seniors; family households priced out of the homeownership market require enough bedrooms in a rental unit to accommodate all members of their household.

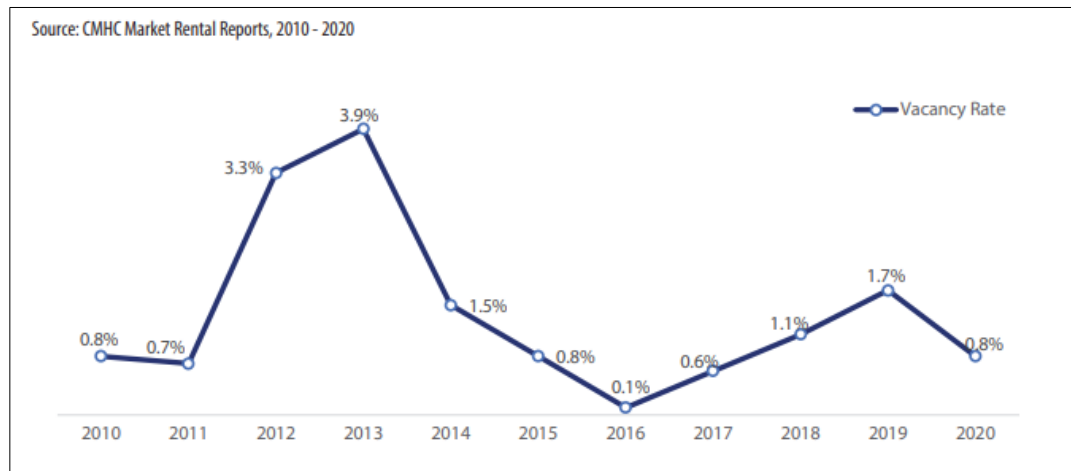


Figure 9 - Rental Vacancy Rate Over Time, White Rock

There has not been an increase in the number of purpose-built rental units in White Rock over the past decade (see figure 10). In 2010, there were 1,402 rental units compared to 1,394 in 2020. The fluctuation in the total number of rental units can result from several factors, such as new units coming online. At the same time, some sites are demolished and redeveloped, and possibly some rental conversions. The key takeaway is that despite new housing projects being built in White Rock, there has not been a net gain in available rental units. This gives fewer choices to renters and is likely a key contributor to the tightening rental vacancy rate in White Rock. It might also explain the incidence of overcrowding (affecting 10% of renters) when there are not enough bedrooms to accommodate everyone in a household.

The purpose-built rental stock is augmented by privately owned units in condos and basement suites that are rented. These are not purpose-built rental units and are less secure for tenants. However, these units provide an alternative. In 2017, approximately 888 condos and between 1,400 and 1,600 secondary suites were available for rent. However, concerns raised by the public and stakeholders during consultation on the Housing Needs Report suggest that several condo stratas are age-restricted (e.g., seniors-oriented), which means not all the secondary rental market is available to renters who need them.

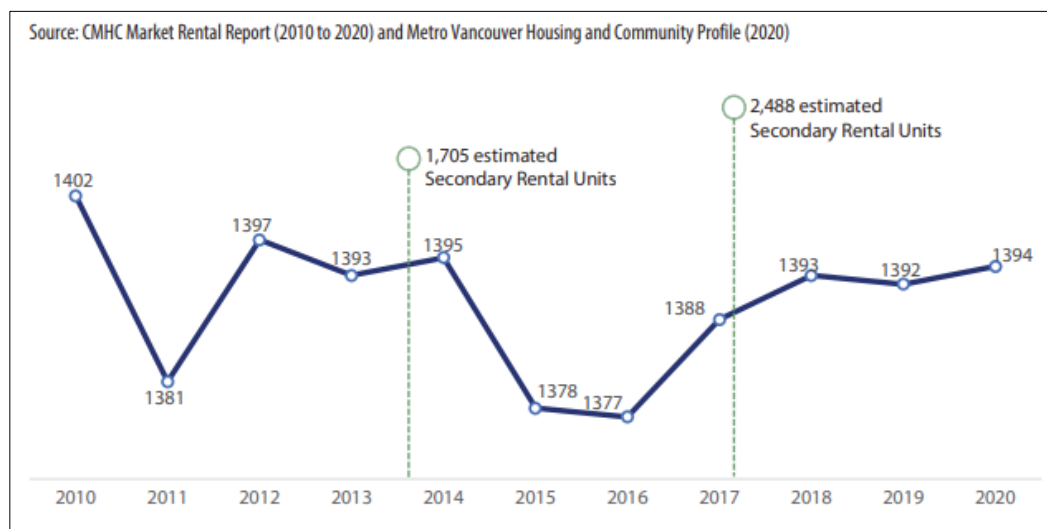


Figure 10- Purpose-Built Rental Units Over Time, White Rock

White Rock Zoning Bylaw

The properties subject to this rezoning application are currently zoned One-Unit Residential Zone (RS-1) (applicable to: 14937 Thrift Avenue & 1441 Vidal Street), Two-Unit (Duplex) Residential Zone (RT-1) (1443-45 Vidal Street), and CD-36 Comprehensive Development Zone (1465 Vidal Street) (see Figure 11).



Figure 11 - Zoning Context

The Figure illustrates the presence of RM-2 zoning east and several property-specific CD zones to the north and west. The maximum height in the RM-2 Zone is 10.7 metres or approximately three-storeys. The CD zones immediately west of the property enable townhome buildings of up to three storeys, with a pitched roof (~11-12 metres).

The proposal, if approved, would change the property's zoning to a site-specific Comprehensive Development (CD) Zone. The CD Zone would establish standards to realize the building massing, form and character as illustrated in Figures 4-7; Appendix B to this report includes several pages from the architectural design packages submitted with the revised proposal.

The current 70-unit proposal would be supported by 123 parking spaces provided within a three-storey below-grade parkade. The parking supply satisfies the zoning bylaw's requirements for residents and visitors and includes the required barrier-free parking. The zoning bylaw currently only requires 105 stalls for a development of this size. Therefore, the proposal exceeds the parking requirements. Ten percent (10%) of the stalls would be provided with charging infrastructure for an electric vehicle, and another ten percent (10%) would have a rough-in for future electric vehicle use.

Overall, this development is intended to have minimal traffic impacts. The estimated trip generation is not enough to require any intersection upgrades. The original traffic study was completed for 129 units (Appendix C) and this number has been reduced to 70 units. Because of the decrease in units staff did not request an updated traffic impact study. If Council would like further information this can be brought forward before third reading at the public hearing.

Advisory Design Panel (ADP)

This development has gone to ADP several times a summary of these meetings is in Appendix A - Corporate Report – November 22, 2021: Initial Review (Revised Submission) 1441, 1443-45,

and 1465 Vidal Street, and 14973 Thrift Avenue, Zoning Bylaw Amendment and Major Development Permit (File No. 19-01).

Staff have reviewed and compared the application changes to the previous plans and the ADP roles and mandate. In the review of development permits applications, the panel's mandate is to consider the following:

- *The alignment with applicable policies of the Official Community Plan and the Development Permit Area Guidelines:*
 - A local government may designate certain lands as Development Permit Areas (DPA) within an Official Community Plan (OCP). The Form and Character DPA controls the form and character objectives for commercial, industrial and residential development (e.g. landscaping, siting, exterior design).
 - The form, character, appearance and landscaping are important to make a place attractive and livable.
 - Staff have concluded through the review that the application still meets the intent of the guidelines, the density has been reduced, but the overall form and characteristics of the development have not changed. However, overall massing has changed in the following ways:
 - Building massing:
 - Reduced building massing significantly reduced from what is allowable under the OCP. The increase in height is now only proposed along the East side of Vidal street, where an additional dedication has been required for the future roadway widening. The façade of the building facing the adjacent townhouses to the west has not increased.
 - Corridor length:
 - Corridor length has been reduced by approximately 12' at the lower levels. Due to this the building length being reduced and greenspace being added to the southern portion of the site.
 - Views
 - The views of surrounding buildings are obstructed by trees, not the building, even with the height changes along the east side of Vidal.
 - The building's height is shorter than the existing old-growth trees surrounding the site. Therefore, no views will be obstructed by the building that isn't already obscured by the existing foliage.
 - Views have not been further obstructed on the west side of the site.
- *Form and character impacts that may arise out of a request for relief from a zoning bylaw standard(s) (e.g., reduction in yard setbacks, additional building height, etc.):*
 - There is no request for relief from zoning bylaw standards.
- *The intended function of the project and how the development fits within the neighbourhood context (e.g., urban design, site design, compatibility of built form, the potential for land use impacts such as shadowing, insufficient parking, negative impacts to traffic volumes, etc.):*

- Urban design has not changed significantly. The proposed parking meets the requirements of our zoning bylaw and has been verified by staff.
- The development meets the intent of the form and character guidelines.
- Traffic volumes will be reduced due to the reduction in units, and parking exceeds the zoning requirements.
- *The overall quality of building and site design considering:*
 - *the livability of the project for future occupants/site users, including specific regard for public safety (CPTED) and accessibility;*
 - The applicant included these features in their design. No significant changes have been made to the current submission that would affect this.
 - *the constructability of the design with regard to the potential impact on building longevity and cost (affordability for future owners/renters);*
 - No further changes are needed.
 - See the report section on Purpose Built Rental.
 - *the environmental sustainability of the design considering efforts for stormwater; retention, passive solar gain, electric vehicle use, and other measures;*
 - See the Climate Change Implications section of the report.
 - *the way in which the building design interacts with, and positively contributes to, the public realm (e.g., interface of the building with the street, landscaping treatment, pedestrian connections, variability in design and massing, etc.);*
 - The project focuses on providing open and engaging spaces for public life, enhancing the character of the built environment and public realm. The addition of an urban park for the community will benefit the City and its residents.
 - *the impact of the siting of buildings and structures, as well as other site features (e.g., driveways, impermeable amenity spaces, etc.) on protected trees, both private and public, and the ability to avoid tree removals and/or support tree plantings through the design of the building(s) and the layout of the site;*
 - The tree protection plan has not changed significantly. See the implications for Tree Preservation and Tree Canopy Enhancement section.
 - *the potential for conflicts with other municipal bylaws where such conflicts may affect the overall form and character of the development or the way in which the project upholds the policy objectives of the Official Community Plan.*
 - There are no known conflicts with City Bylaws. The development does align with the OCP, and no OCP amendment is needed.

Based on the above information and comments, staff concluded that the changes made to the current submission are not enough to be sent back to the ADP for further comments. The revised current submission meets the intent of the design guidelines, and it aligns with the mandate of the ADP. Council may choose to direct this application back to the ADP should Council deem a further review necessary.

FINANCIAL IMPLICATIONS

The following fees would be collected if approved for rezoning and subdivision (Table 2). Note these fees are subject to change:

Table 2: Applicable Development Costs

	Fee (per unit)	Units Subject to Fee	Sub-Total
City of White Rock Development Cost Charges (DCCs)	\$11,253.3	70	\$787,731.00
Metro Vancouver (Regional) DCCs	\$4,269.00	70	\$298,830.00
TransLink DCCs	\$1,554.00	70	\$108,780.00
Surrey School District School Site Acquisition Charges (SSAC)	\$800.00	70	\$ 56,000.00
Total			\$1,251,341.00

This development does not meet the threshold for Community Amenity Contributions.

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

The original proposal was presented to the public through a Public Information Meeting (PIM) held on August 20, 2020. Approximately 40 people attended the PIM. Since this meeting, staff have maintained regular contact with several residents who have expressed an ongoing interest in the project. If the project proceeds to the next step in the approvals process, the public would have an opportunity to express their views direct to Council through a statutory public hearing.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The application and revisions made to the project have been reviewed by city staff from several municipal departments. Before presenting the project to the City's Advisory Design Panel, the original design underwent several rounds of review and revision to ensure it satisfies municipal bylaw standards and the directions provided in the City's DPA Guidelines.

CLIMATE CHANGE IMPLICATIONS

The properties subject to this application are currently vacant. The lands sit immediately south of a 12-storey condominium building and fall within a neighbourhood predominantly comprised of three-storey apartments. The four-storey proposal is, in the opinion of staff, appropriately scaled to the context of development and will allow for the more intensive use of lands that are readily served by municipal infrastructure (e.g., roads, water, sewer, etc.). In addition, compatible development, through infill, lessens the need for sprawl into the periphery on lands which may be more appropriately left as undeveloped, naturalized spaces. Further, bringing residential use into areas within walking distance of commercial and recreational uses lessens the demand for private automobile use, a known contributor to climate change.

IMPLICATIONS FOR TREE PRESERVATION AND TREE CANOPY ENHANCEMENT

The Arborist Report prepared by Van Der Zalm (VDZ) and Associates has undergone several iterations of review and revision since the application was received in 2019. Staff have been working with the applicant to ensure the design of the building, specifically the parkade, allows

for the most significant level of tree retention. This is particularly important along the western and northern boundaries of the Subject Properties, where several mature trees exist.

In response to the City's comments, the applicant has created notches in the parkade to avoid the tree protection zones associated with several large off-site (OS) trees. Furthermore, the applicant has reduced the footprint of the building along its northern façade to provide greater accommodation to several off-site trees falling within the property tied to the Beverley development. Trees for which retention is proposed would require the posting of securities in accordance with the City's Tree Management Bylaw. Trees proposed for removal would be subject to replacement requirements. Where replacements are not feasible, cash-in-lieu of such may be considered by the City.

In summary, \$117,000 in securities (held for tree retention) and 27 replacement trees (\$40,500 value) would be required. The most recent Landscape Plan illustrates 24 replacement trees, plus 11 trees proposed for the fourth storey (rooftop amenity areas). If the project were to proceed, staff would work with the applicant and their Landscape Architect to ensure tree species and required spacing, amongst other matters, were addressed to the satisfaction of the city. If Council gives the first and second reading, the tree locations will be legally surveyed as per our bylaw, and any revision will be made before the third reading.

ALIGNMENT WITH STRATEGIC PRIORITIES

Council has expressed a desire to support a high quality of life in the city. The ability to support residential infill can help lessen the demand for sprawl while also making the best use of existing infrastructure.

OPTIONS / RISKS / ALTERNATIVES

The following options for Council's consideration are:

1. Give first and second readings to "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439"; or
2. Reject "White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439".; or
3. Defer consideration of ""White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439" pending further information to be identified.

Staff recommends Option 1, which is incorporated into the recommendations at the beginning of this corporate report.

CONCLUSION

This application proposes to rezone four properties from the "RS-1 One Unit Residential Zone" to the "Comprehensive Development Zone (CD) 68". If approved, it would enable the proposed multi-building residential project that consists of 70 purpose built rental apartments ranging from 1 to 3-bedroom units to be built. If Council supports this application staff will bring the

development permit forward at third reading. Overall, staff supports the proposal subject to the recommended conditions noted.

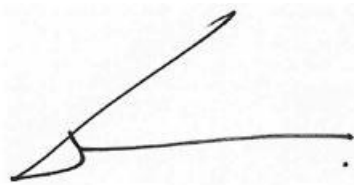
Respectfully submitted,

A handwritten signature in black ink, appearing to read 'Anne Berry', with a stylized, cursive script.

Anne Berry
Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, appearing to read 'Guillermo Ferrero', with a stylized, cursive script.

Guillermo Ferrero
Chief Administrative Officer

- Appendix A: 1441 Vidal - Report to Council - November 22, 2021
- Appendix B: Zoning Amendment Bylaw No. 2439
- Appendix C: Traffic Impact Study
- Appendix D: Arborist Report

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: November 22, 2021

TO: Land Use and Planning Committee

FROM: Greg Newman, Acting Director, Planning and Development Services

SUBJECT: Initial Review (Revised Submission) 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue, Zoning Bylaw Amendment and Major Development Permit (File No. 19-011)

RECOMMENDATIONS

THAT the Land Use and Planning Committee recommends that Council:

1. Direct staff to advance the Zoning Bylaw Amendment Application at 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue, to the next stage in the application review process; and
2. Update the on-site development signage to reflect the revised development proposal as described in the corporate report titled "Initial Review (Revised Submission) 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue, Zoning Bylaw Amendment and Major Development Permit (File No. 19-011)."

EXECUTIVE SUMMARY

In July, 2019, the City received applications for Zoning Bylaw Amendment and a Major Development Permit tied to the properties at 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue. The original proposal included a six-storey 129-unit apartment building which was intended to be rental in tenure. Over the past 24 months the project has undergone a series of changes in response to City-initiated amendments to the Official Community Plan and to address feedback received from the Advisory Design Panel. The project now presents a four-storey 82-unit rental apartment building. The original proposal was presented to the Land Use and Planning Committee (LUPC) on October 19, 2020. At that time, the Committee was supportive of seeing the applications continue through the normal review process. Staff are bringing this proposal back to LUPC to receive direction regarding the overall, revised, scope of the project.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
Motion No. 2020-LU/P-036 October 19, 2020	THAT the Land Use and Planning Committee recommends that Council resolve that the zoning amendment application at 1441 Vidal Street proceed to the next stage in the application review process.

INTRODUCTION/BACKGROUND

Applications for Zoning Bylaw Amendment and a Major Development Permit were received by the City of White Rock in July, 2019. The properties subject to these applications include 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue (‘Properties’) (see Figure 1).



Figure 1: Subject Properties

The Properties are situated within a block largely comprised of multi-family dwellings. Immediately north of the site is the 12-storey “Beverley” building (1501 Vidal Street). To the east there are several two and three-storey apartment buildings and to the west there are several blocks of three-storey townhomes (see Site Photos in Appendix A).

Official Community Plan

The Properties fall within the “Town Centre Transition” designation per Schedule A to the Official Community Plan (OCP). Within this designation, the Plan permits multi-unit residential uses intended to support the commercial uses in the Town Centre. When the original proposal was made in 2019, the policies of the OCP enabled maximum height of approximately 18 storeys along North Bluff Road transitioning down to four storeys along Thrift Avenue (see Figure 2).

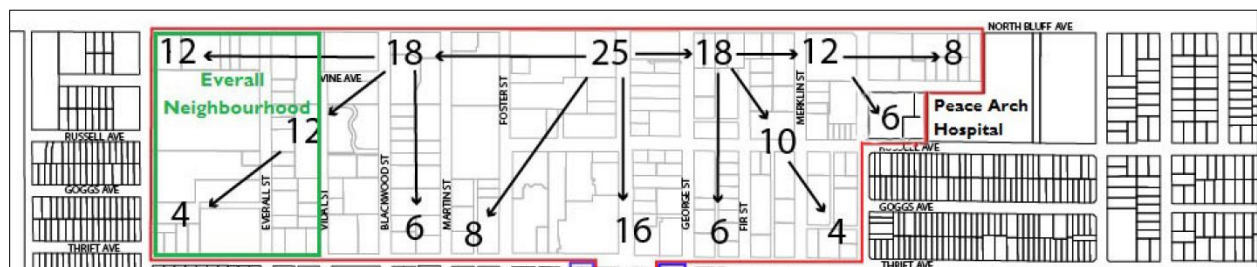


Figure 2: 2019 OCP (Figure 10 “Conceptual Height Transitions”)

The original proposal presented a six-storey building that stepped down as it approached Thrift Avenue (see Figure 3). The height of the project was compliant with the policies of the OCP therefore an amendment was not required. The design had a floor area ratio (FAR) of 2.1 which was enabled by the policies of the Plan. Specifically, the Plan set a base density of 1.5 FAR and Policy 8.2.3 allowed for a 40 percent increase in density, to 2.1 FAR, where at least half of the additional floor area was secured as rental units. The entire 129 unit project was proposed as rental in tenure and therefore benefited from the density bonus.



Figure 3: Original Proposal, July, 2019 (File No. 19-011)

In 2021, the OCP was amended to limit height within the Town Centre Transition area. As it relates to the Properties, height was reduced to four storeys with the opportunity to support six storeys if an “affordable housing component” was included. Policy amendments were also made to lower the base density available within the area to 1.5 FAR, with the ability to increase this density to 2.5 FAR with an affordable housing component, and 2.8 FAR with both an affordable housing component and replacement units. Although the amended OCP allows for consideration of a six-storey building, the Applicant has held their revised proposal to a four-storey building with 82 units, all of which would be rental in tenure (see Figure 4).



Figure 4: Revised Proposal, October, 2021 (File No. 19-011)

The revised proposal includes 17 three-bedroom units (20.7%), 17 two-bedroom units (20.7%), 38 one-bedroom units (46.4%) and 10 studio units (12.2%). The allocation of two and three-bedroom units is supportive of the City’s “family-friendly housing” policy which seeks a minimum 10% supply.

White Rock Zoning Bylaw

The properties subject to this rezoning application are currently zoned One-Unit Residential Zone (RS-1) (applicable to: 14937 Thrift Avenue & 1441 Vidal Street), Two-Unit (Duplex) Residential Zone (RT-1) (1443-45 Vidal Street), and CD-36 Comprehensive Development Zone (1465 Vidal Street) (see Figure 5). The CD-36 Zone allows for a ten-unit apartment use in addition to a retail service group 1 (commercial) use. The commercial use would be limited to the first and second storeys of a potential four storey building.



Figure 5: Zoning Context

The Figure illustrates the presence of RM-2 zoning to the east and several property-specific CD zones to the north and west. The maximum height in the RM-2 Zone is 10.7 metres or approximately three-storeys. The CD zones immediately west of the property enable townhome buildings of up to three storeys, with a pitched roof (~11-12 metres). The Beverley building north of the Properties permits maximum height of 37 metres or roughly 12 storeys.

The proposal, if approved, would change the zoning of the property to a site-specific Comprehensive Development (CD) Zone. The CD Zone would establish standards used to realize the building massing, form and character as illustrated in Figure 4; Appendix B to this report includes several pages from the architectural design packages submitted with the revised proposal. The current 82-unit proposal would be supported by 123 parking spaces, provided within a three-storey below-grade parkade. The supply of parking satisfies the requirements of the zoning bylaw for residents and visitors and includes the required barrier free parking. Ten percent (10%) of the stalls would be provided charging infrastructure for an electric vehicle and another ten percent (10%) would have a rough-in for future electric vehicle use.

Advisory Design Panel Review

The original six-storey project was presented to the Advisory Design Panel on October 20, 2020, and May 18, 2021. Table 1 that follows summarizes some of the main points raised during these two meetings and the Applicant's response to the points; more detail regarding the Panel's feedback can be found within the meeting minutes available on the City's webpage ([direct link](#)).

Table 1: Advisory Design Panel Comments from October 20, 2020 and May 18, 2021 Meetings

Comments from the Panel	Applicant's Response
Potential impacts to trees, particularly those immediately north of the Properties.	North face of the building moved south to lessen potential impacts to trees. Parkade "notched" to allow for tree retention along western limit of property.
Structural reliability of the design is unclear. Related concerns noted about the ability of the building to support rooftop plantings.	Architect referenced options (e.g., structural concrete columns, stone clad architectural columns, composite metal panel frames, etc.) to support design as presented.
Horizontal scale of the building (travel distances for tenants) too long.	Additional stepping of the building introduced along with introduction of architectural features that create visual breaks in massing (see Figure 6).
Need for higher number of electric vehicle charging stations.	Applicant committed to 100 percent rough-in with original proposal; since reduced to minimum required.
Amount of hardscaping may have negative impacts to stormwater management.	Applicant referenced inclusion of plantings and other landscaping elements to aid in stormwater retention.
Uncertainty about tenure of building.	Rental tenure to be secured through zoning controls.
Accessible design (units).	All units have wheelchair access. Fourteen (14) adaptable units with larger doorways / corridors.
Limited regard for rooftop storage (amenity materials) and mechanical units.	Introduction of storage spaces and spaces for mechanical units, the latter of which will be confirmed with the retention of a mechanical engineer at the building permit stage of design.



Figure 6: Rendering Illustrating Stepping in Building Design moving South towards Thrift Avenue

The motion carried by the Panel during their October 20, 2020 meeting was:

“THAT the Advisory Design Panel recommends that staff work with the applicant to further the design of their project based on the comments received by the Panel and summarized in the October 20, 2020 meeting minutes and that it be brought back to the Panel for further review.”

The motion carried by the Panel on May 18, 2020 was:

“THAT the project be conditionally accepted subject to addressing the comments provided by the Panel during the meeting.”

On March 29, 2021 the Land Use and Planning Committee passed Motion LU/P-038, being:

“THAT the Land Use and Planning Committee endorse in relation to Town Centre Transition area Option C as noted in the March 8, 2021 corporate report, with an amendment noting four (4) to six (6) stories where it is defined that along North Bluff on the east or west side permit six (6) stories; and

For the remaining sites it be noted as four (4) stories to six (6) stories with a notation that proposals over four (4) stores would be considered when there is an affordable housing component.”

The motion above was followed by formal amendments to the OCP which were ultimately adopted by Council on June 28, 2021. The amendments had the effect of reducing maximum height permissions available to the Properties to four storeys, or six storeys with the inclusion of an “affordable housing component.” In anticipation of the adoption of the OCP amendments, the Applicant revised their proposal to a four-storey rental building. This proposal was presented to the ADP on June 15, 2021 and September 21, 2021. Table 2 below outlines the main points raised during these two meetings and the Applicant’s response.

Table 2: Advisory Design Panel Comments from June 15, 2021 and September 21, 2021 Meetings

Comments from the Panel	Applicant’s Response
Incorporation of “west coast design” unclear; desire to see more natural materials (less hardy board and aluminum) incorporated into the project.	Natural materials including stone, heavy timber, wood accents and rich natural tones have been incorporated into the design (see Figure 7). Revised design includes stained lumber for the entry column, natural stone (Westcoast Ledge stone) at the base of the building, and stone (Hearthstone, Black Rundle) at the entry frame (see Appendix B for material summary).
Questions raised about access to the private rooftop amenity spaces and the programming of these spaces.	Private amenity spaces on fourth storey programmed with privacy screens and cedar partitions. Common amenity along the southern limit of the development with furniture and a children’s play space.
Plantings proposed within the northwest corner of the property may suffer due to limited solar exposure.	Decking along north façade shifted to the east and west to allow more natural light to the north. Landscaping changes made to reduce the number, and size (at maturity) of the trees proposed. The trees are now concentrated on the mid-portion of the north elevation allowing solar exposure from the northeast and northwest (see Appendix C for landscape plan).
Central hallway too long. Creates a break between units on the fourth storey and private rooftop amenity, opposite the hallway from the unit entrance. Suggested relocation of the hallway to enable units on fourth storey to be tied directly to amenity.	Design elements proposed to break up the visual appearance of the hallway and to link the unit entrances to the private amenity access. Relocating the hallway itself would result in structural challenges. Looking into options to open up portions of the exterior wall to bring light / visibility to the hallway.
Clarification sought regarding proposed tenure of the building.	Original proposal was rental. Dropping to four storeys the revised project (when presented to the ADP) was proposed as a strata building; the building is now intended to be rental in tenure.

Comments from the Panel	Applicant's Response
Concern with the interface of the development (exposed blank parkade wall) with the dwellings to the southwest.	Revisions made to west façade cladding to break up the massing. Updated with planter boxes and alternating material patterns on the lower stone walls. Updates create visual interest and comfort for pedestrians in accordance with Development Permit Area Guideline (22.6.c). Introduction of evergreen plant materials and cable trellis system with climbing plants to soften transition between project and nearby dwellings.



Figure 7: Rendering Illustrating use of Natural Colours and Wood Element

The motion carried by the Panel during their June 15, 2021 meeting was:

“THAT Panel defers making a recommendation on the project pending the resolution of issues to be listed by the Panel (being the issues raised during this meeting) following which the application be brought back to the Panel and the Applicant speak to how the issues have been addressed.”

The motion carried by the Panel on September 21, 2021 was:

“THAT the Panel deny the application, as presented.”

The Advisory Design Panel’s review of the original and revised proposals led to changes which have undoubtedly benefited the project. Although the Panel has recommended that the project be denied, staff are of the opinion that the design of the project is consistent with the broad policy objectives of the Official Community Plan and the more explicit design direction provided by the Multi-Family Development Permit Area Guidelines, found in Section 22.6 of the Plan. The project will bring much-needed rental tenure housing to the City and, at four-storeys, is considered compatible in scale to that of adjacent development. Technical matters relating to servicing, access,

tree retention, and building code compliance will be further vetted through subsequent permitting processes.

FINANCIAL IMPLICATIONS

Municipal Development Cost Charges (DCCs) in the amount of \$922,770 would be required for the 82-unit rental apartment with the potential for credits to be applied for DCCs that may have been paid when the now-demolished single-family homes were constructed; this will be confirmed upon receipt of a building permit application. Community Amenity Contributions would not be applicable to the project as it does not meet the eligibility criteria (i.e., exceeding 1.5 FAR), per the City of White Rock Density Bonus / Amenity Contribution Policy (No. 511).

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

The original proposal was presented to the public through a Public Information Meeting (PIM) held August 20, 2020. Approximately 40 people attended the PIM. Since this meeting, staff have maintained regular contact with several residents who have expressed an on-going interest in the project. If the project proceeds to the next step in the approvals process, the public would have an opportunity to express their views direct to Council through a statutory public hearing.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The application and revisions made to the project have been reviewed by City staff from several municipal departments. Prior to presenting the project to the City's Advisory Design Panel, the original design underwent several rounds of review and revision to ensure it satisfies municipal bylaw standards and the aforementioned direction provided in the City's DPA Guidelines.

CLIMATE CHANGE IMPLICATIONS

The Properties subject to this application are currently vacant. The lands sit immediately south of a 12-storey condominium building and fall within a neighbourhood predominantly comprised of three-storey apartments. The four-storey proposal is, in the opinion of staff, appropriately scaled to the context of development and will allow for the more intensive use of lands that are readily served by municipal infrastructure (e.g., roads, water, sewer, etc.). Compatible development, through infill, lessens the need for sprawl into the periphery, on lands which may be more appropriately left as undeveloped, naturalized spaces. Further, bringing residential use into areas that are within walking distance of commercial and recreational uses lessens the demand for private automobile use, which is a known contributor to climate change.

IMPLICATIONS FOR TREE PRESERVATION AND CANOPY ENHANCEMENT

The original application was supported with an Arborist Report prepared by Van Der Zalm (VDZ) and Associates. The Report has undergone several iterations of review, and revision, since the application was received in 2019. Staff have been working with the Applicant to ensure the design of building, and more specifically the parkade, allows for the greatest level of tree retention. This is particularly important along the western and northern boundaries of the Subject Properties, where several mature trees exist (see Page "L-02" in Appendix C).

In response to the City's comments the Applicant has created notches in the parkade to avoid the tree protection zones associated with several large off-site (OS) trees. Furthermore, the Applicant has reduced the footprint of the building along its northern façade to provide greater accommodation to several off-site trees falling within the property tied to the Beverley development. Appendix D includes

the tree assessment data (inventory) prepared by VDZ and Associates. Trees for which retention is proposed would require the posting of securities in accordance with the City's Tree Management Bylaw. Trees proposed for removal, would be subject to replacement requirements. Where replacements are not feasible, cash-in-lieu of such may be considered by the City. In summary, \$117,000 in securities (held for tree retention) and a total of 27 replacement trees (\$40,500 value) would be required. The most recent Landscape Plan, dated October 15, 2021, illustrates a total of 24 replacement trees, plus 11 trees proposed for the fourth storey (rooftop amenity areas). If the project were to proceed, staff would work with the Applicant and their Landscape Architect to ensure tree species and required spacing, amongst other matters, were addressed to the satisfaction of the City.

ALIGNMENT WITH STRATEGIC PRIORITIES

The proposal is generally aligned with the Corporate Vision established as part of Council's Strategic Priorities, particularly with respect to protecting the environment, and supporting a community where people can live, work and play in an enjoyable atmosphere.

OPTIONS / RISKS / ALTERNATIVES

The following options are available for Council's consideration:

1. LUPC may direct staff to obtain additional project-specific information prior to deciding whether to advance or deny the Application;
2. LUPC may deny the Application; or
3. LUPC may direct the Application to proceed to the next stage in the process and give additional direction on any additional focus or scrutiny during the review process.

CONCLUSION

Applications for zoning bylaw amendment and a major development permit were received in 2019 to support the development of a six-storey, now four-storey, rental apartment building at 1441, 1443-45, and 1465 Vidal Street, and 14937 Thrift Avenue. The project has undergone an iterative review by City staff and the Advisory Design Panel. Staff believe the revised, rental tenure, proposal warrants further consideration through the municipal review process and an opportunity for more formal review during a future public hearing.

Respectfully submitted,

Greg Newman, MCIP, RPP
Acting Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

Guillermo Ferrero
Chief Administrative Officer

Appendix A: Site Photos
Appendix B: Architectural Designs (select pages)
Appendix C: Landscape Designs (select pages)
Appendix D: Tree Inventory

**The Corporation of the
CITY OF WHITE ROCK
BYLAW No. 2439**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled ENACTS as follows:

1. THAT Schedule C of the *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended by rezoning the following lands:

Lot 8 Section 10 Township 1 New Westminster District Plan NWP13684
PID: 007-208-677
(1441 Vidal Street)

Lot 2 Section 10 Township 1 New Westminster District Plan NWS2236
PID: 001-267-761
(1443 Vidal Street)

Lot 1 Section 9 Township 1 New Westminster District Plan NWS2236
PID: 001-267-744
(1445 Vidal Street)

Lot 1 Section 10 Township 1 New Westminster District Plan EPP46879
PID: 029-484-413
(1465 Vidal Street)

Lot 41 Section 10 Township 1 New Westminster District Plan NWP35379
PID: 007-223-480
(14937 Thrift Avenue)

as shown on Schedule "1" attached hereto, from the 'RS-1 One Unit Residential Zone' and the 'RT-1 Two Unit (Duplex) Residential Zone' and the 'CD-32 Comprehensive Zone' to the 'CD-68 Comprehensive Development Zone (Thrift/Vidal Street).'

2. THAT *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended:

(1) by adding to the Table of Contents for ‘Schedule B (Comprehensive Development Zones)’, Section 7.68 CD-68 Comprehensive Development Zone’, and

(2) by adding the attached Schedule “2” to ‘Schedule B (Comprehensive Development Zones)’ Section 7.68 CD-68 Comprehensive Development Zone’.

3. This bylaw may be cited for all purposes as “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439*”.

Public Information Meeting held this 20th day of August, 2020

Read a first time this day of , 2022

Read a second time this day of , 2022

Considered at a Public Hearing this day of , 2022

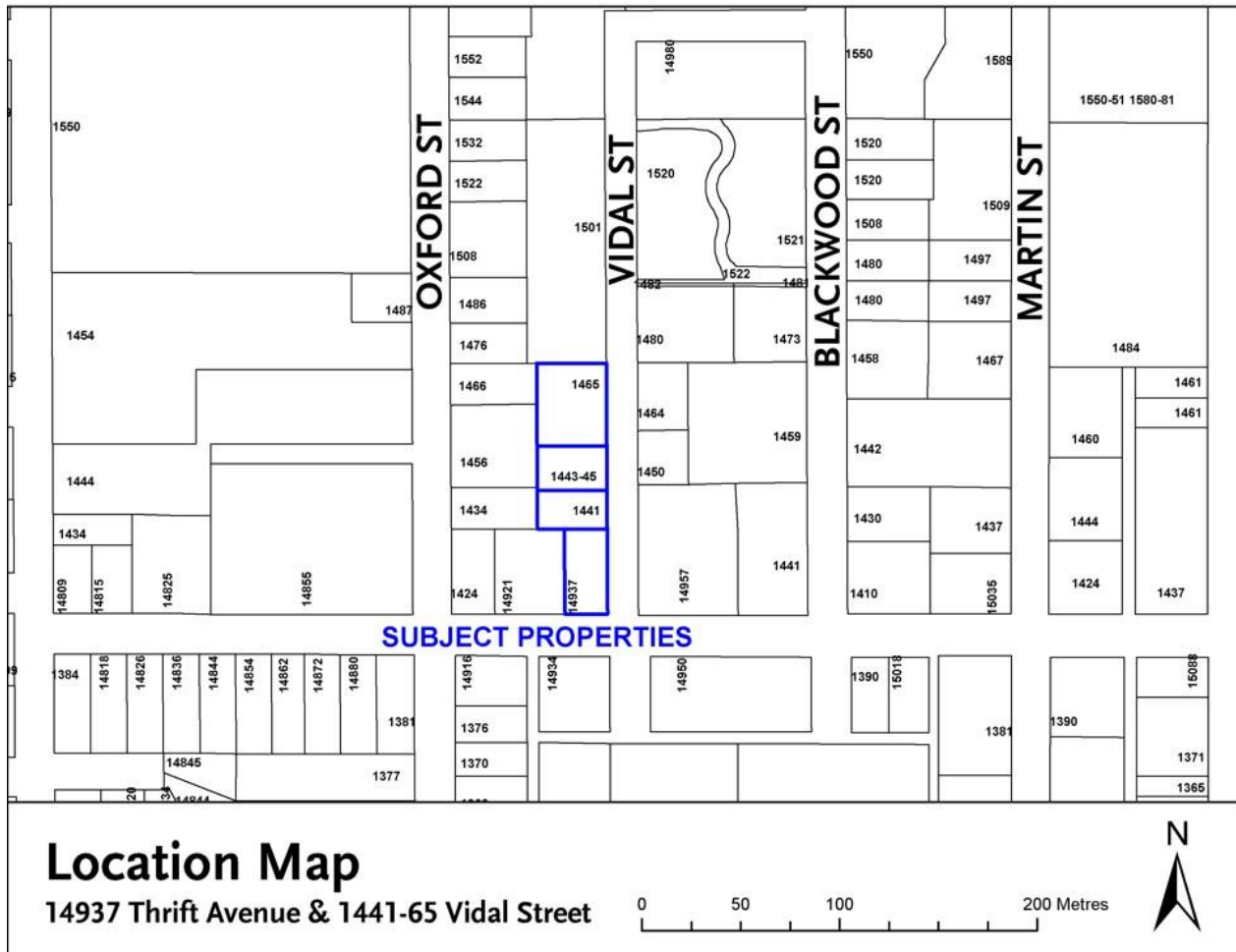
Read a third time this day of , 2022

Adopted this day of , 2022

Mayor

Director of Corporate Administration

Schedule "1"



7.68 CD-68 COMPREHENSIVE DEVELOPMENT ZONE

INTENT

The intent of this zone is to accommodate the development of a multi-unit rental residential building, being four storeys and with 70 dwelling units, on four consolidated sites of approximately 3,875.4 square meters (41,714 square feet or 0.958 acres).

1. Permitted Uses:

- (1) *multi-unit residential use*
- (2) *accessory home occupation* use in accordance with the provisions of section 5.3 and that does not involve clients directly accessing the *principal building*.

2. Lot Coverage:

- (1) The maximum *lot coverage* in the CD-68 zone is 39.6 %.

3. Gross Floor Area and Floor Area Ratio:

- (1) The gross floor area, including amenity areas, shall be no greater than 5,799 square metres (62,425 square feet).
- (2) The maximum density shall not exceed 1.5 times the lot area and the gross floor area ratio shall be no greater than 1.50 FAR

4. Regulations for Size, Shape and Siting of Building and Structures

- (a) The *principal building* for *multi-unit residential use*, inclusive of elevator shafts, stair housing, and all mechanical equipment, shall not exceed a *height* of 20.32 metres (66.66 feet)
- (b) The minimum setbacks for *multi-unit residential uses* are as follows:
 - (i) Setback from interior (north) lot line = 6.83 metres
 - (ii) Setback from exterior (south) lot line = 24.47 metres
 - (iii) Setback from rear (west) lot line = 7.87 metres
 - (iv) Setback from front (east) lot line = 5.27 metres
 - (v) Setback from south interior side lot line = 8.20 metres

5. Parking:

Parking for *multi-unit residential use* shall be provided in accordance with Sections 4.14 and 4.17, with the minimum number of spaces required as follows:

- (a) A minimum of one hundred and twenty-three (123) spaces shall be provided for the *multi-unit residential use*
- (b) A minimum of twenty-one (21) spaces shall be provided for visitors and marked as “visitor”
- (c) A minimum of three (3) of the required one hundred and twenty-three (123) spaces shall be provided as accessible parking spaces and shall be clearly marked as such and shall have a minimum length of 5.5 metres. Of the five accessible parking spaces,

one space shall be provided as a van-accessible loading space with a minimum width of 2.8 metres, and the other four spaces shall have a minimum width of 2.5 metres, provided that the four parking spaces have a shared or non-shared access aisle with a minimum width of 1.5 metres.

- (d) The minimum height clearance at the accessible parking spaces and along the vehicle access and egress routes from the accessible parking spaces must be at least 2.3 metres to accommodate over-height vehicles equipped with a wheelchair lift or ramp.
- (e) A minimum of twelve (12) stalls must have electric charging capacity.
- (f) A minimum of twelve (12) stalls must be roughed for electric charging capacity.

6. Bicycle Parking:

Bicycle parking shall be provided in accordance with Section 4.16, with the minimum number of spaces required as follows:

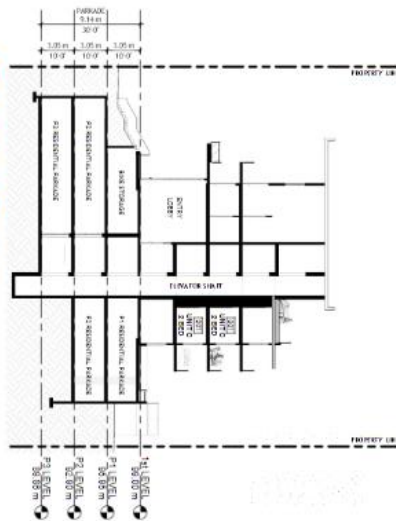
- (a) A minimum of 70 Class I spaces shall be provided
- (b) A minimum of 14 Class II spaces shall be provided

9. Loading:

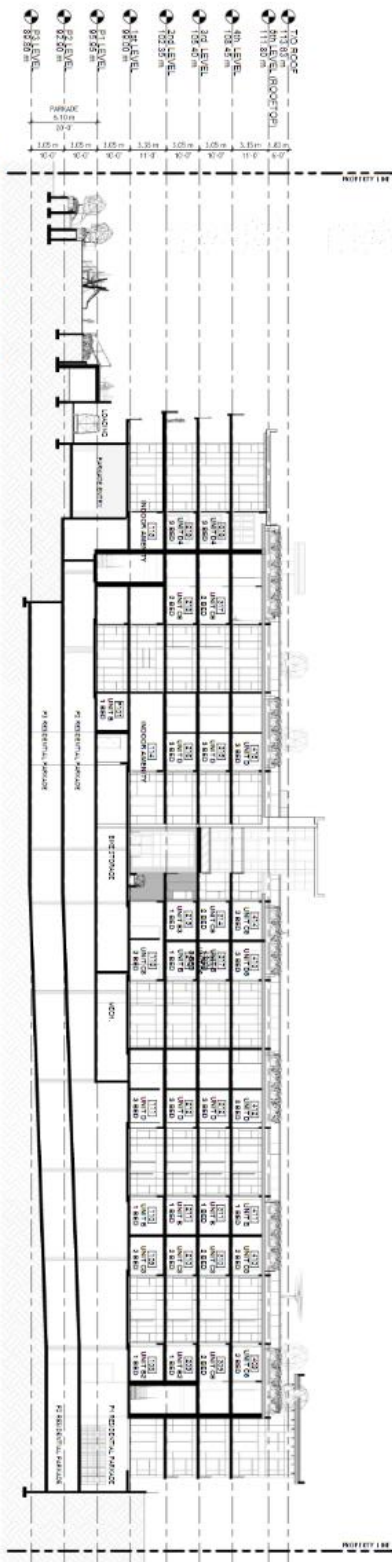
- (a) One loading space shall be provided for a *multi-unit residential use* in accordance with Section 4.15

10. General:

Development in this zone shall substantially conform to the Plans prepared by Keystone Architecture dated October 18, 2021, and titled DP Resubmission that are attached hereto and on file at the City of White Rock.



site section a



site section b









roof deck - plaza & elevator lobby



roof deck - trellis seating area & picnic area



roof deck - green roof & plaza

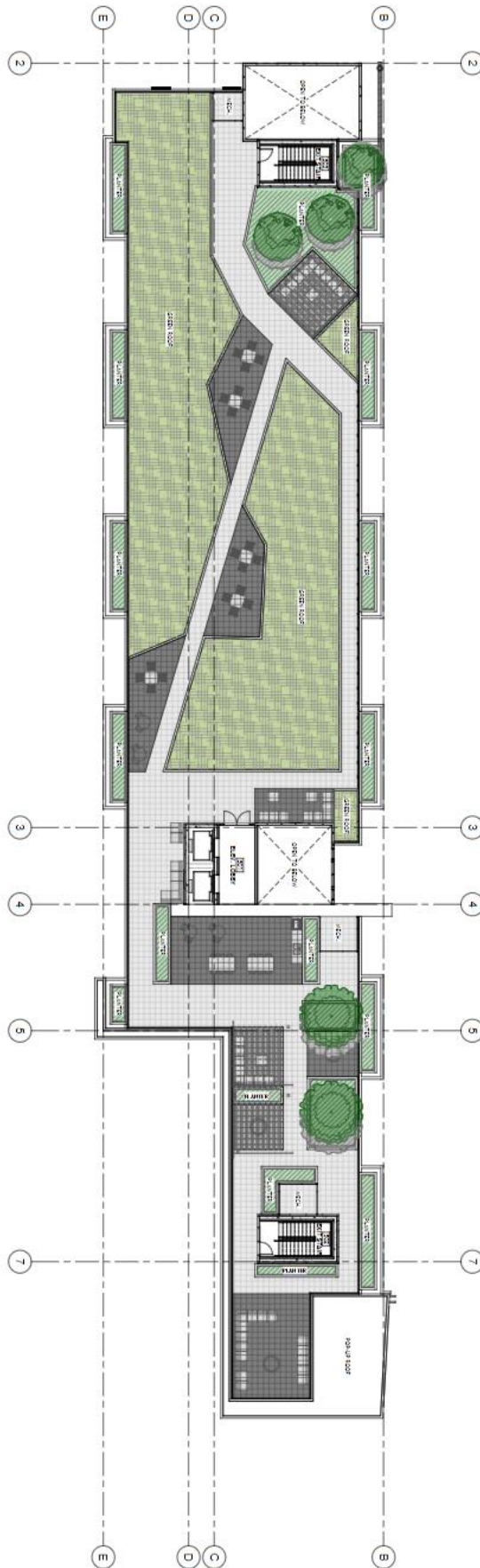


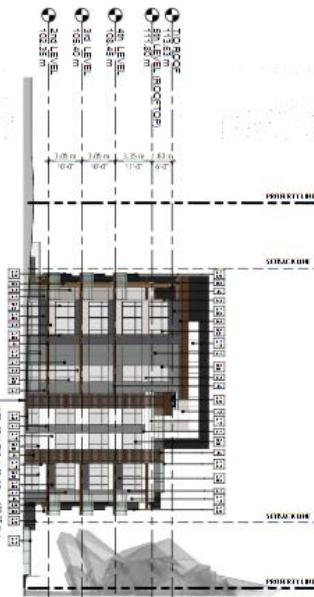
ground level greenspace - street view



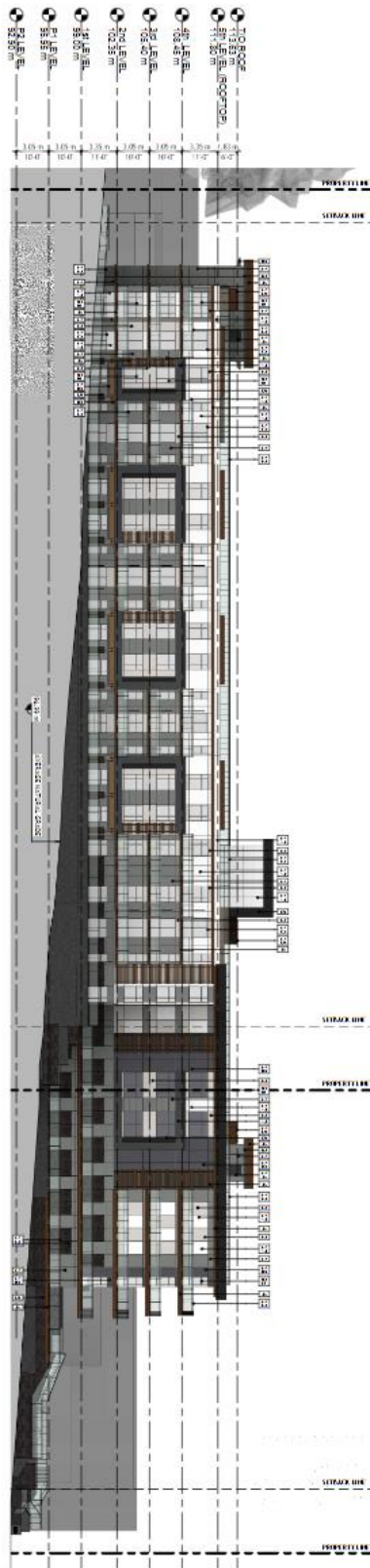
ground level greenspace - aerial view







north elevation (facing highrise)



west elevation (facing townhouses)

[illegible]



east perspective elevation



west perspective elevation



south perspective elevation



north perspective elevation

Traffic Study FINAL - Rev.0

WS Vidal Properties LP
14937 Thrift Avenue Development


June 29, 2020

R.F. BINNIE & ASSOCIATES LTD.

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- Appendix A: Proposed Site Plan
- Appendix B: AutoTURN Site Circulation Analysis
- Appendix C: ITE Trip Generation Reference
- Appendix D: Synchro Analysis Results

1 INTRODUCTION

1.1 Background

R.F. Binnie & Associates Ltd. (Binnie) was retained by WestStone Group (the Developer) to prepare a traffic study for a proposed multi-family residential development in the City of White Rock (the City), BC. The proposed development is located at 14937 Thrift Avenue, as shown in **Figure 1-1**. The proposed site plan of the study development is provided in **Appendix A** and it is expected to provide a total of 129 residential units.

1.2 Study Objectives

The purpose of this study is to review the background conditions within the study area and analyze traffic and parking impacts generated by the proposed development. If any traffic issues are identified, this report will recommend improvements to mitigate them.

The objectives of this traffic study are to make considerations regarding the following items:

- Review the background information available for this study development and the surrounding area;
- Assess pedestrian, cyclist and transit connections and accessibility;
- Examine the City's parking requirements according to the Bylaw and review the on-site vehicle and bicycle parking supply;
- Estimate the site generated traffic volumes from the proposed development and the trip distribution;
- Analyze existing and forecast traffic operations on the study road network with and without site generated traffic volumes;
- Review existing development plan for pedestrian access, vehicle access, bicycle and storage access, and loading provisions;
- Assess the proposed access and internal traffic circulation within the proposed development; and
- Recommend necessary transportation improvements based on the traffic analysis findings.



Figure 1-1: Proposed Development Location

2 EXISTING CONDITIONS

2.1 Site Location

The proposed development is located in White Rock at the northwest corner of the intersection of Thrift Avenue and Vidal Street. The land-use surrounding the development is medium density multi-unit residential (RM-2) to the east, and single-unit residential to the southwest (RS-1 and RS-2). There are also several comprehensive district developments such as “Beverly” at 1501 Vidal Street directly to the north, and “Royce” located to the east at 14855 Thrift Avenue.

Adjacent to the proposed development, Thrift Avenue, Oxford Street, and Martin Street are classified as primary collectors with two lanes and on-street parking. Vidal Street, Vine Avenue, and Blackwood Street are classified as local roads with two lanes and on-street parking. North Bluff Road to the north is classified as an arterial road with four lanes and on-street parking along portions of the south side of the street.

The study development is also located at approximately 900 m to the southwest of Semiahmoo Shopping Centre and White Rock Centre Transit Exchange.

2.2 Adjacent Road Network

2.2.1 Thrift Avenue

Thrift Avenue is a two-lane roadway classified as a primary collector that runs in the east-west direction within the study area. It begins just west of Oxford Street before becoming 14th Avenue at 160th Street. Within the study area, on-street parking is permitted on Thrift Avenue. Thrift Avenue also has a posted speed limit of 50 km/h.

2.2.2 Oxford Street

Oxford Street is a two-lane roadway classified as a primary collector that runs in the north-south direction within the study area. Oxford Street travels from Marine Drive to North Bluff Road, where it becomes 148th Street north of North Bluff Road. Oxford street has a posted speed limit of 50km/h and within the study area, on-street parking is permitted on either side of the road.

2.2.3 Everall Street

Everall Street is a two-lane roadway classified as a local road that runs in the north-south direction within the study area. Everall Street travels between North Bluff Road and Buena Vista Avenue. On-street parking is permitted on either side of the road.

2.2.4 Vidal Street

Vidal Street is a two-lane roadway classified as a local road that runs in the north-south direction within the study area. Vidal Street begins at Vine Avenue and ends just south of Thrift Avenue. On-street parking is permitted on either side of the road.

2.2.5 Blackwood Street

Blackwood Street is a two-lane roadway classified as a local road that runs in the north-south direction within the study area. Blackwood Street begins at North Bluff Road and ends just south of Thrift Avenue. On-street parking is permitted on either side of the road.

2.3 Study Intersections

2.3.1 Thrift Avenue and Oxford Street

The Thrift Avenue and Oxford Street intersection is currently an offset four-legged two-way stop-controlled intersection. The west approach is located slightly north of the east approach. Thrift Avenue is stop-controlled while traffic on Oxford Street is free-flowing. All approaches have a single lane with no dedicated lanes or storage lanes for vehicles turning left or right. Pedestrian crosswalks are currently provided on the north, east, and south approaches of this intersection.

2.3.2 Thrift Avenue and Everall Street

The Thrift Avenue and Everall Street intersection is currently a four-legged two-way stop-controlled intersection. Everall Street is stop-controlled while traffic on Thrift Avenue is free-flowing. All approaches have a single lane with no dedicated lanes or storage lanes for vehicles turning left or right. There is a pedestrian crosswalk with overhead flashing lights on the west approach of this intersection, and a standard pedestrian crosswalk on the north approach.

2.3.3 Thrift Avenue and Vidal Street

The Thrift Avenue and Vidal Street intersection is currently a four-legged two-way stop-controlled intersection. Vidal Street is stop-controlled while traffic on Thrift Avenue is free-flowing. All approaches have a single lane with no dedicated lanes or storage lanes for vehicles turning left or right.

2.3.4 Thrift Avenue and Blackwood Street

The Thrift Avenue and Blackwood Street intersection is currently a four-legged two-way stop-controlled intersection. Blackwood Street is stop-controlled while traffic on Thrift Avenue is free-flowing. All approaches have a single lane with no dedicated lanes or storage lanes for vehicles turning left or right.

The existing laning configurations and traffic controls at the study intersections are shown in **Figure 2-1**.

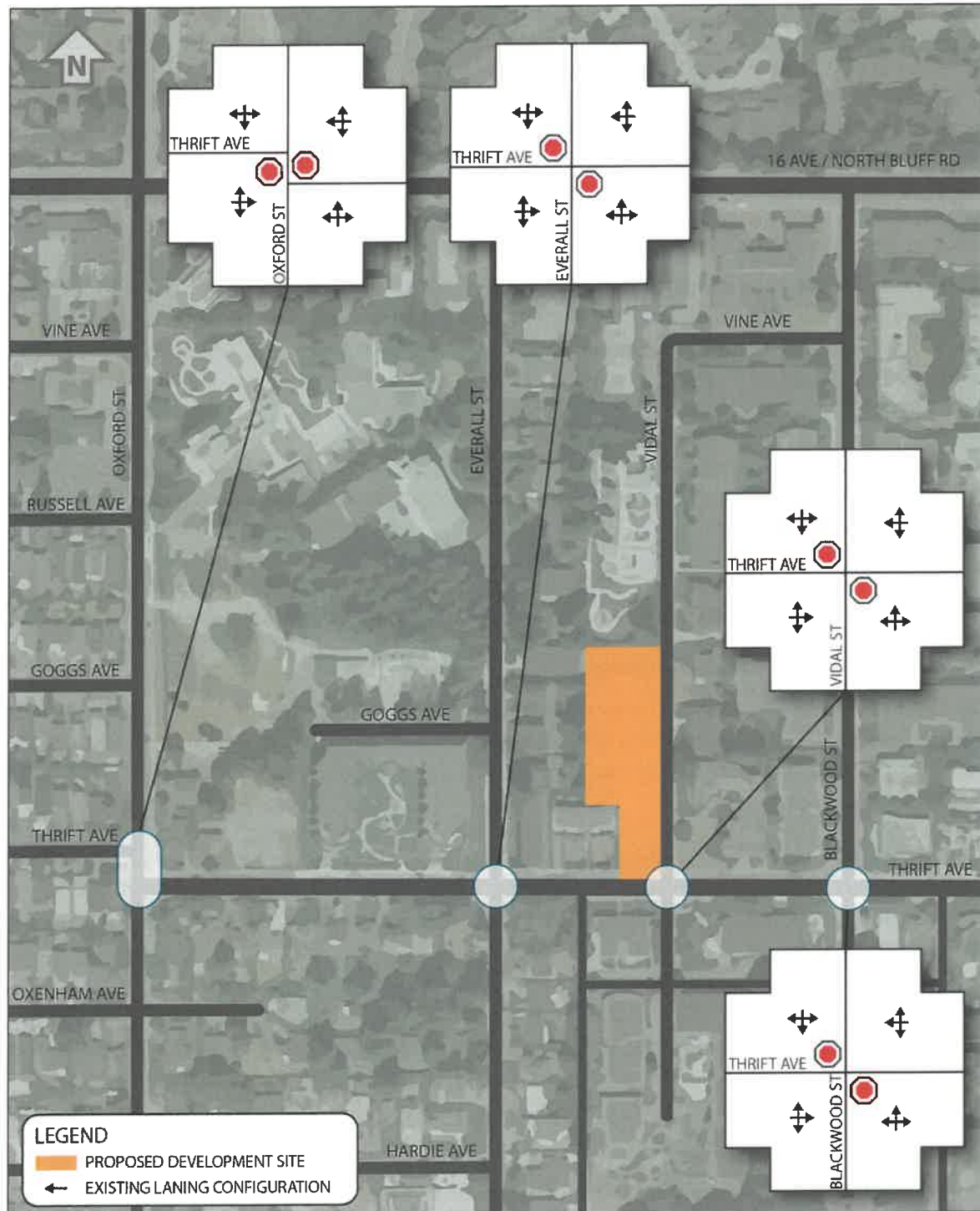


Figure 2-1: Existing Intersection Laning Configuration and Traffic Controls

2.4 Existing Traffic Volumes

Existing traffic volumes for the study intersections along Thrift Avenue were collected by Binnie on May 20, 2019. This included the intersections of Vidal Street at Thrift Avenue, Everall Street at Thrift Avenue, and Blackwood Street at Thrift Avenue. Traffic data for Oxford Street at Thrift Avenue was provided in the background documents from the City. The 2019 existing traffic volumes for the AM peak hour and PM peak hour are summarized in **Figure 2-2**.

2.4.1 Thrift Avenue and Oxford Street

During the AM peak hour, 98 vehicles were observed to be travelling northbound on Oxford Street and 207 were observed to be travelling in the southbound direction. 212 vehicles were observed to be travelling westbound along Thrift Avenue and ten vehicles were observed to be travelling eastbound along Thrift Avenue.

During the PM peak hour, 69 vehicles were observed to be travelling northbound on Oxford Street and 271 were observed to be travelling in the southbound direction. 232 vehicles were observed to be travelling westbound along Thrift Avenue and 14 vehicles were observed to be travelling eastbound along Thrift Avenue.

The predominant turning movements at this intersection were observed to be the southbound left-turn movement and the westbound right-turn movement during both the AM peak hour and PM peak hour.

2.4.2 Thrift Avenue and Everall Street

During the AM peak hour, nine vehicles were observed to be travelling northbound on Everall Street and 24 were observed to be travelling in the southbound direction. 226 vehicles were observed to be travelling westbound along Thrift Avenue and 174 vehicles were observed to be travelling eastbound along Thrift Avenue.

During the PM peak hour, six vehicles were observed to be travelling northbound on Everall Street and 16 were observed to be travelling in the southbound direction. 252 vehicles were observed to be travelling westbound along Thrift Avenue and 170 vehicles were observed to be travelling eastbound along Thrift Avenue.

2.4.3 Thrift Avenue and Vidal Street

During the AM peak hour, 22 vehicles were observed to be travelling northbound on Vidal Street and 24 were observed to be travelling in the southbound direction. 226 vehicles were observed to be travelling westbound along Thrift Avenue and 174 vehicles were observed to be travelling eastbound along Thrift Avenue.

During the PM peak hour, 14 vehicles were observed to be travelling northbound on Vidal Street and 22 were observed to be travelling in the southbound direction. 250 vehicles were observed to be travelling westbound along Thrift Avenue and 178 vehicles were observed to be travelling eastbound along Thrift Avenue.

2.4.4 Thrift Avenue and Blackwood Street

During the AM peak hour, 19 vehicles were observed to be travelling northbound on Blackwood Street and 21 were observed to be travelling in the southbound direction. 203 vehicles were observed to be travelling westbound along Thrift Avenue and 212 vehicles were observed to be travelling eastbound along Thrift Avenue.

During the PM peak hour, two vehicles were observed to be travelling northbound on Blackwood Street and 42 were observed to be travelling in the southbound direction. 252 vehicles were observed to be travelling westbound along Thrift Avenue and 182 vehicles were observed to be travelling eastbound along Thrift Avenue.

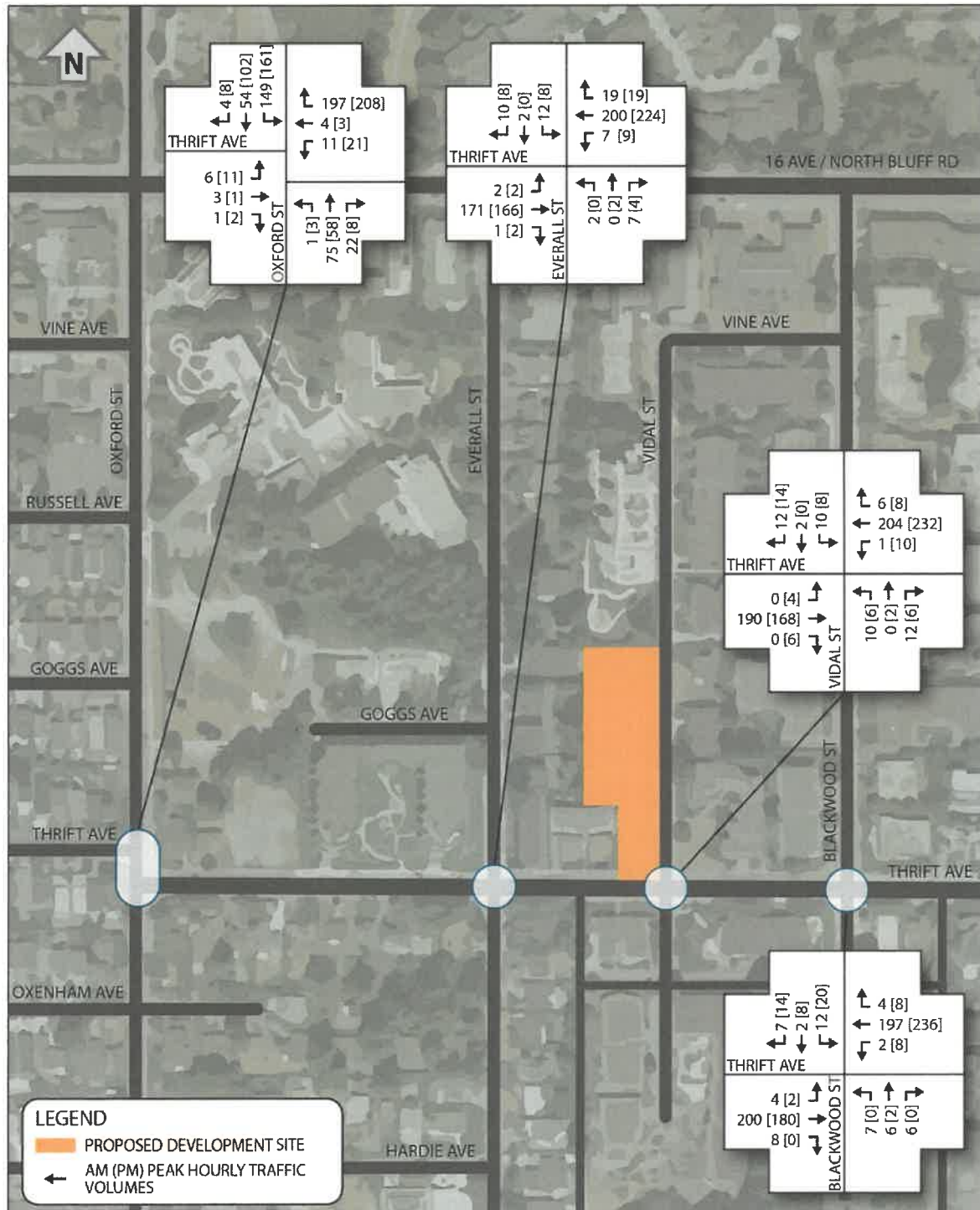


Figure 2-2: 2019 Existing Traffic Volumes

2.5 Pedestrian Accommodations

As described in **Section 2.1**, pedestrian facilities are currently provided on both the north side and south side of Thrift Avenue, and the east side and west side of Vidal Street immediately adjacent to the development. There are pedestrian crosswalks that facilitate crossing Everall Street and Thrift Avenue located approximately 100 m to the west. There is a painted crosswalk on the north approach at the intersection of Vidal Street and Thrift Avenue.

2.6 Cyclist Accommodations

Within the study area, there are several cyclist facilities in close proximity to the proposed development, which are shown in **Figure 2-3**. Thrift Avenue is classified as a Major Street Shared Lane which provides an east-west cycling connection through White Rock and south Surrey. Nearby, Martin Street is also classified as a Major Street Shared Lane which provides cycling access to the north.

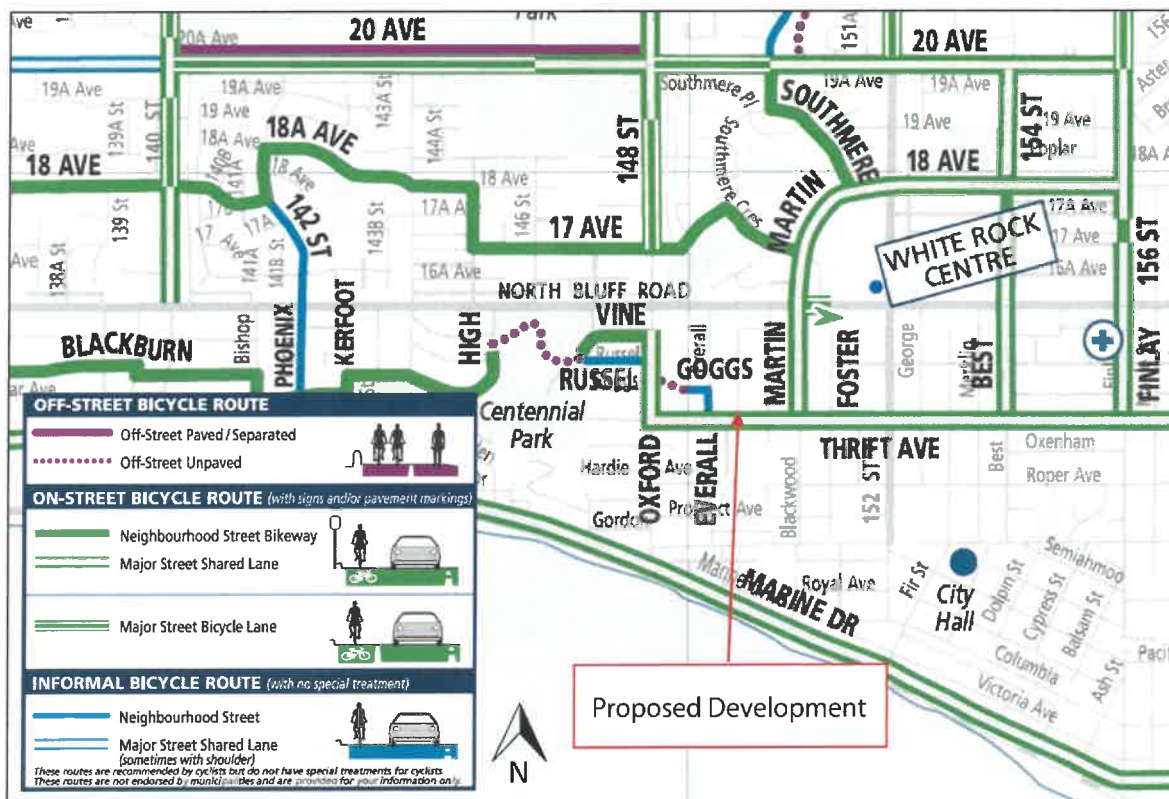


Figure 2-3: White Rock Cycling Map (TransLink, December 2016)

2.7 Transit Services

The public transit service within the City are currently provided by TransLink with three bus routes located along Thrift Avenue with stops within one block of the development:

- Route #351 Bridgeport Station/Crescent Beach – This bus route operates between Bridgeport Station in Richmond and Crescent Beach in Surrey, travelling along Highway 99, King George

Highway and White Rock Centre. There is a westbound bus stop located one block east of the proposed development and an eastbound bus stop located directly across the street from the development.

- Route #345 King George Station/White Rock Centre – This bus route operates between King George Station Surrey and White Rock Centre, travelling along 152 Street and Fraser Highway. There is a westbound bus stop located one block east of the proposed development.
- Route #321 Surrey Central Station/Newton Exchange / White Rock Centre – This bus route operates between Surrey Centre Station, Newton Exchange, and White Rock Centre, travelling along King George Boulevard. There is a westbound bus stop located one block east of the proposed development.

The existing transit routes are shown in **Figure 2-4**.

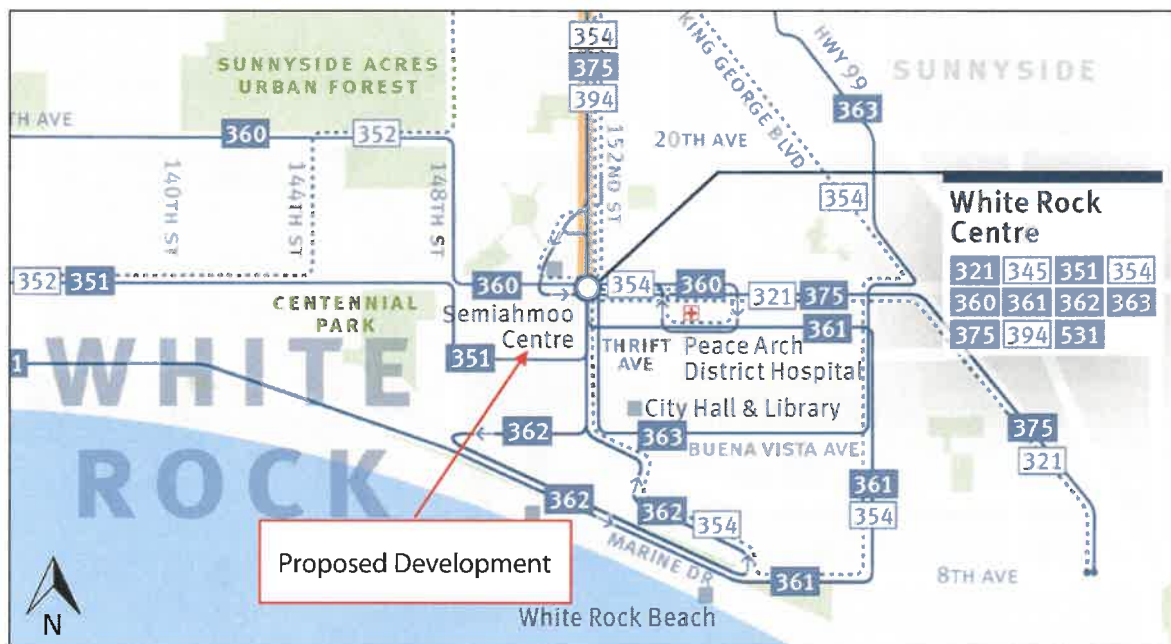


Figure 2-4: Existing Transit Routes in White Rock (TransLink, 2018)

3 FUTURE CONDITIONS

3.1 Proposed Development Concept

The proposed development is located at the northwest corner of the intersection of Thrift Avenue and Vidal Street. Based on the information received from the Developer on May 1, 2019, the proposed development is expected to provide 129 rental residential units when it is completed.

The study development is proposing a two-way access on the east side of the property which will allow vehicles to enter and exit the underground parking levels from Vidal Street. This driveway will be located to the north of the intersection of Vidal Street and Thrift Avenue.

3.2 Phasing and Timing

The proposed opening day of the study development is expected to be in the 2022 horizon year. It is expected that all 129 rental residential units will be built out in one phase.

3.3 Pedestrian Accommodations

The proposed entrance of the development is expected to be located on Vidal Street. Pedestrians will be able to access proposed development using the existing crosswalk at the north approach of the intersection of Vidal Street and Thrift Avenue. As previously mentioned in **Section 2.5**, there are also pedestrian facilities provided as part of the proposed development along the south and east sides of the property.

3.4 Cycling Accommodations

There are also a number of bicycle routes in the vicinity of the proposed development. As mentioned in **Section 2.5** and shown in **Figure 2-3**, Thrift Avenue is classified as a Major Street Shared Lane which provides an east-west cycling connection through White Rock and south Surrey. Nearby, Martin Street is also classified as a Major Street Shared Lane which provides cycling access to the north.

As part of the proposed development, a total of 129 Class 1 long-term secure bicycle parking spaces will be provided and they will be accessed from Vidal Street. A total of 27 Class 2 short-term bicycle parking spaces will be provided and they will be accessed at ground level from Vidal Street.

3.5 Transit Services

Based on the information provided by the TransLink website, there are currently no planned changes to the transit routes nearby to the development. The three bus routes summarized in **Section 2.7** are expected to continue operating on Thrift Avenue in front of the proposed development.

3.6 Road Network Upgrades

It is assumed that there will be no major changes to the existing road network at the study intersections for the opening day or horizon year scenarios. According to the City's Strategic Transportation Plan, published 2014, Thrift Avenue is identified as a potential complete street. This would include the

possible installation of protected bicycle lanes along the corridor while the existing two travel lanes appear to be maintained for the foreseeable future.

3.7 Trip Generation and Distribution

The forecast trip generation for the proposed study and adjacent developments was estimated based on the rates published in the Institute of Transportation Engineers' (ITE) *Trip Generation, 10th Edition*. The trip generation rates for the developments are assumed to be consistent with the following land use category outlined in the ITE guidelines:

- Residential Mid-Rise Multifamily Housing (ITE Ref. 221)

This land use is for multi-unit residential buildings between three and ten floors, including rental and non-rental units with recent data collected from 35 locations across North America. The ITE parking rates include both resident and visitor parking demands. Further details on ITE Ref. 221 are provided in Appendix C.

The forecast site generated traffic volumes for the proposed study developments are summarized in **Table 3-1**.

Table 3-1: Forecast Study Development Site Generated Traffic

Description	Size	Unit	ITE Ref.	Avg. Trip Ends / Unit	Trip Gen.	% Entering	% Exiting	Vehicle Entering	Vehicle Exiting
AM Peak Hour									
Apartments	129	Units	221	0.36	47	26	74	12	35
Total:								12	35
PM Peak Hour									
Apartments	129	Units	221	0.44	57	61	39	35	22
Total:								35	22

The forecast trip distribution for the site generated traffic volumes was estimated based on the existing travel patterns as identified by the turning movement counts. The forecast trip distribution and site generated traffic volumes are shown in **Figure 3-1** and **Figure 3-2**, respectively.

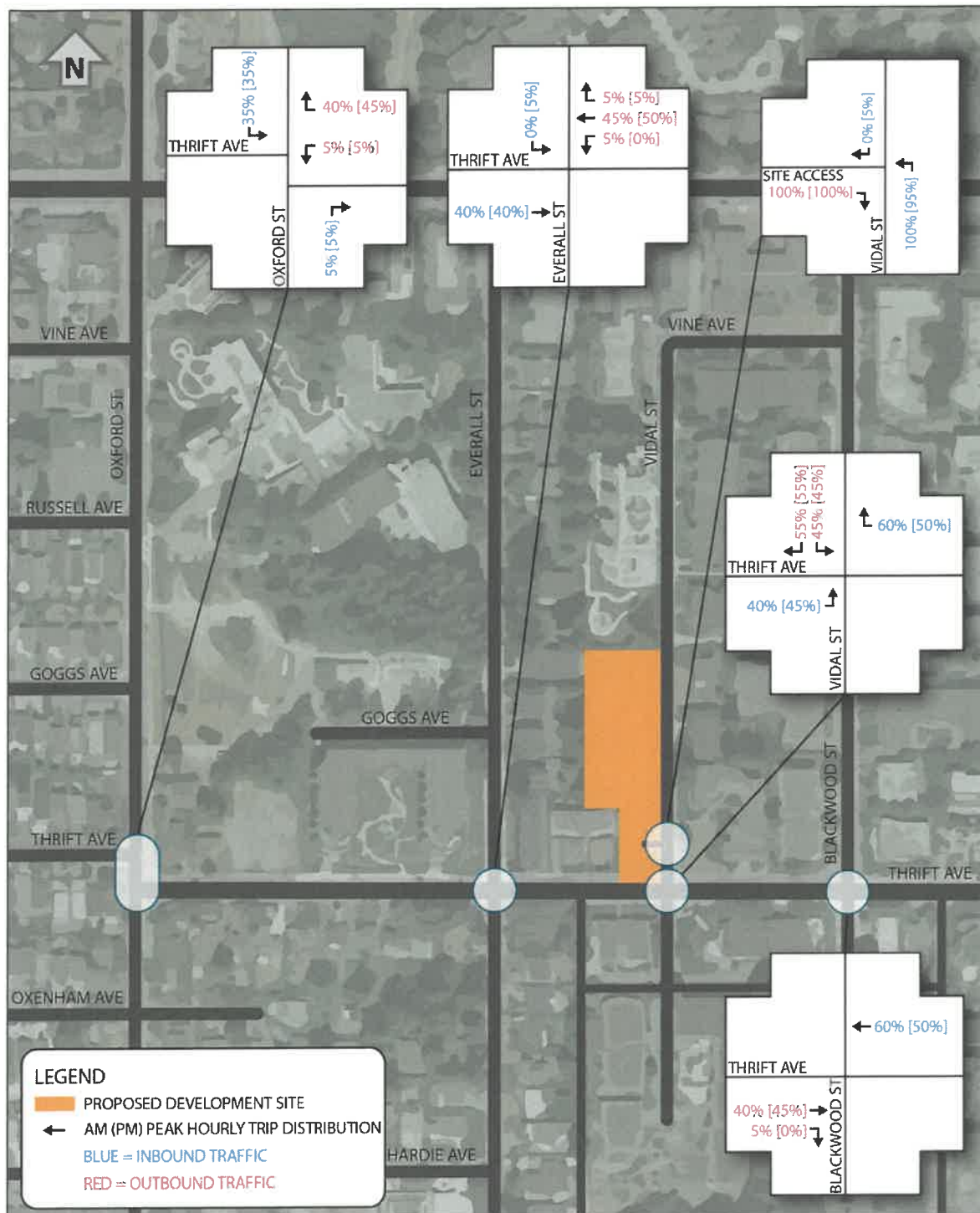


Figure 3-1: Forecast Trip Distribution

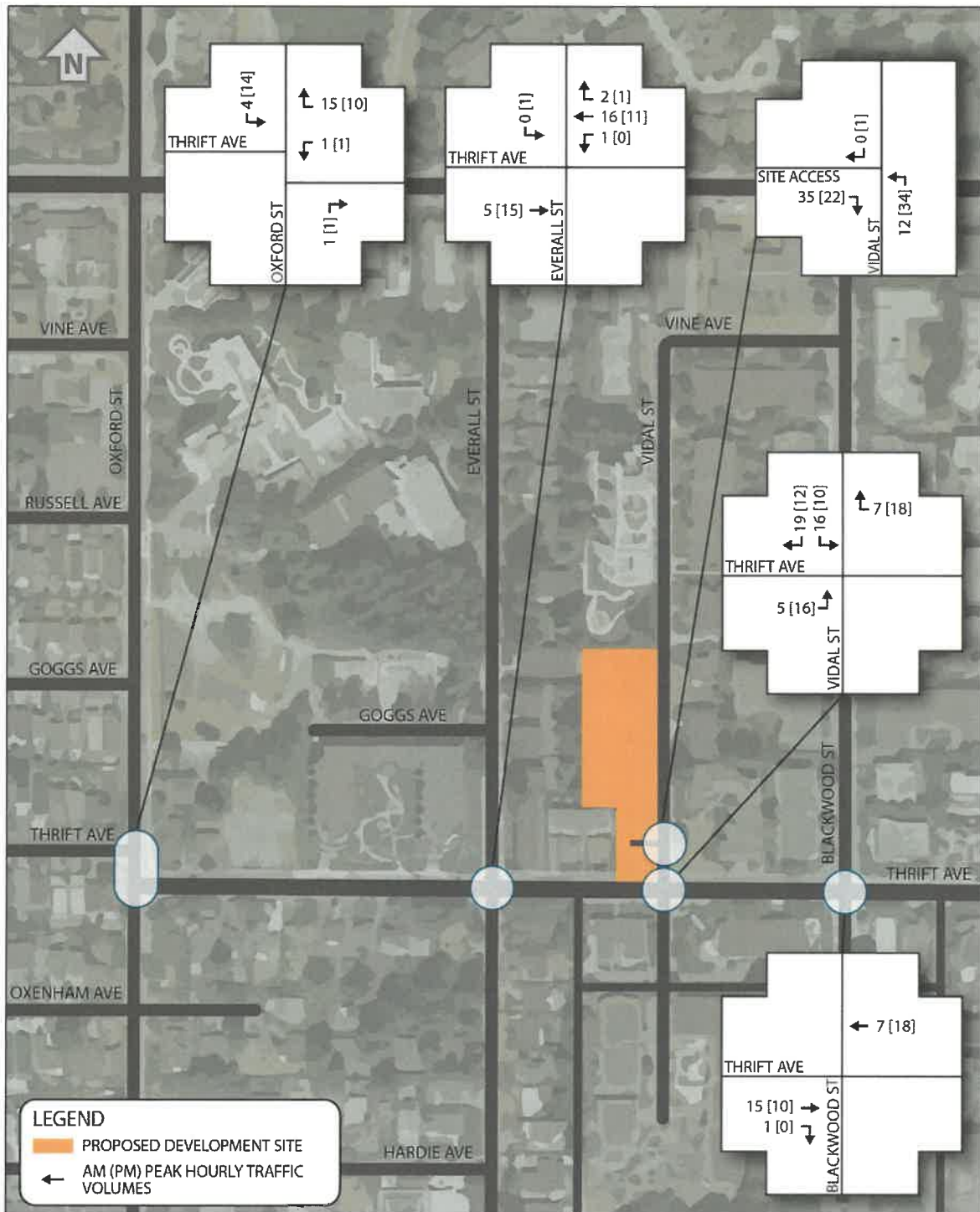


Figure 3-2: Forecast Site Generated Traffic Volumes

4 TRAFFIC OPERATIONS ANALYSIS

4.1 Methodologies

The traffic operations analysis in this report was performed using the Synchro 10 software suite for unsignalized intersections, which is generally based on the Highway Capacity Manual (HCM) methodologies. The traffic operations for each scenario were evaluated to estimate the volume-to-capacity (v/c) ratio, delay, level-of-service (LOS), and 95th percentile queue length at the study intersections.

When reviewing the traffic analysis results, a v/c ratio at or above 1.0 typically indicates that traffic volumes exceed the intersection capacity. Delay, in terms of seconds, represents the average wait time experienced by a driver on the approach to the intersection. LOS is a grading system on intersection operation based on the calculated delay as shown in **Table 4-1** for a signalized intersection and **Table 4-2** for an unsignalized intersection. LOS A means that the intersection experiences little to no delay whereas a LOS F indicates significant delay is present.

Table 4-1: HCM LOS Criteria for Signalized Intersection

Level of Service	Average Control Delay (s/veh)
A	0 - 10
B	> 10 - 20
C	> 20 - 35
D	> 35 - 55
E	> 55 - 80
F	> 80

Table 4-2: HCM LOS Criteria for Unsignalized Intersection

Level of Service	Average Control Delay (s/veh)
A	0 - 10
B	> 10 - 15
C	> 15 - 25
D	> 25 - 35
E	> 35 - 50
F	> 50

The traffic operation analysis is conducted based on the following study thresholds:

- Overall intersection and individual movement of LOS D or better
- Individual movement v/c ratio of 0.85 or less
- Delay less than 35 s (unsignalized)
- Delay less than 55 s (signalized)
- 95th percentile queue lengths impacting adjacent intersections or accesses

The detailed traffic analysis results from Synchro will be provided for the final report in **Appendix D**.

4.2 Existing Traffic Operations

The existing traffic operations analysis on the study road network was completed using the 2019 existing traffic volumes summarized in **Figure 2-2**. The analysis was performed based on the existing intersection configurations and controls shown in **Figure 2-1**.

4.2.1 AM Peak Hour

During the AM peak hour, all the target intersections were found to be operating within the study thresholds. The maximum v/c ratio was found to be 0.25 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

4.2.2 PM Peak Hour

During the PM peak hour, all the target intersections were found to be operating within the study thresholds. The maximum v/c ratio was found to be 0.28 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The traffic operations analysis results based on the 2019 existing traffic volumes are summarized in **Table 4-3**.

Table 4-3: 2019 Existing Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	10.3	0.25	7.6	B	10.6	0.28	8.8
	NBT/R	A	-	0.06	-	A	-	0.04	-
	SBL/T	A	5.9	0.11	2.8	A	5.0	0.12	3.0
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	11.8	0.03	0.7	B	12.4	0.03	0.7
	NBL/T	A	0.2	0.00	0.1	A	0.2	0.01	0.1
	SBT/R	A	-	0.13	-	A	-	0.17	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	-	A	0.1	0.00	-
	WBL/T/R	A	0.3	0.01	0.1	A	0.3	0.01	0.2
	NBL/T/R	A	9.8	0.01	0.3	B	10.3	0.01	0.2
	SBL/T/R	B	11.2	0.04	1.0	B	11.1	0.03	0.7
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	-	0.00	-	A	0.2	0.00	0.1
	WBL/T/R	A	-	0.00	-	A	0.4	0.01	0.2
	NBL/T/R	B	10.8	0.04	0.9	B	11.2	0.03	0.6
	SBL/T/R	B	11.0	0.04	1.0	B	10.9	0.04	0.9
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.2	0.00	0.1	A	0.1	0.00	-
	WBL/T/R	A	0.1	0.00	-	A	0.3	0.01	0.2
	NBL/T/R	B	11.5	0.04	0.9	B	12.6	0.00	0.1
	SBL/T/R	B	11.5	0.04	0.9	B	12.1	0.08	2.0
	Int. LOS	A				A			

4.3 Background Traffic Operation Analysis

The forecast background traffic volumes were projected based on an annual growth rate of 2%. The background traffic analysis notes that the following developments are built-out as of the writing of this report. These include developments and facility improvements at the following locations:

- Evergreen Baptist of Care Facility Expansion;
- 14855 Thrift Avenue; and
- 1501 Vidal Street.

Additionally, several proposed developments were identified through documents provided by the City. These locations were noted yet to be completed and are assumed to be fully built out in the future background traffic volumes. The Institute of Transportation Engineers' (ITE) *Trip Generation, 10th Edition* trip generation rates were used when required and their site-generated traffic was assumed to be inclusive in the background traffic volumes without being subject to the annual growth:

- 1454 Oxford Street;
- 1434 Oxford Street, 14809 Thrift Avenue, 14815 Thrift Avenue; and
- 14825 Thrift Avenue, 14835 Thrift Avenue.

4.3.1 2022 Opening Day Background Traffic Operations

The 2022 opening day background traffic operations analysis assumes the existing intersection and laning configurations. The 2022 opening day background traffic volumes are shown in **Figure 4-1**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.28 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.31 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The 2022 opening day background traffic analysis results are summarized in **Table 4-4**.

Table 4-4: 2022 Opening Day Background Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	10.5	0.28	8.7	B	10.9	0.31	10.2
	NBT/R	A	-	0.07	-	A	-	0.05	-
	SBL/T	A	6.0	0.12	3.2	A	5.2	0.13	3.4
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	11.6	0.02	0.5	B	12.9	0.03	0.8
	NBL/T	A	0.2	0.00	0.1	A	0.2	0.01	0.1
	SBT/R	A	-	0.14	-	A	-	0.19	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	-	A	0.1	0.00	-
	WBL/T/R	A	0.3	0.01	0.1	A	0.4	0.01	0.2
	NBL/T/R	B	10.0	0.01	0.3	B	10.5	0.01	0.2
	SBL/T/R	B	11.5	0.05	1.1	B	11.4	0.03	0.7
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	-	0.00	-	A	0.2	0.00	0.1
	WBL/T/R	A	-	0.00	-	A	0.4	0.01	0.2
	NBL/T/R	B	11.1	0.04	1.0	B	11.6	0.03	0.7
	SBL/T/R	B	11.3	0.05	1.1	B	11.2	0.04	1.0
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	-
	WBL/T/R	A	0.1	0.00	-	A	0.3	0.01	0.2
	NBL/T/R	B	11.9	0.04	1.0	B	13.0	0.00	0.1
	SBL/T/R	B	11.9	0.04	1.0	B	12.5	0.09	2.3
	Int. LOS	A				A			

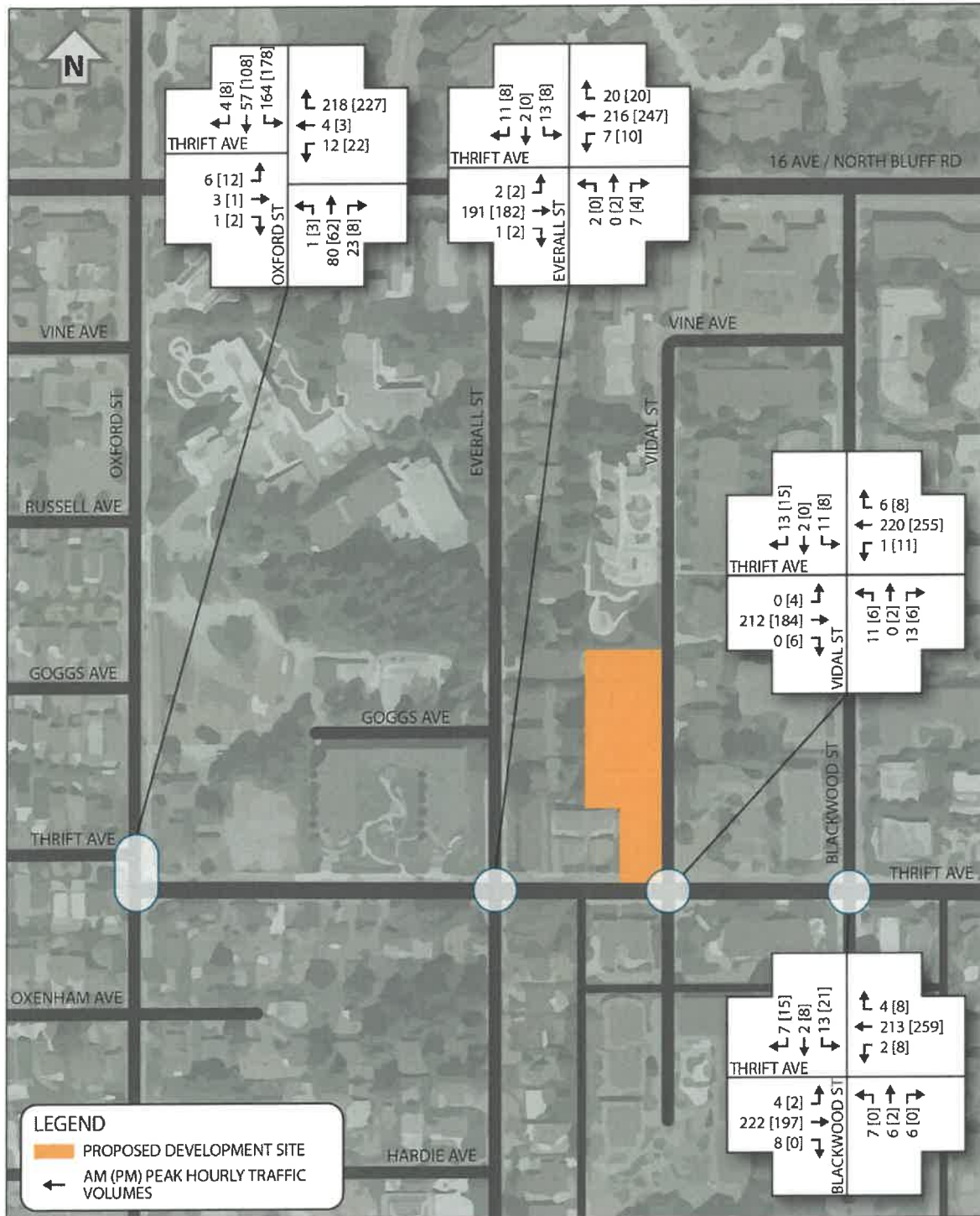


Figure 4-1: 2022 Opening Day Background Traffic Volumes

4.3.2 2032 Opening Day +10 Years Background Traffic Operations

The 2032 opening day plus 10 years background traffic operations analysis assumes the existing intersection and laning configurations. The 2032 background traffic volumes were determined by applying a growth factor of 2% per year to the existing traffic volumes. These are shown in **Figure 4-2**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.37 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.40 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The 2032 background traffic analysis results are summarized in **Table 4-5**.

Table 4-5: 2032 Opening Day +10 Years Background Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	11.7	0.37	12.9	B	12.2	0.40	14.7
	NBT/R	A	-	0.08	-	A	-	0.06	-
	SBL/T	A	6.2	0.15	4.1	A	5.4	0.16	4.2
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	11.6	0.03	0.6	B	14.5	0.05	1.1
	NBL/T	A	0.2	0.01	0.1	A	0.3	0.01	0.2
	SBT/R	A	-	0.11	-	A	-	0.23	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.3	0.01	0.2	A	0.4	0.01	0.2
	NBL/T/R	B	10.7	0.02	0.5	B	11.3	0.01	0.3
	SBL/T/R	B	12.6	0.07	1.6	B	12.5	0.04	1.0
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	-	0.00	-	A	0.2	0.00	0.1
	WBL/T/R	A	-	0.00	-	A	0.4	0.01	0.2
	NBL/T/R	B	12.0	0.06	1.4	B	12.8	0.04	1.0
	SBL/T/R	B	12.3	0.06	1.6	B	12.1	0.06	1.4
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.2	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.1	0.00	0.1	A	0.3	0.01	0.2
	NBL/T/R	B	13.1	0.06	1.4	B	14.3	0.01	0.2
	SBL/T/R	B	13.2	0.06	1.6	B	14.1	0.13	3.4
	Int. LOS	A				A			

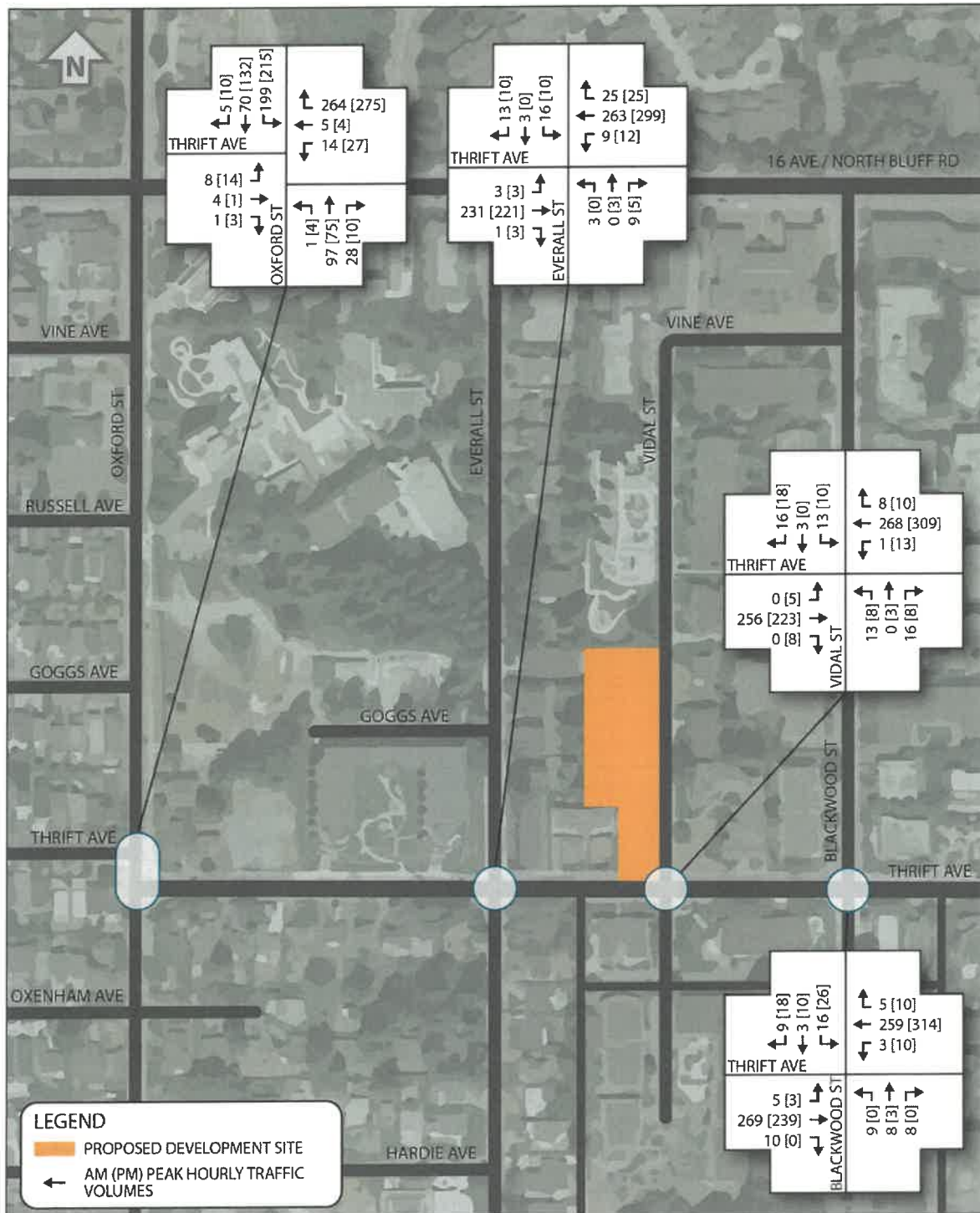


Figure 4-2: 2032 Opening Day +10 Years Background Traffic Volumes

4.3.3 2045 Horizon Year Background Traffic Operations

The 2045 horizon year background traffic operations analysis assumes the existing intersection and laning configurations. The 2045 horizon year background traffic volumes were determined by applying a growth factor of 2% per year to the existing traffic volumes. These are shown in **Figure 4-3**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.50 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.57 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The 2045 horizon year background traffic analysis results are summarized in **Table 4-6**.

Table 4-6: 2045 Horizon Year Background Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	13.9	0.50	21.4	C	15.9	0.57	27.8
	NBT/R	A	-	0.11	-	A	-	0.07	-
	SBL/T	A	6.6	0.20	5.8	A	5.7	0.20	5.8
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	14.7	0.05	1.2	C	18.5	0.09	2.1
	NBL/T	A	0.3	0.01	0.2	A	0.3	0.01	0.2
	SBT/R	A	-	0.23	-	A	-	0.29	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.4	0.01	0.2	A	0.4	0.01	0.3
	NBL/T/R	B	11.4	0.03	0.7	B	13.2	0.03	0.7
	SBL/T/R	B	14.9	0.11	2.7	B	14.6	0.07	1.7
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	-	0.00	-	A	0.3	0.01	0.2
	WBL/T/R	A	0.1	0.00	-	A	0.5	0.01	0.3
	NBL/T/R	B	14.0	0.09	2.3	C	15.1	0.07	1.6
	SBL/T/R	B	14.3	0.10	2.5	B	14.1	0.09	2.2
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.2	0.01	0.2	A	0.1	0.00	0.1
	WBL/T/R	A	0.1	0.00	0.1	A	0.4	0.01	0.3
	NBL/T/R	C	15.6	0.09	2.3	C	16.9	0.01	0.2
	SBL/T/R	C	15.9	0.10	2.6	C	17.7	0.21	5.9
	Int. LOS	A				A			

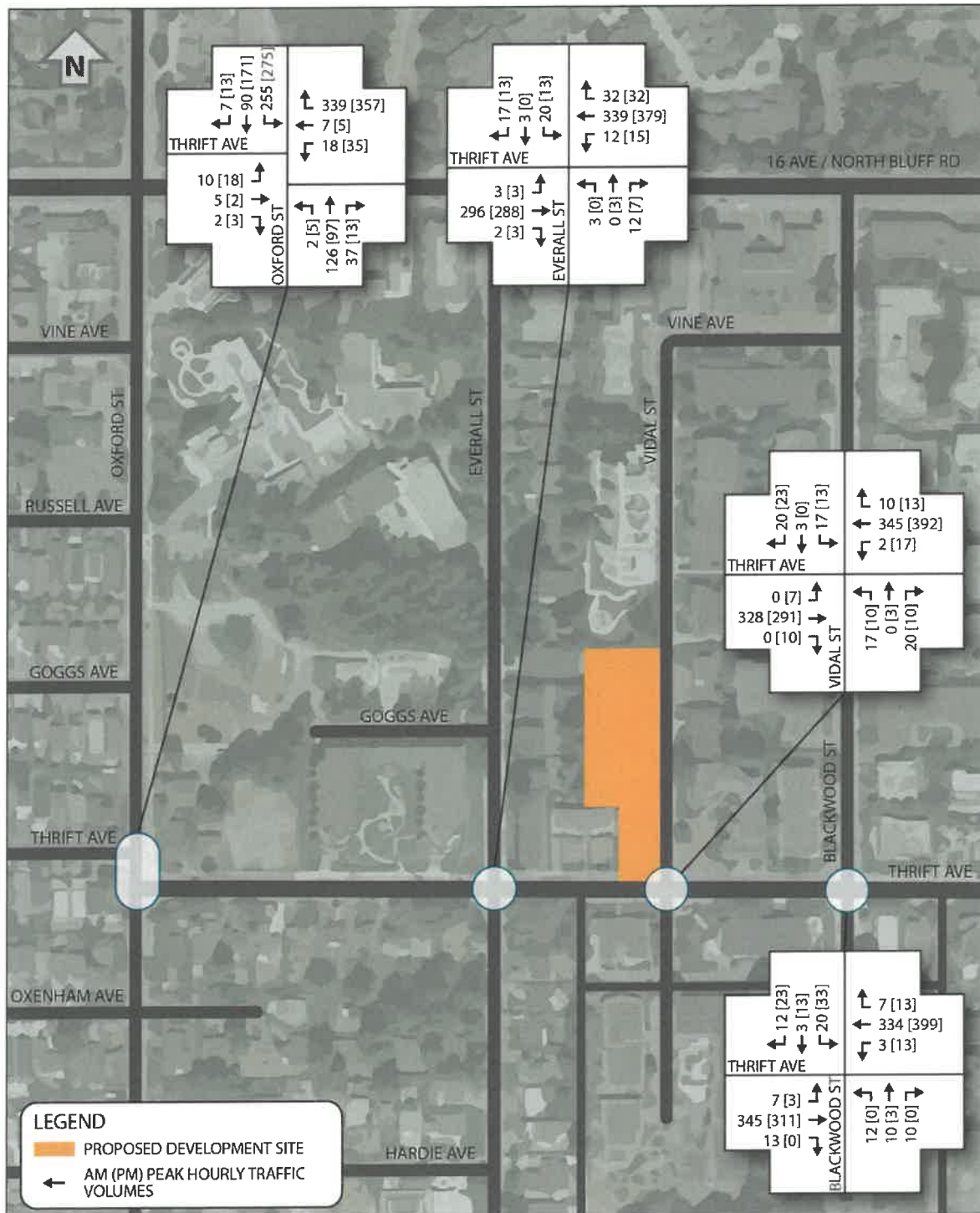


Figure 4-3: 2045 Horizon Year Background Traffic Volumes

4.4 Combined Traffic Operation Analysis

The combined traffic operation analysis considers both the background traffic growth as well as the site generated traffic volumes. The study development is expected to have one stop-controlled access on Vidal Street where free-flow traffic will be maintained on the local roadway. The proposed laning for the 2022 horizon year combined traffic operations analysis is shown in **Figure 4-4**.

4.4.1 2022 Opening Day Combined Traffic Conditions

The opening day 2022 horizon year combined traffic operation analysis is performed based on the forecast traffic volumes shown in **Figure 4-5**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.30 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.33 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The combined traffic analysis for the 2022 horizon year is shown in **Table 4-7**.

Table 4-7: 2022 Opening Day Combined Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	10.7	0.30	9.7	B	11.2	0.33	11.0
	NBL/T	A	-	0.07	-	A	-	0.05	-
	SBL/T	A	6.1	0.13	3.3	A	5.4	0.14	3.7
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	11.7	0.02	0.5	B	13.2	0.04	0.8
	NBL/T	A	0.1	0.00	0.1	A	0.2	0.01	0.1
	SBT/R	A	-	0.15	-	A	-	0.20	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	-	A	0.1	0.00	-
	WBL/T/R	A	0.3	0.01	0.2	A	0.3	0.01	0.2
	NBL/T/R	B	10.1	0.01	0.3	B	10.7	0.01	0.2
	SBL/T/R	B	11.7	0.05	1.2	B	11.7	0.03	0.8
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	0.2	0.00	5.3	A	0.9	0.02	0.4
	WBL/T/R	A	-	0.00	-	A	0.4	0.01	0.2
	NBL/T/R	B	11.5	0.04	2.2	B	12.2	0.03	0.7
	SBL/T/R	B	11.7	0.11	2.5	B	12.0	0.09	2.2
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	-
	WBL/T/R	A	0.1	0.00	-	A	0.3	0.01	0.2
	NBL/T/R	B	12.1	0.04	1.0	B	13.3	0.00	0.1
	SBL/T/R	B	12.1	0.05	1.1	B	12.8	0.09	2.4
	Int. LOS	A				A			
Vidal Street at Development Access (Unsignalized)	EBL/R	A	8.6	0.04	0.9	A	8.5	0.02	0.5
	NBL/T	A	4.8	0.01	0.2	A	5.3	0.02	0.5
	SBT/R	A	-	0.02	-	A	-	0.02	-
	Int. LOS	A				A			

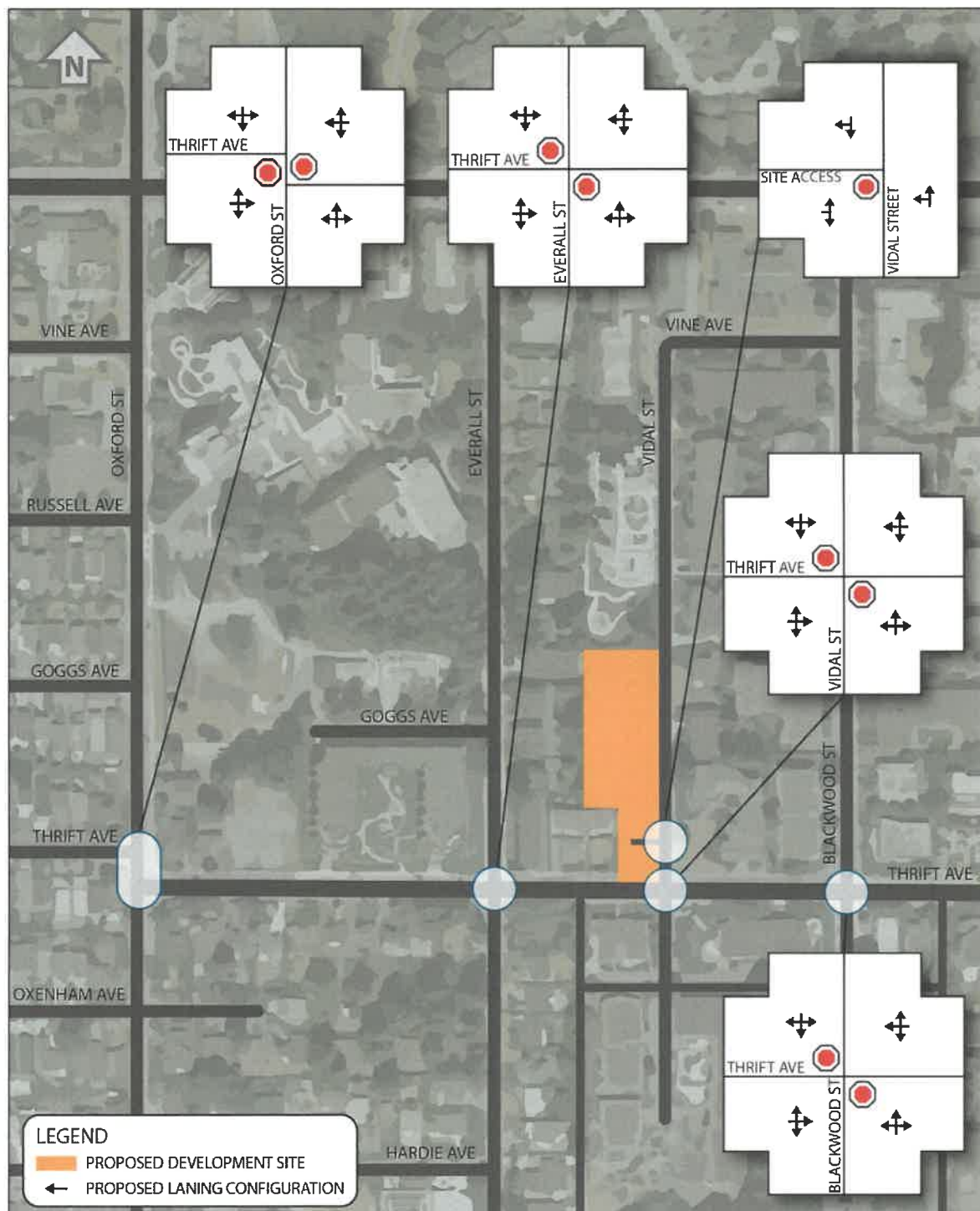


Figure 4-4: 2022 Opening Day Laning Configuration and Traffic Controls



4.4.2 2032 Opening Day +10 Years Combined Traffic Operations

The 2032 horizon year background traffic operations analysis assumes the existing intersection and laning configurations. The 2032 horizon year combined traffic volumes are shown in **Figure 4-6**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.38 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.48 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The 2032 horizon year background traffic analysis results are summarized in **Table 4-8**.

Table 4-8: 2032 Opening Day +10 Years Combined Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	11.7	0.38	13.4	B	12.9	0.48	20.4
	NBT/R	A	-	0.08	-	A	-	0.06	-
	SBL/T	A	6.3	0.16	4.2	A	5.5	0.17	4.6
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	13.0	0.03	0.7	B	14.8	0.05	1.2
	NBL/T	A	0.3	0.01	0.2	A	0.3	0.01	0.2
	SBT/R	A	-	0.18	-	A	-	0.24	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.3	0.01	0.2	A	0.4	0.01	0.2
	NBL/T/R	B	11.2	0.02	0.5	B	11.5	0.01	0.3
	SBL/T/R	B	14.4	0.08	2.0	B	12.9	0.05	1.1
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	0.2	0.00	0.1	A	0.8	0.02	0.4
	WBL/T/R	A	-	0.00	-	A	0.4	0.01	0.2
	NBL/T/R	B	12.4	0.06	1.5	B	13.5	0.05	1.1
	SBL/T/R	B	12.9	0.14	3.6	B	13.1	0.11	2.8
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.2	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.1	0.00	0.1	A	0.3	0.01	0.2
	NBL/T/R	B	13.4	0.06	1.5	B	14.7	0.01	0.2
	SBL/T/R	B	13.5	0.07	1.6	B	14.5	0.14	3.5
	Int. LOS	A				A			
Vidal Street at Development Access (Unsignalized)	EBL/R	A	8.6	0.04	0.9	A	8.5	0.02	0.5
	NBL/T	A	4.3	0.01	0.2	A	4.8	0.02	0.5
	SBT/R	A	-	0.02	-	A	-	0.02	-
	Int. LOS	A				A			

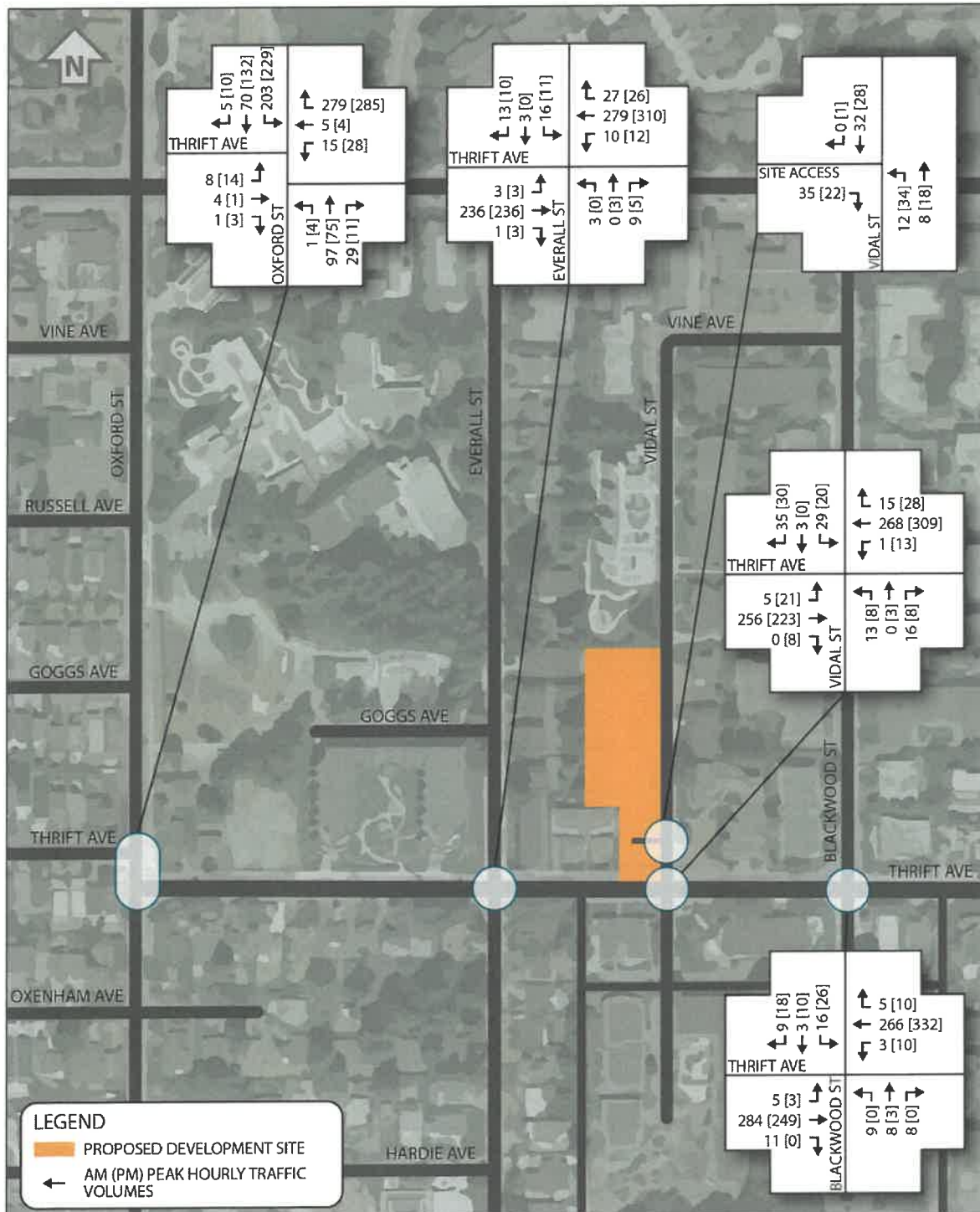


Figure 4-6: 2032 Opening Day +10 Years Combined Traffic Volumes

4.4.3 2045 Horizon Year Combined Traffic Operations

The 2045 horizon year background traffic operations analysis assumes the existing intersection and laning configurations. The 2045 horizon year combined traffic volumes are shown in **Figure 4-7**.

AM Peak Hour

During the AM peak hour, all of the study intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.52 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

PM Peak Hour

During the PM peak hour, all the target intersections are expected to operate within the study thresholds. The maximum v/c ratio is expected to be 0.52 for the westbound movements at the intersection of Thrift Avenue and Oxford Street.

The 2045 horizon year background traffic analysis results are summarized in **Table 4-9**.

Table 4-9: 2045 Horizon Year Combined Traffic Operations

Intersection	Turning Movement	AM Peak Hour				PM Peak Hour			
		LOS	Delay (s)	V/C Ratio	95% Q (m)	LOS	Delay (s)	V/C Ratio	95% Q (m)
Thrift Avenue at Oxford Street South (Unsignalized)	WBL/R	B	14.4	0.52	23.3	B	14.2	0.52	23.6
	NBT/R	A	-	0.11	-	A	-	0.07	-
	SBL/T	A	6.6	0.21	5.9	A	5.8	0.22	6.3
	Int. LOS	A				A			
Thrift Avenue at Oxford Street North (Unsignalized)	EBL/R	B	14.8	0.05	1.2	C	16.5	0.06	1.4
	NBL/T	A	0.3	0.01	0.2	A	0.3	0.01	0.2
	SBT/R	A	-	0.23	-	A	-	0.30	-
	Int. LOS	A				A			
Thrift Avenue at Everall Street (TWSC)	EBL/T/R	A	0.1	0.00	0.1	A	0.1	0.00	0.1
	WBL/T/R	A	0.4	0.01	0.3	A	0.4	0.01	0.3
	NBL/T/R	B	11.6	0.03	0.7	B	12.1	0.02	0.5
	SBL/T/R	C	15.3	0.11	2.8	C	15.1	0.08	1.8
	Int. LOS	A				A			
Thrift Avenue at Vidal Street (TWSC)	EBL/T/R	A	0.2	0.00	0.1	A	0.8	0.02	0.5
	WBL/T/R	A	0.1	0.00	-	A	0.5	0.01	0.3
	NBL/T/R	B	14.6	0.10	2.4	C	16.1	0.07	1.8
	SBL/T/R	C	15.5	0.19	5.3	C	15.7	0.16	4.2
	Int. LOS	A				A			
Thrift Avenue at Blackwood Street (TWSC)	EBL/T/R	A	0.2	0.01	0.2	A	0.1	0.00	0.1
	WBL/T/R	A	0.1	0.00	0.1	A	0.4	0.01	0.3
	NBL/T/R	C	16.0	0.10	2.4	C	17.4	0.01	0.2
	SBL/T/R	C	16.3	0.11	2.7	C	18.4	0.22	6.2
	Int. LOS	A				A			
Vidal Street at Development Access (Unsignalized)	EBL/R	A	8.6	0.04	0.9	A	8.6	0.02	0.5
	NBL/T	A	4.0	0.01	0.2	A	4.5	0.02	0.6
	SBT/R	A	-	0.03	-	A	-	0.02	-
	Int. LOS	A				A			

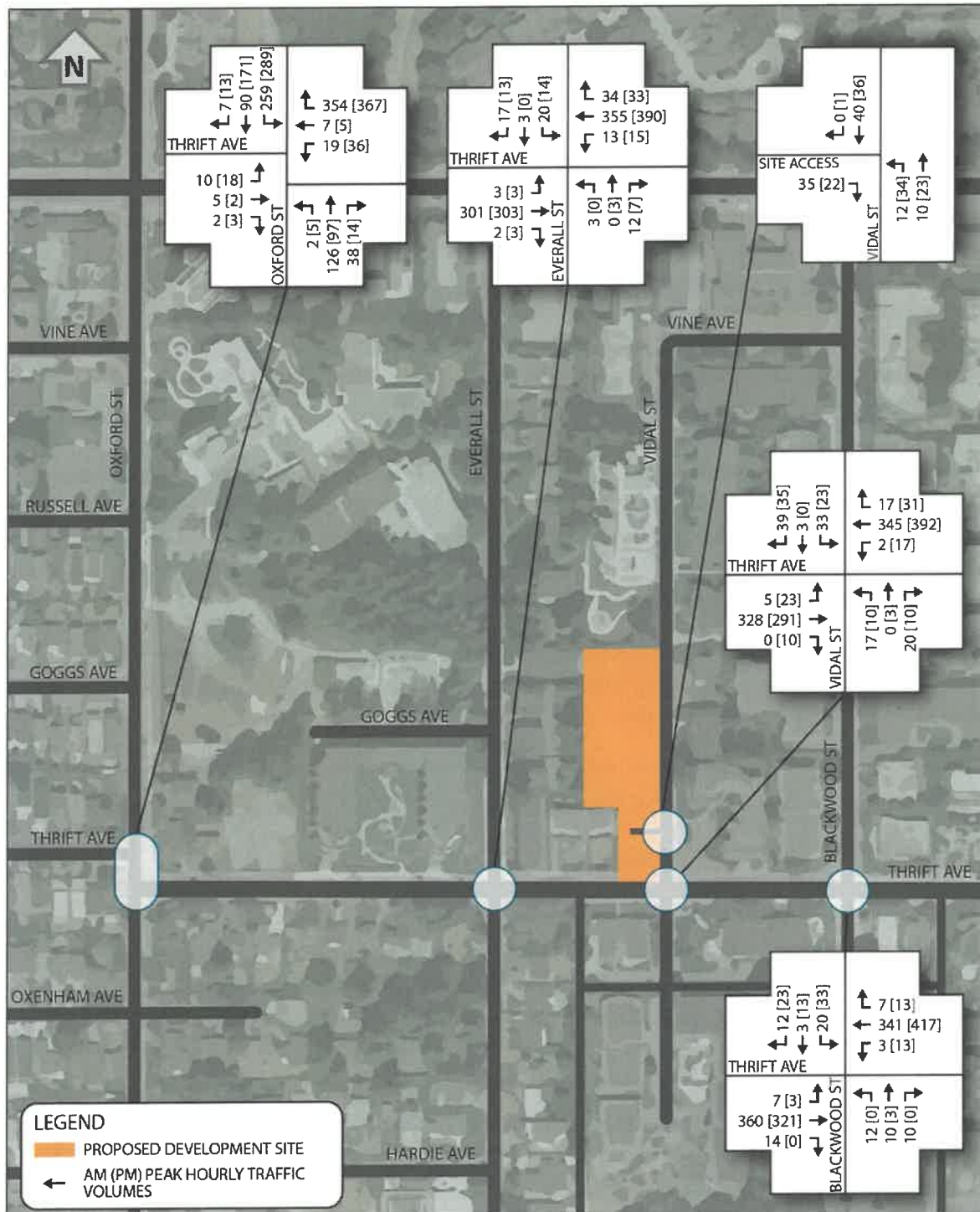


Figure 4-7: 2045 Horizon Year Combined Traffic Volumes

5 TRAFFIC CIRCULATION AND OFF-STREET PARKING

5.1 Site Access Review

The study development is expected to have one access on Vidal Street. All ingress and egress movements for the underground parkade are expected to be made from this single entrance.

The driveway spacing was reviewed according to the Transportation Association of Canada's (TAC) *Geometric Design Guide for Canadian Roads* Chapter 8 – Section 8.9.7. Within residential land uses adjacent to a minor intersection, a minimum distance of 2.0 m is suggested between the driveway and the back of the curb corner. The proposed driveway location for the study development is expected to exceed the minimum TAC guidelines. In order to provide adequate sightlines for the site traffic to safely enter Vidal Street, a no-stopping zone should be established on either side of the driveway.

5.2 AutoTURN Review

An AutoTURN analysis was conducted for the ingress and egress movements of a standard passenger vehicle at each of the parkade levels based on design guidelines published by TAC. The AutoTURN analysis found that this design vehicle is able to perform the ingress and egress movements without issues at the proposed access. It is also expected to manoeuvre without issues within the study development's parkade as per the current site plan.

It is noted that solid waste collection bins are located near the parkade entrance within the building. It is anticipated these will be moved to Vidal Street for collection and that solid waste collection vehicles are not expected to enter the parkade.

The analysis also reviewed the critical parking manoeuvres within the parkade, typically parking spaces that are against walls, and found that they could be accessed by the design vehicle without issues.

The AutoTURN analysis for the study development is provided in **Appendix B**.

5.3 Off-Street Parking

The off-street parking requirements for the proposed development were calculated based on the City's Bylaw No. 2000 (2019). Based on Section 4.14, a total of 194 parking spaces are required with 155 spaces for resident parking and 39 spaces for visitor parking, which is presented in **Table 5-1** below.

Table 5-1: Bylaw Parking Requirements

Description	Bylaw Ref.	Size	Unit	Demand Per Unit	Stalls Required
Required Parking Spaces - Apartment	2000 - 4.14	129	Units	1.20	155
Visitor Parking Spaces - Apartment	2000 - 4.14	129	Units	0.30	39
Total:					194

Based on the January 9, 2020 site plan, there are 179 parking spaces for the proposed development with 39 spaces for visitor parking and 140 spaces for resident parking. As there is a 15 stall shortfall and that a variance from the Bylaw is required, the forecast parking demand for the proposed development was

reviewed based on the rate published in the Institute of Transportation Engineers' (ITE) *Parking Generation Manual, 5th Edition*. The parking generation rate for the development is assumed to be consistent with the following land use category outlined in the ITE guidelines:

- Residential Mid-Rise Multifamily Housing (ITE Ref. 221)

The forecast site generated traffic volumes for the proposed study development are summarized in **Table 5-2**.

Table 5-2: Forecast Study Development Generated Parking Demand

Description	Size	Unit	ITE Ref.	Avg. Parking Gen Per Unit	Generated Parking Demand
Weekday Peak Period	129	Units	221	1.31	169

During the peak parking period, which is likely overnight within the development, there is an estimated parking demand for 169 parking spaces, inclusive of the visitor parking, which is less than the proposed 179 parking spaces in the proposed development. Although the proposed 179 parking spaces do not meet the Bylaw requirements, they were found to exceed the projected parking demand for similar residential sites based on ITE parking generation rates. The ITE parking demand rate is generally based on market residential developments; therefore, the study rental development is anticipated to generate less parking demand than the projection using ITE's parking generation rate. Further, with existing bus routes operating on Thrift Avenue that are directly adjacent to the proposed development, the dependency on using personal vehicles for local trips are expected to be reduced.

Based on the review performed in this section, the provided on-site parking supply would be sufficient for this proposed development.

5.4 Bicycle Parking

Based on section 4.16 of the City's Bylaw No. 2000 (2019), a total of 155 bicycle parking spaces are required with 129 spaces for Class 1 secure long-term parking and 26 spaces for Class 2 short-term parking. Based on the January 9, 2020 site plan, there are 156 bicycle parking spaces provided in the proposed development, which meet the Bylaw requirements presented in **Table 5-3**.

Table 5-3: Bicycle Parking Requirements

Description	Bylaw Ref.	Size	Unit	Demand Per Unit	Stalls Required
Bicycle Parking Stall Class 1	2000 - 4.16	129	Units	1.00	129
Bicycle Parking Stall Class 2	2000 - 4.16	129	Units	0.20	26
Total:					155

5.5 Loading

According to section 4.15 of the City's Bylaw No. 2000 (2019), an off-street loading space is required if there are greater than ten units in an apartment complex. Based on the site plan, a single off-street loading space is located perpendicular to the parkade driveway on the east side of the building. An

AutoTURN analysis found that an MSU (medium single unit) truck design vehicle is able to reverse into this loading space from Vidal Street without issues. A Building Manager is expected to be on-site at all times to help plan and facilitate access to the loading bay to minimize impacts to on-street parking.

In order to accommodate the MSU turning movements into the loading bay, a variance will be required to permit the driveway to widen in excess of 7 metres at the new property line. This is considered to be acceptable as the widest point of the driveway is near the building where conflicts with pedestrians would be low.

To provide waste collection services on-site, the study development is expected to accommodate that completely on-site with low-profile compactors. Waste collection would then be performed by small trucks that could manoeuvre through the parkade.

6 CONCLUSIONS AND RECOMMENDATIONS

The purpose of this study is to evaluate the transportation effects of the proposed multi-family residential development and its traffic impacts on the adjacent transportation network. The proposed development is expected to provide a total of 129 residential units at its full build-out.

A summary of the study findings are as follows:

- The study area is located in close proximity to existing transit routes and regional cycling routes. There are existing pedestrian facilities provided on the study roadways as well.
- A new pedestrian sidewalk is proposed along Vidal Street and Thrift Avenue adjacent to the proposed development.
- The development is expected to provide a total of 129 'Class 1' bicycle parking spaces and 27 'Class 2' bicycle parking spaces which meet the City's Bylaw requirements.
- At full build-out, the proposed development is expected to generate 47 vehicle trips onto the study road network during the AM peak hour, with 12 inbound trips, and 35 outbound trips.
- The proposed development is expected to generate 57 vehicle trips onto the study road network during the PM peak hour, with 35 inbound trips and 22 outbound trips.
- The proposed development is expected to provide one access point on Vidal Street north of Thrift Avenue with free-flow traffic maintained on Vidal Street. The proposed driveway distance from Thrift Avenue is expected to meet and exceed the minimum TAC requirement.
- The existing study intersections along Thrift Avenue are currently operating within the study thresholds during both the AM and PM peak hours. During the 2022 opening day, 2032 horizon year, and 2045 horizon year scenarios, the existing intersections and the study development access are expected to continue operating within study thresholds.
- The study development is required to provide 155 residential parking spaces and 39 visitor parking spaces for a total of 194 parking spaces based on the City's Bylaw; however, the proposed parking supply is expected to be sufficient in accommodating the parking demand based on the ITE's parking generation rate.
- Solid waste collection will be performed fully within the site.
- A variance will be required to permit the driveway to widen in excess of 7 metres at the new property line in order to accommodate loading activities. The area where the driveway would exceed 7 metres is away from the pedestrian sidewalk so safety is not expected to be impacted.

Based on the study findings, all of the intersections within the study area are expected to operate acceptably; thus, no transportation improvements are expected to be required to accommodate the traffic demand generated by the proposed development.

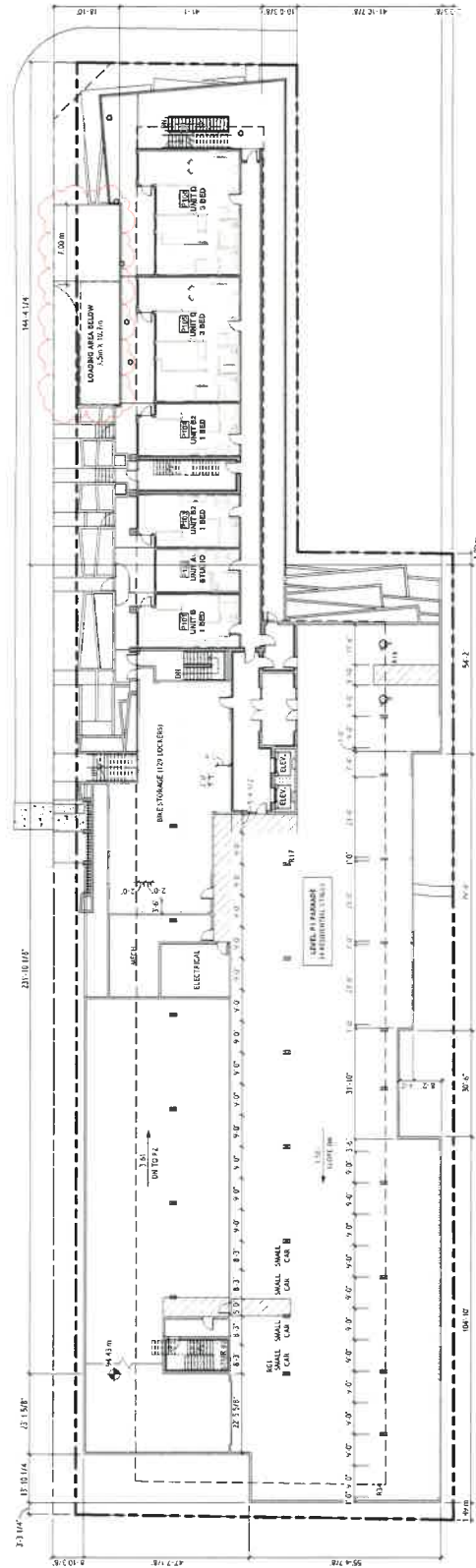
An on-site Building Manager is recommended to monitor and facilitate access to the loading bay in order to coordinate its used by future residents and to reduce impacts to on-street parking.

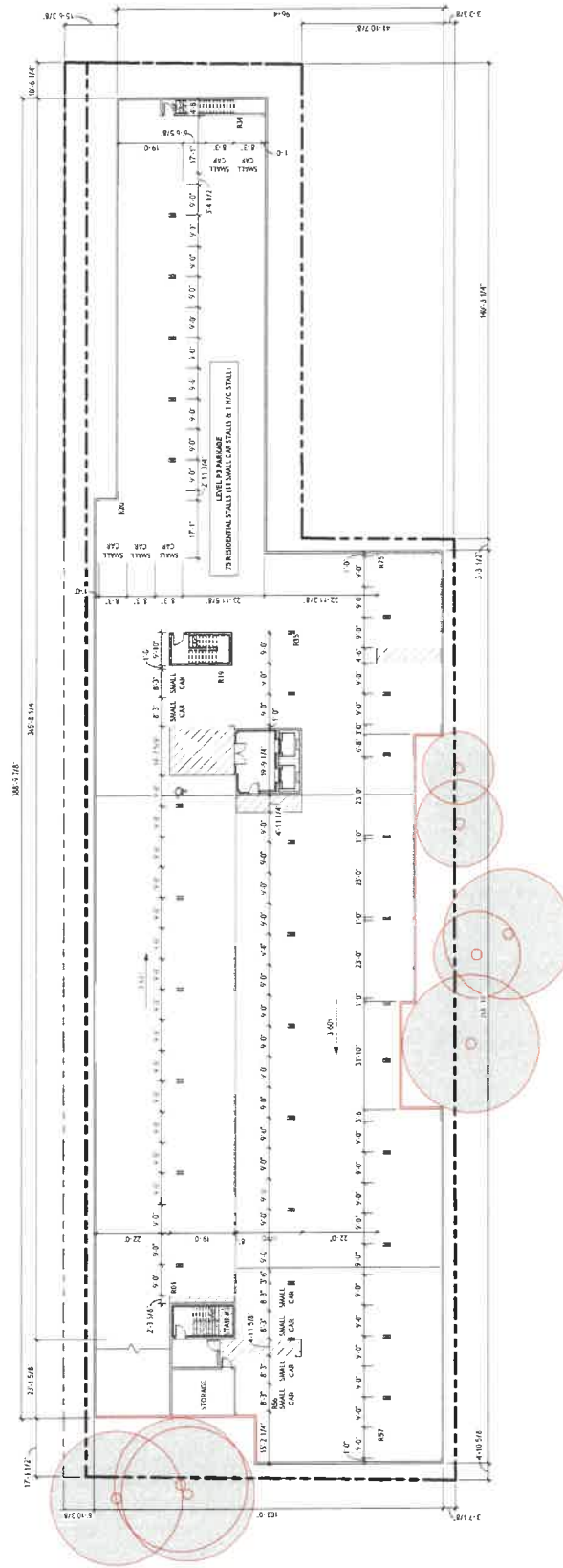
APPENDIX A

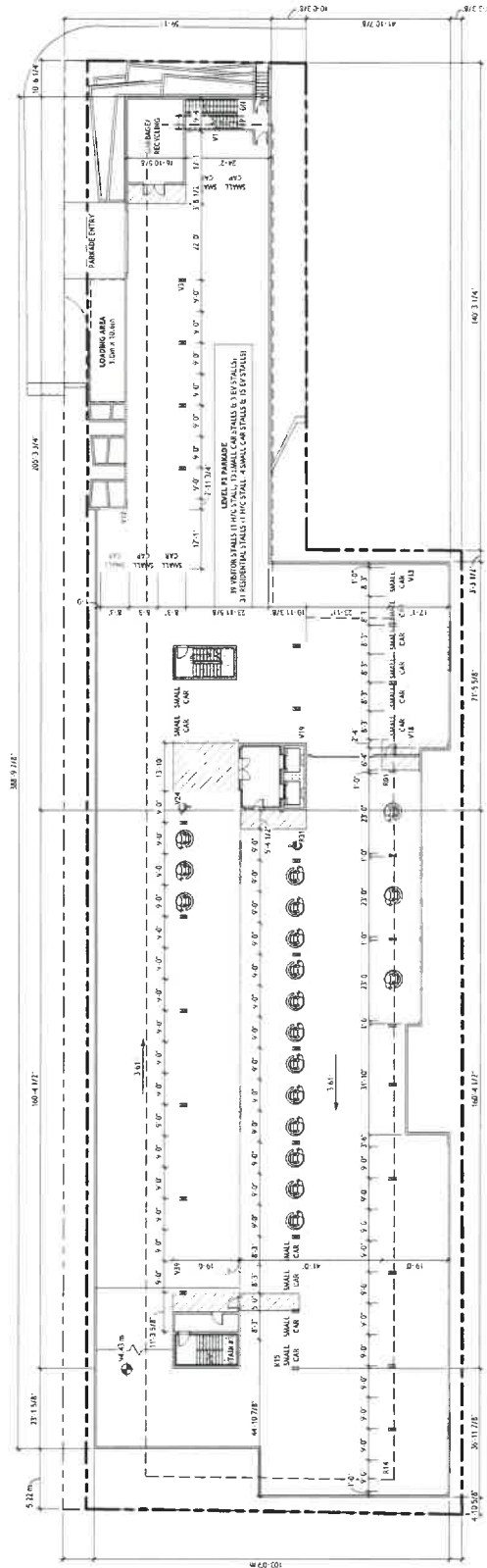
PROPOSED SITE PLAN

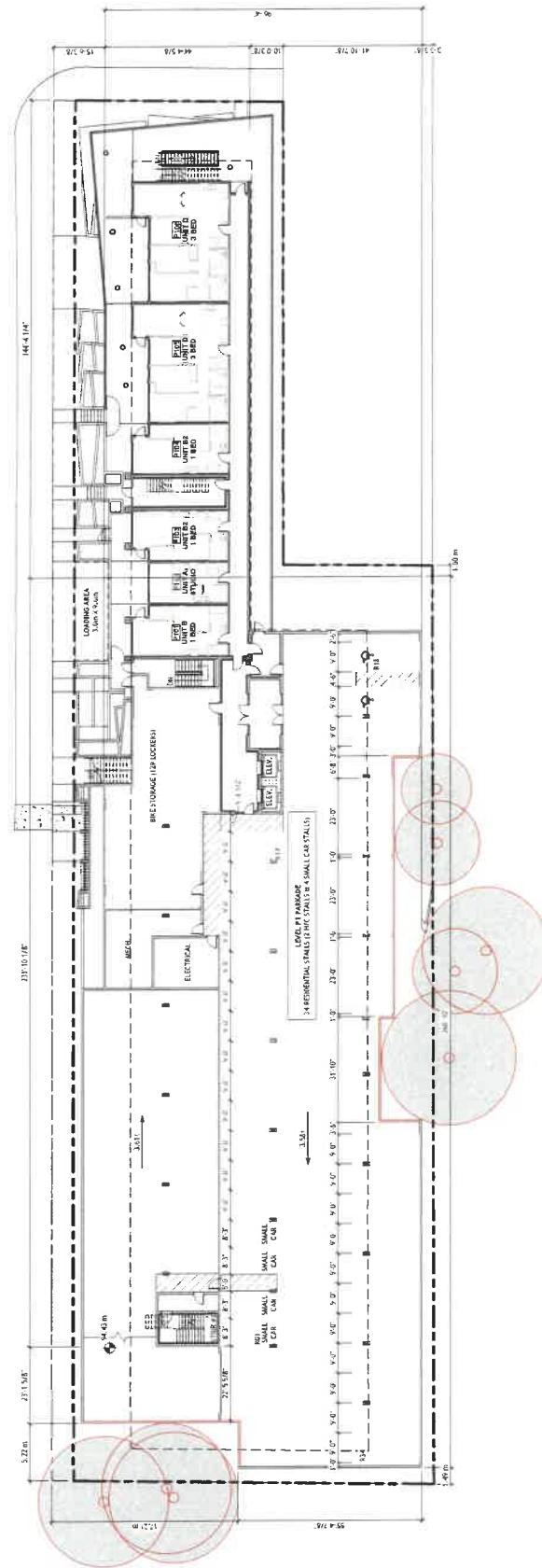


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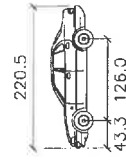
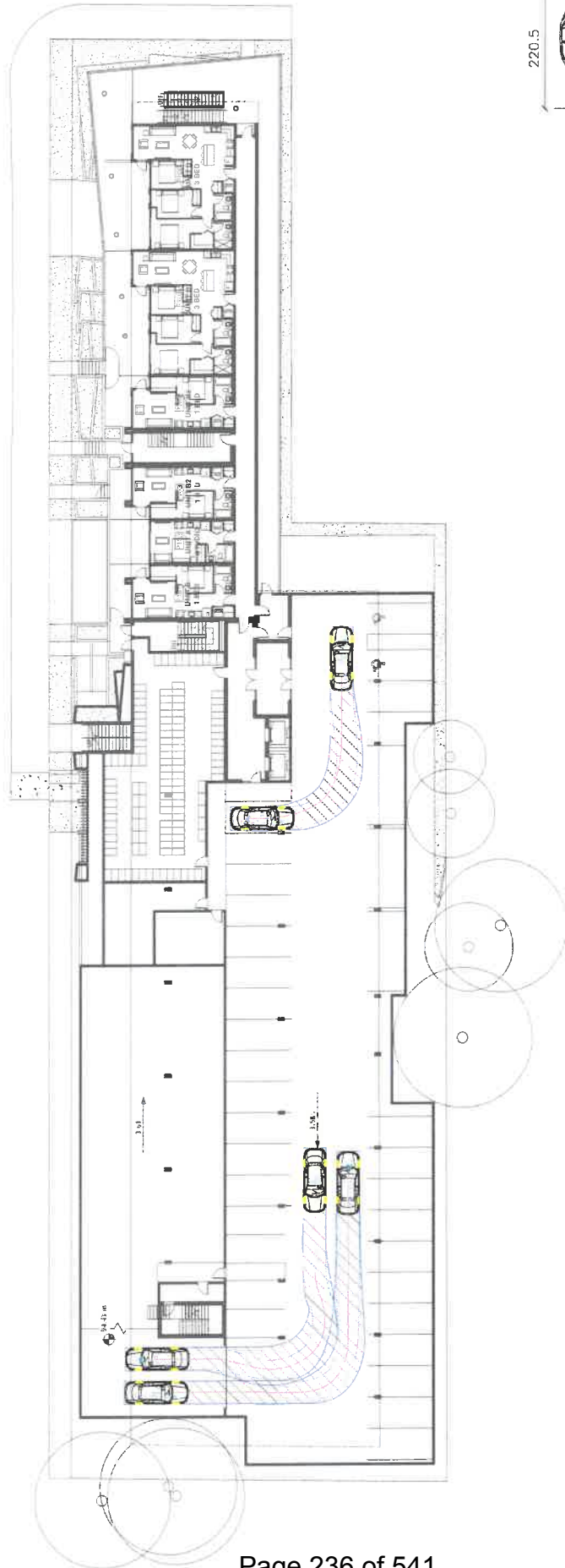






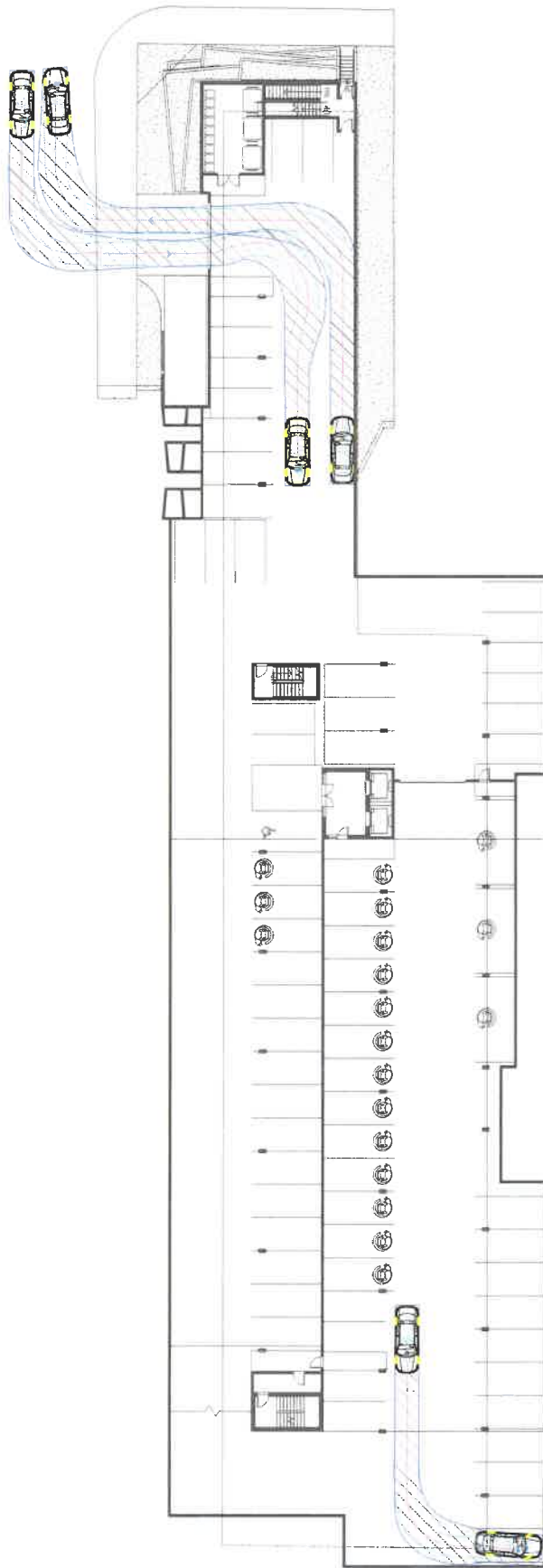
APPENDIX B

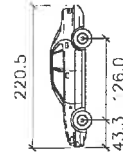
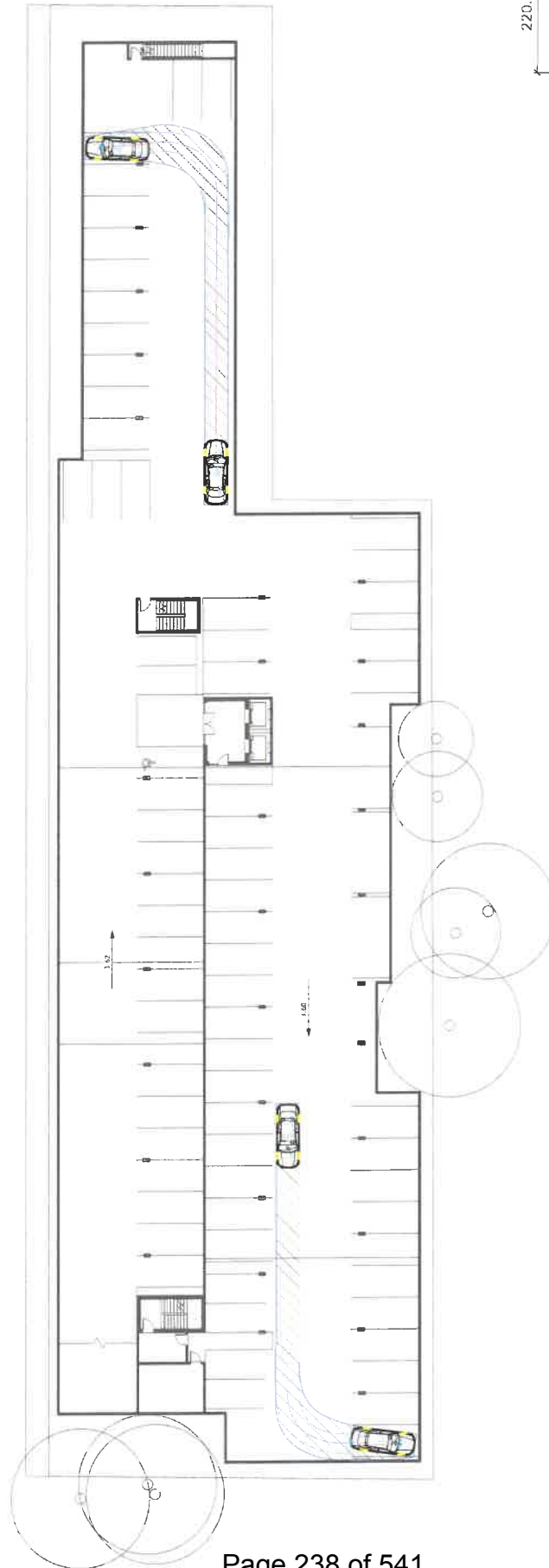
AUTOTURN SITE CIRCULATION ANALYSIS



inches
Width : 78.7
Track : 78.7
Lock to Lock Time : 6.0
Steering Angle : 35.9

P





P

inches
Width : 78.7
Track : 78.7
Lock to Lock Time : 6.0
Steering Angle : 35.9



TITLE PARKADE CIRCULATION LEVEL P3
AUTOTURN ANALYSIS
VIDAL STREET DEVELOPMENT, WHITE ROCK

DATE 8/26/2020
DWG. No. : 03
SCALE : NOT TO SCALE

ACCOMMODATED



CONFLICT





DATE : 6/26/2020	TITLE : LOADING BAY - LSU DESIGN VEHICLE AUTOTURN ANALYSIS VIDAL STREET DEVELOPMENT, WHITE ROCK	 The people behind your infrastructure.	ACCOMMODATED 	CONFLICT 
DWG. No. : 04				
SCALE : NOT_TO_SCALE				



TITLE	DATE	6/26/2020
	DWG. No.	05
	SCALE	NOT TO SCALE
LOADING BAY - MSU DESIGN VEHICLE AUTOTURN ANALYSIS VIDAL STREET DEVELOPMENT, WHITE ROCK		
BINNIE The people behind your infrastructure.		
ACCOMMODATED CONFLICT		

APPENDIX C

ITE TRIP GENERATION REFERENCE

Land Use: 221

Multifamily Housing (Mid-Rise)

Description

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors). Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (Land Use 225), and mid-rise residential with 1st-floor commercial (Land Use 231) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the mid-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.46 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 95.7 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 4:45 and 5:45 p.m., respectively.

For the four dense multi-use urban sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:15 and 5:15 p.m., respectively. For the three center city core sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 6:45 and 7:45 a.m. and 5:00 and 6:00 p.m., respectively.

For the six sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.46 residents per occupied dwelling unit.

For the five sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 95.7 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the five center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 1.84 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.94 during Weekday, AM Peak Hour of Generator
- 2.07 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.59 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 32 dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.90 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.90 during Weekday, AM Peak Hour of Generator
- 2.00 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.08 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 13 general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.56 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.88 during Weekday, AM Peak Hour of Generator
- 1.70 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.07 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), British Columbia (CAN), California, Delaware, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, New Hampshire, New Jersey, Ontario, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Utah, Virginia, and Wisconsin.

Source Numbers

168, 188, 204, 305, 306, 321, 357, 390, 436, 525, 530, 579, 638, 818, 857, 866, 901, 904, 910, 912, 918, 934, 936, 939, 944, 947, 948, 949, 959, 963, 964, 966, 967, 969, 970

Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

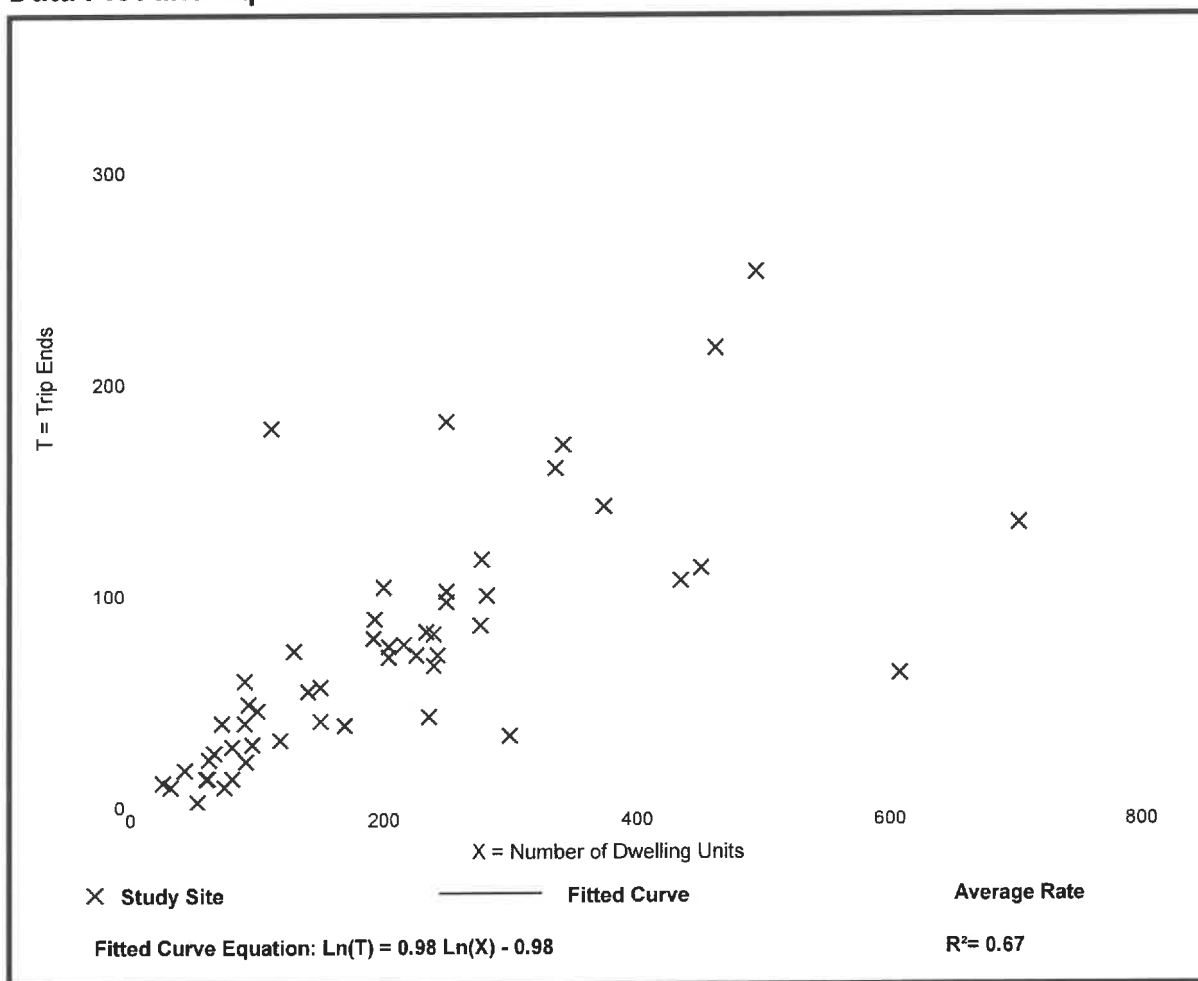
Avg. Num. of Dwelling Units: 207

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.06 - 1.61	0.19

Data Plot and Equation



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Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

**On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.**

Setting/Location: General Urban/Suburban

Number of Studies: 60

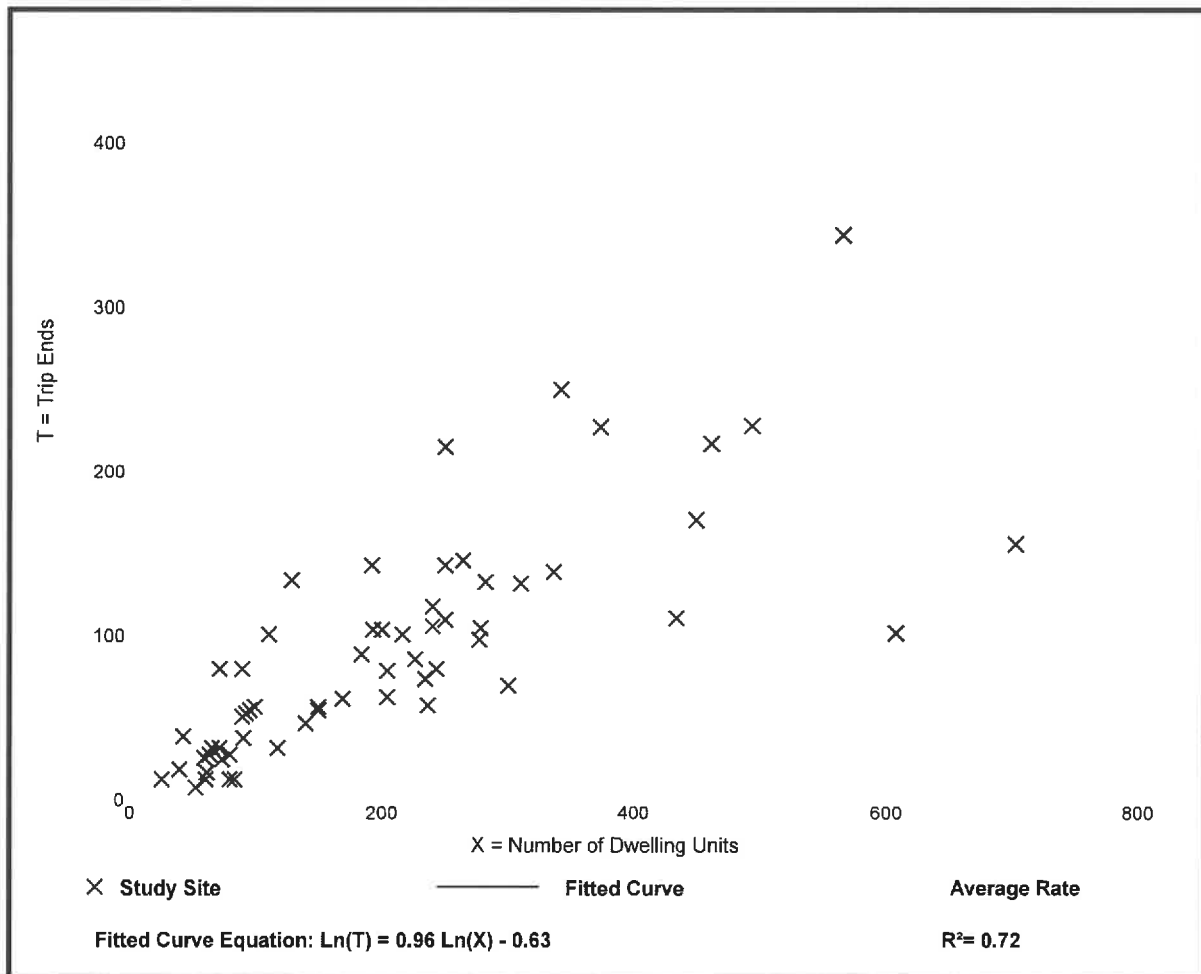
Avg. Num. of Dwelling Units: 208

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

Data Plot and Equation












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APPENDIX D

SYNCHRO ANALYSIS RESULTS

















EXISTING 2019 AM
1: Oxford Street & Thrift Avenue

06/24/2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	11	201	76	22	152	55
Future Volume (Veh/h)	11	201	76	22	152	55
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	12	218	83	24	165	60
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	485	95			107	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	485	95			107	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	98	77			89	
cM capacity (veh/h)	481	962			1484	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	230	107	225			
Volume Left	12	0	165			
Volume Right	218	24	0			
cSH	914	1700	1484			
Volume to Capacity	0.25	0.06	0.11			
Queue Length 95th (m)	7.6	0.0	2.8			
Control Delay (s)	10.3	0.0	5.9			
Lane LOS	B		A			
Approach Delay (s)	10.3	0.0	5.9			
Approach LOS	B					
Intersection Summary						
Average Delay		6.6				
Intersection Capacity Utilization		37.7%		ICU Level of Service	A	
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	171	1	7	200	19	2	0	7	12	2	10
Future Volume (Veh/h)	2	171	1	7	200	19	2	0	7	12	2	10
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	186	1	8	217	21	2	0	8	13	2	11
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	238			187			446	444	186	442	434	228
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	238			187			446	444	186	442	434	228
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	99	97	100	99
cM capacity (veh/h)	1329			1387			511	504	856	518	511	812
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	189	246	10	26								
Volume Left	2	8	2	13								
Volume Right	1	21	8	11								
cSH	1329	1387	754	611								
Volume to Capacity	0.00	0.01	0.01	0.04								
Queue Length 95th (m)	0.0	0.1	0.3	1.0								
Control Delay (s)	0.1	0.3	9.8	11.2								
Lane LOS	A	A	A	B								
Approach Delay (s)	0.1	0.3	9.8	11.2								
Approach LOS			A	B								
Intersection Summary												
Average Delay			1.0									
Intersection Capacity Utilization			26.0%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	190	0	1	204	6	10	0	12	10	2	12
Future Volume (Veh/h)	0	190	0	1	204	6	10	0	12	10	2	12
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	207	0	1	222	7	11	0	13	11	2	13
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	229			207			448	438	207	448	434	226
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	229			207			448	438	207	448	434	226
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			98	100	98	98	100	98
cM capacity (veh/h)	1339			1364			510	512	833	513	514	814
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	207	230	24	26								
Volume Left	0	1	11	11								
Volume Right	0	7	13	13								
cSH	1339	1364	646	629								
Volume to Capacity	0.00	0.00	0.04	0.04								
Queue Length 95th (m)	0.0	0.0	0.9	1.0								
Control Delay (s)	0.0	0.0	10.8	11.0								
Lane LOS		A	B	B								
Approach Delay (s)	0.0	0.0	10.8	11.0								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			21.9%		ICU Level of Service					A		
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	4	200	8	2	197	4	7	6	6	12	2	7
Future Volume (Veh/h)	4	200	8	2	197	4	7	6	6	12	2	7
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	4	217	9	2	214	4	8	7	7	13	2	8
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	218			226			458	452	222	460	454	216
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	218			226			458	452	222	460	454	216
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			98	99	99	97	100	99
cM capacity (veh/h)	1352			1342			504	501	818	500	500	824
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	230	220	22	23								
Volume Left	4	2	8	13								
Volume Right	9	4	7	8								
cSH	1352	1342	573	579								
Volume to Capacity	0.00	0.00	0.04	0.04								
Queue Length 95th (m)	0.1	0.0	0.9	0.9								
Control Delay (s)	0.2	0.1	11.5	11.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.1	11.5	11.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.2									
Intersection Capacity Utilization			23.5%		ICU Level of Service				A			
Analysis Period (min)			15									










15: Oxford Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	12	3	5	272	203	4
Future Volume (Veh/h)	12	3	5	272	203	4
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	3	5	296	221	4
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	529	223	225			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	529	223	225			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	97	100	100			
cM capacity (veh/h)	508	817	1344			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	16	301	225			
Volume Left	13	5	0			
Volume Right	3	0	4			
cSH	547	1344	1700			
Volume to Capacity	0.03	0.00	0.13			
Queue Length 95th (m)	0.7	0.1	0.0			
Control Delay (s)	11.8	0.2	0.0			
Lane LOS	B	A				
Approach Delay (s)	11.8	0.2	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		28.3%		ICU Level of Service	A	
Analysis Period (min)		15				

















EXISTING 2019 PM
1: Oxford Street & Thrift Avenue

06/24/2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	21	211	61	8	162	104
Future Volume (Veh/h)	21	211	61	8	162	104
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	23	229	66	9	176	113
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	536	70			75	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	536	70			75	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	95	77			88	
cM capacity (veh/h)	447	992			1524	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	252	75	289			
Volume Left	23	0	176			
Volume Right	229	9	0			
cSH	893	1700	1524			
Volume to Capacity	0.28	0.04	0.12			
Queue Length 95th (m)	8.8	0.0	3.0			
Control Delay (s)	10.6	0.0	5.0			
Lane LOS	B		A			
Approach Delay (s)	10.6	0.0	5.0			
Approach LOS	B					
Intersection Summary						
Average Delay		6.7				
Intersection Capacity Utilization		42.0%		ICU Level of Service		A
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	166	2	9	224	19	0	2	4	8	0	8
Future Volume (Veh/h)	2	166	2	9	224	19	0	2	4	8	0	8
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	180	2	10	243	21	0	2	4	9	0	9
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	264			182			468	469	181	464	460	254
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	264			182			468	469	181	464	460	254
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	100	98	100	99
cM capacity (veh/h)	1300			1393			497	488	862	501	494	785
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	184	274	6	18								
Volume Left	2	10	0	9								
Volume Right	2	21	4	9								
cSH	1300	1393	686	612								
Volume to Capacity	0.00	0.01	0.01	0.03								
Queue Length 95th (m)	0.0	0.2	0.2	0.7								
Control Delay (s)	0.1	0.3	10.3	11.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	10.3	11.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			0.8									
Intersection Capacity Utilization			32.7%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	4	168	6	10	232	8	6	2	6	8	0	14
Future Volume (Veh/h)	4	168	6	10	232	8	6	2	6	8	0	14
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	4	183	7	11	252	9	7	2	7	9	0	15
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	261			190			488	478	186	481	476	256
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	261			190			488	478	186	481	476	256
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	99	98	100	98
cM capacity (veh/h)	1303			1384			477	481	856	486	482	782
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	194	272	16	24								
Volume Left	4	11	7	9								
Volume Right	7	9	7	15								
cSH	1303	1384	592	636								
Volume to Capacity	0.00	0.01	0.03	0.04								
Queue Length 95th (m)	0.1	0.2	0.6	0.9								
Control Delay (s)	0.2	0.4	11.2	10.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.4	11.2	10.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			27.6%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	180	0	8	236	8	0	2	0	20	8	14
Future Volume (Veh/h)	2	180	0	8	236	8	0	2	0	20	8	14
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	196	0	9	257	9	0	2	0	22	9	15
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	266			196			499	484	196	480	480	262
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	266			196			499	484	196	480	480	262
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	100	96	98	98
cM capacity (veh/h)	1298			1377			463	479	845	491	482	777
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	198	275	2	46								
Volume Left	2	9	0	22								
Volume Right	0	9	0	15								
cSH	1298	1377	479	556								
Volume to Capacity	0.00	0.01	0.00	0.08								
Queue Length 95th (m)	0.0	0.2	0.1	2.0								
Control Delay (s)	0.1	0.3	12.6	12.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	12.6	12.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			33.5%		ICU Level of Service					A		
Analysis Period (min)			15									

15: Oxford Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	11	3	6	266	263	8
Future Volume (Veh/h)	11	3	6	266	263	8
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	12	3	7	289	286	9
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	594	290	295			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	594	290	295			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	97	100	99			
cM capacity (veh/h)	465	749	1266			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	15	296	295			
Volume Left	12	7	0			
Volume Right	3	0	9			
cSH	503	1266	1700			
Volume to Capacity	0.03	0.01	0.17			
Queue Length 95th (m)	0.7	0.1	0.0			
Control Delay (s)	12.4	0.2	0.0			
Lane LOS	B	A				
Approach Delay (s)	12.4	0.2	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		28.8%		ICU Level of Service	A	
Analysis Period (min)		15				

EXISTING 2022 AM

















1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W	R	T	R	L	R
Traffic Volume (veh/h)	18	269	98	28	203	71
Future Volume (Veh/h)	18	269	98	28	203	71
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	20	292	107	30	221	77
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	641	122			137	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	641	122			137	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	95	69			85	
cM capacity (veh/h)	372	929			1447	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	312	137	298			
Volume Left	20	0	221			
Volume Right	292	30	0			
cSH	848	1700	1447			
Volume to Capacity	0.37	0.08	0.15			
Queue Length 95th (m)	12.9	0.0	4.1			
Control Delay (s)	11.7	0.0	6.2			
Lane LOS	B		A			
Approach Delay (s)	11.7	0.0	6.2			
Approach LOS	B					
Intersection Summary						
Average Delay		7.4				
Intersection Capacity Utilization		49.5%		ICU Level of Service		A
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	231	1	9	263	25	3	0	9	16	3	13
Future Volume (Veh/h)	3	231	1	9	263	25	3	0	9	16	3	13
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	251	1	10	286	27	3	0	10	17	3	14
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	313			252			592	590	252	587	578	300
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	313			252			592	590	252	587	578	300
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	99	96	99	98
cM capacity (veh/h)	1247			1313			404	416	787	413	423	740
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	255	323	13	34								
Volume Left	3	10	3	17								
Volume Right	1	27	10	14								
cSH	1247	1313	646	506								
Volume to Capacity	0.00	0.01	0.02	0.07								
Queue Length 95th (m)	0.1	0.2	0.5	1.6								
Control Delay (s)	0.1	0.3	10.7	12.6								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	10.7	12.6								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			30.8%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	256	0	1	268	8	13	0	16	13	3	16
Future Volume (Veh/h)	0	256	0	1	268	8	13	0	16	13	3	16
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	278	0	1	291	9	14	0	17	14	3	17
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	300			278			594	580	278	592	576	296
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	300			278			594	580	278	592	576	296
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
pD queue free %	100			100			97	100	98	97	99	98
cM capacity (veh/h)	1261			1285			405	425	761	408	428	744
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	278	301	31	34								
Volume Left	0	1	14	14								
Volume Right	0	9	17	17								
cSH	1261	1285	544	530								
Volume to Capacity	0.00	0.00	0.06	0.06								
Queue Length 95th (m)	0.0	0.0	1.4	1.6								
Control Delay (s)	0.0	0.0	12.0	12.3								
Lane LOS		A	B	B								
Approach Delay (s)	0.0	0.0	12.0	12.3								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.2									
Intersection Capacity Utilization			25.4%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	269	10	3	259	5	9	8	8	16	3	9
Future Volume (Veh/h)	5	269	10	3	259	5	9	8	8	16	3	9
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	292	11	3	282	5	10	9	9	17	3	10
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	287			303			610	600	298	612	604	284
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	287			303			610	600	298	612	604	284
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			97	98	99	96	99	99
cM capacity (veh/h)	1275			1258			397	412	742	392	410	754
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	308	290	28	30								
Volume Left	5	3	10	17								
Volume Right	11	5	9	10								
cSH	1275	1258	473	469								
Volume to Capacity	0.00	0.00	0.06	0.06								
Queue Length 95th (m)	0.1	0.1	1.4	1.6								
Control Delay (s)	0.2	0.1	13.1	13.2								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.1	13.1	13.2								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			27.8%		ICU Level of Service					A		
Analysis Period (min)			15									

15: Oxford Street




06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	8	5	6	361	169	5
Future Volume (Veh/h)	8	5	6	361	169	5
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	9	5	7	392	184	5
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	592	186	189			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	592	186	189			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	98	99	99			
cM capacity (veh/h)	466	856	1385			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	14	399	189			
Volume Left	9	7	0			
Volume Right	5	0	5			
cSH	557	1385	1700			
Volume to Capacity	0.03	0.01	0.11			
Queue Length 95th (m)	0.6	0.1	0.0			
Control Delay (s)	11.6	0.2	0.0			
Lane LOS	B	A				
Approach Delay (s)	11.6	0.2	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		33.8%		ICU Level of Service	A	
Analysis Period (min)		15				

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1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	27	279	79	10	216	135
Future Volume (Veh/h)	27	279	79	10	216	135
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	29	303	86	11	235	147
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	708	92			97	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	708	92			97	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	91	69			84	
cM capacity (veh/h)	338	966			1496	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	332	97	382			
Volume Left	29	0	235			
Volume Right	303	11	0			
cSH	831	1700	1496			
Volume to Capacity	0.40	0.06	0.16			
Queue Length 95th (m)	14.7	0.0	4.2			
Control Delay (s)	12.2	0.0	5.4			
Lane LOS	B		A			
Approach Delay (s)	12.2	0.0	5.4			
Approach LOS	B					
Intersection Summary						
Average Delay			7.5			
Intersection Capacity Utilization			51.1%	ICU Level of Service		A
Analysis Period (min)			15			

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	ESR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	221	3	12	299	25	0	3	5	10	0	10
Future Volume (Veh/h)	3	221	3	12	299	25	0	3	5	10	0	10
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	240	3	13	325	27	0	3	5	11	0	11
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	352			243			623	626	242	618	614	338
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	352			243			623	626	242	618	614	338
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	99	97	100	98
cM capacity (veh/h)	1207			1323			389	396	797	393	402	704
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	246	365	8	22								
Volume Left	3	13	0	11								
Volume Right	3	27	5	11								
cSH	1207	1323	578	504								
Volume to Capacity	0.00	0.01	0.01	0.04								
Queue Length 95th (m)	0.1	0.2	0.3	1.0								
Control Delay (s)	0.1	0.4	11.3	12.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	11.3	12.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			0.8									
Intersection Capacity Utilization			38.9%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	223	8	13	309	10	8	3	8	10	0	18
Future Volume (Veh/h)	5	223	8	13	309	10	8	3	8	10	0	18
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	242	9	14	336	11	9	3	9	11	0	20
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None				None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	347			251			646	632	246	636	630	342
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	347			251			646	632	246	636	630	342
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			98	99	99	97	100	97
cM capacity (veh/h)	1212			1314			369	392	792	379	392	701
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	256	361	21	31								
Volume Left	5	14	9	11								
Volume Right	9	11	9	20								
cSH	1212	1314	484	539								
Volume to Capacity	0.00	0.01	0.04	0.06								
Queue Length 95th (m)	0.1	0.2	1.0	1.4								
Control Delay (s)	0.2	0.4	12.8	12.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.4	12.8	12.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			33.4%	ICU Level of Service					A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	239	0	10	314	10	0	3	0	26	10	18
Future Volume (Veh/h)	3	239	0	10	314	10	0	3	0	26	10	18
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	260	0	11	341	11	0	3	0	28	11	20
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	352			260			660	640	260	636	634	346
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	352			260			660	640	260	636	634	346
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	100	93	97	97
cM capacity (veh/h)	1207			1304			355	389	779	385	392	697
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	263	363	3	59								
Volume Left	3	11	0	28								
Volume Right	0	11	0	20								
cSH	1207	1304	389	456								
Volume to Capacity	0.00	0.01	0.01	0.13								
Queue Length 95th (m)	0.1	0.2	0.2	3.4								
Control Delay (s)	0.1	0.3	14.3	14.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	14.3	14.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			39.5%		ICU Level of Service				A			
Analysis Period (min)			15									

15: Oxford Street










06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	14	4	8	350	347	10
Future Volume (Veh/h)	14	4	8	350	347	10
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	4	9	380	377	11
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	780	382	388			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	780	382	388			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	361	665	1170			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	19	389	388			
Volume Left	15	9	0			
Volume Right	4	0	11			
cSH	399	1170	1700			
Volume to Capacity	0.05	0.01	0.23			
Queue Length 95th (m)	1.1	0.2	0.0			
Control Delay (s)	14.5	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.5	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.5				
Intersection Capacity Utilization		34.8%		ICU Level of Service	A	
Analysis Period (min)		15				

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1: Oxford Street & Thrift Avenue

06/24/2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	12	220	81	23	167	58
Future Volume (Veh/h)	12	220	81	23	167	58
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	239	88	25	182	63
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	528	100			113	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	528	100			113	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	97	75			88	
cM capacity (veh/h)	448	955			1476	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	252	113	245			
Volume Left	13	0	182			
Volume Right	239	25	0			
cSH	902	1700	1476			
Volume to Capacity	0.28	0.07	0.12			
Queue Length 95th (m)	8.7	0.0	3.2			
Control Delay (s)	10.5	0.0	6.0			
Lane LOS	B		A			
Approach Delay (s)	10.5	0.0	6.0			
Approach LOS	B					
Intersection Summary						
Average Delay		6.8				
Intersection Capacity Utilization		39.9%		ICU Level of Service	A	
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	191	1	7	216	20	2	0	7	13	2	11
Future Volume (Veh/h)	2	191	1	7	216	20	2	0	7	13	2	11
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	208	1	8	235	22	2	0	8	14	2	12
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	257			209			488	486	208	482	475	246
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	257			209			488	486	208	482	475	246
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	99	97	100	98
cM capacity (veh/h)	1308			1362			479	478	832	487	485	793
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	211	265	10	28								
Volume Left	2	8	2	14								
Volume Right	1	22	8	12								
cSH	1308	1362	725	583								
Volume to Capacity	0.00	0.01	0.01	0.05								
Queue Length 95th (m)	0.0	0.1	0.3	1.1								
Control Delay (s)	0.1	0.3	10.0	11.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	10.0	11.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.0									
Intersection Capacity Utilization			27.0%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	212	0	1	220	6	11	0	13	11	2	13
Future Volume (Veh/h)	0	212	0	1	220	6	11	0	13	11	2	13
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	230	0	1	239	7	12	0	14	12	2	14
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	246			230			490	478	230	488	474	242
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	246			230			490	478	230	488	474	242
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			97	100	98	98	100	98
cM capacity (veh/h)	1320			1338			479	486	809	481	488	796
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	230	247	26	28								
Volume Left	0	1	12	12								
Volume Right	0	7	14	14								
cSH	1320	1338	614	601								
Volume to Capacity	0.00	0.00	0.04	0.05								
Queue Length 95th (m)	0.0	0.0	1.0	1.1								
Control Delay (s)	0.0	0.0	11.1	11.3								
Lane LOS		A	B	B								
Approach Delay (s)	0.0	0.0	11.1	11.3								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.2									
Intersection Capacity Utilization			22.7%		ICU Level of Service					A		
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	4	222	8	2	213	4	7	6	6	13	2	7
Future Volume (Veh/h)	4	222	8	2	213	4	7	6	6	13	2	7
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	4	241	9	2	232	4	8	7	7	14	2	8
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	236			250			500	494	246	502	496	234
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	236			250			500	494	246	502	496	234
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			98	99	99	97	100	99
cM capacity (veh/h)	1331			1316			473	475	793	469	473	805
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	254	238	22	24								
Volume Left	4	2	8	14								
Volume Right	9	4	7	8								
cSH	1331	1316	543	545								
Volume to Capacity	0.00	0.00	0.04	0.04								
Queue Length 95th (m)	0.1	0.0	1.0	1.0								
Control Delay (s)	0.1	0.1	11.9	11.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.1	11.9	11.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			24.7%		ICU Level of Service					A		
Analysis Period (min)			15									

15: Oxford Street




06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	6	4	5	298	221	4
Future Volume (Veh/h)	6	4	5	298	221	4
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	7	4	5	324	240	4
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	576	242	244			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	576	242	244			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	100			
cM capacity (veh/h)	477	797	1322			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	11	329	244			
Volume Left	7	5	0			
Volume Right	4	0	4			
cSH	559	1322	1700			
Volume to Capacity	0.02	0.00	0.14			
Queue Length 95th (m)	0.5	0.1	0.0			
Control Delay (s)	11.6	0.2	0.0			
Lane LOS	B	A				
Approach Delay (s)	11.6	0.2	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.3				
Intersection Capacity Utilization		29.7%		ICU Level of Service	A	
Analysis Period (min)		15				

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1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	22	230	65	8	179	110
Future Volume (Veh/h)	22	230	65	8	179	110
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	24	250	71	9	195	120
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	586	76			80	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	586	76			80	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	94	75			87	
cM capacity (veh/h)	412	986			1518	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	274	80	315			
Volume Left	24	0	195			
Volume Right	250	9	0			
cSH	879	1700	1518			
Volume to Capacity	0.31	0.05	0.13			
Queue Length 95th (m)	10.2	0.0	3.4			
Control Delay (s)	10.9	0.0	5.2			
Lane LOS	B		A			
Approach Delay (s)	10.9	0.0	5.2			
Approach LOS	B					
Intersection Summary						
Average Delay			6.9			
Intersection Capacity Utilization		44.5%		ICU Level of Service		A
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	182	2	10	247	20	0	2	4	8	0	8
Future Volume (Veh/h)	2	182	2	10	247	20	0	2	4	8	0	8
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	198	2	11	268	22	0	2	4	9	0	9
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	290			200			513	515	199	509	505	279
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	290			200			513	515	199	509	505	279
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	100	98	100	99
cM capacity (veh/h)	1272			1372			463	459	842	467	465	760
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	202	301	6	18								
Volume Left	2	11	0	9								
Volume Right	2	22	4	9								
cSH	1272	1372	659	579								
Volume to Capacity	0.00	0.01	0.01	0.03								
Queue Length 95th (m)	0.0	0.2	0.2	0.7								
Control Delay (s)	0.1	0.4	10.5	11.4								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	10.5	11.4								
Approach LOS			B	B								
Intersection Summary												
Average Delay			0.7									
Intersection Capacity Utilization			34.7%		ICU Level of Service					A		
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	4	184	6	11	255	8	6	2	6	8	0	15
Future Volume (Veh/h)	4	184	6	11	255	8	6	2	6	8	0	15
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	4	200	7	12	277	9	7	2	7	9	0	16
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	286			207			533	522	204	525	520	282
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	286			207			533	522	204	525	520	282
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			98	100	99	98	100	98
cM capacity (veh/h)	1276			1364			444	454	837	454	455	757
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	211	298	16	25								
Volume Left	4	12	7	9								
Volume Right	7	9	7	16								
cSH	1276	1364	561	610								
Volume to Capacity	0.00	0.01	0.03	0.04								
Queue Length 95th (m)	0.1	0.2	0.7	1.0								
Control Delay (s)	0.2	0.4	11.6	11.2								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.4	11.6	11.2								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			29.5%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	197	0	8	259	8	0	2	0	21	8	15
Future Volume (Veh/h)	2	197	0	8	259	8	0	2	0	21	8	15
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	214	0	9	282	9	0	2	0	23	9	16
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	291			214			543	527	214	524	522	286
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	291			214			543	527	214	524	522	286
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	100	95	98	98
cM capacity (veh/h)	1271			1356			432	453	826	460	455	753
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	216	300	2	48								
Volume Left	2	9	0	23								
Volume Right	0	9	0	16								
cSH	1271	1356	453	527								
Volume to Capacity	0.00	0.01	0.00	0.09								
Queue Length 95th (m)	0.0	0.2	0.1	2.3								
Control Delay (s)	0.1	0.3	13.0	12.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	13.0	12.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			35.0%		ICU Level of Service					A		
Analysis Period (min)			15									

















15: Oxford Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	12	3	6	289	286	8
Future Volume (Veh/h)	12	3	6	289	286	8
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	3	7	314	311	9
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	644	316	320			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	644	316	320			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	97	100	99			
cM capacity (veh/h)	435	725	1240			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	16	321	320			
Volume Left	13	7	0			
Volume Right	3	0	9			
cSH	470	1240	1700			
Volume to Capacity	0.03	0.01	0.19			
Queue Length 95th (m)	0.8	0.1	0.0			
Control Delay (s)	12.9	0.2	0.0			
Lane LOS	B	A				
Approach Delay (s)	12.9	0.2	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		30.0%	ICU Level of Service	A		
Analysis Period (min)		15				

2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	296	2	12	339	32	3	0	12	20	3	17
Future Volume (Veh/h)	3	296	2	12	339	32	3	0	12	20	3	17
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	322	2	13	368	35	3	0	13	22	3	18
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	403			324			760	758	323	754	742	386
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	403			324			760	758	323	754	742	386
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	98	93	99	97
cM capacity (veh/h)	1156			1236			309	332	718	317	339	662
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	327	416	16	43								
Volume Left	3	13	3	22								
Volume Right	2	35	13	18								
cSH	1156	1236	575	408								
Volume to Capacity	0.00	0.01	0.03	0.11								
Queue Length 95th (m)	0.1	0.2	0.7	2.7								
Control Delay (s)	0.1	0.4	11.4	14.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	11.4	14.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			39.0%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Volume (veh/h)	0	328	0	2	345	10	17	0	20	17	3	20
Future Volume (Veh/h)	0	328	0	2	345	10	17	0	20	17	3	20
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	357	0	2	375	11	18	0	22	18	3	22
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	386			357			765	747	357	764	742	380
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	386			357			765	747	357	764	742	380
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			94	100	97	94	99	97
cM capacity (veh/h)	1172			1202			307	341	687	310	343	667
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	357	388	40	43								
Volume Left	0	2	18	18								
Volume Right	0	11	22	22								
cSH	1172	1202	441	431								
Volume to Capacity	0.00	0.00	0.09	0.10								
Queue Length 95th (m)	0.0	0.0	2.3	2.5								
Control Delay (s)	0.0	0.1	14.0	14.3								
Lane LOS		A	B	B								
Approach Delay (s)	0.0	0.1	14.0	14.3								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.4									
Intersection Capacity Utilization			30.4%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	345	13	3	334	7	12	10	10	20	3	12
Future Volume (Veh/h)	7	345	13	3	334	7	12	10	10	20	3	12
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	375	14	3	363	8	13	11	11	22	3	13
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	371			389			786	775	382	788	778	367
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	371			389			786	775	382	788	778	367
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	99			100			96	97	98	93	99	98
cM capacity (veh/h)	1188			1170			300	326	665	294	325	678
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	397	374	35	38								
Volume Left	8	3	13	22								
Volume Right	14	8	11	13								
cSH	1188	1170	374	368								
Volume to Capacity	0.01	0.00	0.09	0.10								
Queue Length 95th (m)	0.2	0.1	2.3	2.6								
Control Delay (s)	0.2	0.1	15.6	15.9								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.2	0.1	15.6	15.9								
Approach LOS			C	C								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			33.4%		ICU Level of Service				A			
Analysis Period (min)			15									

15: Oxford Street




06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	10	7	9	474	345	7
Future Volume (Veh/h)	10	7	9	474	345	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	11	8	10	515	375	8
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	914	379	383			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	914	379	383			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	301	668	1175			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	19	525	383			
Volume Left	11	10	0			
Volume Right	8	0	8			
cSH	391	1175	1700			
Volume to Capacity	0.05	0.01	0.23			
Queue Length 95th (m)	1.2	0.2	0.0			
Control Delay (s)	14.7	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.7	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		42.2%		ICU Level of Service		A
Analysis Period (min)		15				

EXISTING 2045 PM

















1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	35	362	97	13	275	171
Future Volume (Veh/h)	35	362	97	13	275	171
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	38	393	105	14	299	186
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	896	112			119	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	896	112			119	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	85	58			80	
cM capacity (veh/h)	247	941			1469	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	431	119	485			
Volume Left	38	0	299			
Volume Right	393	14	0			
cSH	755	1700	1469			
Volume to Capacity	0.57	0.07	0.20			
Queue Length 95th (m)	27.8	0.0	5.8			
Control Delay (s)	15.9	0.0	5.7			
Lane LOS	C		A			
Approach Delay (s)	15.9	0.0	5.7			
Approach LOS	C					
Intersection Summary						
Average Delay			9.3			
Intersection Capacity Utilization			61.9%	ICU Level of Service		B
Analysis Period (min)			15			

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	288	3	15	379	32	3	3	7	13	0	13
Future Volume (Veh/h)	3	288	3	15	379	32	3	3	7	13	0	13
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	313	3	16	412	35	3	3	8	14	0	14
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	447			316			796	800	314	792	784	430
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	447			316			796	800	314	792	784	430
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	99	99	95	100	98
cM capacity (veh/h)	1113			1244			295	313	726	298	320	626
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	319	463	14	28								
Volume Left	3	16	3	14								
Volume Right	3	35	8	14								
cSH	1113	1244	455	404								
Volume to Capacity	0.00	0.01	0.03	0.07								
Queue Length 95th (m)	0.1	0.3	0.7	1.7								
Control Delay (s)	0.1	0.4	13.2	14.6								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	13.2	14.6								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.0									
Intersection Capacity Utilization			41.4%		ICU Level of Service					A		
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	291	10	17	392	13	10	3	10	13	0	23
Future Volume (Veh/h)	7	291	10	17	392	13	10	3	10	13	0	23
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	316	11	18	426	14	11	3	11	14	0	25
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	440			327			832	814	322	819	812	433
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	440			327			832	814	322	819	812	433
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	99			99			96	99	98	95	100	96
cM capacity (veh/h)	1120			1233			272	306	719	283	306	623
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	335	458	25	39								
Volume Left	8	18	11	14								
Volume Right	11	14	11	25								
cSH	1120	1233	382	435								
Volume to Capacity	0.01	0.01	0.07	0.09								
Queue Length 95th (m)	0.2	0.3	1.6	2.2								
Control Delay (s)	0.3	0.5	15.1	14.1								
Lane LOS	A	A	C	B								
Approach Delay (s)	0.3	0.5	15.1	14.1								
Approach LOS			C	B								
Intersection Summary												
Average Delay			1.4									
Intersection Capacity Utilization			39.8%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	311	0	13	399	13	0	3	0	33	13	23
Future Volume (Veh/h)	3	311	0	13	399	13	0	3	0	33	13	23
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	338	0	14	434	14	0	3	0	36	14	25
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	448			338			845	820	338	814	813	441
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	448			338			845	820	338	814	813	441
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	100	88	95	96
cM capacity (veh/h)	1112			1221			259	305	704	291	308	616
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	341	462	3	75								
Volume Left	3	14	0	36								
Volume Right	0	14	0	25								
cSH	1112	1221	305	358								
Volume to Capacity	0.00	0.01	0.01	0.21								
Queue Length 95th (m)	0.1	0.3	0.2	5.9								
Control Delay (s)	0.1	0.4	16.9	17.7								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.1	0.4	16.9	17.7								
Approach LOS			C	C								
Intersection Summary												
Average Delay			1.8									
Intersection Capacity Utilization			47.3%		ICU Level of Service					A		
Analysis Period (min)			15									

15: Oxford Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	18	5	10	467	446	13
Future Volume (Veh/h)	18	5	10	467	446	13
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	20	5	11	508	485	14
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	1022	492	499			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1022	492	499			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	92	99	99			
cM capacity (veh/h)	259	577	1065			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	25	519	499			
Volume Left	20	11	0			
Volume Right	5	0	14			
cSH	291	1065	1700			
Volume to Capacity	0.09	0.01	0.29			
Queue Length 95th (m)	2.1	0.2	0.0			
Control Delay (s)	18.5	0.3	0.0			
Lane LOS	C	A				
Approach Delay (s)	18.5	0.3	0.0			
Approach LOS	C					
Intersection Summary						
Average Delay		0.6				
Intersection Capacity Utilization		42.6%		ICU Level of Service	A	
Analysis Period (min)		15				

2022 COMBINED AM

















1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	13	237	81	24	171	58
Future Volume (Veh/h)	13	237	81	24	171	58
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	258	88	26	186	63
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	536	101			114	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	536	101			114	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	97	73			87	
cM capacity (veh/h)	442	954			1475	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	272	114	249			
Volume Left	14	0	186			
Volume Right	258	26	0			
cSH	901	1700	1475			
Volume to Capacity	0.30	0.07	0.13			
Queue Length 95th (m)	9.7	0.0	3.3			
Control Delay (s)	10.7	0.0	6.1			
Lane LOS	B		A			
Approach Delay (s)	10.7	0.0	6.1			
Approach LOS	B					
Intersection Summary						
Average Delay		7.0				
Intersection Capacity Utilization		41.2%		ICU Level of Service		A
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	2	196	1	8	232	22	2	0	7	13	2	11
Future Volume (Veh/h)	2	196	1	8	232	22	2	0	7	13	2	11
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	2	213	1	9	252	24	2	0	8	14	2	12
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	276			214			512	512	214	508	500	264
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	276			214			512	512	214	508	500	264
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	100	99	97	100	98
cM capacity (veh/h)	1287			1356			460	462	827	468	469	775
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	216	285	10	28								
Volume Left	2	9	2	14								
Volume Right	1	24	8	12								
cSH	1287	1356	713	564								
Volume to Capacity	0.00	0.01	0.01	0.05								
Queue Length 95th (m)	0.0	0.2	0.3	1.2								
Control Delay (s)	0.1	0.3	10.1	11.7								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	10.1	11.7								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.0									
Intersection Capacity Utilization			28.6%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	212	0	1	220	13	11	0	13	27	2	32
Future Volume (Veh/h)	5	212	0	1	220	13	11	0	13	27	2	32
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	230	0	1	239	14	12	0	14	29	2	35
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	253			230			524	495	230	502	488	246
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	253			230			524	495	230	502	488	246
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			97	100	98	94	100	96
cM capacity (veh/h)	1312			1338			440	474	809	470	478	793
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	235	254	26	66								
Volume Left	5	1	12	29								
Volume Right	0	14	14	35								
cSH	1312	1338	584	600								
Volume to Capacity	0.00	0.00	0.04	0.11								
Queue Length 95th (m)	0.1	0.0	1.1	2.8								
Control Delay (s)	0.2	0.0	11.5	11.7								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.0	11.5	11.7								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.9									
Intersection Capacity Utilization			25.9%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	4	237	9	2	220	4	7	6	6	13	2	7
Future Volume (Veh/h)	4	237	9	2	220	4	7	6	6	13	2	7
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	4	258	10	2	239	4	8	7	7	14	2	8
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	243			268			525	518	263	526	521	241
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	243			268			525	518	263	526	521	241
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			98	98	99	97	100	99
cM capacity (veh/h)	1323			1296			455	460	776	451	458	798
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	272	245	22	24								
Volume Left	4	2	8	14								
Volume Right	10	4	7	8								
cSH	1323	1296	526	528								
Volume to Capacity	0.00	0.00	0.04	0.05								
Queue Length 95th (m)	0.1	0.0	1.0	1.1								
Control Delay (s)	0.1	0.1	12.1	12.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.1	12.1	12.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.1									
Intersection Capacity Utilization			25.6%		ICU Level of Service					A		
Analysis Period (min)			15									










14: Vidal Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	35	12	6	26	0
Future Volume (Veh/h)	0	35	12	6	26	0
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	38	13	7	28	0
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	61	28	28			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	61	28	28			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	96	99			
cM capacity (veh/h)	938	1047	1585			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	38	20	28			
Volume Left	0	13	0			
Volume Right	38	0	0			
cSH	1047	1585	1700			
Volume to Capacity	0.04	0.01	0.02			
Queue Length 95th (m)	0.9	0.2	0.0			
Control Delay (s)	8.6	4.8	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	4.8	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		4.9				
Intersection Capacity Utilization		17.6%		ICU Level of Service		A
Analysis Period (min)		15				

15: Oxford Street





06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	6	4	5	313	225	4
Future Volume (Veh/h)	6	4	5	313	225	4
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	7	4	5	340	245	4
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	597	247	249			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	597	247	249			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	98	99	100			
cM capacity (veh/h)	464	792	1317			
Direction: Lane #	EB 1	NB 1	SB 1			
Volume Total	11	345	249			
Volume Left	7	5	0			
Volume Right	4	0	4			
cSH	546	1317	1700			
Volume to Capacity	0.02	0.00	0.15			
Queue Length 95th (m)	0.5	0.1	0.0			
Control Delay (s)	11.7	0.1	0.0			
Lane LOS	B	A				
Approach Delay (s)	11.7	0.1	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay			0.3			
Intersection Capacity Utilization		30.5%		ICU Level of Service	A	
Analysis Period (min)		15				

2022 COMBINED PM

















1: Oxford Street & Thrift Avenue

06/24/2020

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	19	372	479	11	230	135
Future Volume (Veh/h)	19	372	479	11	230	135
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	21	404	521	12	250	147
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None		None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	1174	527			533	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1174	527			533	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	87	27			76	
cM capacity (veh/h)	161	551			1035	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	425	533	397			
Volume Left	21	0	250			
Volume Right	404	12	0			
cSH	492	1700	1035			
Volume to Capacity	0.86	0.31	0.24			
Queue Length 95th (m)	69.2	0.0	7.2			
Control Delay (s)	43.2	0.0	7.0			
Lane LOS	E		A			
Approach Delay (s)	43.2	0.0	7.0			
Approach LOS	E					
Intersection Summary						
Average Delay			15.6			
Intersection Capacity Utilization			79.8%		ICU Level of Service	D
Analysis Period (min)			15			

















2: Evrall Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	236	3	12	310	26	0	3	5	11	0	10
Future Volume (Veh/h)	3	236	3	12	310	26	0	3	5	11	0	10
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	257	3	13	337	28	0	3	5	12	0	11
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	365			260			652	656	258	648	643	351
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	365			260			652	656	258	648	643	351
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	99	97	100	98
cM capacity (veh/h)	1194			1304			371	381	780	375	387	692
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	263	378	8	23								
Volume Left	3	13	0	12								
Volume Right	3	28	5	11								
cSH	1194	1304	560	480								
Volume to Capacity	0.00	0.01	0.01	0.05								
Queue Length 95th (m)	0.1	0.2	0.3	1.1								
Control Delay (s)	0.1	0.4	11.5	12.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	11.5	12.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			0.8									
Intersection Capacity Utilization			39.7%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	20	184	6	11	255	26	6	2	6	18	0	27
Future Volume (Veh/h)	20	184	6	11	255	26	6	2	6	18	0	27
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	22	200	7	12	277	28	7	2	7	20	0	29
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	305			207			592	576	204	570	566	291
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	305			207			592	576	204	570	566	291
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	98			99			98	100	99	95	100	96
cM capacity (veh/h)	1256			1364			394	416	837	418	422	748
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	229	317	16	49								
Volume Left	22	12	7	20								
Volume Right	7	28	7	29								
cSH	1256	1364	517	566								
Volume to Capacity	0.02	0.01	0.03	0.09								
Queue Length 95th (m)	0.4	0.2	0.7	2.2								
Control Delay (s)	0.9	0.4	12.2	12.0								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.9	0.4	12.2	12.0								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.8									
Intersection Capacity Utilization			28.3%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/24/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	249	0	10	332	10	0	3	0	26	10	18
Future Volume (Veh/h)	3	249	0	10	332	10	0	3	0	26	10	18
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	271	0	11	361	11	0	3	0	28	11	20
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	372			271			691	671	271	667	666	366
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	372			271			691	671	271	667	666	366
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	100	92	97	97
cM capacity (veh/h)	1186			1292			338	373	768	367	376	679
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	274	383	3	59								
Volume Left	3	11	0	28								
Volume Right	0	11	0	20								
cSH	1186	1292	373	437								
Volume to Capacity	0.00	0.01	0.01	0.14								
Queue Length 95th (m)	0.1	0.2	0.2	3.5								
Control Delay (s)	0.1	0.3	14.7	14.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	14.7	14.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			40.5%		ICU Level of Service				A			
Analysis Period (min)			15									










14: Vidal Street

06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	22	34	14	23	1
Future Volume (Veh/h)	0	22	34	14	23	1
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	24	37	15	25	1
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	114	26	26			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	114	26	26			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	98	98			
cM capacity (veh/h)	861	1050	1588			
Direction: Lane #	EB 1	NB 1	SB 1			
Volume Total	24	52	26			
Volume Left	0	37	0			
Volume Right	24	0	1			
cSH	1050	1588	1700			
Volume to Capacity	0.02	0.02	0.02			
Queue Length 95th (m)	0.5	0.5	0.0			
Control Delay (s)	8.5	5.3	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.5	5.3	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		4.7				
Intersection Capacity Utilization		19.3%		ICU Level of Service		A
Analysis Period (min)		15				

15: Oxford Street










06/24/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	14	4	8	360	361	10
Future Volume (Veh/h)	14	4	8	360	361	10
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	4	9	391	392	11
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	806	398	403			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	806	398	403			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	348	652	1156			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	19	400	403			
Volume Left	15	9	0			
Volume Right	4	0	11			
cSH	386	1156	1700			
Volume to Capacity	0.05	0.01	0.24			
Queue Length 95th (m)	1.2	0.2	0.0			
Control Delay (s)	14.8	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.8	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.5				
Intersection Capacity Utilization		35.4%		ICU Level of Service	A	
Analysis Period (min)		15				

2032 COMBINED AM

















1: Oxford Street & Thrift Avenue

06/26/2020

1. Oxford Street & Thrift Avenue						
						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	15	284	98	29	207	71
Future Volume (Veh/h)	15	284	98	29	207	71
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	16	309	107	32	225	77
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	650	123			139	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	650	123			139	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	96	67			84	
cM capacity (veh/h)	366	928			1445	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	325	139	302			
Volume Left	16	0	225			
Volume Right	309	32	0			
cSH	863	1700	1445			
Volume to Capacity	0.38	0.08	0.16			
Queue Length 95th (m)	13.4	0.0	4.2			
Control Delay (s)	11.7	0.0	6.3			
Lane LOS	B		A			
Approach Delay (s)	11.7	0.0	6.3			
Approach LOS	B					
Intersection Summary						
Average Delay			7.4			
Intersection Capacity Utilization			50.5%	ICU Level of Service	A	
Analysis Period (min)			15			

















2: Evrall Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	236	1	10	379	27	3	0	9	16	3	13
Future Volume (Veh/h)	3	236	1	10	379	27	3	0	9	16	3	13
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	257	1	11	412	29	3	0	10	17	3	14
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	441			258			728	726	258	722	712	426
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	441			258			728	726	258	722	712	426
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	99	95	99	98
cM capacity (veh/h)	1119			1307			327	347	781	335	353	628
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	261	452	13	34								
Volume Left	3	11	3	17								
Volume Right	1	29	10	14								
cSH	1119	1307	591	417								
Volume to Capacity	0.00	0.01	0.02	0.08								
Queue Length 95th (m)	0.1	0.2	0.5	2.0								
Control Delay (s)	0.1	0.3	11.2	14.4								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	11.2	14.4								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.0									
Intersection Capacity Utilization			37.6%			ICU Level of Service			A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	256	0	1	268	15	13	0	16	29	3	35
Future Volume (Veh/h)	5	256	0	1	268	15	13	0	16	29	3	35
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	278	0	1	291	16	14	0	17	32	3	38
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	307			278			628	597	278	606	589	299
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	307			278			628	597	278	606	589	299
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			96	100	98	92	99	95
cM capacity (veh/h)	1254			1285			371	414	761	398	419	741
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	283	308	31	73								
Volume Left	5	1	14	32								
Volume Right	0	16	17	38								
cSH	1254	1285	516	526								
Volume to Capacity	0.00	0.00	0.06	0.14								
Queue Length 95th (m)	0.1	0.0	1.5	3.6								
Control Delay (s)	0.2	0.0	12.4	12.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.0	12.4	12.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			2.0									
Intersection Capacity Utilization			28.7%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	284	11	3	266	5	9	8	8	16	3	9
Future Volume (Veh/h)	5	284	11	3	266	5	9	8	8	16	3	9
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	309	12	3	289	5	10	9	9	17	3	10
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	294			321			634	625	315	636	628	292
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	294			321			634	625	315	636	628	292
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			97	98	99	95	99	99
cM capacity (veh/h)	1268			1239			382	399	725	377	397	748
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	326	297	28	30								
Volume Left	5	3	10	17								
Volume Right	12	5	9	10								
cSH	1268	1239	458	455								
Volume to Capacity	0.00	0.00	0.06	0.07								
Queue Length 95th (m)	0.1	0.1	1.5	1.6								
Control Delay (s)	0.2	0.1	13.4	13.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.2	0.1	13.4	13.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			28.7%		ICU Level of Service				A			
Analysis Period (min)			15									










14: Vidal Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	35	12	8	32	0
Future Volume (Veh/h)	0	35	12	8	32	0
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	38	13	9	35	0
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	70	35	35			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	70	35	35			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	96	99			
cM capacity (veh/h)	927	1038	1576			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	38	22	35			
Volume Left	0	13	0			
Volume Right	38	0	0			
cSH	1038	1576	1700			
Volume to Capacity	0.04	0.01	0.02			
Queue Length 95th (m)	0.9	0.2	0.0			
Control Delay (s)	8.6	4.3	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	4.3	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		4.4				
Intersection Capacity Utilization		17.8%		ICU Level of Service	A	
Analysis Period (min)		15				

15: Oxford Street










06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	8	5	9	376	273	5
Future Volume (Veh/h)	8	5	9	376	273	5
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	9	5	10	409	297	5
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	728	300	302			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	728	300	302			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	98	99	99			
cM capacity (veh/h)	387	740	1259			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	14	419	302			
Volume Left	9	10	0			
Volume Right	5	0	5			
cSH	467	1259	1700			
Volume to Capacity	0.03	0.01	0.18			
Queue Length 95th (m)	0.7	0.2	0.0			
Control Delay (s)	13.0	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	13.0	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay			0.4			
Intersection Capacity Utilization			37.0%	ICU Level of Service	A	
Analysis Period (min)			15			

2032 COMBINED PM

















1: Oxford Street & Thrift Avenue

06/26/2020

1. Oxford Street & Thirsk Avenue						
						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	19	372	79	11	230	135
Future Volume (Veh/h)	19	372	79	11	230	135
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	21	404	86	12	250	147
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	739	92			98	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	739	92			98	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	93	58			83	
cM capacity (veh/h)	320	965			1495	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	425	98	397			
Volume Left	21	0	250			
Volume Right	404	12	0			
cSH	878	1700	1495			
Volume to Capacity	0.48	0.06	0.17			
Queue Length 95th (m)	20.4	0.0	4.6			
Control Delay (s)	12.9	0.0	5.5			
Lane LOS	B		A			
Approach Delay (s)	12.9	0.0	5.5			
Approach LOS	B					
Intersection Summary						
Average Delay			8.3			
Intersection Capacity Utilization			57.2%	ICU Level of Service	B	
Analysis Period (min)			15			

















2: Evrall Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	236	3	12	310	26	0	3	5	11	0	10
Future Volume (Veh/h)	3	236	3	12	310	26	0	3	5	11	0	10
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	257	3	13	337	28	0	3	5	12	0	11
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	365			260			652	656	258	648	643	351
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	365			260			652	656	258	648	643	351
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	99	97	100	98
cM capacity (veh/h)	1194			1304			371	381	780	375	387	692
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	263	378	8	23								
Volume Left	3	13	0	12								
Volume Right	3	28	5	11								
cSH	1194	1304	560	480								
Volume to Capacity	0.00	0.01	0.01	0.05								
Queue Length 95th (m)	0.1	0.2	0.3	1.1								
Control Delay (s)	0.1	0.4	11.5	12.9								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.4	11.5	12.9								
Approach LOS			B	B								
Intersection Summary												
Average Delay			0.8									
Intersection Capacity Utilization			39.7%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	21	223	8	13	309	28	8	3	8	20	0	30
Future Volume (Veh/h)	21	223	8	13	309	28	8	3	8	20	0	30
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	23	242	9	14	336	30	9	3	9	22	0	33
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	366			251			704	686	246	682	676	351
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	366			251			704	686	246	682	676	351
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	98			99			97	99	99	94	100	95
cM capacity (veh/h)	1193			1314			327	359	792	349	364	692
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	274	380	21	55								
Volume Left	23	14	9	22								
Volume Right	9	30	9	33								
cSH	1193	1314	445	497								
Volume to Capacity	0.02	0.01	0.05	0.11								
Queue Length 95th (m)	0.4	0.2	1.1	2.8								
Control Delay (s)	0.8	0.4	13.5	13.1								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.8	0.4	13.5	13.1								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.9									
Intersection Capacity Utilization			31.5%		ICU Level of Service				A			
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	249	0	10	332	10	0	3	0	26	10	18
Future Volume (Veh/h)	3	249	0	10	332	10	0	3	0	26	10	18
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	271	0	11	361	11	0	3	0	28	11	20
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	372			271			691	671	271	667	666	366
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	372			271			691	671	271	667	666	366
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			100	99	100	92	97	97
cM capacity (veh/h)	1186			1292			338	373	768	367	376	679
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	274	383	3	59								
Volume Left	3	11	0	28								
Volume Right	0	11	0	20								
cSH	1186	1292	373	437								
Volume to Capacity	0.00	0.01	0.01	0.14								
Queue Length 95th (m)	0.1	0.2	0.2	3.5								
Control Delay (s)	0.1	0.3	14.7	14.5								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.1	0.3	14.7	14.5								
Approach LOS			B	B								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			40.5%		ICU Level of Service				A			
Analysis Period (min)			15									

14: Vidal Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	22	34	18	28	1
Future Volume (Veh/h)	0	22	34	18	28	1
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	24	37	20	30	1
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	124	30	31			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	124	30	31			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	98	98			
cM capacity (veh/h)	850	1044	1582			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	24	57	31			
Volume Left	0	37	0			
Volume Right	24	0	1			
cSH	1044	1582	1700			
Volume to Capacity	0.02	0.02	0.02			
Queue Length 95th (m)	0.5	0.5	0.0			
Control Delay (s)	8.5	4.8	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.5	4.8	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		4.3				
Intersection Capacity Utilization		19.5%		ICU Level of Service	A	
Analysis Period (min)		15				

15: Oxford Street










06/26/2020



Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			4	2	
Traffic Volume (veh/h)	14	4	8	360	361	10
Future Volume (Veh/h)	14	4	8	360	361	10
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	4	9	391	392	11
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	806	398	403			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	806	398	403			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	348	652	1156			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	19	400	403			
Volume Left	15	9	0			
Volume Right	4	0	11			
cSH	386	1156	1700			
Volume to Capacity	0.05	0.01	0.24			
Queue Length 95th (m)	1.2	0.2	0.0			
Control Delay (s)	14.8	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.8	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay			0.5			
Intersection Capacity Utilization			35.4%	ICU Level of Service		A
Analysis Period (min)			15			

















2045 COMBINED AM
1: Oxford Street & Thrift Avenue

06/26/2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	19	361	128	38	264	92
Future Volume (Veh/h)	19	361	128	38	264	92
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	21	392	139	41	287	100
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	834	160			180	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	834	160			180	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	92	56			79	
cM capacity (veh/h)	269	886			1396	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	413	180	387			
Volume Left	21	0	287			
Volume Right	392	41	0			
cSH	793	1700	1396			
Volume to Capacity	0.52	0.11	0.21			
Queue Length 95th (m)	23.3	0.0	5.9			
Control Delay (s)	14.4	0.0	6.6			
Lane LOS	B		A			
Approach Delay (s)	14.4	0.0	6.6			
Approach LOS	B					
Intersection Summary						
Average Delay		8.7				
Intersection Capacity Utilization		61.9%		ICU Level of Service		B
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	301	2	13	355	34	3	0	12	20	3	17
Future Volume (Veh/h)	3	301	2	13	355	34	3	0	12	20	3	17
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	327	2	14	386	37	3	0	13	22	3	18
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	423			329			786	785	328	780	768	404
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	423			329			786	785	328	780	768	404
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	98	93	99	97
cM capacity (veh/h)	1136			1231			296	320	713	304	328	646
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	332	437	16	43								
Volume Left	3	14	3	22								
Volume Right	2	37	13	18								
cSH	1136	1231	564	393								
Volume to Capacity	0.00	0.01	0.03	0.11								
Queue Length 95th (m)	0.1	0.3	0.7	2.8								
Control Delay (s)	0.1	0.4	11.6	15.3								
Lane LOS	A	A	B	C								
Approach Delay (s)	0.1	0.4	11.6	15.3								
Approach LOS			B	C								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			40.6%		ICU Level of Service				A			
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	328	0	2	345	17	17	0	20	33	3	39
Future Volume (Veh/h)	5	328	0	2	345	17	17	0	20	33	3	39
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	357	0	2	375	18	18	0	22	36	3	42
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	393			357			798	764	357	777	755	384
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	393			357			798	764	357	777	755	384
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			100			94	100	97	88	99	94
cM capacity (veh/h)	1166			1202			281	332	687	303	336	664
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	362	395	40	81								
Volume Left	5	2	18	36								
Volume Right	0	18	22	42								
cSH	1166	1202	417	424								
Volume to Capacity	0.00	0.00	0.10	0.19								
Queue Length 95th (m)	0.1	0.0	2.4	5.3								
Control Delay (s)	0.2	0.1	14.6	15.5								
Lane LOS	A	A	B	C								
Approach Delay (s)	0.2	0.1	14.6	15.5								
Approach LOS			B	C								
Intersection Summary												
Average Delay			2.2									
Intersection Capacity Utilization			32.8%		ICU Level of Service					A		
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	360	14	3	341	7	12	10	10	20	3	12
Future Volume (Veh/h)	7	360	14	3	341	7	12	10	10	20	3	12
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	391	15	3	371	8	13	11	11	22	3	13
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	379			406			810	800	398	812	803	375
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	379			406			810	800	398	812	803	375
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	99			100			95	97	98	92	99	98
cM capacity (veh/h)	1179			1153			288	315	651	283	314	671
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	414	382	35	38								
Volume Left	8	3	13	22								
Volume Right	15	8	11	13								
cSH	1179	1153	361	356								
Volume to Capacity	0.01	0.00	0.10	0.11								
Queue Length 95th (m)	0.2	0.1	2.4	2.7								
Control Delay (s)	0.2	0.1	16.0	16.3								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.2	0.1	16.0	16.3								
Approach LOS			C	C								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			34.3%		ICU Level of Service					A		
Analysis Period (min)			15									










14: Vidal Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SSR
Lane Configurations						
Traffic Volume (veh/h)	0	35	12	10	40	0
Future Volume (Veh/h)	0	35	12	10	40	0
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	38	13	11	43	0
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (m)						
pX. platoon unblocked						
vC, conflicting volume	80	43	43			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	80	43	43			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	96	99			
cM capacity (veh/h)	915	1027	1566			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	38	24	43			
Volume Left	0	13	0			
Volume Right	38	0	0			
cSH	1027	1566	1700			
Volume to Capacity	0.04	0.01	0.03			
Queue Length 95th (m)	0.9	0.2	0.0			
Control Delay (s)	8.6	4.0	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	4.0	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		4.0				
Intersection Capacity Utilization		17.9%		ICU Level of Service	A	
Analysis Period (min)		15				

















15: Oxford Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	10	7	9	480	349	7
Future Volume (Veh/h)	10	7	9	480	349	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	11	8	10	522	379	8
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	925	383	387			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	925	383	387			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	296	664	1171			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	19	532	387			
Volume Left	11	10	0			
Volume Right	8	0	8			
cSH	386	1171	1700			
Volume to Capacity	0.05	0.01	0.23			
Queue Length 95th (m)	1.2	0.2	0.0			
Control Delay (s)	14.8	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.8	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		42.5%		ICU Level of Service	A	
Analysis Period (min)		15				

















2: Evrall Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	3	301	2	13	355	34	3	0	12	20	3	17
Future Volume (Veh/h)	3	301	2	13	355	34	3	0	12	20	3	17
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	3	327	2	14	386	37	3	0	13	22	3	18
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	423			329			786	785	328	780	768	404
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	423			329			786	785	328	780	768	404
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	100			99			99	100	98	93	99	97
cM capacity (veh/h)	1136			1231			296	320	713	304	328	646
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	332	437	16	43								
Volume Left	3	14	3	22								
Volume Right	2	37	13	18								
cSH	1136	1231	564	393								
Volume to Capacity	0.00	0.01	0.03	0.11								
Queue Length 95th (m)	0.1	0.3	0.7	2.8								
Control Delay (s)	0.1	0.4	11.6	15.3								
Lane LOS	A	A	B	C								
Approach Delay (s)	0.1	0.4	11.6	15.3								
Approach LOS			B	C								
Intersection Summary												
Average Delay			1.3									
Intersection Capacity Utilization			40.6%		ICU Level of Service					A		
Analysis Period (min)			15									

















3: Vidal Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	23	291	10	17	392	31	10	3	10	23	0	35
Future Volume (Veh/h)	23	291	10	17	392	31	10	3	10	23	0	35
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	25	316	11	18	426	34	11	3	11	25	0	38
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	460			327			888	868	322	863	856	443
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	460			327			888	868	322	863	856	443
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	98			99			95	99	98	90	100	94
cM capacity (veh/h)	1101			1233			241	280	719	261	284	615
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	352	478	25	63								
Volume Left	25	18	11	25								
Volume Right	11	34	11	38								
cSH	1101	1233	349	400								
Volume to Capacity	0.02	0.01	0.07	0.16								
Queue Length 95th (m)	0.5	0.3	1.8	4.2								
Control Delay (s)	0.8	0.5	16.1	15.7								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.8	0.5	16.1	15.7								
Approach LOS			C	C								
Intersection Summary												
Average Delay			2.1									
Intersection Capacity Utilization			38.0%		ICU Level of Service					A		
Analysis Period (min)			15									










4: Blackwood Street & Thrift Avenue

06/26/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	360	14	3	341	7	12	10	10	20	3	12
Future Volume (Veh/h)	7	360	14	3	341	7	12	10	10	20	3	12
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	391	15	3	371	8	13	11	11	22	3	13
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	379			406			810	800	398	812	803	375
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	379			406			810	800	398	812	803	375
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	99			100			95	97	98	92	99	98
cM capacity (veh/h)	1179			1153			288	315	651	283	314	671
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	414	382	35	38								
Volume Left	8	3	13	22								
Volume Right	15	8	11	13								
cSH	1179	1153	361	356								
Volume to Capacity	0.01	0.00	0.10	0.11								
Queue Length 95th (m)	0.2	0.1	2.4	2.7								
Control Delay (s)	0.2	0.1	16.0	16.3								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.2	0.1	16.0	16.3								
Approach LOS			C	C								
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			34.3%		ICU Level of Service				A			
Analysis Period (min)			15									










14: Vidal Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	22	34	23	36	1
Future Volume (Veh/h)	0	22	34	23	36	1
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	24	37	25	39	1
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	138	40	40			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	138	40	40			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	98	98			
cM capacity (veh/h)	835	1032	1570			
Direction, Lane #	EB 1	NB 1	SB 1			
Volume Total	24	62	40			
Volume Left	0	37	0			
Volume Right	24	0	1			
cSH	1032	1570	1700			
Volume to Capacity	0.02	0.02	0.02			
Queue Length 95th (m)	0.5	0.6	0.0			
Control Delay (s)	8.6	4.5	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	4.5	0.0			
Approach LOS	A					
Intersection Summary						
Average Delay		3.8				
Intersection Capacity Utilization		19.8%		ICU Level of Service	A	
Analysis Period (min)		15				

15: Oxford Street

06/26/2020

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	10	7	9	480	349	7
Future Volume (Veh/h)	10	7	9	480	349	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	11	8	10	522	379	8
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	925	383	387			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	925	383	387			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	96	99	99			
cM capacity (veh/h)	296	664	1171			
Direction: Lane #	EB 1	NB 1	SB 1			
Volume Total	19	532	387			
Volume Left	11	10	0			
Volume Right	8	0	8			
cSH	386	1171	1700			
Volume to Capacity	0.05	0.01	0.23			
Queue Length 95th (m)	1.2	0.2	0.0			
Control Delay (s)	14.8	0.3	0.0			
Lane LOS	B	A				
Approach Delay (s)	14.8	0.3	0.0			
Approach LOS	B					
Intersection Summary						
Average Delay		0.4				
Intersection Capacity Utilization		42.5%		ICU Level of Service		A
Analysis Period (min)		15				

PROJECT: **VIDAL STREET DEVELOPMENT**

SITE ADDRESS: 14937 Thrift Ave & 1441/1443-45/1465 Vidal Street, White Rock, BC

CLIENT: **WESTSTONE GROUP**
10090 152ND St.
Surrey, BC, V3R 8X8

VDZ PROJECT # DP2018-59

SITE REVIEW DATE(s): October 16, 2018
September 15, 2020
July 8, 2022

PREPARED BY: **VDZ+A Consulting Ltd.**
102 – 355 Kingsway
Vancouver, BC
V5T 3J7

PROJECT ARBORIST: **D. Glyn Romaine**
ISA Certified Arborist PN 7929A
TRAQ

FORMER PROJECT ARBORIST: **KELLY KOOME**
ISA Certified Arborist PN 5962A
ISA Tree Risk Assessment Qualified Certified
Wildlife Danger Tree Assessor #P2546

Original Report November 5, 2018
Revision 1 May 8, 2019
Revision 2 September 23, 2020 – A.L.
Revision 3 July 11, 2022 – D.G.R. - Updated Survey.



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INTRODUCTION

ASSIGNMENT

VDZ + A Consulting Inc. (VDZ) have been retained by the client to prepare an arborist report to assess the tree(s) located at 14937 Thrift Avenue & 1441 / 1443-45 / 1465 Vidal Street, White Rock, BC. VDZ arborists performed site reviews entailing identification and visual assessment of the tree(s) on-site. A tree survey of all off-site trees was completed by the client or representative(s).

The Project Arborist will provide recommendations for the retention of tree(s) based on the existing site conditions and the proposed use of the site. Mitigation of development impact on the tree(s) has been considered as part of the tree assessment process.

LIMITS OF THE ASSIGNMENT

VDZ's observations were limited to site visits on October 16, 2018, September 15, 2020, and July 8, 2022. No tissue or soil samples were sent to a lab for identification or analysis. VDZ + A Consulting Inc. located the trees using existing landmarks and onsite navigation.

TESTING AND ANALYSIS

VDZ arborists used visual tree assessment and mallet sounding to test the trees' health, condition, and risk level.

PURPOSE AND USE OF REPORT

The purpose of this report is to assist the property owner in compliance with the White Rock Tree Protection Bylaw, 2021 No. 2407.

SITE DESCRIPTION

SITE REVIEW



Fig. 1 – Aerial view of property (WROMS)

PROPOSED SITE DEVELOPMENT

The demolition of existing structure and the development of midrise multi-family residential building.

ENVIRONMENTAL DESCRIPTION

ISA Certified Arborist Austin Peterson of VDZ + A Consulting Inc. conducted a site review and evaluation of the trees located at the above referenced property on October 16, 2018. A site review was also conducted September 15th, 2020 by Kelly Koome and on July 8, 2022 by Glyn Romaine

The site consists of four residential lots, three of which have existing houses. All four lots have established landscapes composed of mature trees and shrubs. The southernmost lot is a single-



family residential home that fronts onto Thrift Avenue. It is joined via the north property line to the first three lots proceeding up the west side of Vidal Street. From Thrift Avenue, Vidal Street inclines north. To the west lay an assortment of low-rise multifamily residences and to the north is a newer high-rise development.

There are no seasonal creeks that transect the property.

There is no evidence of raptors nests, osprey nests or heron colonies on the site. Removal of trees however between March 15 – August 15 (date subject to change depending on seasonal nesting behavior and therefore must be confirmed with City of Maple Ridge) will require a bird nesting survey. This is as prescribed by the federal Migratory Birds Convention Act (MBCA), 1994 and Section 34 of the BC Wildlife Act. It is the responsibility of the owner/developer to ensure they are in compliance with the city's regulations governing nesting birds on sites where development is occurring.

Off-site Trees – There are private off-site trees associated with this project.

Municipal Trees – There are City of White Rock trees associated with this project.

Trees Straddling the Property Line – There are trees straddling the property line associated with this project

TREE PRESERVATION SUMMARY

All the Trees identified on the Tree Retention/Removal Plan and within the Tree Assessment Data Table have been given their Retention/Removal recommendation on a preliminary basis. Final recommendations will be based upon design/construction and grading details.

Long-term tree preservation success is dependent on minimizing the impact caused during pre-construction clearing operations, construction, and post construction activities. Best efforts must be made to ensure the Tree Protection Zone remains undisturbed.

Ongoing monitoring of retained trees through the development process and implementation of mitigating works (watering, mulching, etc.) is essential for success. Once excavation starts, the consulting arborist needs to be contacted to monitor the work that is done near the trees.

TREE HEALTH CARE PLAN DURING CONSTRUCTION

To ensure continued health of the protected trees during construction, the following is recommended:

1. Remove dead, dying, and diseased branches prior to the start of construction.
2. Install tree protection barriers per bylaw specifications.



3. Regular weekly watering of trees between June 1 – October 1.
4. Application of wood chips within the tree protection zone (1-3 inches).
5. Monthly monitoring of protected trees by assigned Arborist.

Retained protected trees will require supplemental watering on a weekly basis (weather dependent), as well as the application of wood chips or mulch to the tree protection zone within the tree protection barriers. Wood chips are preferred to ensure porous movement through soil and protection from compaction during construction. The mulch or wood chip height should not exceed the root collar (not to exceed 10cm) to avoid moisture retention concentrated on the stem. In addition to the City's requirements, recommendations include the pruning of dead or dying limbs, if applicable, prior to construction for worker safety, as well as monthly monitoring of the trees by an Arborist to ensure the health and well-being of the protected trees.

As there are off-site trees with driplines that extend into the subject property, there may be interconnected root systems within the grouping (OS9-OS11) which likely extend onto the property. BC Plant Health Care Root Radar results determined the roots of tree 06 has poor structure and multiple trunks with decay. In addition, OS2-OS6 have feeder or structural roots which grow towards the property. Any work done within the critical root zone will need to be monitored by the arborist. Any retention wall should be maintained to avoid root disruption and destabilization.

SUMMARY OF FINDINGS

- Tree 03, grows adjacent to foundation of the existing house
- Tree 04, noticeable pruning completed prior to visit. Potentially for utility clearance.
- Tree 06 noticeable decay on single stem of the multi-stemmed tree.
- OS 02-OS 08, dripline extends to/over subject property line. Root radar used to assess root systems. Will need an arborist present to monitor excavation on the property line, and during installation of the proposed retaining wall / landscape features.
- Tree 05 suffered a failed limb prior to September 15th, 2020 visit.
- Tree protection fencing requires repairs and placing for all protected trees prior to any land clearing activities.
- Knotweed was observed at 1441 Vidal. This should be managed, and all plant parts must be disposed of separately.
- Significant amount of Scots broom onsite – to be kept separate from other vegetation debris upon removal.
- Hypodermic needles were observed at 1445 Vidal.

TABLE 1

TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
Comments written for 376 and OS2-OS8, in italics, have been transferred from the BC Plant Health Care Inc. Arborist Report for Tree Root Mapping, dated March 18, 2019.								
The following trees are located on 14937 Thrift Avenue.								
01	370	English holly <i>Ilex aquifolium</i>	Yes	-	-	-	Listed as an invasive species by City of White Rock. Dash ("-") indicates the arborist was not required to measure this species. WITHIN BUILDING FOOTPRINT	Remove
02	371	English holly <i>Ilex aquifolium</i>	Yes	-	-	-	Listed as an invasive species by City of White Rock. Dash ("-") indicates the arborist was not required to measure this species. WITHIN BUILDING FOOTPRINT	Remove
The following trees are located on 1441 Vidal Street.								
03	373	Threadleaf false-cypress <i>Chamaecyparis pisifera</i> 'Filifera'	Yes	54 (17,18,19)	3.0	60	Fair form and structure. TRUNK – Growing directly adjacent to the foundation of the existing house. WITHIN BUILDING FOOTPRINT	Remove
The following trees are located on 1465 Vidal Street.								
04	374	Crimson King Norway maple <i>Acer platanoides</i> 'Crimson King'	Yes	44	5.1	80	DBH measured at 1 m. Fair form and structure. CROWN – Previously side pruned for utility line clearance. Previously topped. WITHIN PARKADE FOOTPRINT	Remove



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
05	375	Common lilac <i>Syringa vulgaris</i>	No	31 (10,10, 11)	3.0	30	HANDPLOTTED Poor form and structure. TRUNK – Multi-stem from base. Single limb failure since original visit. WITHIN PARKADE FOOTPRINT	Remove
06	376	Red alder <i>Alnus rubra</i>	Yes	114 (42, 41, 31)	9.4	80	Fair form and structure. TRUNK – 3stems from base. Decay present in one stem (0.5 meters in length). Rope girdling eastern trunk, previous tear-out on western trunk. Natural lean east. <i>BC Plant Health Care root radar results:</i> <i>Poor structure with multiple trunks and decay. Conflict with proposed development.</i> WITHIN PARKADE FOOTPRINT	Remove
07	377	Flowering plum <i>Prunus cerasifera</i>	No	62 (15,18, 29)	5.8	80	HANDPLOTTED Fair form and structure. CROWN: Heavy ivy up trunk into crown. Some dieback at branch ends. WITHIN PARKADE FOOTPRINT	Remove
08	378	Mountain ash <i>Sorbus aucuparia</i>	No	38 (11, 12, 15)	4.5	80	HANDPLOTTED Fair form and structure. CROWN: Heavy ivy up trunk into crown. Some dieback at branch ends. WITHIN BUILDING FOOTPRINT	Remove

TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
09	379	Japanese maple <i>Acer palmatum</i>	No	36 (10, 13, 13)	5.6	75	HANDPLOTTED Fair form and structure. TRUNK: Ivy up trunk. WITHIN LIKELY EXCAVATION ZONE	Remove
10	380	Mountain ash <i>Sorbus aucuparia</i>	No	37 (11, 13, 13)	4.5	40	HANDPLOTTED Fair form and structure. CROWN – Shade suppressed on north and east sides. TRUNK: Ivy up trunk. WITHIN PARKADE FOOTPRINT	Remove
11	381	Vine maple <i>Acer circinatum</i>	No	51 (15, 16, 20)	4.0	80	HANDPLOTTED Fair form and structure. TRUNK: Multi-stemmed. Ivy up trunk. WITHIN LIKELY EXCAVATION ZONE	Remove
12	382	Bitter cherry <i>Prunus emarginata</i>	No	54 (16, 16, 22)	4.5	80	HANDPLOTTED Fair form and structure. Multi-stemmed. CROWN: Dieback on one stem. WITHIN LIKELY EXCAVATION ZONE	Remove
13	435	Fruiting cherry. <i>Prunus</i> sp.	No	31	4.3	50	Good form and structure TRUNK: Ivy up trunk. WITHIN LIKELY EXCAVATION ZONE	Remove

TREE ASSESSMENT



TREE #	TAG #	COMMON NAME <i>BOTANICAL NAME</i>	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
14	300	Crimson King Norway maple <i>Acer platanoides</i> 'Crimson King'	No	23	5.5	60	Good form and structure TRUNK: Ivy up trunk. WITHIN LIKELY EXCAVATION ZONE	Remove
The following trees are located offsite.								
Trees OS 1 – OS 8 were inspected visually from a distance. DBH figures have been estimated by the Project Arborist.								
OS 01	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	25	3.5	90	Good form and structure. TRUNK – Located within (0.25 meters) of retaining wall on two sides. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain
OS 02	No tag	Paper birch <i>Betula papyrifera</i>	Yes	55	8.0	50	Good form and structure. CROWN – Dripline extends 3.0 meters onto subject property. BC Plant Health Care root radar results: Feeder roots detected in the 0-20 cm depth range. The tree is about 6 meters from the proposed development. Critical Root Zone does not enter the subject lot. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain

TREE ASSESSMENT



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
OS 03	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	95	6.0	75	<p>Good form and structure.</p> <p>OS 03 – OS 05 are part of a larger grouping of trees with approximately 6.0 meter dripline(s) that extend to subject property line.</p> <p>ROOTS – Interconnected within grouping and likely extending onto subject property.</p> <p>BC Plant Health Care root radar results: Feeder roots detected in the 0 – 20 cm depth range. The tree is about 8 meters from the proposed development. Critical Root Zone does not enter the subject lot. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain
OS 04	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	50	5.8	75	<p>Good form and structure.</p> <p>OS 03 – OS 05 are part of a larger grouping of trees with approximately 6.0 meter dripline(s) that extend to subject property line.</p> <p>ROOTS – Interconnected within grouping and likely extending onto subject property.</p> <p>BC Plant Health Care root radar results: Assessment blocked by a shed. Roots may grow towards the shed. About 24% of Critical Root Zone will be impacted. Retain with no cut at Property Line. Design a point-footing retaining wall with suspended beams. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain

TREE ASSESSMENT



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
OS 05	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	60	8.0	60	<p>Good form and structure.</p> <p>OS 03 – OS 05 are part of a larger grouping of trees with approximately 6.0 meters dripline(s) that extend to subject property line.</p> <p>ROOTS – Interconnected within grouping and likely extending onto subject property.</p> <p><i>BC Plant Health Care root radar results:</i> May have structural, lateral, and feeder roots growing towards the east in the 0 – 20 cm depth range. About 27% of Critical Root Zone will be impacted. Retain with no cut at Property Line. Design a point-footing retaining wall with suspended beams. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain
OS 06	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	90	8.8	75	<p>Good form and structure.</p> <p>CROWN – Dripline extends 3.5 meters onto subject property.</p> <p><i>BC Plant Health Care root radar results:</i> The tree is about 6 meters from the proposed development. Critical Root Zone does not enter the subject lot. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
OS 07	No tag	Western redcedar <i>Thuja plicata</i>	Yes	60	6.2	60	<p>Good form and structure. CROWN – Dripline extends 3.8 meters onto subject property.</p> <p><i>BC Plant Health Care root radar results:</i> May have structural, lateral, and feeder roots growing towards its southeast in the 0 – 20 cm depth range. About 6% of Critical Root Zone may be impacted. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain
OS 08	No tag	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	95	9.1	50	<p>Good form and structure. CROWN – Dripline extends 7.0 meters onto subject property.</p> <p><i>BC Plant Health Care root radar results:</i> Assessment blocked by Tree 376 and shrubs. About 25% of Critical Root Zone will be impacted. Retain with no cut at Property Line. Design a point-footing retaining wall with suspended beams. Arborist oversight recommended for the excavation at Property Line for the installation of the proposed retaining wall / landscape feature.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain
Trees OS 9 – OS 13 form the edge of a larger grouping of private off-site trees.								
OS 9	6346	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	67	6.0	50	<p>Good form and structure. TRUNK: Crook at 16 m.</p> <p>Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.</p>	Retain



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
OS 10	6411	Western redcedar <i>Thuja plicata</i>	Yes	38	4.7	80	Fair form and structure. CROWN: Sheared on south side. TRUNK – Previously topped. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain
OS 11	6336	Western redcedar <i>Thuja plicata</i>	Yes	38	4.7	80	Fair form and structure. CROWN: Sheared on south side. TRUNK – Previously topped. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain
OS12	6332	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	41	6.9	80	Good form and structure. Crown: Previous shearing or clearance pruning on south side. Minor flagging. ROOTS: Large exposed roots. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain
OS13	6334	Douglas-fir <i>Pseudotsuga menziesii</i>	Yes	71	7.1	80	Good form and structure. Trunk: Resinosis. Tree Protection Barrier (TPB) required. Arborist supervision required during excavation and any construction activities within 1.5 m of the dripline.	Retain



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
The following trees are straddling the City of White Rock property.								
SH 01	No tag	Common privet hedge <i>Ligustrum vulgare</i>	Yes	-	1.3	100	Height = 2.2M Shared with 14937 Thrift Ave. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
SH 02	No tag	Boxwood hedge <i>Buxus</i> <i>Sempervirens</i>	Yes	-	1.0	100	Height = 2.0M Shared with 14937 Thrift Ave. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
SH 03	No tag	Common privet hedge <i>Ligustrum vulgare</i>	Yes	-	1.5	100	Height = 2.5M Shared with 14937 Thrift Ave. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
SH 04	No tag	English laurel <i>Prunus laurocerasus</i>	Yes	-	2.2	100	Height = 5.0M Shared with 1441 Vidal St. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove



TREE #	TAG #	COMMON NAME BOTANICAL NAME	LOCATED ON THE SURVEY	DBH (cm)	Crown Radius (m)	LCR (%)	COMMENTS	RETAIN / REMOVE
SH 05	No tag	English laurel <i>Prunus laurocerasus</i>	Yes	-	1.8	100	Height = 3.5M Shared with 1443-45 Vidal St. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
SH 06	372	Cherry <i>Prunus spp.</i>	Yes	59	5.5	30	Growing within the SH 04 hedge. Fair condition. CROWN: Some dieback. Shared with 1441 Vidal St. Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
The following trees belong to the City of White Rock.								
C 1	No tag	Pyramidalis hedge <i>Thuja occidentalis</i> 'Pyramidalis'	Yes	-	1.0	100	HANDPLOTTED Height = 6.0M Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove
C 2	No tag	Mixed hedge	No	-	2.5	100	HANDPLOTTED Height = 6.0M Indirect conflict with civil sidewalk upgrades and proposed street trees. Written permission required from City to remove.	Remove



TREE REPLACEMENT SUMMARY

Onsite & Straddling:

Size	To be Removed	Replacement Trees Required
Undersized (<20cm dbh), (hedges, invasive holly)	5	0
≤ 50cm dbh	7	14
51-65cm dbh	5	15
66-75cm dbh	0	0
76-85cm dbh	0	0
>85cm dbh	1	6
Total	20	35

Offsite City:

Size	To be Removed	Replacement Trees
(<30cm dbh) (hedges)	2	0
≤ 50cm dbh	0	0
51-65cm dbh	0	0
66-75cm dbh	0	0
76-85cm dbh	0	0
>85cm dbh	0	0
Total	2	0

TREE PROTECTION AND REPLACEMENT SECURITIES

Tree Protection securities:

Size of Tree Retained	Securities
Dbh ≤ 50cm	\$3,000.00 per retained tree
Dbh of 51-65cm	\$4,500.00 per retained tree
Dbh > 65cm	\$10,000 per retained tree

Tree Replacement securities:

Size Tree Removed*	Replacement Ratio	Securities / Cash-in-lieu (\$1,500 per replacement tree)
≤ 50cm dbh	2:1	\$3,000
51-65cm dbh	3:1	\$4,500
66-75cm dbh	4:1	\$6,000
76-85cm dbh	5:1	\$7,500
>85cm dbh	6:1	\$9,000

PHOTOS



Fig. 2 - View facing south along Vidal Street to Thrift Avenue.



Fig. 3 – Off-site Douglas-fir tree



Fig. 4 – Tree 03 growing within S4



Fig. 5 – View of Trees OS2 – OS8



Fig. 6 – Stand of off-site conifers located directly west of 1441/1443-45/1465 Vidal Street.



Fig. 7 – View facing north/northwest. OS 9 – OS 11 make up part of the edge of a larger grouping of conifers.



Fig. 8 – Alternate view of Trees OS 9 – OS 11



Fig. 9 – Red alder (376) located on 1465 Vidal Street.

**PHOTOS – September 15, 2020**

Fig. 9 – View facing east on 1465 Vidal st, tree protection fencing damaged. Needs repair.

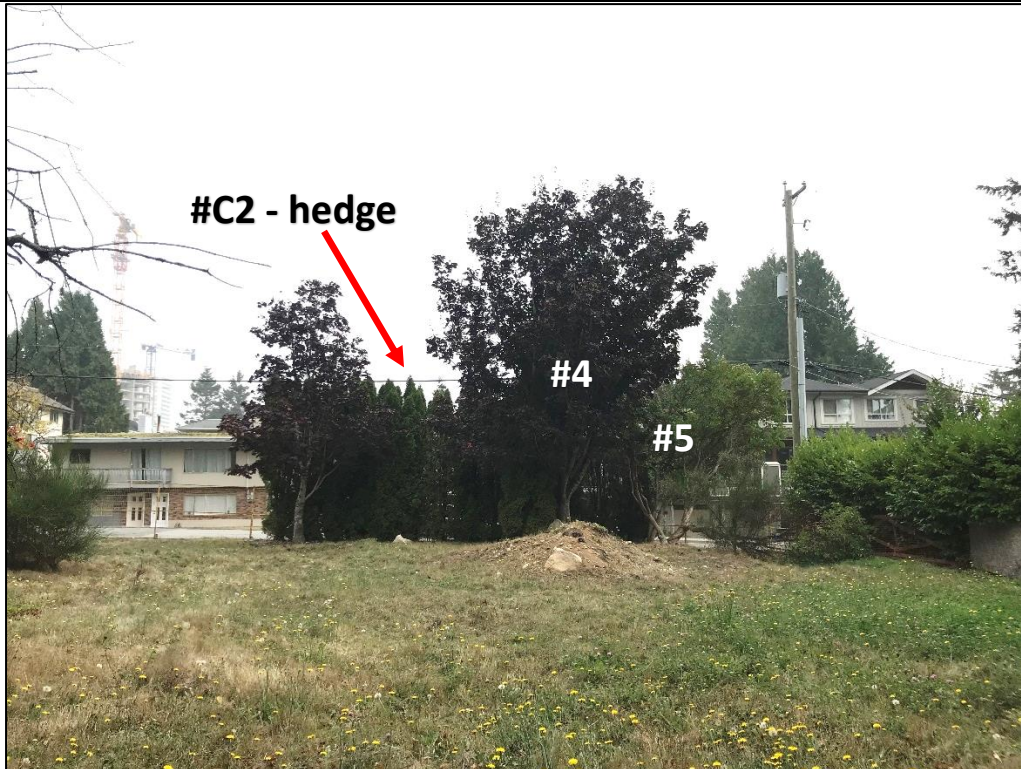




Fig. 10 – View facing east. Southeast corner of 1445 Vidal st. C2 hedge, #4 norway maple, and #5 lilac.



Fig. 11 – Northwest corner of 1465 Vidal.



Fig. 12 – Looking south from 1443-45 Vidal St.



Fig. 13 – Tree #5, failed limb.



Fig. 14 – Pruning of tree branches along east property line, 1465, 1443-45.



Fig. 15 – North property line of 1441 Vidal St, east corner.



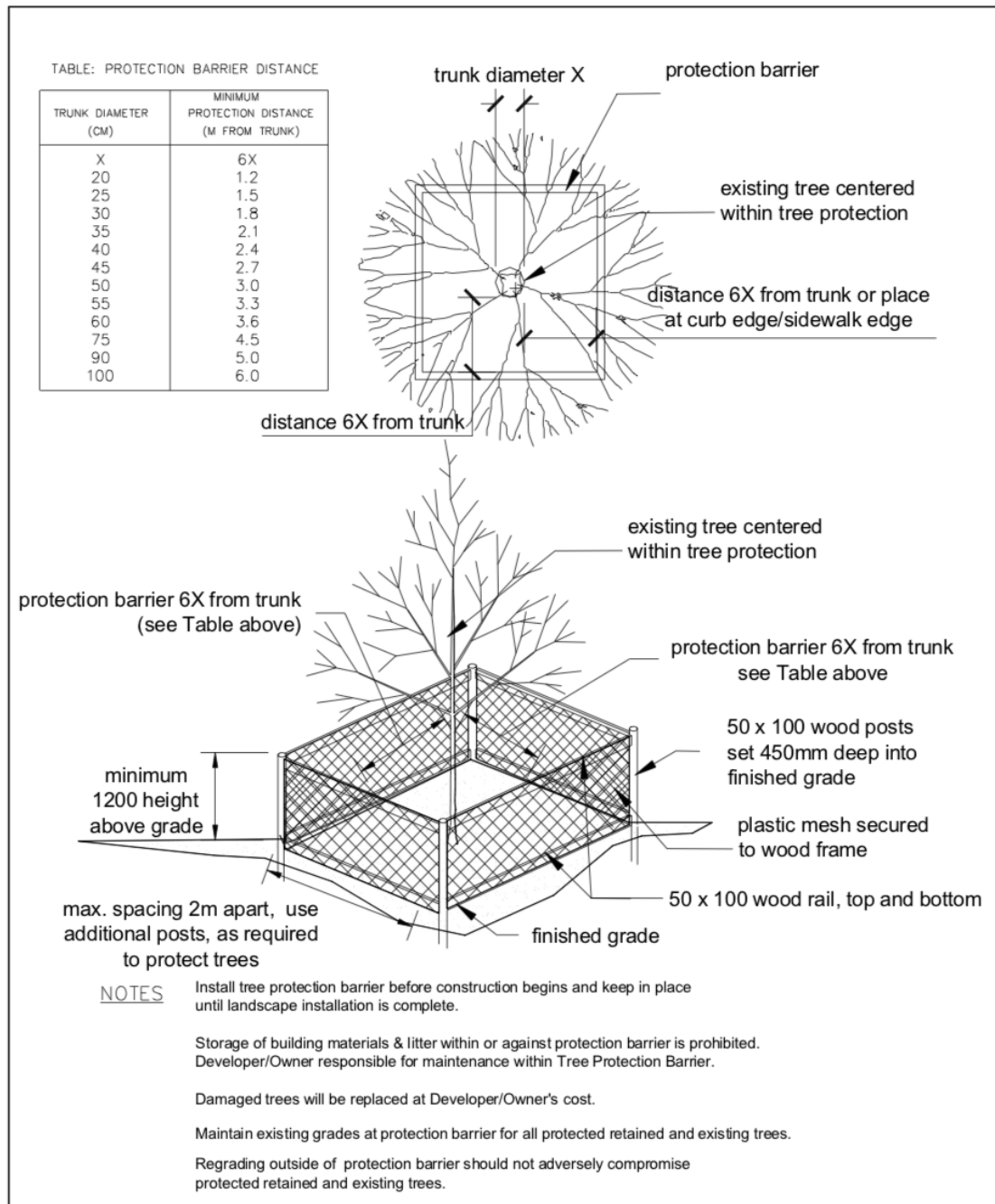
Fig. 16 – North property line of 1441 Vidal St, west corner.



CONSTRUCTION ACTIVITY AROUND TREE PROTECTION ZONE

TREE PROTECTION FENCING

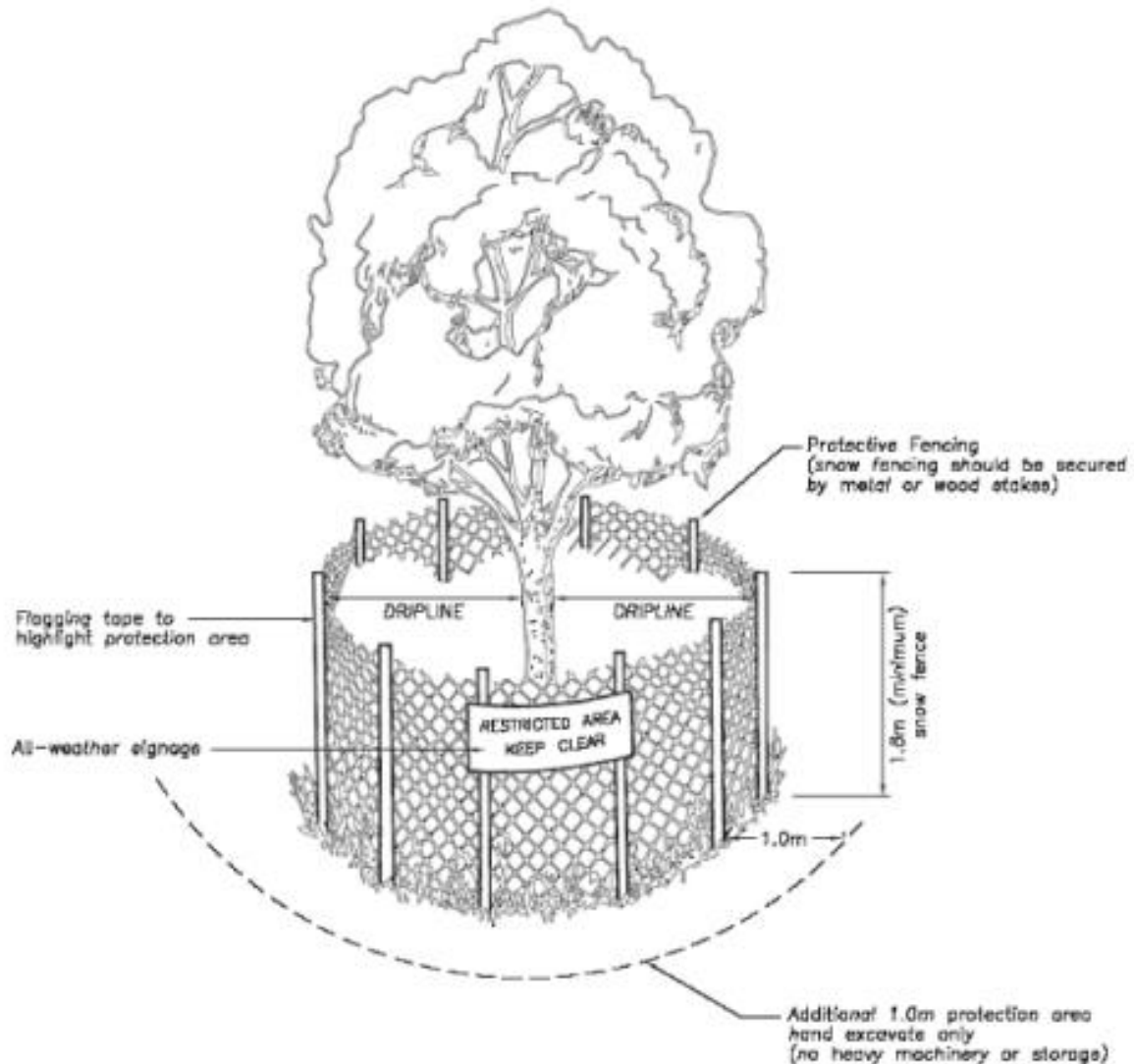
Specifications for Tree Protection Barriers



TREE PROTECTION

How do I safely retain trees on, or adjacent to, the property?

Prior to construction activity you should erect temporary fencing at the dripline of the tree to protect the roots and canopy.





GENERAL REQUIREMENTS AND LIMITATIONS FOR OPERATIONS WITHIN THE TREE PROTECTION ZONE

- The Contractor shall not engage in any construction activity within the Tree Protection Zone (TPZ) without the approval of the Project Arborist including: operating, moving or storing equipment; storing supplies or materials; locating temporary facilities including trailers or portable toilets and shall not permit employees to traverse the area to access adjacent areas of the project or use the area for lunch or any other work breaks. Permitted activity, if any, within the Tree Protection Zone maybe indicated on the drawings along with any required remedial activity as listed below.
- If construction activity is unavoidable within the Tree Protection Zone, notify the Project Arborist and submit a detailed written plan of action for approval. The plan shall include: a statement detailing the reason for the activity including why other areas are not suited; a description of the proposed activity; the time period for the activity, and a list of remedial actions that will reduce the impact on the Tree Protection Zone from the activity. Remedial actions shall include but shall not be limited to the following:
 - In general, demolition and excavation within the drip line of trees and shrubs shall proceed with extreme care either using hand tools, directional boring and/or Air Spade. If any excavation work is required within the Tree Protection Zone (TPZ), the Project Arborist must be present during excavation, and a trench should be 'hand dug' to a depth of 60 cm outside the Drip Line, to uncover any potential roots. The Project Arborist should cleanly prune roots and recommend the appropriate treatment for any structural roots encountered.
 - Knife excavation where indicated or with other low impact equipment that will not cause damage to the tree, roots soil.
 - When encountered, exposed roots, 1 inches and larger in diameter shall be worked around in a manner that does not break the outer layer of the root surface (bark). These roots shall be covered in Wood Chips and shall be maintained above permanent wilt point at all times. Roots one inch and larger in diameter shall not be cut without the approval of the Project Arborist. Excavation shall be tunnelled under these roots without cutting them. In the areas where roots are encountered, work shall be performed and scheduled to close excavations as quickly as possible over exposed roots.
- Tree branches that interfere with the construction may be tied back or pruned to clear only to the point necessary to complete the work. Other branches shall only be RETAINED when specifically indicated by the Project Arborist. Tying back or trimming of all branches and the cutting of roots shall be in accordance with accepted arboriculture practices (ANSI A300, part 8) and be performed under supervision of the Project Arborist.
- Do not permit foot traffic, scaffolding or the storage of materials within the Tree Protection Zone.
- Protect the Tree Protection Zone at all times from compaction of the soil; damage of any kind to trunks, bark, branches, leaves and roots of all plants; and contamination of the soil, bark or leaves with construction materials, debris, silt, fuels, oils, and any chemicals substance. Notify the Project Arborist of any spills, compaction or damage and take corrective action immediately using methods approved by the Project Arborist



GLOSSARY OF KEY TERMS

Abutment: A structure built to support the lateral pressure of an arch or span, e.g., at the ends of a bridge.

Adapted Trunk Diameter Method: This method uses the trees age and tolerance to construction damage to determine the factor that will be multiplied by the diameter to provide a sufficient tree protection zone given these factors.

Age: The relative age (young, intermediate, mature) within the particular stand of trees or forest.

Algae: Is a simple, nonflowering plant (includes seaweeds and many single-celled forms). They do contain chlorophyll (but lack true stems, roots, and vascular tissue)

ALR: The Agricultural Land Reserve in which agriculture is recognized as the priority.

Bole: The stem or trunk of a tree.

Chlorotic: Yellowing of plant tissues caused by nutrient deficiency &/or pathogen.

Co-dominant Leaders: Forked dominant stems nearly the same size in diameter, arising from a common junction.

Co-dominant Within Stand: Individual tree whose height is generally equal to trees (regardless of species) within the same stand.

Compaction: Compression of the soil that breaks down soil aggregates and reduces soil volume and total pore space, especially macropore space.

Conk: A fungal fruiting structure typically found on trunks and indicating internal decay.

Dead Standing: A tree that has died but is still standing erect.

DBH: The Diameter of the tree at 1.40 meters above the ground.

Dominant Within Stand: Individual tree whose height is significantly greater than adjacent trees (regardless of species) within the same stand.

C-rad: Crown radius, is the dripline measured from the edge of the trunk to the outermost branches of the crown.

CRZ: Critical Root Zone - means the area of land surrounding the trunk of a tree contained within a radius equal to the DBH of the tree multiplied by six (6), or one (1) metre beyond the drip line of the tree, whichever is greater.

Fair: Healthy but has some defects such as co-dominant trunk, dead branches.



Feeder Roots: The smaller roots responsible for water and nutrient absorption and gas exchange. These roots can extend far beyond the Drip Line (or outer canopy) of the tree.

Fungus (singular) / Fungi (plural): Unicellular, multicellular or syncytial spore-producing organisms that feed on organic matter (including molds, yeast, mushrooms and toadstools)

Girdling Root: Root that encircles all or part of the trunk of a tree or other roots and constricts the vascular tissue and inhibits secondary growth and the movement of water.

Good: Good form and structure, healthy with no defects.

Hazardous: Significant hazard exists with a high risk of immediate failure; which could result in serious damage to property or person(s).

Height: Height of tree is approximate.

LCR: Live Crown Ratio – The ratio of crown length to total tree length.

Level 1 Limited Visual Assessment: Limited visual assessment looking for obvious defects such as, but not limited to dead trees, large cavity openings, large dead or broken branches, fungal fruiting structures, large cracks, and severe leans.

Level 2 Basic Visual Assessment: Detailed visual inspection (aboveground roots, trunk, canopy) of tree(s) may include the use of simple tools to perform assessment (i.e. sounding mallet, trowel, measuring tape, binoculars). The assessment does not include advanced resistance drilling of trunk.

Level 3 Advanced Assessment: To provide detailed information about specific tree parts, defects, targets, or side conditions. May included aerial inspection, resistance drilling of tree parts, laboratory diagnosis of fungal or plant tissue.

Mildew: Is a minute powdery or web-like fungi (of different colours) that is found on diseased or decaying substances.

Moss: A small, green, seedless plant that grows on stones, trees or ground.

No Disturbance Zone: $(\text{Trunk Diameter} \times 6) + \text{Trunk Radius} + (60 \text{ cm excavation zone})$. For example, a 50-cm diameter tree would have a No Disturbance Zone = 3.85 meters measured from the edge of the trunk.

Poor: multiple defects, disease, poor structure and or form, root and or canopy damage.

Phloem: Plant vascular tissue that transports sugar and growth regulators. Situated on the inside of the bark, just outside the cambium. Is bidirectional (transports up and down). Contrast with xylem.

Phototropic: Growth toward light source or stimulant.



Retain & Monitor: Monitor health and condition of tree every 12 months for signs of deterioration.

Root Crown: Also, called the root collar, it includes the flare at the base of the trunk and the initial roots that develop below the trunk. These roots generally taper and subdivide rapidly to form the root system of the tree.

SPEA: Streamside Protection and Enhancement Area

Spiral Decline: The health and condition of the tree is deteriorating.

Sub-dominant Within Stand: Individual tree whose height is significantly less than adjacent trees (regardless of species) within the same stand.

Suppressed: Individual tree whose growth, health and condition are negatively impacted by adjacent tree(s).

TPZ: Tree Protection Zone - The area between the trunk and the Tree Protection Barrier.

Wildlife Tree: A tree or a group of trees that are identified to be retained to provide future wildlife habitat. Wildlife habitat can exist in tree risks (cavities, dead snags, broken tops). Often times the tree risk to potential targets (people & property) is reduced by removing that part of the tree posing the risk of failure, but the tree (or portion of) is retained to provide future habitat.

Witches Broom: A dense mass of shoots growing from a single point, with the resulting structure resembling a broom or a bird's nest.

Xylem: Thin overlapping cells that helps provide support and that conducts water and nutrients upward from the roots all the way to the leaves.



LIMITATIONS

This report is valid for the day the trees were reviewed. This report is not to be re-printed, copied, published, or distributed without prior approval by VDZ + A Consulting Inc.

Sketches, diagrams, and photographs contained in this report being intended as visual aids, should not be construed as engineering reports or legal surveys.

Only the subject tree(s) was inspected and no others. This report does not imply or in any other way infer that other trees on this site or near this site are sound and healthy.

The tendency of trees or parts of trees to fall due to environmental conditions and internal problems are unpredictable. Defects are often hidden within the tree or underground. The project arborist has endeavored to use his skill, education, and judgment to assess the potential for failure, with reasonable methods and detail. It is the owner's responsibility to maintain the trees and inspect the trees to reasonable standards and to carry out recommendations for mitigation suggested in this report.

REFERENCES

Bond, Jerry & Buchanan, Beth (2006) *Best Management Practices: Tree Inventories*, International Society of Arboriculture, Champaign, IL.

Dunster, Dr. Julian (2003) *Preliminary Species Profiles for Tree Failure Assessment*. ISA Pacific Northwest Chapter, Silverton, OR, USA

Dunster, Dr. Julian & Edmonds, Dr. R. (2014) *Common Fungi Affecting Pacific Northwest Trees*, ISA Pacific Northwest Chapter, Silverton, OR, USA

Fite, Kelby & Smiley, E. Thomas (2016) *Best Management Practices: Managing Trees During Construction*, International Society of Arboriculture, Champaign, IL.

Sibley, David Allen (2009) *The Sibley Guide to Trees*. Alfred A. Knopf, New York, NY

Smiley, E.T., Matheny, N., Lilly, S. (2011) *Best Management Practices: Tree Risk Assessment*. International Society of Arboriculture, Champaign, IL.



TREE MANAGEMENT PLAN

See attached Tree Mangement Plan

Original size: 24x36

Print as 11x17 for foldout



THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: July 25, 2022

TO: Mayor and Council

FROM: Anne Berry, Director, Planning and Development Services

SUBJECT: Consideration of 1st and 2nd Reading of White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438

RECOMMENDATIONS

THAT Council:

1. Give first and second readings to “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438;”
 2. Direct staff to schedule the public hearing for “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438;” and
 3. Direct staff to address the following conditions prior to bringing “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438” back for consideration of final adoption:
 - a) Ensure that all engineering requirements and issues, including road dedication and the execution of a Works and Servicing Agreement, are addressed to the satisfaction of the Director of Engineering and Municipal Operations;
 - b) Ensure that the east pedestrian walkway is revised, and updated drawings are submitted and approved by the Director of Planning and Development Services;
 - c) Registration of a Section No. 219 Covenant to restrict basement suites;
 - d) Ensure that all matters pertaining to tree protection and retention are addressed to the satisfaction of the Director of Planning and Development Services.
 - e) Confirm and ensure the recommendations of the final arborist report, approved by the Director of Planning and Development Services and, more specifically the city’s Arboricultural Technician, are implemented and maintained through future demolition and construction activities; and
 - f) Complete the demolition of the existing dwelling to the satisfaction of the Director of Planning and Development Services.
-

EXECUTIVE SUMMARY

This application proposes to rezone the property from the “RS-1 One-Unit Residential Zone” to the “RT-1 Two-Unit (duplex) Residential Zone” at 1091 Stayte Road. If approved, it would allow for the construction of a duplex on the property. A minor development permit for form and character is also required. The subject property meets the lot area and dimension requirements of

the RT-1 zone and the siting and massing of the proposed two-storey duplex is similar to a single-unit residential building that could be constructed under the existing zoning. The duplex would provide a more ‘affordable’ ownership option in the city, as compared to one large single-family home. Overall, staff supports the proposal subject to the recommended conditions.

A copy of draft Zoning Amendment Bylaw No. 2438 is included in this corporate report as Attachment A, and the proposed architectural and landscape drawings are included as Attachment B.

PREVIOUS COUNCIL DIRECTION

Motion & Meeting Date	Motion Details
2020-LU/P-040 October 26, 2020	THAT Land Use and Planning Committee recommend that Council resolve that the zoning amendment application at 1091 Stayte Road proceed to the next stage in the application review process.
2020-542 November 9, 2020	THAT Council resolve that the zoning amendment application at 1091 Stayte Road proceed to the next stage in the application review process.

INTRODUCTION/BACKGROUND

If approved, the zoning would be amended from RS-1 to RT-1, which enables the construction of a duplex. Figure 1.0 illustrates the Subject Property and the context of the site. The properties immediately west and south are zoned RS-1. The properties to the east of the subject property fall within the City of Surrey.

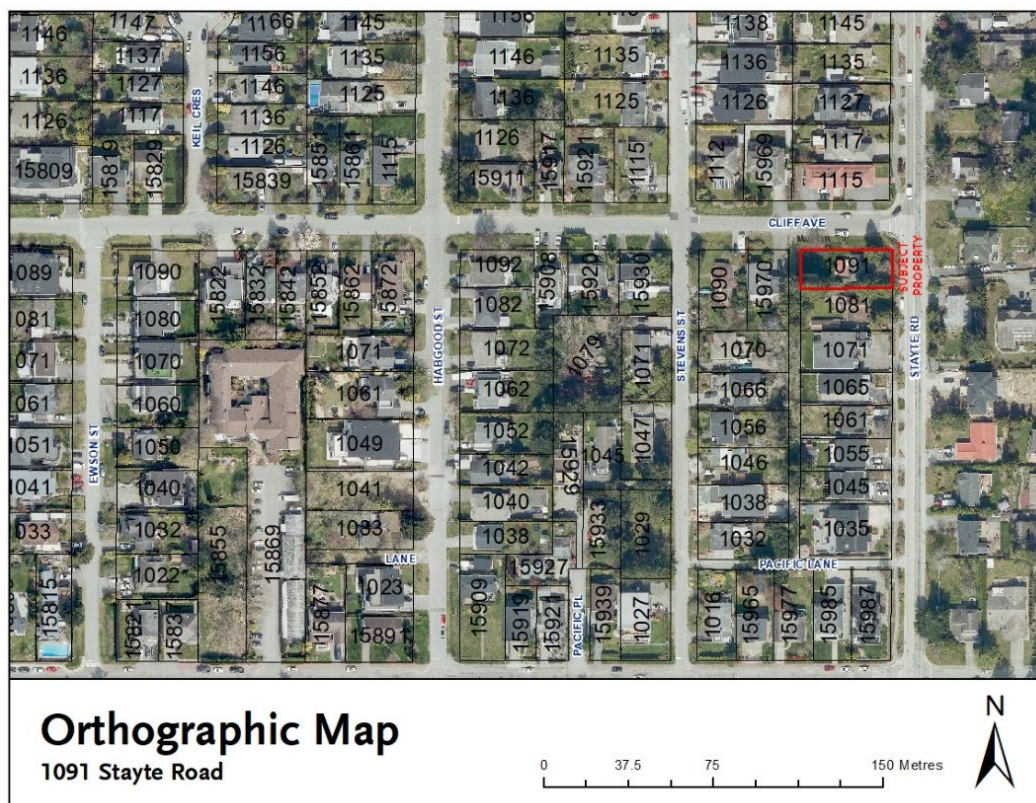


Figure 1: Site Context and Subject Property

Official Community Plan

The property is within the City’s “Mature Neighbourhood” designation as outlined in the Official Community Plan (OCP). This OCP designation goal is to create low-scale housing forms (for example, housing that includes single-family homes with secondary suites, duplexes, and triplexes). The OCP policies are intended to maintain residential character within established neighbourhoods by encouraging gentle infill while supporting a variety of housing choices and different levels of affordability. Under OCP Section 7: Growth Management, Policy 7.4.2, duplexes are encouraged to be spread throughout the City by limiting the number of duplexes along a single block frontage to 20% of the total number of properties. This would be the first duplex on the west side of Stayte Road and is therefore supported by OCP Policy 7.4.2.

ANALYSIS

The proposal has undergone a series of revisions to ensure alignment with the City’s Development Permit Area and Duplex Design Guidelines. The project is consistent with the OCP’s Mature Neighbourhood DPA policies. The following sections describe details of the proposal and key land use planning considerations made in preparing the staff recommendation outlined in this report.

Zoning

The subject property is approximately 18.59 m (62.0 ft) wide, 40.26 m (132.3 ft) deep, and has a lot area of 748.06 sq.m (8,052 ft²), exceeding the minimum requirements of both the existing RS-1 zone and the proposed RT-1 zone. The zoning requirements that relate to the siting and massing of buildings are largely identical for the RS-1 and RT-1 zones. The only variation is to lot coverage with a maximum of 40% required in the RS-1 zone as compared to 45% in the RT-1 zone. The proposal at 1091 Stayte Road is in conformance with all RT-1 standards. Table 1.0 below summarizes the current and proposed zoning and configuration for the proposed duplex. The proposed use of the property is permitted within the RT-1 zone. Secondary suites are not permitted in the RT-1 zone.

Table 1: Existing and Proposed Zoning Matrix

Zone Standard	RS-1 (Current zone)	RT-1 (Standard) zone	RT-1 (Proposed)
Lot Width (min)	15.0 m	18.0 m	18.59 m (62.0ft)
Lot Depth (min)	27.4 m	30.5 m	40.26 m (132.3ft)
Lot Area (min)	464.0 m ² (4,994.5ft ²)	742.0 m ² (7,986.8ft ²)	748.06 m ²
Lot Coverage (max)	40%	45%	39.6%
Floor Area	0.5 times the lot area	0.5 times the lot area	0.5 times the lot area
Height (max)	7.7 m	7.7 m	6.97 m
Setbacks (min)			
Front	7.5 m	7.5 m	7.83 m
Rear	7.5 m	7.5 m	7.81 m
Exterior sideyard	3.8 m	3.8 m	3.94 m
Interior side	1.5 m	1.5 m	1.91 m

Public Realm and Streetscape Improvements

Improvements to the City boulevard would be completed by the applicant at the time of redevelopment (e.g., sidewalks, street tree planting, etc.).

Mature Neighbourhood DPA Guidelines

The applicant has submitted a response to the Mature Neighbourhood Development Permit Area Guidelines, which apply to the proposal under OCP Policy 22.1. The response to the guidelines is attached as Attachment C. Staff consider the submitted response to be in conformance with the Development Permit Guidelines. Figure 2.0 below is a rendering of the proposed duplex development on the subject property.



Figure 2: Rendering of proposed duplex

The applicant has adequately identified how the proposed development (see meets the development permit guidelines by providing the following key aspects:

- a) The individuality of each unit has been achieved through variations in the floor plans, setbacks, window sizes, front door locations, and by using different cladding accent materials.
- b) The site is to be landscaped with plant materials using a variety of evergreen and flowering trees, shrubs, grasses and perennials. The overall result will be the creation of an enhanced streetscape that maintains a strong separation between private and public space in the front yard and exterior side yard.

Advisory Design Panel Review

During the Advisory Design Panel (ADP) meeting on March 15, 2022, the panel recommended that the application for the development proposal at 1091 Stayte Road proceed to Council, subject to consideration of comments pertaining to the following items (see Table 2.0 below; see Attachment D for related ADP meeting minutes):

Table 2: ADP comments and Applicant's responses

ADP Comments	Applicant's response
Consider using a different variety of tree and plant species along the property line to reduce the need for a large fence	<ul style="list-style-type: none"> The plantings have been revised. The proposed fence does not exceed the zoning bylaw's maximum fence height requirement. It was advised that coordination with the Arborist is required to avoid damaging the tree roots. Notes have been included in the updated Landscape Plan; the posts are to be hand dug under Arborist supervision.
To use colours and elements from the front elevation on the rear elevation.	<ul style="list-style-type: none"> The colours and elements on the rear elevation have been revised to match that on the front elevation.
Revise the design to replace the horizontal windows with larger vertical windows.	<ul style="list-style-type: none"> The horizontal window has been replaced by a larger window.
To include roughed-in electric vehicle charging infrastructure.	<ul style="list-style-type: none"> Plans updated to include a rough-in outlet for EV charging.
Ensuring that minimum setbacks for the RT-1 zone are maintained and no building and/or structure falls within these setbacks.	<ul style="list-style-type: none"> The garbage enclosures have been removed from the Site Plan to meet the RT-1 zoning setback requirement.

Staff believe the applicant has provided a satisfactory response to the comments noted above.

Tree Management and Protection

As per the OCP, development projects should be designed to preserve and protect mature, healthy trees. The applicant has submitted an arborist report which identifies a total of seventeen (17) "protected trees" which are subject to the provisions of the city's Tree Protection Bylaw, No. 2407 (see Attachment E) There are four on-site trees, seven off-site trees plus six City trees. Table 3.0 below identifies the protected trees, their size, and whether they are proposed for removal or retention. The Table also identifies the value of securities for tree retention and the number and value of replacement trees where removals are proposed.

Table 3: Summary of Protected Trees tied to the Development

Tree Ref.	Species	DBH (cm)	Retain / Remove	Security for Retention / Replacement Value	# of Replacements
665 (City)	English Oak	42	Retain	3000	NA
666 (City)	Western Red Cedar	57	Retain	4500	NA
667 (City)	Western Red Cedar	33	Retain	3000	NA
668 (City)	Western Red Cedar	74	Retain	10000	NA
669 (OS)	Douglas Fir	59	Retain	4500	NA
670 (OS)	Western Red Cedar	71	Retain	10000	NA

671 (OS)	Western Red Cedar	93	Retain	10000	NA
672 (OS)	Western Red Cedar	90	Retain	10000	NA
673 (OS)	Western Red Cedar	54	Retain	4500	NA
674	Mountain Ash	56	Remove	4500	3
675 (OS)	Western Red Cedar	80	Retain	10000	NA
676 (OS)	Western Red Cedar	37	Retain	3000	NA
677 *	Western Red Cedar*	94	Retain	10000	NA
678	Pacific Dogwood	92	Retain	10000	NA
679 (City)	Paperbark Maple	21	Retain	3000	NA
680 (City)	Paperbark Maple	13	Retain	3000	NA
681	Spruce	54	Remove	4500	3
Total				107500	6

* This tree is shared with the City

City staff have reviewed the recommendations of the Project Arborist and are comfortable with the proposed tree protection measures and removals subject to the following requirements, noted by the city's Arboricultural Technician, being fulfilled prior to final adoption:

- The emerald cedar hedging shrubs planted on Cliff Avenue will require removal under the Engineering Department's lot servicing permit.
- Trees 677 and 678 will not require pruning for overhead service drop. Hydro will be undergrounded and trenched and will need to be supplied from the power pole on Cliff Avenue siding 1115 Stayte Road.
- The Architect and Engineer will need to approve and sign off that all prescriptions made by the project arborist are feasible.
- Walkways and encroachments on city boulevard must be approved through Engineering. The east pedestrian walkway proposed within the critical root zone of the Dogwood will negatively impact the Pacific Dogwood tree. Additionally, it cannot be supported as there is a city bench and bus stop 56190 on city sidewalk where the proposed pathway is.
- Posting \$107,500 in securities for trees as required by the Tree Protection Bylaw.

Six new trees are proposed as part of the development. All the trees identified on the Tree Retention/Removal Plan in the Arborist report (see Attachment E) have been given their retention/removal recommendation on a preliminary basis. This will be further reviewed upon receipt of an application for a Tree Management Permit (TMP), likely to accompany a future request for demolition of the existing building. Final recommendations will be based upon design/construction and grading details.

FINANCIAL IMPLICATIONS

The following fees would be collected if approved for rezoning and subdivision (Table 4). Note these fees are subject to change:

Table 4: Applicable Development Costs

	Fee (per Unit)	Units Subject to Fee	Sub-Total
--	-------------------	----------------------------	-----------

City of White Rock Development Cost Charges (DCCs)	\$19,294.76	1	\$19,294.76
TransLink DCCs	\$2,993.00	1	\$2,993.00
Metro Vancouver (Regional) DCCs	\$6,254.00	1	\$6,254.00
Surrey School District School Site Acquisition Charges (SSAC)	\$1,000.00	1	\$1,000.00
Total			\$29,541.76

COMMUNICATION AND COMMUNITY ENGAGEMENT IMPLICATIONS

A Public Information Meeting (PIM) for this proposal was held on October 8, 2020. One person attended the PIM. A copy of the Applicant's PIM Summary is included in Attachment F. Comments received during the PIM are summarized in Table 5.

Table 5: Comment and Response from PIM Summary

Comment / Question	Applicant's Response
Concern about offsite tree removal and potential damage to the health of the trees with the duplex development.	<ul style="list-style-type: none"> An effort to save as many healthy trees as possible, based on the arborist's professional experience and recommendations has been made. The arborist will be present onsite during excavation and construction near TPZ to ensure no damage is done to the trees to be retained. The developer reached out to the neighbours after PIM to obtain permission for off-site tree removal. Since the neighbours declined to provide their permission, the design was revised to retain all offsite trees by incorporating a Screw Pile foundation.

A public hearing will be scheduled if the zoning amendment receives 1st and 2nd reading. This will allow the community to express their thoughts on the proposal to Council.

INTERDEPARTMENTAL INVOLVEMENT/IMPLICATIONS

The zoning bylaw amendment application was referred to municipal departments for comments. Comments provided to the applicant have been addressed related to this stage of the rezoning proposal.

Approval of the final detailed (civil) designs and the execution of a "Works and Servicing Agreement" to the satisfaction of the Director of Engineering and Operations would be required as a condition of 3rd reading.

The relocation of the east pedestrian walkway would need to be addressed to the satisfaction of the Director of Planning and Development Services. The submission of drawings and final approval of the revised location by city departments would be required as a condition of 3rd reading.

The tree retention requirements would need to be addressed to the satisfaction of the Director of Planning and Development Services. The completion of covenants and the final approval of the applicant's arborist report by the city's Arboricultural Technician is also recommended to be required as a condition of 3rd reading.

CLIMATE CHANGE IMPLICATIONS

The application will enable modest infill within the ‘Mature Neighbourhood’ designation, thereby lessening the demand for outward sprawl otherwise necessary to accommodate growth.

IMPLICATIONS FOR TREE PRESERVATION AND TREE CANOPY ENHANCEMENT

Tree retention is recognized as a benefit to the uptake of greenhouse gases and reducing factors that contribute to climate change. The subject property contains mature trees subject to the city’s Tree Protection Bylaw. Several off-site mature trees border the property line of the subject property and neighbouring property south of the subject property that would be impacted by this development. Staff have made efforts to ensure optimal long-term tree preservation occurs.

ALIGNMENT WITH STRATEGIC PRIORITIES

The proposal is generally aligned with the Corporate Vision established as part of Council’s Strategic Priorities, particularly with respect to supporting a community where people can live, work and play in an enjoyable atmosphere.

OPTIONS / RISKS / ALTERNATIVES

The following options are available for the Council’s consideration:

1. Reject “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438,” or
2. Defer consideration of “White Rock Zoning Bylaw, 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2022, No. 2438,” and refer the application to staff to address any issues identified by Council.

CONCLUSION

The City has received an application for a zoning bylaw amendment, to allow the construction of a duplex at 1091 Stayte Road. Staff have brought forward a draft Zoning Amendment Bylaw to move the application forward to a Public Hearing. Staff recommend that the Zoning Amendment Bylaw be given first and second reading and that a Public Hearing be scheduled. If Council adopts the zoning amendment bylaw, the subsequent issuance of the Minor Development Permit for the form and character of the duplex would be considered by staff, as the authority is delegated to the City Manager by the Planning Procedures Bylaw. Overall, staff supports the proposal subject to the recommended conditions noted.

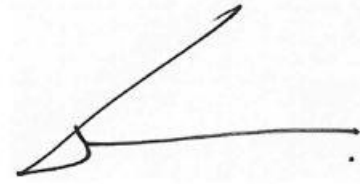
Respectfully submitted,



Anne Berry, MCIP, RPP
Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, consisting of a stylized 'G' followed by a horizontal line and a small dot at the end.

Guillermo Ferrero
Chief Administrative Officer

Attachment A: Draft Zoning Amendment Bylaw No. 2438

Attachment B: Architectural and Landscape Drawings

Attachment C: Response to Mature Neighbourhood Development Permit Area Guidelines

Attachment D: March 15 2022 ADP Meeting Minutes

Attachment E: Arborist Report

Attachment F: Public Information Meeting (PIM) Summary

**The Corporation of the
CITY OF WHITE ROCK
BYLAW 2438**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock, in open meeting assembled, ENACTS as follows:

1. Schedule "C" of the "White Rock Zoning Bylaw, 2012, No. 2000" as amended is further amended by rezoning the following lands:

Lot 1 Section 11 Township 1 New Westminster District Plan 14850
(1091 Stayte Road)
PID: 002-267-144

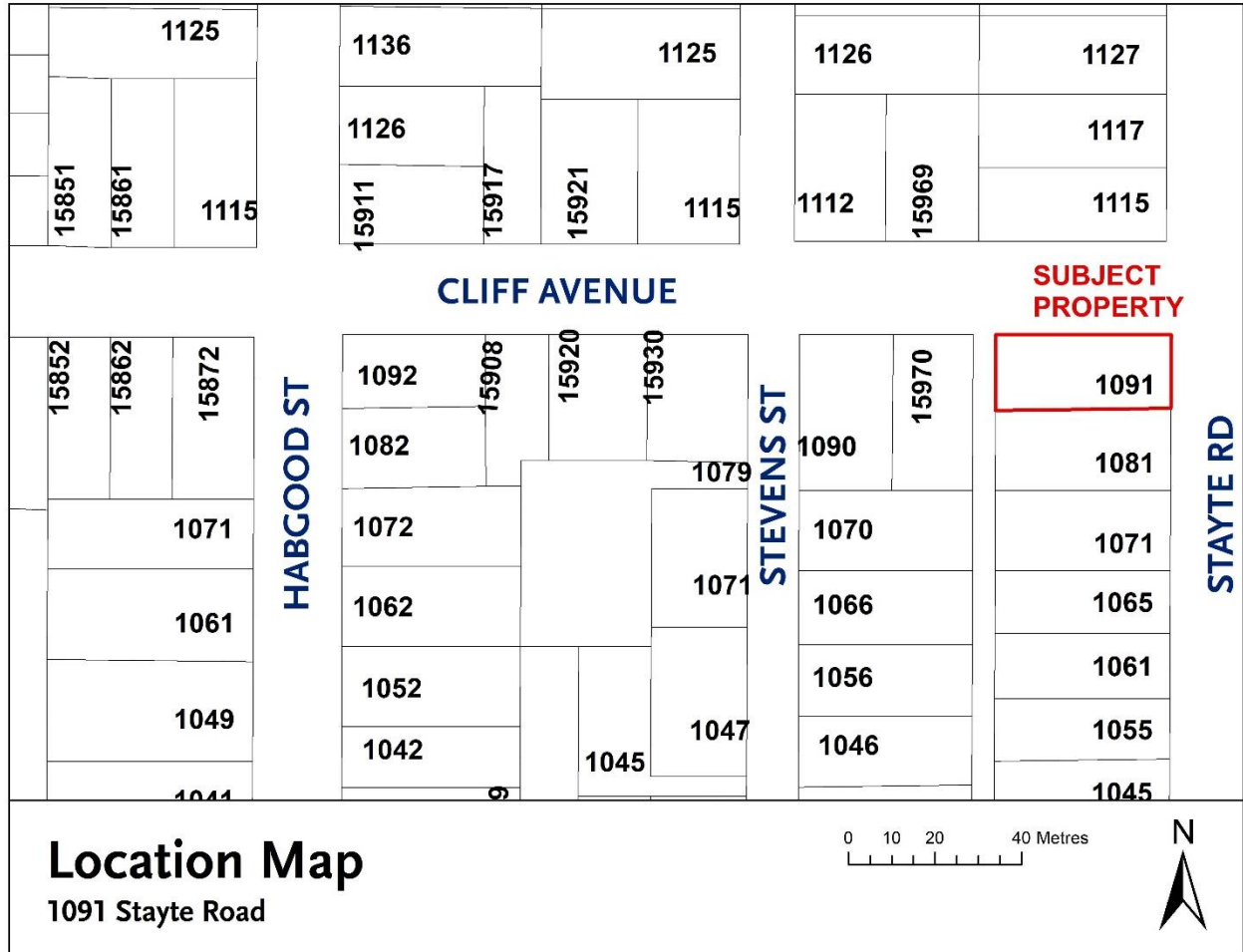
as shown on Schedule "1" attached hereto, from 'RS-1 One Unit Residential Zone' to 'RT-1 Two Unit (Duplex) Residential Zone'.
2. This Bylaw may be cited for all purposes as the "White Rock Zoning Bylaw 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2020, No. 2438".

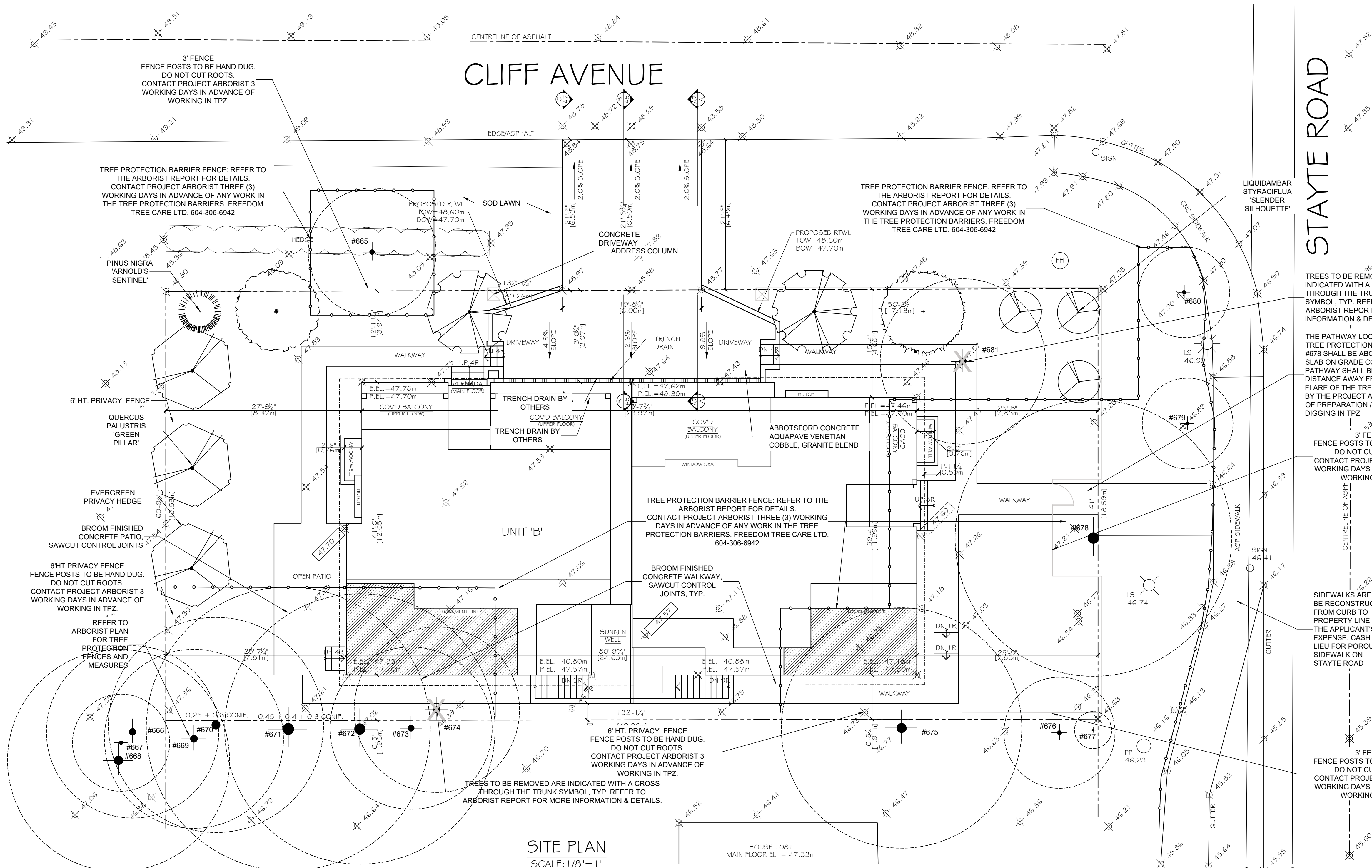
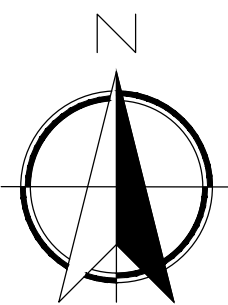
PUBLIC INFORMATION MEETING on the	8 th day of	October, 2020
RECEIVED FIRST READING on the	day of	
RECEIVED SECOND READING on the	day of	
PUBLIC HEARING held on the	day of	
RECEIVED THIRD READING on the	day of	
RECONSIDERED AND FINALLY ADOPTED on the	day of	

Mayor

City Clerk

Schedule "1"





SITE PLAN
SCALE: 1/8" = 1'

PROPOSED SINGLE FAMILY DWELLING			
CIVIC ADDRESS	1091 STAYTE ROAD, WHITE ROCK		
LEGAL DESCRIPTION	LOT 1 BLK 17 SEC 11 NWD PL NWP 14850 TWP 1		
AVERAGE GRADE	48.38m, 47.60m, 47.57, 47.70m		
MAX. BUILDING HEIGHT EL.	47.81 m		
	55.52m		
BASEMENT FLOOR SLAB EL.	45.72m		
GARAGE SLAB EL.	48.38m		
MAIN FLOOR EL.	48.51m		
UPPER FLOOR EL.	51.61m		
USIDE OF TRUSS EL.	54.37m		
ROOF PEAK EL.	54.79m		
RT-1 ZONING			
LOT AREA	8052 SQ. FT.		
	748.06 SQ. FT.		
ITEMS	PERMITTED	PROPOSED	
LOT COVERAGE (45%)	3623 SQ. FT.	3189 SQ. FT.	
FLOOR AREA RATIO (50%)	4026 SQ. FT.	MF UNIT 'A'	1149 SQ. FT.
		MF UNIT 'B'	1138 SQ. FT.
		VERANDA 'A'	27,000 SQ. FT.
		VERANDA 'B'	24 SQ. FT.
		UF UNIT 'A'	830 SQ. FT.
GARAGE AREA	N/A	UF UNIT 'B'	854 SQ. FT.
		UNIT 'A'	4022 SQ. FT.
		UNIT 'B'	423 SQ. FT.
		UNIT 'B'	420 SQ. FT.
		TOTAL	844 SQ. FT.
FRONT YARD SETBACK	7.50m	7.83m	
RIGHT SIDE SETBACK (EXTERIOR)	3.80m	3.94m	
LEFT SIDE SETBACK (INTERIOR)	1.50m	1.91m	
REAR YARD SETBACK	7.50m	7.81m	
COVID DECK AREA	N/A	UNIT 'A'	82 SQ. FT.
		UNIT 'B'	194 SQ. FT.
		TOTAL	276.5347 SQ. FT.
HEIGHT	7.7m	6.97m	

STAYTE ROAD

TREES TO BE REMOVED ARE INDICATED WITH A CROSS THROUGH THE TRUNK SYMBOL. TYP. REFER TO ARBORIST REPORT FOR MORE INFORMATION & DETAILS.

THE PATHWAY LOCATED INSIDE THE TREE PROTECTION ZONE OF TREE #678 SHALL BE ABOVE GRADE USING SLAB ON GRADE CONCRETE. THE PATHWAY SHALL BE A SUITABLE DISTANCE AWAY FROM THE BASAL FLARE OF THE TREE, AS DETERMINED BY THE PROJECT ARBORIST AT TIME OF PREPARATION / INSTALLATION. NO DIGGING IN TPZ.

3' FENCE
FENCE POSTS TO BE HAND DUG.
DO NOT CUT ROOTS.
CONTACT PROJECT ARBORIST 3
WORKING DAYS IN ADVANCE OF
WORKING IN TPZ.

SIDEWALKS ARE TO BE RECONSTRUCTED FROM CURB TO PROPERTY LINE AT THE APPLICANT'S EXPENSE. CASH IN LIEU FOR POROUS SIDEWALK ON STAYTE ROAD

3' FENCE
FENCE POSTS TO BE HAND DUG.
DO NOT CUT ROOTS.
CONTACT PROJECT ARBORIST 3
WORKING DAYS IN ADVANCE OF
WORKING IN TPZ.

- NOTES
- WRITTEN DIMENSIONS SHALL HAVE PRECEDENCE OVER SCALED DIMENSIONS.
 - CONTRACTOR SHALL VERIFY ALL ON SITE CONDITIONS PRIOR TO THE COMMENCEMENT OF WORK.
 - ALL WORK SHALL CONFORM TO THE REQUIREMENT OF THE BRITISH COLUMBIA BUILDING CODE; 2018 EDITION
 - ALL CONCRETE WORK SHALL CONFORM TO THE REQUIREMENT OF CSA 3.A23.1 LATEST EDITION.
 - CONCRETE STRENGTH AT 28 DAYS, MIN. 15 MPa, AND 20 MPa FOR FLOORS, AND 32 MPa FOR GARAGE SLABS
 - ALL FRAMING AND NAILING SHALL CONFORM TO B.C.B.C. PART 9 AND DESIGN TO CSA LATEST EDITION.
 - ALL FRAMING LUMBER SHALL BE DOUGLAS FIR #2 OR BETTER.
 - WOOD TRUSSES SHALL BE DESIGNED AND SEALED BY P. ENG. REGISTERED IN B.C.
 - ALL PLYWOOD SHALL BE DOUGLAS FIR AND CONFORM TO CSA 0121 LATEST EDITION.
 - ALL NEW WINDOWS, DOORS AND SKYLIGHTS AND THEIR INSTALLATION SHALL COMPLY WITH NEW NAFS STANDARDS AND SPECIFICATIONS (9.7.4 B.C.B.C. 2018 EDITION).
 - THE OWNER IS TO BE ENTIRELY RESPONSIBLE FOR ALL STRUCTURAL ENGINEERING REQUIREMENTS.
 - ALL DIMENSIONS ARE TO BE CHECKED ON SITE BEFORE ANY WORK COMMENCES.
 - PROTECTIVE GRATE ENCLOSURE CONFORMING TO 9.9.10.1.(5) TO BE INSTALLED OVER WINDOW WELLS
 - WINDOW SEATS RAISED A MINIMUM OF 0.5m FROM THE FLOOR

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

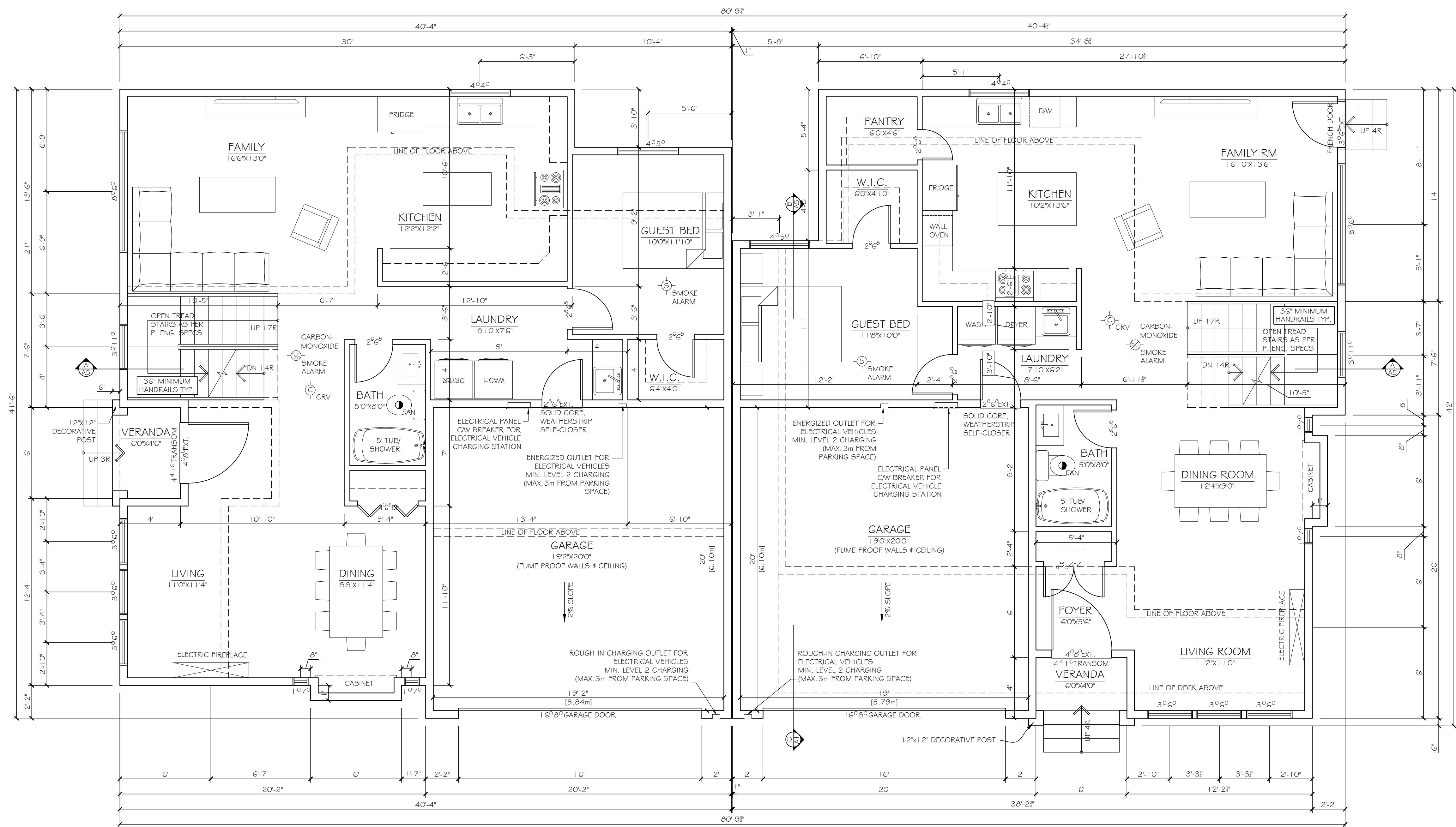
TOORA HOME PLANS

TEL: (604) 951-4343
FAX: (604) 951-4373
EMAIL: toorahomes@gmail.com

ADDRESS:
12968 - 107th AVE
SURREY, B.C. V3T 2E9

PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	SITE PLAN	A I
SCALE:	1/8" = 1'	
DATE:	FEB/2022	
DESIGNER:	RAJ TOORA	
DRAWN BY:	JAQUELINE CRUZ	



MAIN FLOOR UNIT A
SCALE: 1/4"=1'

NET FLOOR AREA =	1149 SQ. FT.
GROSS FLOOR AREA =	1573 SQ. FT.
GARAGE FLOOR AREA =	423 SQ. FT.
VERANDA AREA (PARTLY ENCLOSED) =	27 SQ. FT.

MAIN FLOOR UNIT B
SCALE: 1/4"=1'

NET FLOOR AREA =	1138 SQ. FT.
GROSS FLOOR AREA =	1558 SQ. FT.
GARAGE FLOOR AREA =	420 SQ. FT.
VERANDA AREA (PARTLY ENCLOSED) =	24 SQ. FT.

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

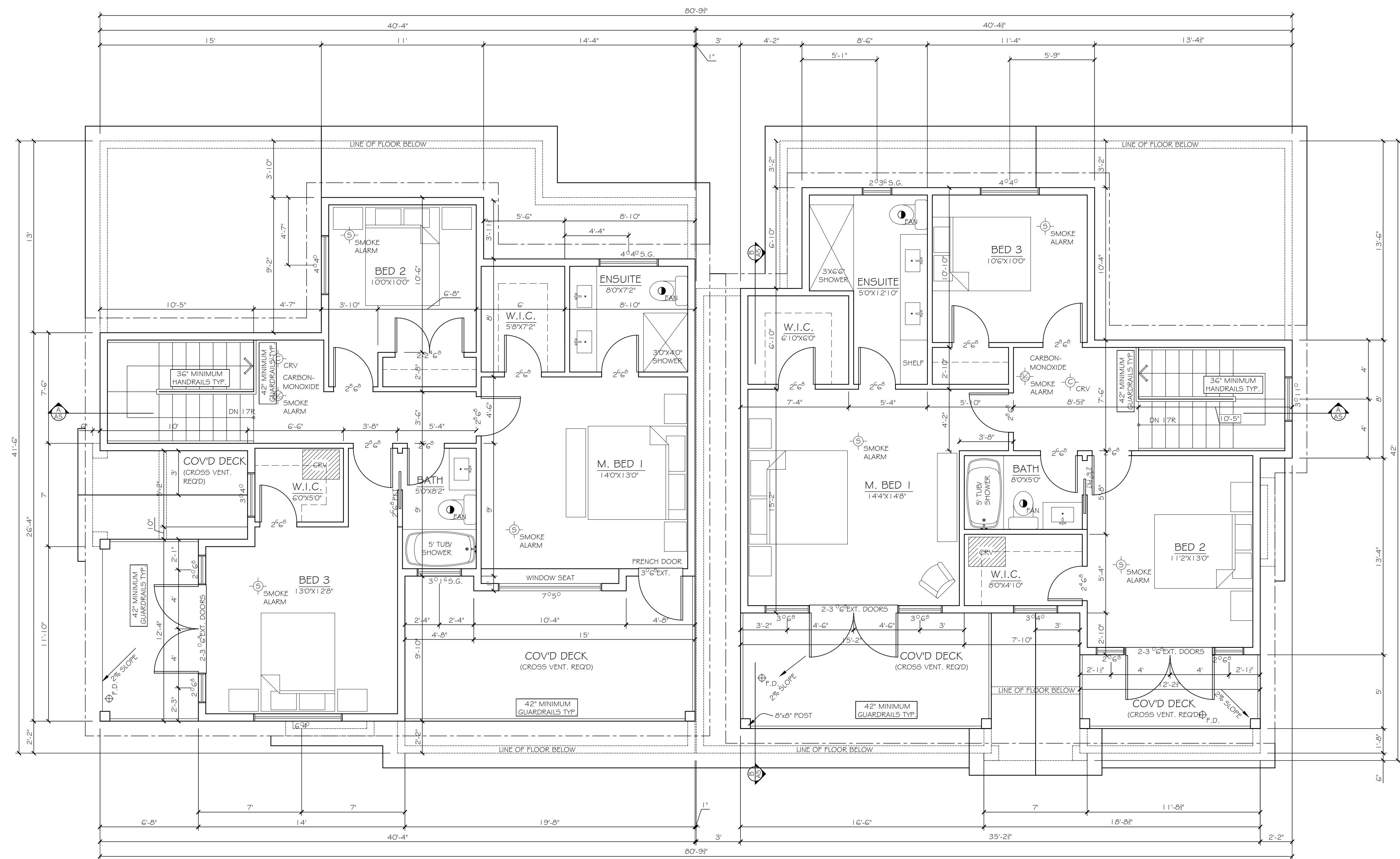
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EMAIL: toorahomes@gmail.com SURREY, B.C. V3T 2E9

PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	MAIN FLOOR PLAN
SCALE:	1/4"=1'
DATE:	FEB/2022
DESIGNER:	RAJ TOORA
DRAWN BY:	JAQUELINE CRUZ

A2



UPPER FLOOR - UNIT A
SCALE: 1/4" = 1'

NET FLOOR AREA =	830 SQ. FT.
LESS OPEN AREA =	69 SQ. FT.
GROSS FLOOR AREA =	899 SQ. FT.
COV'D DECK AREA =	82 SQ. FT.

UPPER FLOOR - UNIT B
SCALE: 1/4" = 1'

NET FLOOR AREA =	854 SQ. FT.
LESS OPEN AREA =	69 SQ. FT.
GROSS FLOOR AREA =	923 SQ. FT.
COV'D DECK AREA =	194 SQ. FT.

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

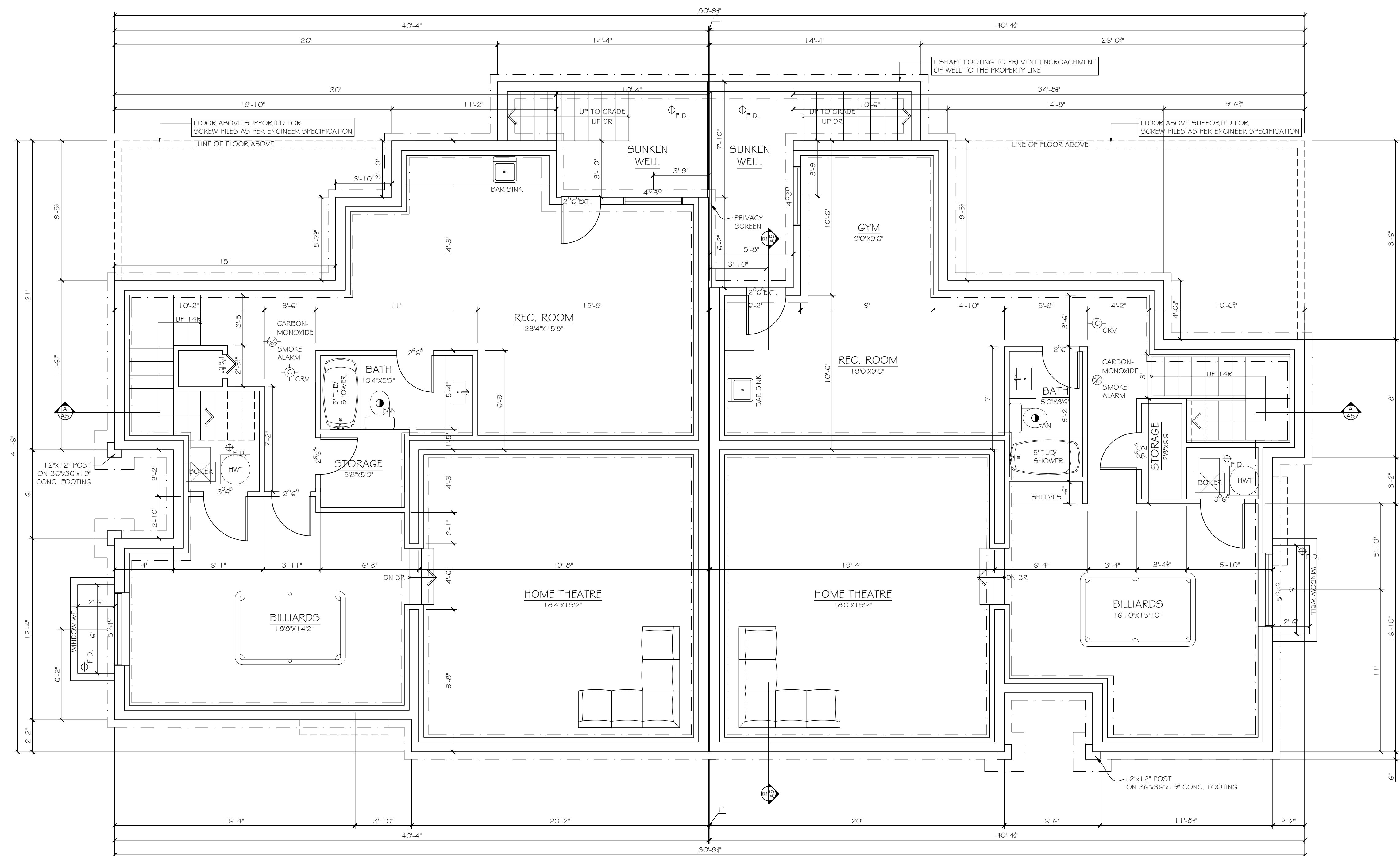
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EMAIL: toorahomes@gmail.com SURREY, B.C. V3T 2E9

PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	UPPER FLOOR PLAN
SCALE:	1/4" = 1'
DATE:	FEB/2022
DESIGNER:	RAJ TOORA
DRAWN BY:	JAQUELINE CRUZ

A3



BASEMENT FLOOR UNIT A
SCALE: 1/4"=1'

GROSS FLOOR AREA = 1410 SQ. FT.
SUNKEN WELL AREA = 96 SQ. FT.

BASEMENT FLOOR UNIT B
SCALE: 1/4"=1'

GROSS FLOOR AREA = 1289 SQ. FT.
SUNKEN WELL AREA = 113 SQ. FT.

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

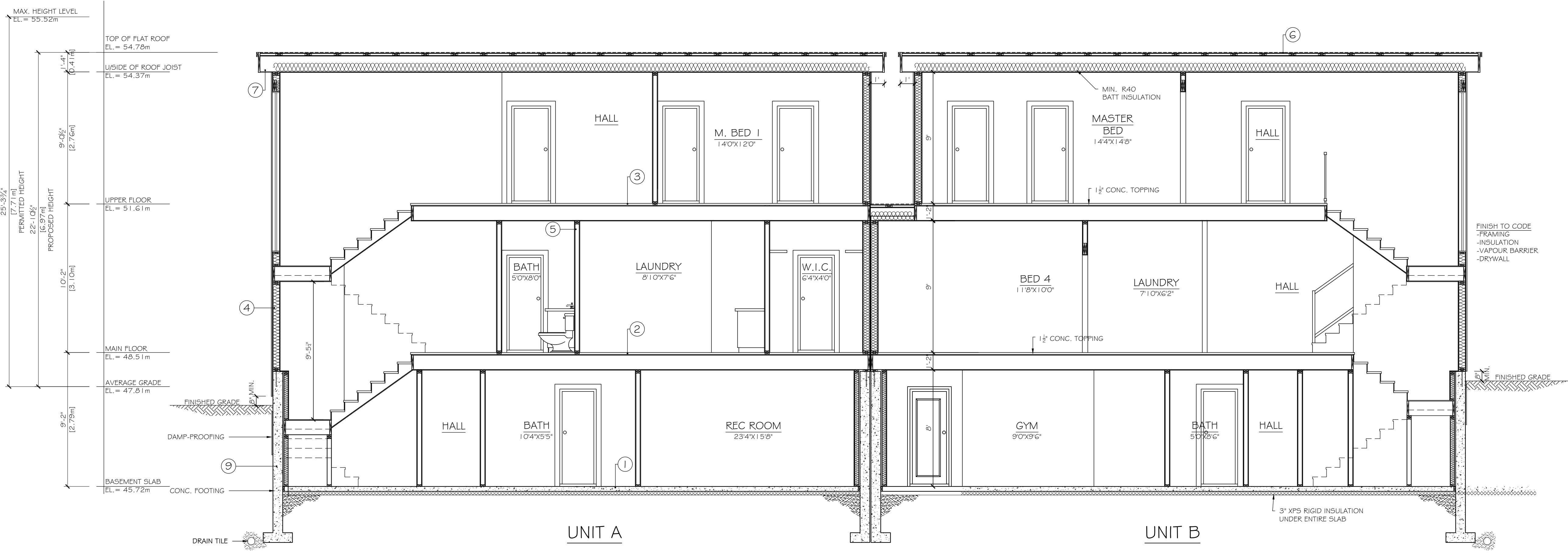
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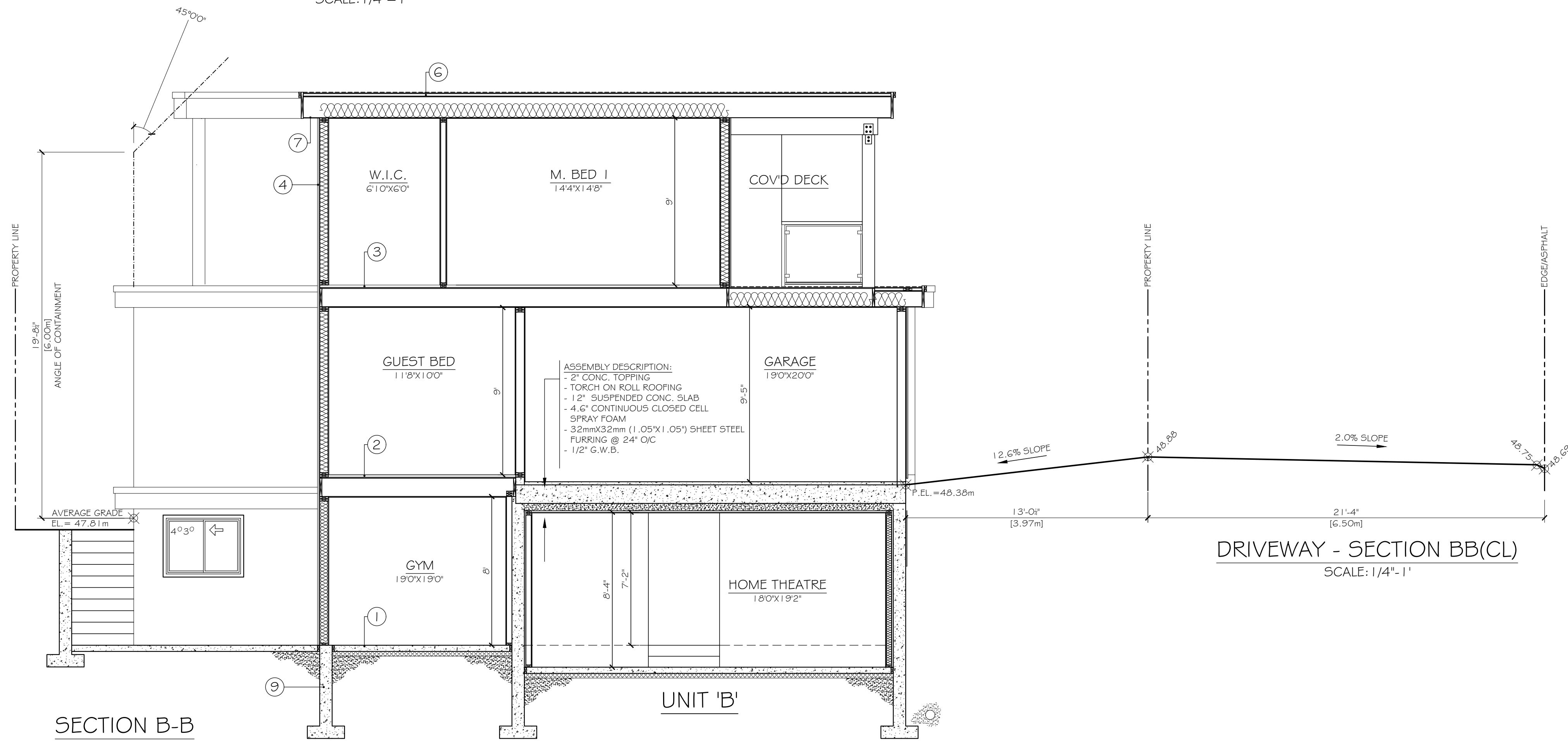
PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	BASEMENT FLOOR PLAN
SCALE:	1/4"=1'
DATE:	FEB/2022
DESIGNER:	RAJ TOORA
DRAWN BY:	JAQUELINE CRUZ

A4



SECTION A-A
SCALE: 1/4"= 1'



SECTION B-B
SCALE: 1/4"= 1'

CONSTRUCTION SPECIFICATIONS

- SLAB**
 - 4" CONC. SLAB
 - 6x6 WELDED WIRE MESH
 - 6 MIL. POLY VAPOR BARRIER
 - 6" MIN. COMPACTED SAND (TYPICAL)
- MAIN FLOOR**
 - 3/4" T&G PLYWOOD SUBFLOOR GLUED & SCREWED
 - 1 1/2" TJI FL. JST. @ 16" O/C (U.N.O.)
 - CROSS-BRIDGING/ BLOCKING AS PER MFG SPECS
 - 1/2" G.W.B.
- UPPER FLOOR**
 - 3/4" T&G PLYWOOD SUBFLOOR GLUED & SCREWED
 - 1 1/2" TJI FL. JST. @ 16" O/C (U.N.O.)
 - CROSS-BRIDGING/ BLOCKING AS PER MFG SPECS
 - 1/2" G.W.B.
- EXTERIOR WALLS (PRINCIPAL BUILDING)**
 - STONE, FIBER CEMENT PANELS & HARDIE SIDING
 - 5/8" CAPILLARY AIR SPACE
 - AIR BARRIER/ SHEATHING MEMBRANE
 - PRE-PAINTED METAL FLASHING
 - 1/2" EXT. GRADE PLYWOOD SHEATHING
 - 2x6 STUDS @ 16" o/c (U.N.O.)
 - R20 BATT INSULATION
 - 6 MIL. POLY VAPOR BARRIER
 - 1/2" G.W.B.
- INTERIOR PARTITIONS**
 - 1/2" G.W.B. BOTH SIDES (U.N.O.)
 - 2x4 STUDS @ 16" O/C (U.N.O.)
 - (* REFER TO DETAILS FOR FIRE-RATED WALLS)
- FLAT ROOF**
 - TORCH-ON ROOF
 - BUILDING PAPER
 - 5/8" T&G PLYWOOD SHEATHING
 - TJI JOISTS AS PER STRUCTURAL ENGINEER DRAWINGS
 - R40 BATT INSULATION
 - 6MIL POLY VAPOR BARRIER
 - 5/8" G.W.B.
- EAVES**
 - INTEGRAL GUTTER SYSTEM
 - 2x10 FASCIA BOARD
 - 2x4 BACK NAILER
 - METAL SOFFIT WITH 2" CONT. SCREEN VENT
 - UNVENTED ALUMINUM SOFFIT @ O/H WITHIN 1.2m OF FL.
- ATTIC VENTILATION**
 - 2" Baffle CLEARANCE WITH INSULATION STOPS
- FOUNDATION**
 - SEE FOUNDATION DETAIL ON STRUCTURAL DRAWINGS
- GARAGE SLAB**
 - 2" CONC. TOPPING
 - TORCH ON ROLL ROOFING
 - 12" SUSPENDED CONC. SLAB
 - 4.6" CONTINUOUS CLOSED CELL SPRAY FOAM
 - 32mmX32mm (1.05"X1.05") SHEET STEEL FURRING @ 24" O/C
 - 1/2" G.W.B.

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

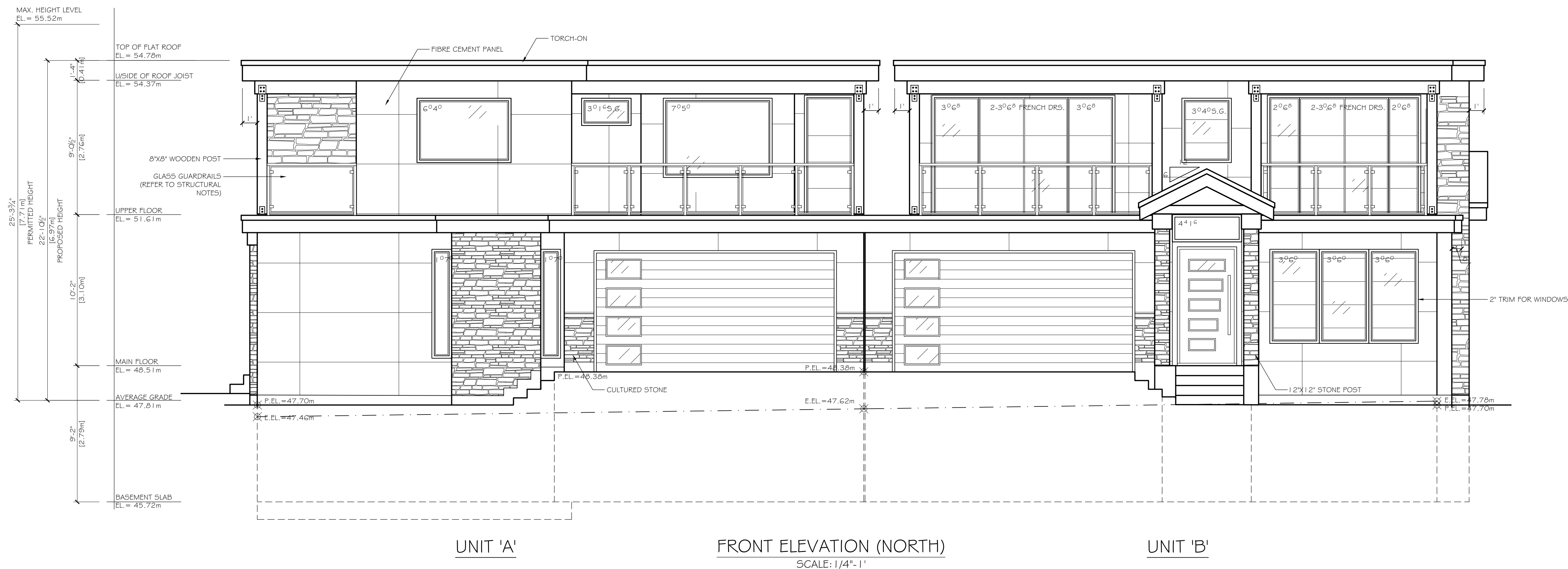
TOORA HOME PLANS

TEL: (604) 951-4343 ADDRESS:
FAX: (604) 951-4373 12968 - 107th AVE
EMAIL: toorahomes@gmail.com SURREY, B.C. V3T 2E9

PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	SECTIONS	
SCALE:	1/4"= 1'	DESIGNER: RAJ TOORA
DATE:	FEB/2022	DRAWN BY: JAQUELINE CRUZ

A5



THESE PLANS CONFORM TO REQUIREMENTS IN THE
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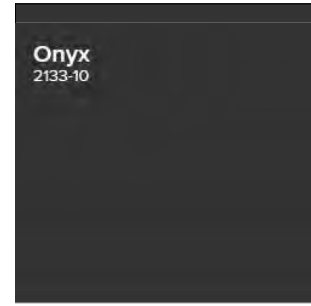
PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	ELEVATIONS	
SCALE:	1/4"=1'	DESIGNER: RAJ TOORA
DATE:	FEB/2022	DRAWN BY: JAQUELINE CRUZ

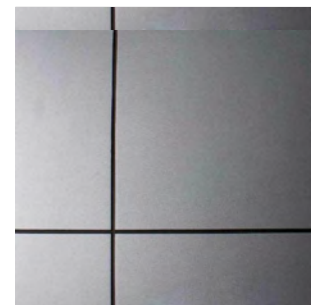
A6



① STONE
ARCTIC PACIFIC ART STONE (OR SIMILAR)



② METAL FASCIA / TRIMS
BLACK ONYX 2133-10 BENJAMIN MOORE

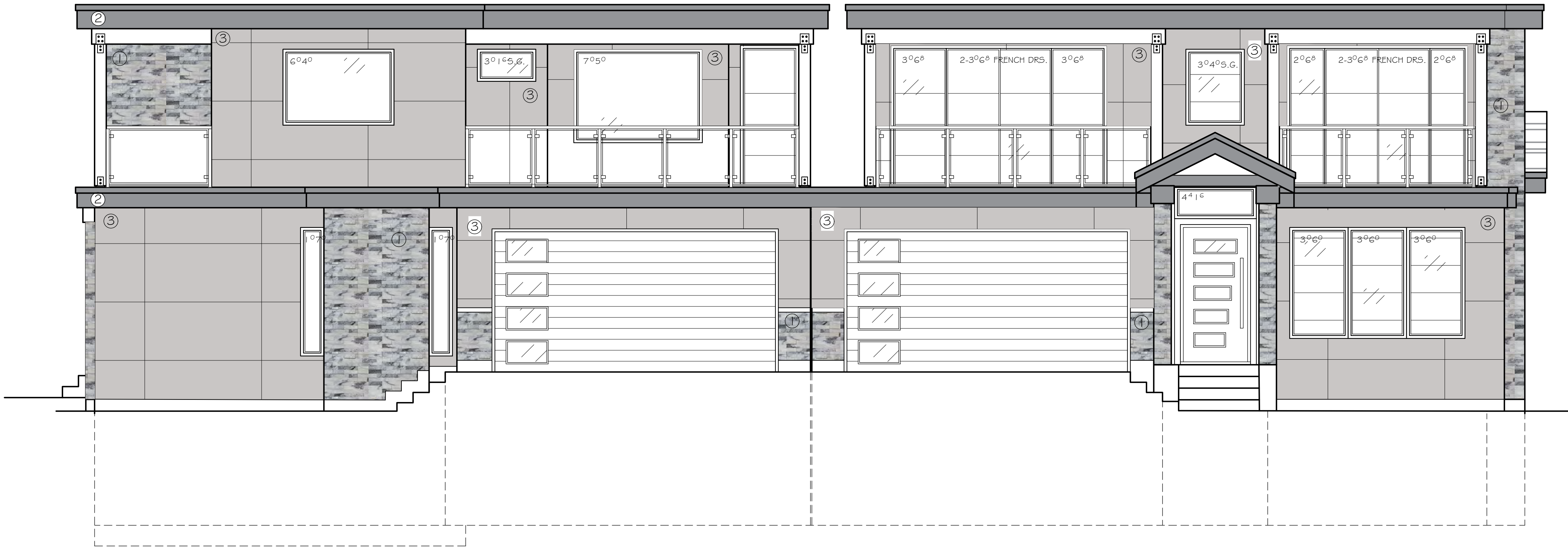


③ FIBER CEMENT PANELS
WHITE DOWN CC50 - BENJAMIN MOORE



④ HARDIE PANEL VERTICAL SIDING
NIGHT GRAY JAMES HARDIE

OTHER MATERIALS:
ALUMINUM & GLASS GUARDRAIL SYSTEM - BLACK
GABLE ROOF - GREY ASPHALT SHINGLES
4" HARDIE TRIM, PAINTED - BLACK ONYX 2133-10 BENJAMIN MOORE
EXT. STEEL DOOR WITH VIEW LITE, PAINTED - BLACK ONYX BENJAMIN MOORE
METAL FASCIA, PAINTED - BLACK ONYX BENJAMIN MOORE
WOOD POST, PAINTED - WHITE DOWN CC50 BENJAMIN MOORE
WINDOWS FRAME - BLACK
STEPS - EXPOSED CONCRETE



UNIT 'A'

FRONT ELEVATION (NORTH)
SCALE: 1/4" = 1'

UNIT 'B'



UNIT 'B'

REAR ELEVATION (SOUTH)
SCALE: 1/4" = 1'

UNIT 'A'

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

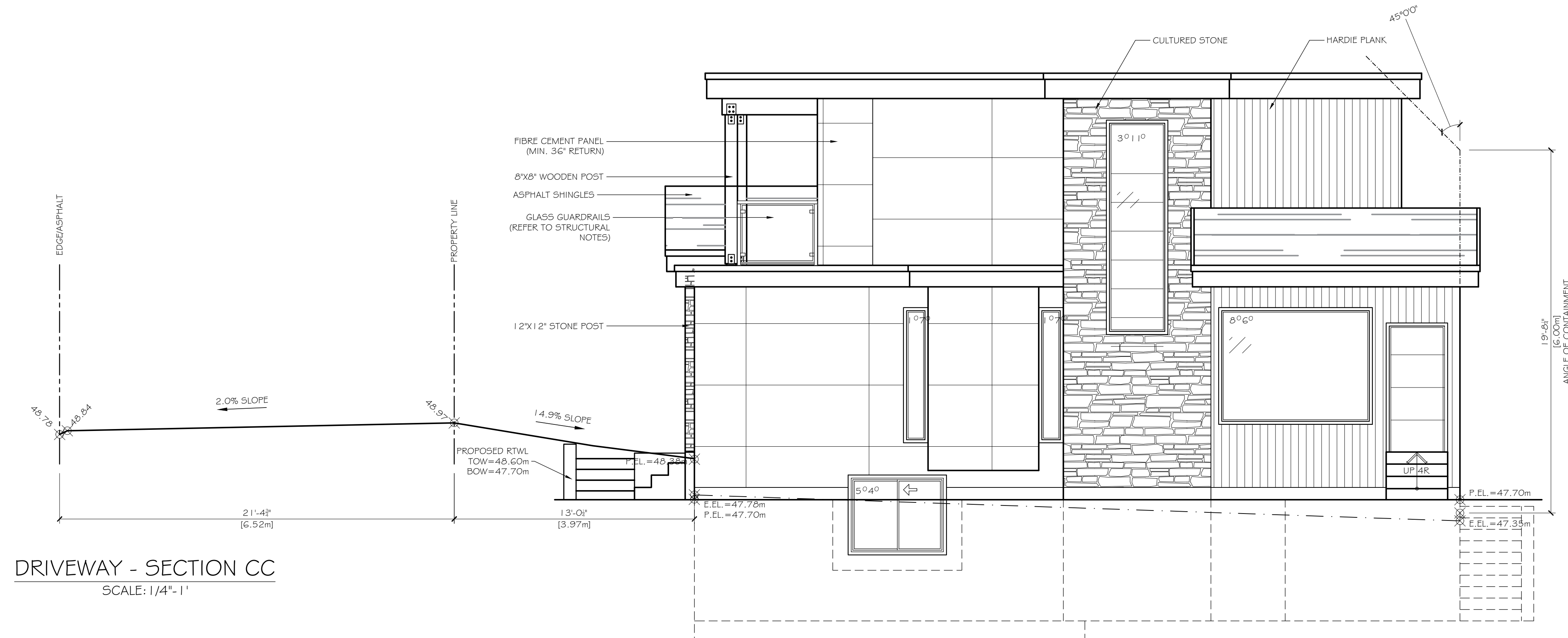
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PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

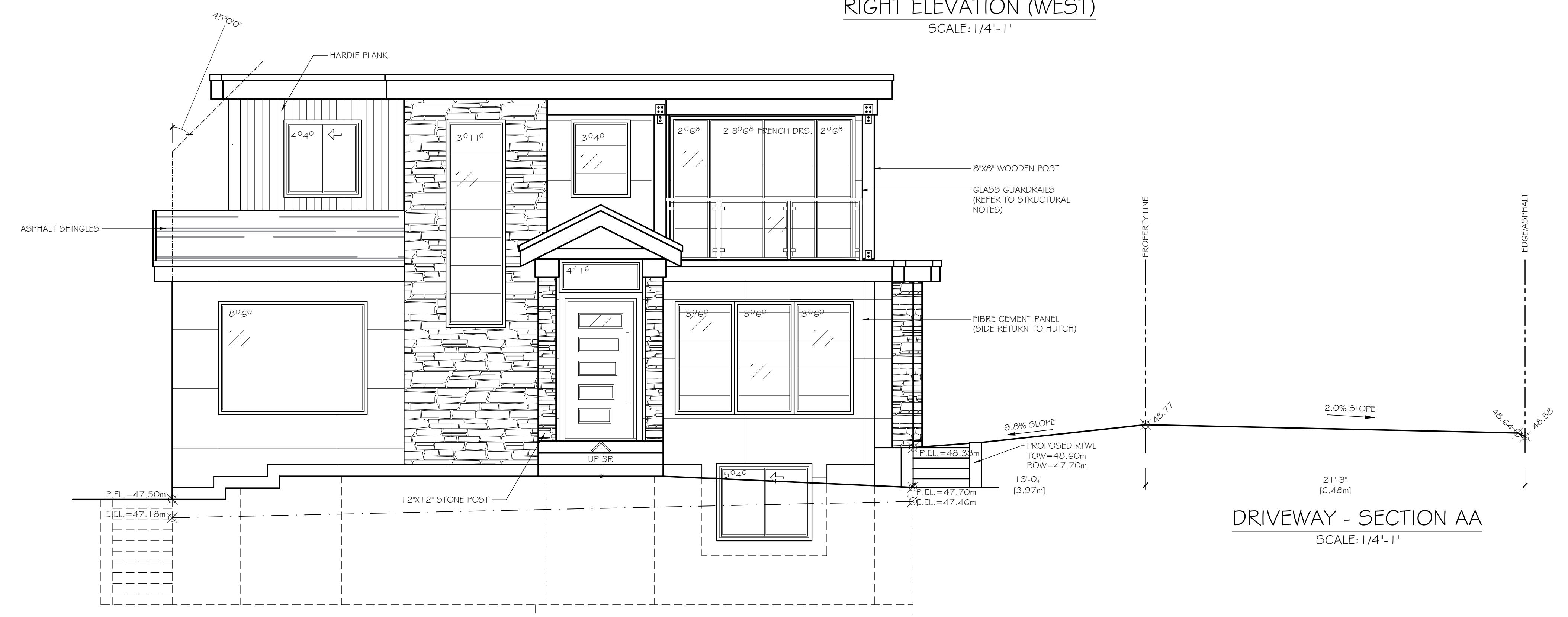
TITLE:	COLOURED ELEVATIONS	
SCALE:	1/4" = 1'	DESIGNER: RAJ TOORA
DATE:	FEB/2022	DRAWN BY: JAQUELINE CRUZ

A6.1



DRIVEWAY - SECTION CC
SCALE: 1/4" = 1'

UNIT 'B'
RIGHT ELEVATION (WEST)
SCALE: 1/4" = 1'



DRIVEWAY - SECTION AA
SCALE: 1/4" = 1'

UNIT 'A'
LEFT ELEVATION (EAST)
SCALE: 1/4" = 1'

THESE PLANS CONFORM TO REQUIREMENTS IN THE
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PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

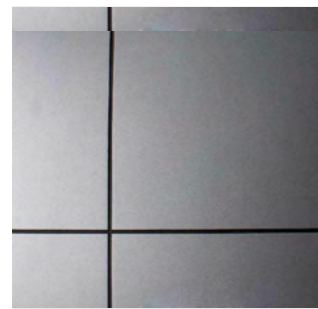
TITLE:	ELEVATIONS & DETAILS	A7
SCALE:	AS NOTED	
DATE:	FEB/2022	
DESIGNER:	RAJ TOORA	
DRAWN BY:	JAQUELINE CRUZ	



① STONE
ARCTIC PACIFIC ART STONE (OR SIMILAR)



② METAL FASCIA / TRIMS
BLACK ONYX 2133-10 BENJAMIN MOORE



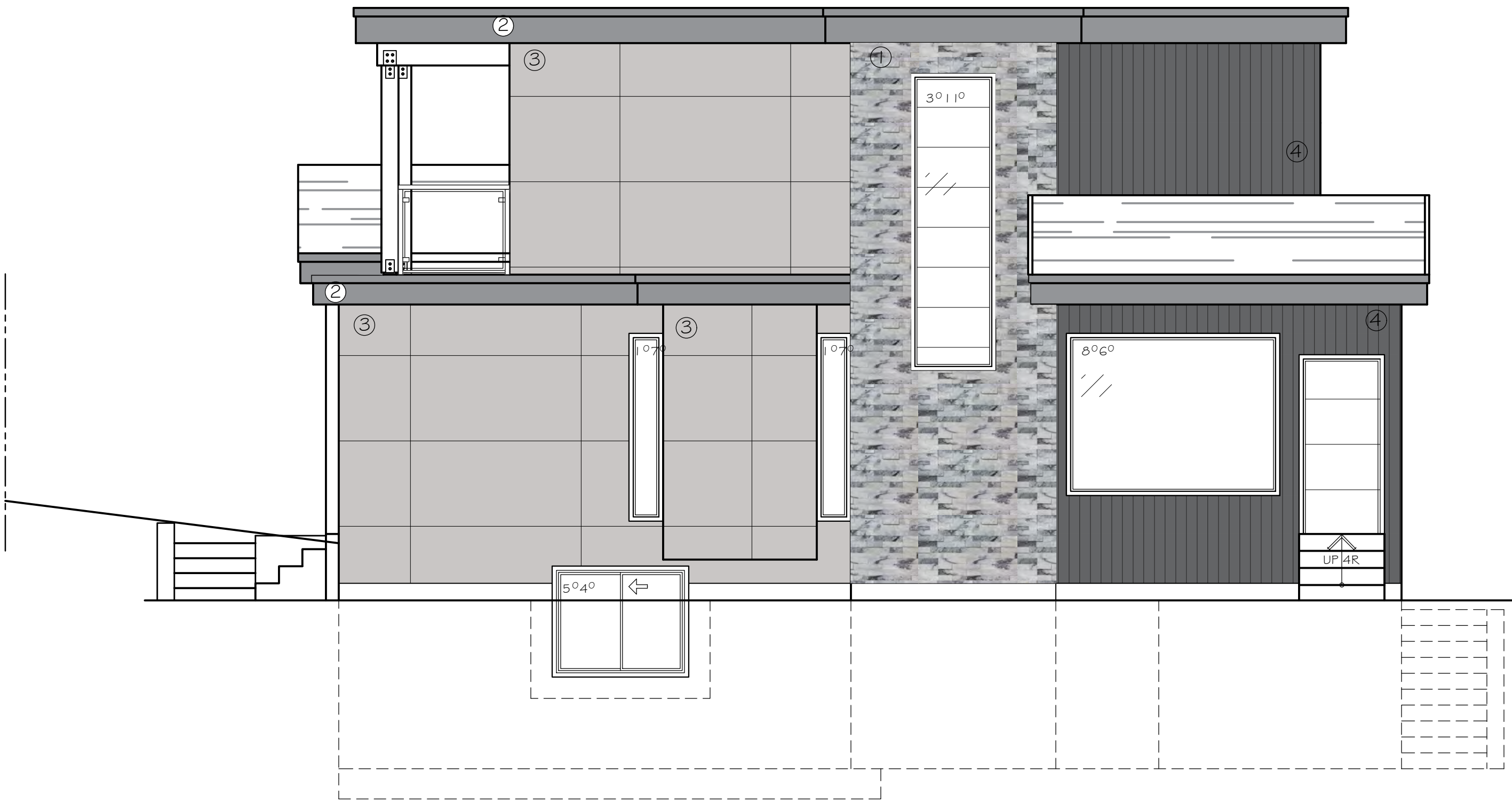
③ FIBER CEMENT PANELS
WHITE DOWN CC50 -BENJAMIN MOORE



④ HARDIE PANEL VERTICAL SIDING
NIGHT GRAY JAMES HARDIE

OTHER MATERIALS:

ALUMINUM 4 GLASS GUARDRAIL SYSTEM - BLACK
4" HARDIE TRIM, PAINTED - BLACK ONYX 2133-10 BENJAMIN MOORE
EXT. STEEL DOOR WITH VIEW LITE, PAINTED - BLACK ONYX BENJAMIN MOORE
METAL FASCIA, PAINTED - BLACK ONYX BENJAMIN MOORE
WOOD POST, PAINTED - WHITE DOWN CC50 BENJAMIN MOORE
WINDOWS FRAME - BLACK
STEPS - EXPOSED CONCRETE



UNIT 'B'
RIGHT ELEVATION (WEST)
SCALE: 1/4"=1'



UNIT 'A'
LEFT ELEVATION (EAST)
SCALE: 1/4"=1'

THESE PLANS CONFORM TO REQUIREMENTS IN THE
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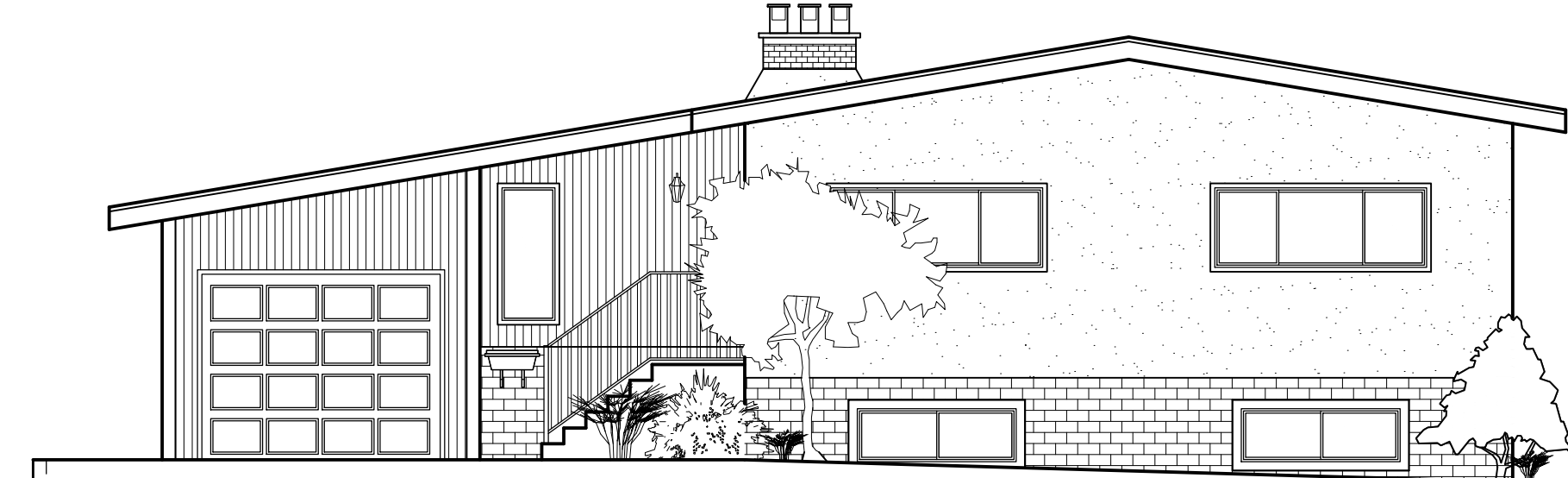
PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	COLOURED ELEVATIONS	
SCALE:	AS NOTED	DESIGNER: RAJ TOORA
DATE:	FEB/2022	DRAWN BY: JAQUELINE CRUZ

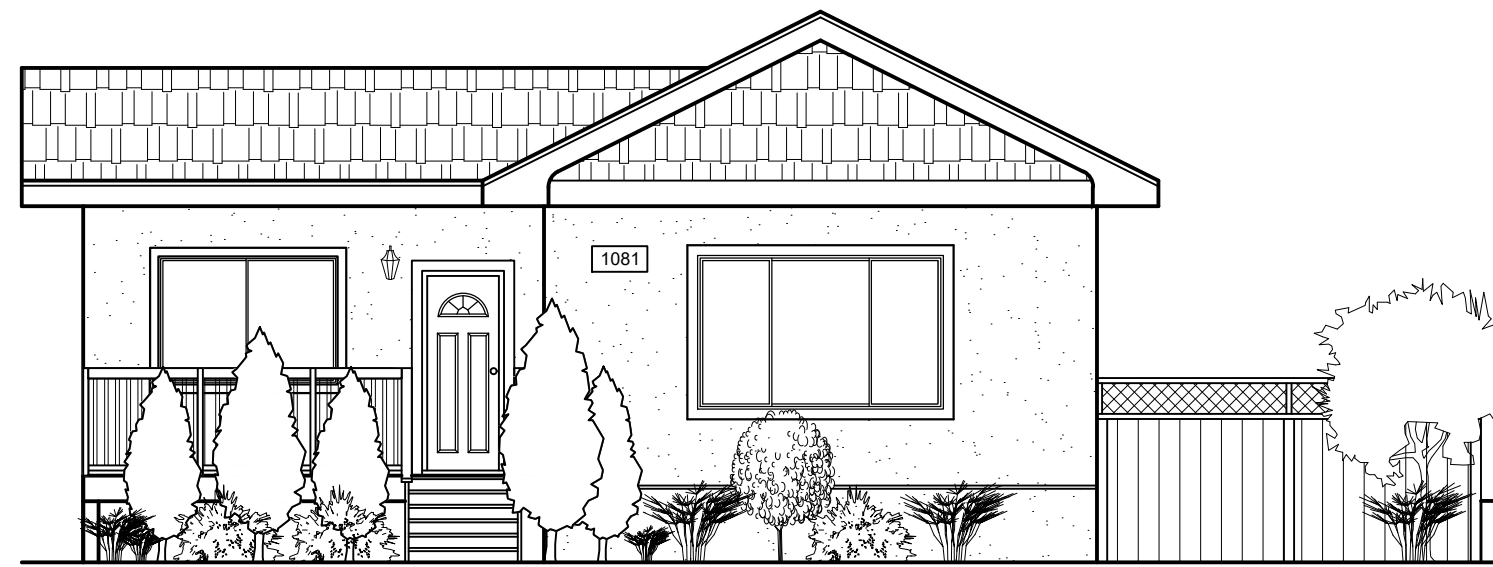
A7.1



1091
NORTH ELEVATION
(CLIFF AVENUE)



15970



1081
EAST ELEVATION
(STAYTE ROAD)



1091

THESE PLANS CONFORM TO REQUIREMENTS IN THE
B.C. BUILDING CODE 2018.

TOORA HOME PLANS

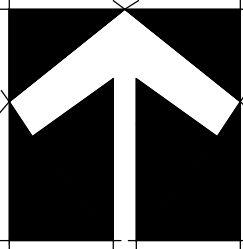
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EMAIL: toorahomes@gmail.com SURREY, B.C. V3T 2E9

PROPOSED DUPLEX FOR
LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK, B.C.

TITLE:	STREET PROFILE & RENDERINGS
SCALE:	NTS
DATE:	FEB/2022
DESIGNER:	RAJ TOORA
DRAWN BY:	JAUQUELINE CRUZ

A8

SEAL:



7	22.JUN.27	UPDATE PER CITY COMMENTS	CLG
6	22.APR.21	COMMENTS FROM CLIENT	WZ
5	22.APR.11	UPDATE PER COMMENTS	WZ
4	21.OCT.25	UPDATE PER ARBORIST PLAN	CLG
3	21.JUL.13	UPDATE PER NEW SITE PLAN	CLG
2	21.JAN.13	UPDATE PER NEW SITE PLAN	CLG
1	20.FEB.04	ISSUED FOR SUBMISSION	CLG

NO. DATE REVISION DESCRIPTION DR.

CLIENT:

PROJECT:

PROPOSED DUPLEX FOR

LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK

DRAWING TITLE:

LANDSCAPE PLAN

DATE: 20.JAN.17

DRAWING NUMBER:

SCALE: 1:100

DRAWN: CLG

DESIGN: CLG

CHK'D: PC

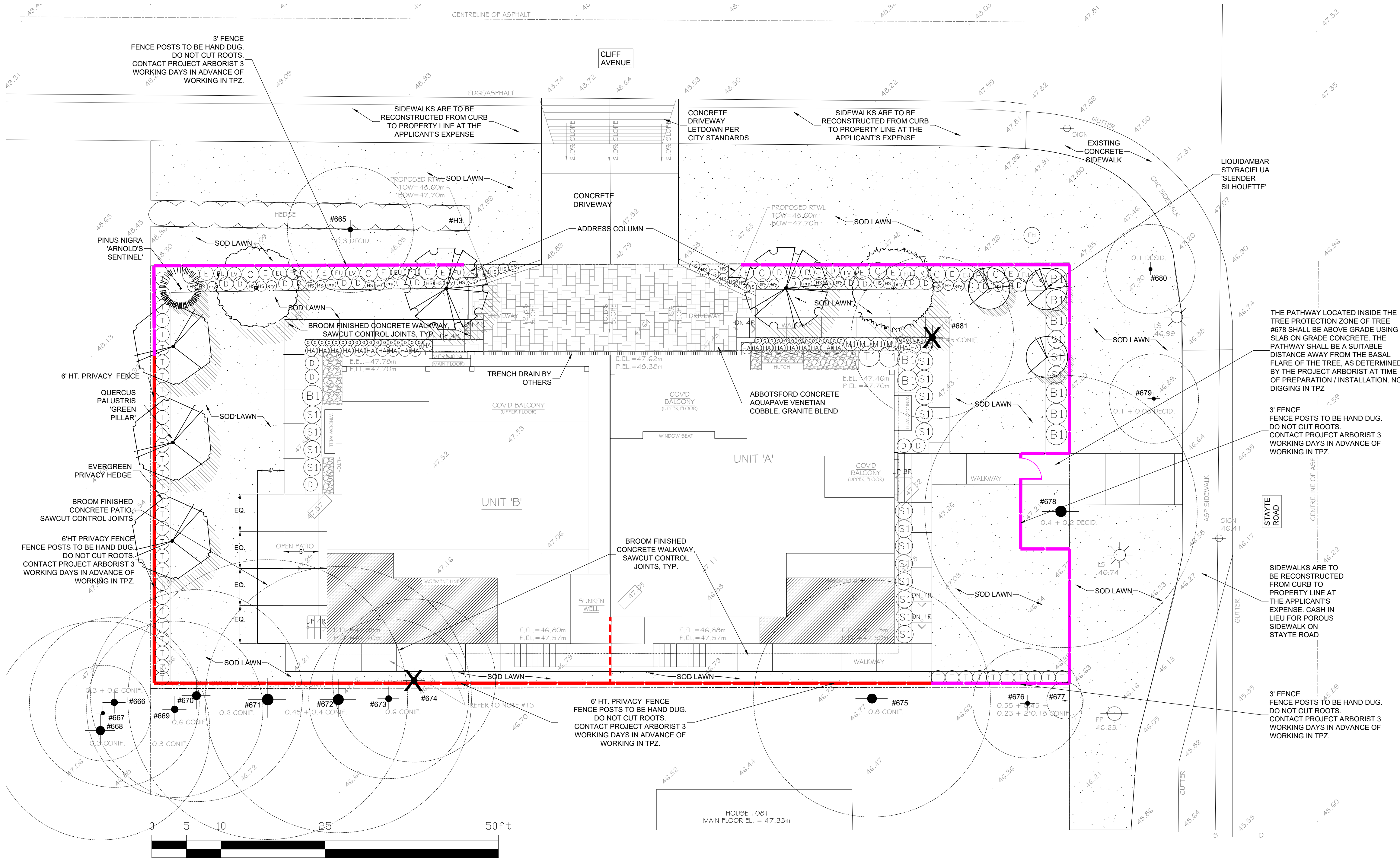
L1

OF 3

20008-7.ZIP

PMG PROJECT NUMBER:

20-008



GINKGO BILOBA 'PRINCETON SENTRY'



QUERCUS PALUSTRIS 'GREEN PILLAR'



CORNUS KOUSA 'SATOMI'

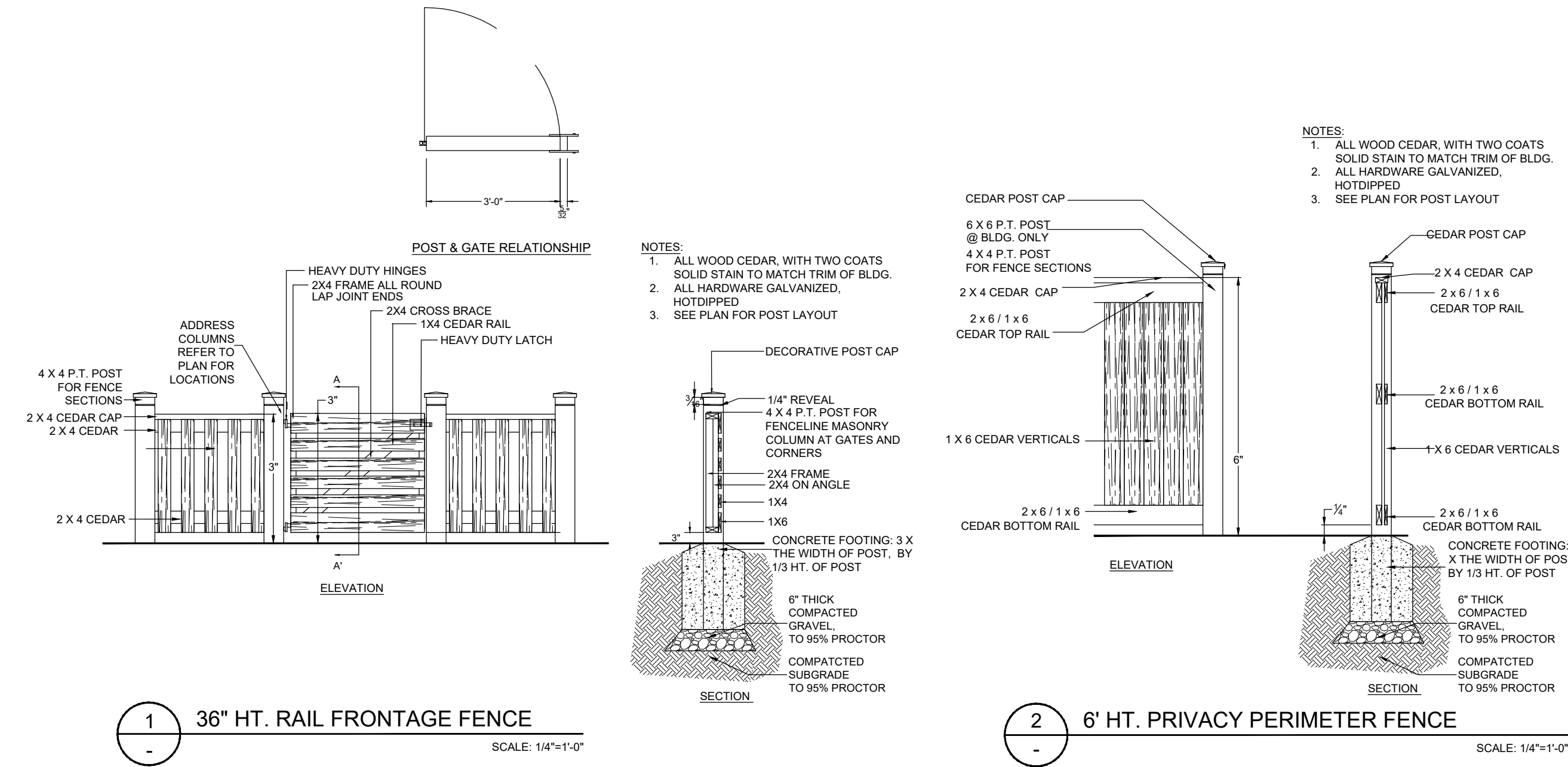
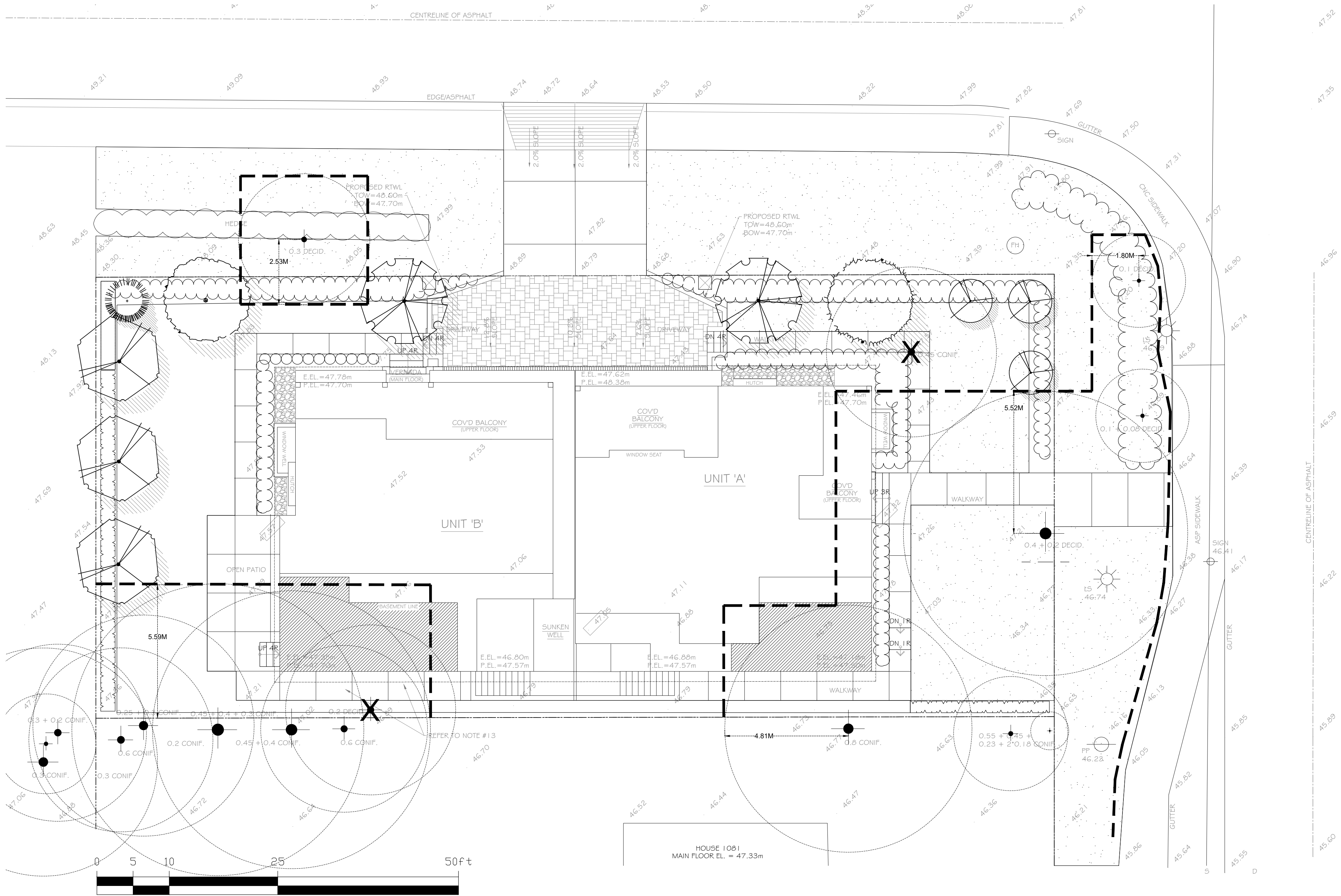
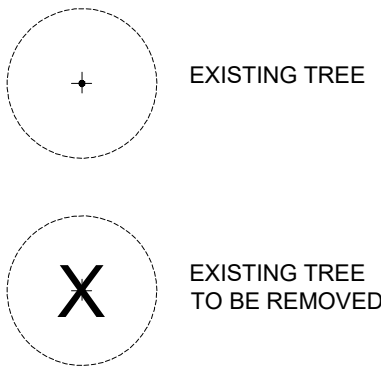


PINUS NIGRA 'ARNOLDS SENTINEL'



MAGNOLIA GRANDIFLORA 'EDITH BOGUE'

PLANT SCHEDULE				PMG PROJECT NUMBER: 20-008
KEY	QTY	BOTANICAL NAME	COMMON NAME	PLANTED SIZE / REMARKS
	1	CORNUS KOUSA 'SATOMI'	PINK KOUSSA DOGWOOD	6.0CM CAL, B&B
	1	LIQUIDAMBAR STYRACIFLUA 'SLENDER SILHOUETTE'	SLENDER LIQUIDAMBAR SWEETGUM	6CM CAL, 2M STD, B&B
	1	MAGNOLIA GRANDIFLORA 'EDITH BOGUE'	'EDITH BOGUE' SOUTHERN MAGNOLIA	6.0CM CAL, B&B
	1	PINUS NIGRA 'ARNOLD SENTINEL'	ARNOLD SENTINEL AUSTRIAN BLACK PINE	3.0M HT, B&B
	1	QUERCUS PALUSTRIS 'GREEN PILLAR'	GREEN PILLAR PIN OAK	6CM CAL, 1.8M STD, B&B
	1	STYRAX JAPONICUS 'SNOWCONE'	SNOWCONE JAPANESE SNOWBELL	6.0CM CAL, 1.5M STD, B&B
	1			
	1			
	9	BERBERIS THUNBERGII 'ROSE GLOW'	ROSE PURPLE FOLIAGE BARBERRY	#3 POT, 40CM
	8	EUONYMUS JAPONICUS 'GREEN SPIRE'	GREEN SPIRE EUONYMUS	#3 POT, 50CM
	4	MAHONIA AQUIFOLIUM	OREGON GRAPE HOLLY	#3 POT, 50CM
	20	SPIRAEA JAPONICA 'GOLDMOUND'	DWARF GOLDMOUND SPIREA	#3 POT, 40CM
	2	TAXUS BACCATA 'FASTIGIATA'	COLUMNAR IRISH YEW	1.0M B&B
	40	THUJA OCCIDENTALIS 'SMARAGD'	EMERALD GREEN CEDAR	1.5M HT, B&B
	10	CALAMAGROSTIS ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#1 POT
	25	DESCHAMPSIA CESPIIOSA 'GOLDTUL'	GOLD DEW TUFTED HAIR GRASS	#1 POT
	32	HELIOTRICHON SEMPERVIRENS	GOLDEN JAPANESE FOREST GRASS	#1 POT
	35	OPIOPOGON PLANISCAPUS 'NIGRESCENS'	BLACK MONDO GRASS	#1 POT
	10	ECHINACEA PURPUREA 'WHITE SWAN'	WHITE CONEFLOWER	15CM POT
	11	ERYNGIUM 'SAPPHIRE BLUE'	SAPPHIRE BLUE SEA HOLLY	#1 POT
	5	LAVANDULA STOECHAS 'OTTO QUAST'	SPANISH LAVENDER, PURPLE	#1 POT
	3	PEROVSKIA ATROPURPUREA 'LITTLE SPIRE'	DWARF RUSSIAN SAGE	15CM POT
	10			
	10			
NOTES: * PLANT SIZES IN THIS LIST ARE SPECIFIED ACCORDING TO THE BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. CONTAINER SIZES SPECIFIED AS PER ONLY STANDARD. BOTH PLANT SIZE AND CONTAINER SIZE ARE THE MINIMUM ACCEPTABLE SIZES. * REFER TO SPECIFICATIONS FOR DEFINED CONTAINER MEASUREMENTS AND OTHER PLANT MATERIAL REQUIREMENTS. * SEARCH AND REVIEW: MAKE PLANT MATERIAL AVAILABLE FOR OPTIONAL REVIEW BY LANDSCAPE ARCHITECT AT SOURCE OF SUPPLY. AREA OF SEARCH TO INCLUDE LOWER MAINLAND AND FRASER VALLEY. * SUBSTITUTIONS: OBTAIN WRITTEN APPROVAL FROM THE LANDSCAPE ARCHITECT PRIOR TO MAKING ANY SUBSTITUTIONS TO THE SPECIFIED MATERIAL. UNAPPROVED SUBSTITUTIONS WILL BE REJECTED. ALLOW A MINIMUM OF FIVE DAYS PRIOR TO DELIVERY FOR REQUEST TO SUBSTITUTE. SUBSTITUTIONS ARE SUBJECT TO BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD - DEFINITION OF CONDITIONS OF AVAILABILITY. * ALL LANDSCAPE MATERIAL AND WORKMANSHIP MUST MEET OR EXCEED BC LANDSCAPE STANDARD AND CANADIAN LANDSCAPE STANDARD, LATEST EDITION. * ALL PLANT MATERIAL MUST BE PROVIDED FROM CERTIFIED DISEASE FREE NURSERY. * BIO-SOLIDS NOT PERMITTED IN GROWING MEDIUM UNLESS AUTHORIZED BY LANDSCAPE ARCHITECT.				
REQUIRED REPLACEMENT TREES: 26 (PER ARBORIST REPORT)				
PROPOSED REPLACEMENT TREES: 12				

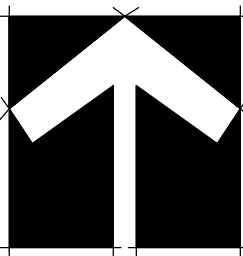


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Suite C100 - 4185 Still Creek Drive
Burnaby, British Columbia, V5C 6G9
p: 604 294-0011 ; f: 604 294-0022

SEAL:



7	22.JUN.27	UPDATE PER CITY COMMENTS	CLG
6	22.APR.21	COMMENTS FROM CLIENT	WZ
5	22.APR.11	UPDATE PER COMMENTS	WZ
4	21.OCT.25	UPDATE PER ARBORIST PLAN	CLG
3	21.JUL.13	UPDATE PER NEW SITE PLAN	CLG
2	21.JAN.13	UPDATE PER NEW SITE PLAN	CLG
1	20.FEB.04	ISSUED FOR SUBMISSION	CLG

NO. DATE REVISION DESCRIPTION DR.

CLIENT:

PROJECT:

PROPOSED DUPLEX FOR

LOT 1 @ 1091 STAYTE ROAD
WHITE ROCK

DRAWING TITLE:

TREE MANAGEMENT
PLAN

DATE: 20.JAN.17 DRAWING NUMBER:
SCALE: 1:100
DRAWN: CLG
DESIGN: CLG
CHK'D: PC

L2

OF 3

11 REFERENCES

1. CCDC Doc 2 008
Comply with all articles in the General Conditions of Contract in conjunction with this section unless superseded by other Contract Documents.

2. Canadian Landscape Standard, latest edition, prepared by the Canadian Society of Landscape Architects and the Canadian Landscape & Nursery Association, jointly. All work and materials shall meet standards as set out in the Canadian Landscape Standard unless superseded by this specification or as directed by Landscape Architect with written instruction.

3. MASTER MUNICIPAL SPECIFICATIONS & STANDARD DETAILS, 2000 edition, prepared by the Consulting Engineers of British Columbia, Roadbuilders and Heavy Construction Association, and the Municipal Engineers Division

4. STANDARD FOR LANDSCAPE IRRIGATION SYSTEM, 2008 Prepared by the Irrigation Industry Association of British Columbia

5. MUNICIPAL BYLAWS AND ENGINEERING SPECIFICATIONS WHERE NOTED

12 TESTING

1. A current test no more than one month test for all growing medium to be used on this site is required. Provide and pay for testing by an independent testing facility pre-approved by the Landscape Architect. Deliver growing medium test results to Landscape Architect for review and approval prior to placement. Refer to Section 34.3 Growing Medium Testing for procedure.

2. Owner reserves the right to test or re-test materials. Contractor responsible to pay for testing if materials do not meet specification.

13 SUBMITTALS

1. Any alternate products differing from that contained in the contract documents must be pre-approved by the Landscape Architect.

2. Submittals to consist of product sample or manufacturer's product description.

14. SITE REVIEW

1. Under the terms of the Landscape Architect's Contract with the Owner and where the Landscape Architect is the designated reviewer, the Landscape Architect will observe construction as is necessary in their opinion to conform to the plans and specifications. Contact Owner Representative to arrange for site observation at the appropriate times. Allow two days notice. Observation schedule may include but will not be limited to the following:
11. Start up Site Meeting, General Contract: Prior to any site disturbance, a meeting with the general contractor to review tree preservation issues, general landscape issues and municipal requirements.
12. Start Up Site Meeting, Landscape Contract (if separate): At the start of work with Owner's Representative, Site Superintendent and Landscape Contractor, a meeting to be held to review expected work and to verify the acceptability of the subgrade and general site conditions to the Landscape Contractor. Provide growing medium test results for this meeting.
13. Progress Site Visits: To observe materials and workmanship as necessary through the course of the work. Review of different aspects of the work may be dealt with on any single visit. Such elements may include Site Layout, Rough Grading, Growing Medium - quality, depths, finish grading, Drainage and Drainage Materials, Lawns or Grass areas, Planting, plant material including negotiations with suppliers, nursery inspections, plant sizes, quality, quantity, planting practice and layout, Tree support, Mulch, Irrigation Systems, Play Equipment, Site Furniture, and other elements of the site development where the Landscape Architect is the designated reviewer such as Pedestrian Paving, Fencing, Non-structural walls and slabs, Unit Paving.
14. Substantial Performance Review of all work, accounting of all substitutions, deletions, plant counts, preparations of deficiency list and recommendations for completion.
15. Certificate of Completion: Upon the declaration of Substantial Performance, a recommendation for the issuance of the Certificate of Completion will be made to the Payment Certifier as defined in the contract.
16. Deficiency Review: Prior to the completion of the holdback period, check for completion of deficiencies. Once completed, a Schedule "C" will be issued where required.
17. Warranty Review: Prior to the completion of the warranty period (-/-) 11 months after issuance of the Certificate of Completion, review all warranty material and report recommendations for warranty replacement.

15. WORKMANSHIP

1. Unless otherwise instructed in the Contract Documents, the preparation of the subgrade shall be the responsibility of the General Contractor. Placement of growing medium constitutes acceptance of the subgrade by the Landscape Contractor. Any subsequent corrections to the subgrade required are the responsibility of the Landscape Contractor.

2. All work and superintendence shall be performed by personnel skilled in landscape contracting. In addition, all personnel applying herbicides and/or pesticides shall hold a current license issued by the appropriate authorities.

3. A site visit is required to become familiar with site conditions before bidding and before start of work.

4. Confirm location of all services before proceeding with any work.

5. Notify Landscape Architect of any discrepancies. Obtain approval from Landscape Architect prior to deviating from the plans.

6. Take appropriate measures to avoid environmental damage. Do not dump any waste materials into water bodies. Conform with all federal, provincial and local statutes and guidelines.

7. Collect and dispose of all debris and/or excess material from landscape operations. Keep paved surfaces clean and repair damage resulting from landscape work. Repairs are to be completed prior to final acceptance.

8. Where new work connects with existing, and where existing work is altered, make good to match existing undisturbed condition.

16. WARRANTIES

1. Guarantee all materials and workmanship for a minimum period of one full year from the date of Certificate of Completion.

2. Refer to individual sections for specific warranties.

PART TWO SCOPE OF WORK

21. SCOPE OF WORK

1. Other conditions of Contract may apply. Confirm Scope of Work at time of tender.

2. Work includes supply of all related items and performing all operations necessary to complete the work in accordance with the drawings and specifications and generally consists of the following:
2.1. Retention of Existing Trees: where shown on drawings.
2.2. Finish Grading and Landscape Drainage.
2.3. Supply and placement of growing medium.
2.4. Testing of imported growing medium and/or site topsoil.
2.5. Supply and incorporation of additives to meet requirements of soil test and Table One.
2.6. Preparation of planting beds, supply of plant material and planting.
2.7. Preparation of rough grass areas, supply of materials and seeding.
2.8. Preparation of lawn areas, supply of materials and sodding.
2.9. Supply and placement of bark mulch.
2.10. Maintenance of planted and seeded/sodded areas until accepted by Owner.
2.11. SEPARATE PRICE: Establishment Maintenance, Section 311.
2.12. Other work: Work other than this list, not specified by Landscape Architect

22. MATERIALS

1. Growing Medium: Conform to Canadian Landscape Standard for definitions of imported and on-site topsoil. Refer to Table One below.

TABLE ONE - PROPERTIES OF GROWING MEDIUM FOR LEVEL 1 GROWNED AND LEVEL 2 MODERATE AREAS Canadian System of Soil Classification Textural Class "Loamy Sand" to "Sandy Loam".			
Applications	Low Traffic Areas Trees and Large Shrubs	High Traffic Lawn Areas	Planting Areas and Planters
Growing Medium Types	ZL	ZH	ZP
Texture	Percent Of Dry Weight of Total Growing Medium		
Coarse Gravel: larger than 25mm	0 - 1%	0 - 1%	0 - 1%
All Gravel: larger than 2mm	0 - 5%	0 - 5%	0 - 5%
	Percent Of Dry Weight of Growing Medium Excluding Gravel		
Sand: larger than 0.05mm smaller than 2.0mm	50 - 80%	70 - 90%	40 - 80%
Silt: larger than 0.002mm smaller than 0.0075mm	10 - 25%	0 - 15%	10 - 25%
Clay: smaller than 0.002mm	0 - 25%	0 - 15%	0 - 25%
Clay and Silt Combined	maximum 35%	maximum 15%	maximum 35%
Organic Content (coast)	3 - 10%	3 - 5%	10 - 20%
Organic Content (interior)	3 - 5%	3 - 5%	15 - 20%
Acidity (pH)	6.0 - 7.0	6.0 - 7.0	4.5 - 6.5
Drainage	Percolation shall be such that no standing water is visible 60 minutes after at least 10 minutes of moderate to heavy rain or irrigation.		

2. Fertilizer: An organic and/or inorganic compound containing Nitrogen (N), Phosphate (P), and Potash (Soluble 2) in proportions required by soil test.

3. Lime: Ground agricultural limestone. Meet requirements of the Canadian Landscape Standard.

4. Organic Additive: Commercial compost product to the requirements of the Canadian Landscape Standard, latest edition and pre-approved by the Landscape Architect. Recommended suppliers: The Answer Garden Products, Fraser Richmond Soils & Fibre, Stream Organics Management.

5. Sand: Clean, washed pump sand to meet requirements of the Canadian Landscape Standard.

6. Composted Bark Mulch: 10mm (3/8") minus Fir/Hemlock bark chips and fines, free of chunks and sticks, dark brown in colour and free of all soil, stones, roots or other extraneous matter. Fresh orange in colour bark will be rejected.

7. Herbicides and Pesticides: If used, must conform to all federal, provincial and local statutes. Applies must hold current licences issued by the appropriate authorities in the area.

8. Filter Fabric: A non biodegradable blanket or other filtering membrane that will allow the passage of water but not fine soil particles. [Such as MRAFI 140 N, GEOLON 140 OR ARMOCO 4545 or alternate product pre-approved by the Landscape Architect.]

9. Drainage Piping if required: Schedule 40 PVC nominal sizes.

10. Drain Rock: Clean, round, inert, durable, and have a maximum size of 19mm and containing no material smaller than 10mm.

11. Plant Material: To the requirements of the Canadian Landscape Standard. Refer to 39, Plants and Planting. All plant material must be provided from a certified disease free nursery. Provide proof of certification.

12. Sod: Refer to individual sections in this specification.

13. Supplier and installers of segmental block walls to provide engineered drawings for all walls; signed and sealed drawings for all walls, individually, in excess of 12m, or combinations of walls collectively in excess of 12m. Installations must be reviewed and signed off by Certified Professional Engineer; include cost of engineering services in Tender price.

14. Miscellaneous: Any other material necessary to complete the project as shown on the drawings and described herein.

PART THREE SOFT LANDSCAPE DEVELOPMENT			
31. RETENTION OF EXISTING TREES			
1. Prior to any work on site - protect individual trees or plant groupings indicated as retained on landscape plans as vegetation retention areas.			
2. In any instance the Landscape Architect will tag trees or areas to remain. Discuss tree retention areas at a start-up meeting with the Landscape Architect.			
3. A physical barrier must be installed to delineate clearing boundaries. Refer to physical barrier detail. If detail not provided, comply with local municipal requirements.			
3. No machine travel through or within vegetation retention areas or under crowns of trees, to be retained is allowed.			
4. Do not stockpile soil, construction materials, or excavated materials within vegetation retention areas.			
5. Do not park, fuel or service vehicles within vegetation retention areas.			
6. No debris fires, clearing fires or trash burning shall be permitted within vegetation retention areas.			
7. No excavations, drain or service trenches nor any other disruption shall be permitted within vegetation retention areas without a review of the proposed encroachment by the Landscape Architect.			
8. Do not cut branches or roots of retained trees without the approval of the Landscape Architect.			
9. No damage to existing vegetation intended for preservation will be subject to evaluation by an I.S.A. Certified Arborist using the "Guide for Plant Appraisal", Eighth Edition, 1992.			
9.1 Replacement planting of equivalent value to the disturbance will be required. The cost of the evaluation and of the replacement planting will be the responsibility of the General Contractor and/or the person(s) responsible for the disturbance.			
10. In municipalities with specific tree retention/replacement bylaws ensure compliance to bylaws.			
11. In situations where required construction may disturb existing vegetation intended for preservation, contact Landscape Architect for review prior to commencing construction.			
32. GRADES			
1. Ensure subgrade is prepared to conform to depths specified in Section 35, Growing Medium Supply, below. Where planting is indicated close to existing trees, prepare suitable planting pockets for material indicated on the planting plan. Shape subgrade to eliminate free standing water and conform to the site grading and drainage plan.			
2. On slopes in excess of 3:1 trench subgrade across slope to 150mm (6") minimum at 15m (5 ft.) intervals minimum.			
3. Scarify the entire subgrade immediately prior to placing growing medium. Re-cultivate where vehicular traffic results in compaction during the construction procedures. Ensure that all planting areas are smoothly contoured after light compaction to finished grades.			
4. Eliminate standing water from all finished grades. Provide a smooth, firm and even grade and conform to grades shown on the Landscape Drawings. Do not exceed maximum and minimum grades defined by the Canadian Landscape Standard.			
5. Construct swales true to line and grade, smooth and free of sags or high points. Minimum slope 2%, maximum side slopes 10%. Assume positive drainage to collection points.			
6. Slope not to exceed the following maximums: Rough Grass 3:1, Lawn 4:1, Landscape plantings 2:1.			
7. Finished soil/mulch elevation at building to comply with municipal requirements.			
8. Inform Landscape Architect of completion of finish grade prior to placement of seed, sod, plants or mulch.			
33. LANDSCAPE DRAINAGE			
1. Related Work: Growing medium and Finish Grading, Grass areas, Trees Shrubs and Groundcovers, Planters, Crib Walls.			
2. Work Included: Site finish grading and surface drainage. Installation of any drainage systems detailed on landscape plans. Note: Catch basins shown on landscape plans for coordination only, confirm scope of work prior to bid.			
2.1 Coordinate all landscape drainage work with test of site drainage. Refer to engineering drawings and specifications for connections and other drainage work.			
2.2 Determine exact location of all existing utilities and structures and underground utilities prior to commencing work, which may not be located on drawings and conduct work so as to prevent interruption of service or other delay. Protect existing structures and utility services and be responsible for damage caused.			
2.3 Planter drains on slab: Refer to Section 3.10, Installing Landscape Structures.			
3. Execution			
3.1 Do trenching and backfilling in accordance with engineering details and specifications.			
3.2 Lay drains on prepared bed, true to line and grade with invert's smooth and free of sags or high points. Ensure barrel of each pipe is in contact with bed throughout full length.			
3.3 Commence laying pipe at outlet and proceed in upstream direction.			
3.4 Lay perforated pipes with perforations at 90 and 180 degree points.			
3.5 Make joints tight in accordance with manufacturer's directions.			
3.6 Do not allow water to flow through the pipes during construction except as approved by Engineer.			
3.7 Make watertight connections to existing drains, new or existing manholes or catchbasins where indicated or as directed by Landscape Architect.			
3.8 Plug upstream ends of pipe with watertight clean out caps.			
3.9 Surround and cover pipe with drain rock in uniform 150mm layers to various depths as shown in details, minimum 100mm.			
3.10 Cover drain rock with non-woven filter cloth lap all edges and seams minimum 150mm.			
3.11 Assure positive drainage.			
3.12 Back fill remainder of trench as indicated.			
3.13 Protect subdrains from flotation during installation.			
34. GROWING MEDIUM TESTING			
1. Submit representative sample of growing medium proposed for use on this project to an independent laboratory. Provide test results to Landscape Architect prior to placing. Test results to include:			
11. Physical properties: % content of gravel, sand, silt, clay and organics.			
12. Acidity pH and quantities of lime or sulphur required to bring within specified range.			
13. Nutrient levels of principle and trace elements and recommendations for required soil amendments.			
14. Carbon/Nitrogen level.			
35. GROWING MEDIUM SUPPLY AND PLACEMENT			
1. Supply all growing medium admixtures for the performance of the Contract. Do not load, transport or spread growing medium when it is so wet that its structure is likely to be damaged.			
2. Supply all growing medium admixtures as required by the soil test. Amended growing medium must meet the specification for growing medium as defined in Table One for the various areas.			
2.1 Thoroughly mix required amendments into the full depth of the growing medium.			
2.2 Special mixes may be required for various situations. Refer to drawing notes for instructions.			
3. Place the amended growing medium in all grass and planting areas. Spread growing medium in uniform layers not exceeding 6" (150mm), over unfrozen subgrade free of standing water.			
4. Minimum depths of growing medium placed and compacted to 80%:			
4.1 On-grade			
4.1.1 Seeded and sodded lawn			6" (150mm)
4.1.2 Mass planted shrubs & groundcovers			18" (450mm)
4.1.3 Groundcover only areas, if defined on plan			9" (225mm)
4.1.4 Tree & large shrub pits			depth to conform to depth of rootball - width shall be at least twice the width of the root ball with saucer shaped sides.
4.2 On-Slab			
4.2.1 Irrigated lawn			9" (220mm)
4.2.2 Groundcover areas			12" (300mm)
4.2.3 Lawn without automatic irrigation			12" (300mm)
4.2.4 Shrub & groundcover areas			18" (450mm)
4.2.5 Trees and specimen shrubs			30" (760mm) over columns and/or edge of slab (verify column locations on-site for tree locations.)
4.2.6 Depth noted includes 1" to 2" (25-50mm) sand over filter fabric.			
4.2.7 Maximum 18" depth growing medium except where rounded for trees over column points.			
5. Manually spread growing medium/planting soil around existing trees, shrubs and obstacles.			
6. In perimeter seeded grass areas, feather growing medium out to nothing at edges and blend into existing grades.			
7. Finished grades shall conform to the elevations shown on landscape and site plans.			
36. ROUGH GRASS AREA - SEEDING			
1. General: Rough grass areas are noted on the drawings as "Rough Grass". Treat all areas defined as rough grass between all property lines of the project including all boulevards to edge of roads and lanes.			
2. Preparation of Surfaces: To Canadian Landscape Standard (Class 3 Grass (Rough grass) Section 11.13			
2.1 Clean existing soil by mechanical means of debris over 50mm in any dimension.			
2.2 Roughly grade surfaces to allow for maintenance seeding and/or positive drainage.			
3. Time of Seeding: Seed from early spring (generally April 1st) to late fall (September 15th) of each year. Further extensions may be obtained on concurrence of the Landscape Architect.			
4. Seed Supply & Testing: All seed must be obtained from a recognized seed supplier and shall be No. 1 grass mixture delivered in containers bearing the following information:			
4.1 Analysis of the seed mixture			
4.2 Percentage of each seed type			
5. Seed Mixture: All varieties shall be rated as strong performers in the Pacific Northwest and are subject to client approval.			
70% Creeping Red Fescue 20% Annual Ryegrass 5% Saturn Perennial Ryegrass 5% Kentucky Bluegrass			
For Wildflower Areas use a mixture of Wildflowers with Hard Fescues (Terraclim Coastal Wildflowers) with Hard Fescue or pre-approved alternate.			
6. Fertilizer: Mechanical seeding: Apply a complete synthetic slow-release fertilizer with maximum 35% water soluble nitrogen and a formulation ratio of 18-18-18 - 50% sulphur area coated, 112 kg/ha(1000lbs/acre) using a mechanical spreader.			
7. Seeding: Apply seed at a rate of 1120/Lb (1000lbs /acre) with a mechanical spreader. Incorporate seed into the top 1/4" (6mm) of soil and lightly compact.			
8. Acceptance: Provide adequate protection of the seeded areas until conditions of acceptance have been met. Comply with Section 3.7 Hydroseeding.			
37. HYDROSEEDING			
1. May be used as an alternate to mechanical seeding in rough grass areas.			
2. May not be used in areas of lawn unless pre-approved by the Landscape Architect prior to bidding.			
3. Preparation and Growing Medium			
3.1 In areas of Rough Grass: Comply with Section 36, Rough Grass.			
3.2 Where approved for use in areas of lawn, comply with Section 3.8 Lawn Areas: Sodding.			
4. Protection: Ensure that fertilizer in solution does not come in contact with the foliage of any trees, shrubs, or other susceptible vegetation. Do not spray seed or mulch on objects not expected to grow grass. Protect existing site equipment, roadways, landscaping, reference points, monuments, markers and structures from damage. Where contamination occurs, remove seeding slurry to satisfaction of and by means approved by the Landscape Architect.			
5. Mulch shall consist of virgin wood fibre or recycled paper fibre designed for hydrolic seeding and dyed to ease of monitoring application. If using recycled paper material for wood fibre substitute use 100% by weight) Conform to Canadian Landscape Standard for mulch requirements.			
6. Water: Shall be free of any impurities that may have an injurious effect on the success of seeding and may be harmful to the environment.			
7. Equipment: Use industry standard hydrolic seeder/mulcher equipment with the tank volume certified by an identification plate or sticker affixed in plain view on the equipment. The hydrolic seeder/mulcher shall be capable of sufficient agitation to mix the material into a homogeneous slurry and to maintain the slurry in a homogeneous state until it is applied. The discharge pumps and gun nozzles shall be capable of applying the materials uniformly over the designated area.			

PART THREE SOFT LANDSCAPE DEVELOPMENT - CONT			
8. Application Rate			
8.1 Seed Mixture: 136 kg/ha (125 lbs/acre)			
8.2 Fertilizer: 112 kg/ha (100 lbs/acre)			
8.3 Coastal Wildflower: Where specified, apply (310 lbs/acre) (114 lb./1.0 of grass seed)			
8.4 Notes			
8.4.1 At the time of Tender provide a complete chart of all components of the mix proposed including mulch, fertilizer, water etc. Sloped sites require fertilizer.			

City of White Rock – Planning & Development Services

Mature Neighbourhood Infill

Development Permit Area Guidelines



The objectives of the **Mature Neighbourhood Infill Development Permit Area** are to:

- Establish an attractive, comfortable, well-connected, pedestrian-oriented environment that fosters vibrant public life
- Ensure the compatibility of infill development (i.e: duplexes, triplexes, small-lot single family) within established neighbourhoods.
- Ensure the compatibility of new development with adjacent existing buildings
- Enhance quality of life
- Conserve energy, conserve water, and reduce GHGs
- Enhance the character of the built environment and public realm in the City of White Rock

<p>Please provide a summary of how your proposal achieves the objectives and policies of the Mature Neighbourhood Infill DPA below:</p>
<p>The proposed building is a simple, modern, and elegant addition to the neighborhood. As a duplex, it will be larger than the older buildings but similar in size to the modern buildings in the area. The height and placement of the building on the lot (subject to zoning requirements) will ensure that it will not dominate, but rather fit in with the existing neighborhood. The modern style combined with natural grey colouring will blend in with both the newer and older characteristics of the surrounding area.</p> <p>Special care has been taken to improve the landscaping, maximizing the area of softscaping. Plenty of trees, shrubs, and bushes will enhance the attractiveness of the exterior and increase quality of life. Tree cover will be increased with the net addition of 5 new trees.</p> <p>Pedestrian access will be improved by adding a new sidewalk on the North side of the property, increasing the connectedness of the neighborhood and improving pedestrian access.</p> <p>Appliances and fixtures will be of the highest quality, with attention to conservation features to ensure reduction of energy and water usage.</p>
<p>NOTE 1: All ‘Applicant Response’ sections must be filled out by the applicant.</p> <p>NOTE 2: If your proposal cannot adequately address one of the below-listed DPA guidelines, provide a rationale (and alternative resolution) above, and in the applicable response section.</p>

Section 22.9.1 - Buildings

Mature Neighbourhood Infill DPA Guideline 22.9.1 (a)

Ensure buildings are compatible with or complementary to adjacent developments in terms of height, density, and design.

Applicant Response

The neighborhood consists of a mix of new developments and older houses. The land slopes southward, resulting in the buildings being stepped as you go down the hillside. Architectural design varies: there are older single or two storey houses with stucco or wood siding and sloped roofs, newer three storey dwellings with vinyl siding, brickwork, stucco, and sloped roofs, and modern three storey developments with modern hardiboard siding, stonework, and flat roofs. There is also a two storey commercial building across the street with stucco siding and a parking lot.

The proposed building would fit in with the newer developments, while avoiding overpowering the smaller, older dwellings. The massing of the building is concentrated in the corner of the property, reducing its apparent size. Height is two stories with basement. This is the same as other modern developments and slightly taller than the oldest buildings in the neighborhood. The building would fit in with the slope of the land and continue the stepping of the buildings with the slope of the hill.

Mature Neighbourhood Infill DPA Guideline 22.9.1 (b)

Consider alternatives to the traditional side- by-side duplexes and triplexes, such as front/ rear and top/bottom layouts. ‘Mirror-image’ designs will not be permitted for single family dwellings, duplexes, or triplexes. Entrances shall be clearly identifiable, and weather protection with overhangs and awnings shall be provided over all entrances.

Applicant Response

The building has avoided a side by side approach, and is not mirrored. Unit A faces East and Unit B faces north. There are differing balconies, window placement, and stonework between the two units in order to provide further differentiation.

The two entrances are clearly visible, each with a small veranda. The verandas are fully covered to provide protection from the weather.

Mature Neighbourhood Infill DPA Guideline 22.9.1 (c)

Create visual interest with architectural details that break up the mass of the building and give each dwelling unit in a duplex or triplex its own visual identity. Open verandas and peaked roofs are encouraged for duplexes, triplexes, and small-lot single family development.

Applicant Response

The building has features that create visual interest and break up the mass of the building. Each unit has its own identity.

The building is a rectangular shape, and each unit has its own separated top floor. This top floor separation breaks the mass of the building from the point of view from the exterior. There are three covered decks on the top floor which provides further differentiation. Unit A has two decks, and unit B has one larger deck.

The arrangement of stonework, windows, and the front verandas provide further differentiation between the two units and makes it clear where one unit ends and the other begins.

Mature Neighbourhood Infill DPA Guideline 22.9.1 (d)

Use a variety cladding colours and/or materials to avoid large, uniform expanses. Different cladding colours or materials can be used to differentiate between units in a duplex or triplex.

Applicant Response

The proposed dwelling will have a modern exterior, and the colours and materials have been chosen and arranged in part to avoid large or uniform expanses.

Cladding materials include hardiboard, stonework, stucco, and wood trim. The placement of these materials, the windows, and the balconies serve to break up any large expanses.

A grey colour scheme has been chosen, with modern stainless steel hardware.

Mature Neighbourhood Infill

Development Permit Area Guidelines

Mature Neighbourhood Infill DPA Guideline 22.9.1 (e)

Follow passive solar design principles for the orientation and siting of buildings. Design roofs to maximize opportunities for solar collection in winter and control solar gain on south-facing facades by blocking high- angle sun in summer. Maximize passive ventilation and passive cooling through building orientation.

Applicant Response

The proposed duplex will be a rectangular building oriented east-west on the long side. Front entrances will be facing North and East. Lot size, tree retention, and other constraints limit the placement of the building on the lot.

Solar gain is minimized by setting some of the windows back in covered decks and under awnings.

Mature Neighbourhood Infill DPA Guideline 22.9.1 (f)

Incorporate west coast design elements with the use of natural materials, including brick, stone, concrete, exposed heavy timber, and/or steel. Vinyl siding and stucco will not be considered for cladding. Use rich natural tones which reflect the natural landscape and seascape as the dominant colours, with brighter colours used only as accents.

Applicant Response

The proposed building will have a modern exterior, incorporating some natural materials. Exterior materials include hardiboard, stone, stucco, and wood trim. Stainless steel hardware will be used, and railings will be glass and stainless steel.

The colour scheme will be grey, a natural tone.

Mature Neighbourhood Infill DPA Guideline 22.9.1 (g)	
Ensure that garages do not dominate the front face of a building. If a garage faces a street, it shall be subordinate to the pedestrian entrance in terms of size, prominence on the streetscape, location, and design emphasis. The use of landscaping to screen and soften the appearance of a garage is encouraged.	
Applicant Response	City requirements allow only one driveway onto Cliff Avenue, so garages must be side by side. Several steps have been taking to mitigate the massing of these garage doors. They are placed on the North side, the wide side of the building, so their appearance is minimized. Hedge, shrub, and tree placement is used to soften and screen the massing of the doors.

Section 22.9.2 – Public Realm and Landscape

Mature Neighbourhood Infill DPA Guideline 22.9.2 (a)

Improve the public realm with widened sidewalks (minimum 1.8 metres). Plant street trees and design curb let-downs to accommodate wheelchairs and scooters.

**Applicant
Response**

A sidewalk will be added to the North side of the property, where currently there is none. Curb let-downs will be added at driveway and sidewalk entrances.

Mature Neighbourhood Infill DPA Guideline 22.9.2 (b)

Site buildings to create through-block walking connections where appropriate. These will create opportunities for a variety of pedestrian-oriented activities and a finer- grained street grid.

**Applicant
Response**

Not applicable to proposed development.

Mature Neighbourhood Infill DPA Guideline 22.9.2 (c)

Use light coloured reflective paving materials such as white asphalt or concrete for paths and driveways to reduce heat absorption and urban heat island effect. Ensure all areas not covered by buildings, structures, and roads are landscaped. Incorporate shared pedestrian accesses where possible to minimize impervious areas.

Applicant Response

White concrete will be used for driveway and sidewalk.

All areas not utilized by the building or hardscaping will be landscaped and planted with resilient and native plantings. The plant specifications provided by our landscape architect require less fertilizer and water inputs than more tender plant selections.

Mature Neighbourhood Infill DPA Guideline 22.9.2 (d)

Ensure all trees are planted with sufficient soil volume, using soil cells where appropriate, and incorporate diverse native shrub layers below trees to intercept stormwater. Projects should be designed to allow for the retention of large, mature, healthy trees, and landscape design should employ CPTED principles.

Applicant Response

Our project arborist has identified trees for retention that are outside of the immediate disturbance zone caused by construction. These retained mature trees and hedgers are maintained by pushing construction-related excavation and planting disturbances outside of the critical root zones. Where possible, under-planting of proposed trees with shrubs will keep stormwater in close proximity and shade sensitive root structures. This work is proposed to be constructed under arborist supervision. CPTED principles are maintained by allowing clear sight lines and low evergreen planting that does not create darkened zones in the proposed yards.

Mature Neighbourhood Infill

Development Permit Area Guidelines

Mature Neighbourhood Infill DPA Guideline 22.9.2 (e)

Select trees that will maximize passive solar gain, natural ventilation, and natural cooling, and increase the entry of natural light into buildings. Maximize the use of drought tolerant species that can withstand the seaside setting and require minimal irrigation. Avoid planting invasive species. The planting of hedges directly adjacent to sidewalks is discouraged, unless they are screening a garbage/recycling area.

Applicant Response

None of the proposed plants are considered invasive by the BC Invasive Species Council. The west and south aspect of the site is shaded by adjacent retained trees or proposed large columnar oak trees. These will provide summertime shade, and are open enough to allow the moderate passage of cooling evening sea breezed. No sidewalk-adjacent hedging is proposed.

Mature Neighbourhood Infill DPA Guideline 22.9.2 (f)

Use Low Impact Development Techniques for stormwater management, where appropriate, in accordance with the City's Integrated Storm Water Management Plan (ISWMP). This includes but is not limited to bio-swales, cisterns, and permeable paving. Narrower lanes/access roads and the use of porous asphalt are encouraged.

Applicant Response

Paving is kept to a minimum in the proposed landscape to allow maximum permeability.

Section 22.9.3 – Parking and Functional Elements

Mature Neighbourhood Infill DPA Guideline 22.9.3 (a)

Minimize paved areas with narrow, shared vehicular accesses. Separate accesses are considered for duplexes or triplexes that are located on corner lots or that have street and lane accesses.

Applicant Response

The paved driveway proposed is the minimum sized possible while still being accessible. Separate access is not permitted as per City requirements.

Mature Neighbourhood Infill DPA Guideline 22.9.3 (b)

Provide sufficient space for garbage, recycling, and composting where appropriate. These areas are to be located so that they are convenient for users and accessible for waste/recycling/ compost collection and removal.

Applicant Response

Room for garbage and recycling is proposed adjacent to the driveways off Cliff Avenue. This will be screened by landscaping, trees, and fencing.

PRESENT: P. Byer, Chairperson
P. Rust
F. Kubacki
J. Vasto
R. Dhall

ABSENT: J. Muego

NON-VOTING MEMBERS: S. Greysen, BIA Representative

GUESTS: S. Gill, Applicant (877 Kent Street)
A. Kulla, Landscape Designer (877 Kent Street)
J. Ogden, Owner (1091 Stayte Road)
J. Kaur, Applicant (1091 Stayte Road)
R. Toora, Designer (1091 Stayte Road)

STAFF: J. Calenda, Interim Director, Planning & Development Services
N. Syam, Planner
J. Pelzman, Planning & Development Assistant II

1. CALL TO ORDER

The meeting was called to order at 3:32pm.

2. ADOPTION OF AGENDA

It was **MOVED** and **SECONDED**

THAT the Advisory Design Panel adopts the March 15, 2022, agenda as circulated.

CARRIED

3. ADOPTION OF MINUTES

It was **MOVED** and **SECONDED**

THAT the Advisory Design Panel adopts the minutes from the February 15, 2022, meeting as circulated.

CARRIED

4. APPLICATION SUBMISSIONS TO THE ADVISORY DESIGN PANEL

At the beginning of this section of the agenda, J. Calenda (Interim Director, Planning & Development Services) provided an overview of the policy and regulatory framework applicable to the applications under review by the ADP. The following subsection outlines the minutes of the meeting as they relate to the applications.

4.1. Application: 877 Kent Street – Proposed Intensive Residential Development – S. Gill (Applicant)

The Applicant provided an overview of the updates to their proposed intensive residential development since the previous ADP review on October 19, 2021.

- A member of the Panel requested clarification on the garage door for the southern house and asked if it was clear glass. The Applicant responded by saying that it was glass but reflective with privacy panels. The Panel recommended using a different material.
- A member of the Panel commented about the landscaping plan only showing small shrubs and columnar trees at the front of the properties and recommended planting some larger shrubs and tree varieties instead.
- A member of the Panel recommended dropping the ceiling height of the front entrances to match the first storey roof line along the sides of the houses.
- A member of the Panel appreciated the change to two single-family homes, which fits in with the character of the street and is less massing for the neighbourhood.
- A member of the Panel noted that the design of the north house could be improved. They recommended implementing larger windows and using some shared colours and elements between the two houses.
- A member of the Panel noted that there are some inconsistencies in the drawings because the landscaping plan appears to show gravel between the houses and along the outer edges of the properties which is not consistent with the site plans. The Applicant acknowledged the inconsistencies and confirmed that they will work with City staff to make sure the landscaping plan is consistent with other plans.

Following the receipt of final comments, the Chairperson asked for a motion.

It was **MOVED** by R. Dhall and **SECONDED** by P. Rust

THAT the Advisory Design Panel supports the application for the development proposal at 877 Kent Street proceeding to Council, subject to the following considerations made to the satisfaction of Staff:

- Revise the design of the garage door on the south house to a different material.
- Recommendation to use larger shrubs and tree varieties at the front of the properties.
- Recommendation to lower the ceiling height of the front entrances to match the first storey roof line along the sides of the houses.
- Revise the design of the north house to have larger windows and some shared colours and elements between the two houses.
- Request for more consistency between the landscaping plan and the other site plans.

CARRIED

4.2. Application: 1091 Stayte Road – Proposed Duplex – J. Ogden (Owner)

The Owner provided an overview of their proposed duplex development.

- A member of the Panel asked the Owner why there was a gap between the units on the second level. The Owner responded that it was an attempt to reduce the mirroring effect of the two units. The Panel noted that there may be some heat loss from having two exterior walls in the design.
- A member of the Panel recommended using a different variety of tree and plant species along the property line. It was suggested that utilizing different tree and plant species may also reduce the need for a large fence.
- A member of the Panel noted that there was a lack of windows on the rear elevation. They recommended using some colours and elements from the front elevation on the rear. They also recommended using vertical windows instead of horizontal windows.
- A member of the Panel had concerns with some mature off-site trees from the adjacent property impacting the building on the first and second floors. The Owner said that there may need to be some trimming done to prevent this. The Panel asked if the Arborist and the neighbour came to an agreement to allow the trees to be trimmed. The Owner responded by saying he will have to discuss this with the Arborist.
- A member of the Panel noted that one of the large off-site trees may not survive the construction that occurs in the root zone area. The Panel also noted that the City's new Tree Protection Bylaw may require revisions to the Arborist Report that was submitted with the application.
- A member of the Panel asked about the location of the garbage/recycling bins. City Staff responded by saying that the Applicant proposed an enclosed structure for the bins, but this may impact the setback requirements and is still under discussion.
- A member of the Panel recommended that the proposal include roughed-in electric vehicle charging infrastructure.

Following the receipt of final comments, the Chairperson asked for a motion.

It was **MOVED** by R. Dhall and **SECONDED** by F. Kubacki.

THAT the Advisory Design Panel supports the application for the development proposal at 1091 Stayte Road proceeding to Council, subject to the following considerations made to the satisfaction of Staff:

- Recommendation to use a different variety of tree and plant species along the property line to reduce the need for a large fence.
- Recommendation to use colours and elements from the front elevation on the rear elevation.
- Revise the design to replace the horizontal windows with larger vertical windows.
- Recommendation to include roughed-in electric vehicle charging infrastructure in the design.

CARRIED

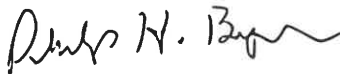
A member of the Panel requested an additional motion to update the unit addresses to align with the streets they are facing.

It was **MOVED** by P. Byer and **SECONDED** by P. Rust.

CARRIED

5. CONCLUSION OF THE MEETING

There being no further business, the Chairperson declared the meeting concluded at 5:29 pm.



P. Byer
Chairperson, Advisory Design Panel



J. Calenda
Interim Director, Planning & Development
Services, City of White Rock



Arborist Report for Development

1091 Stayte Road

White Rock, BC V4B 4Y8

April 28, 2022 Updated

Submitted to:

City of White Rock

Planning & Development Services Department

Submitted by:

Freedom Sukenick

ISA Certified Arborist PN 7712A

White Rock Business License #23377

Client:

Mirus Enterprises Inc.

Jamie Walgren

Jamie.walgren@gmail.com

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If there are any questions or concerns with the contents of this report, please do not hesitate to contact us.

Contact information

Phone: 604-306-6942

Email: info@freedomtreecare.com

Website: www.freedomtreecaretrimmingservice.com

ASSIGNMENT / INTRODUCTION

I was asked to assess all trees located onsite, and all City & offsite trees within 4 meters of the property line. The purpose of my consultation is to determine the viability of the trees and the potential impact in relation to the construction of a new house and upgrading of all services.

I conducted my assessment on September 19, 2019.

March 27, 2022 - I was asked to update the Report to provide recommendations on how to care for the retained trees including a maintenance and watering program. The site plan has also been updated showing new walkway and screw pile locations and a reduced excavation zone on the south side of the proposed building.

NEW April 26, 2022 - Pathway near tree #678 has been modified. Additional comments provided in the Summary and Recommendations on page 8.

METHODOLOGY

- **A level 2 Qualitative approach** with a mallet and probe were used on accessible items without dissection, excavation, climbing or coring.
- All trees were inspected using a ground based visual examination.
- Photos have been included to help with tree identification.
- Tree Survey drawing #1 shows all trees.
- Tree Survey drawing #2 shows trees to be retained, Tree Protection Barriers and Replacement Tree locations.
- Trees were evaluated for their preservation potential based on health, structure, location, biotic, abiotic, pathogenic, decay and species factors. Topping cuts and codominant stems are considered structural defects and under most circumstances are considered structurally poor.
- Trees found to be unsafe, conflicting with the proposed building plans, of poor health, of little long term retentive value or having been requested by the owner are recommended for removal in Table #1 and shown on Tree Survey Drawing #1 with an X.
- The maximum amount of encroachment from excavation is generally around 30%. Further encroachment may de-stabilize the tree and is not recommended.
- A 1.5 meter excavation zone is calculated and used around the new house structure to show any potential conflicts with proposed construction.

OBSERVATIONS

Tree Resource

The tree resource in this report is made up of 17 trees. 10 Western red cedar (*Thuja plicata*), 2 Paper bark maple (*Acer griseum*), and one each of the following; English oak (*Quercus robur*), Douglas fir (*Pseudotsuga menziesii*), Pacific dogwood (*Cornus nuttallii*), Mountain-ash (*Sorbus aucuparia*), and Spruce (*Picea engelmannii*). Three hedges have been found which are on city property. They are labeled H1, H2 and H3 on the tree survey drawings.

All trees have been tagged and located on the tree survey drawing.

Tree Inventory and Assessment - Table 1

The following Inventory Table provides individual tree data for all protected trees. Specific information includes:

- tree/tag number, offsite (OS), shared or City Tree (City, C), species
- diameter at breast height (DBH), approximate height, live crown ratio (LCR) %, deadwood %
- structural integrity - a qualitative rating of a tree's shape and structure when compared to ideal trees of the same species and age class (good, fair, poor, dead)
- health - the trees overall health and vigour (good, fair, poor, dead)
- **Good:** Trees in this category are in good health and structural stability and have the potential for longevity at this site.
- **Fair:** Trees in this category are in moderate health and/or have structural defects that may be mitigated with treatment. These trees may require more intense management and monitoring, and may have shorter life-spans than those in the "good" category.
- **Poor:** Trees in this category are in poor health or have structural defects that may not be mitigated with treatment. These trees can be expected to decline regardless of management.
- comments and recommendations
- Tree Protection Zones (TPZs)

TABLE 1 - Tree Inventory and Assessment

Tag #	Common Name	Botanical Name	DBH (m)	Height (m)	LCR (%)	Dead wood (%)	Structural Integrity	Health	Comments	Recommendations	TPZ 6X DBH (m)
665 City	English oak	<i>Quercus robur</i>	.42	13	90	5	Good	Good	Multi-stemmed at 2m.	Retain and Monitor	2.52
666 City	Western red cedar	<i>Thuja plicata</i>	.35, .22	13	85	10	Poor	Fair	Multi-stemmed at base & 4.5m Previously topped at 2.5m with dieback at the topping cuts	Retain and Monitor	3.42
667 City	Western red cedar	<i>Thuja plicata</i>	.33	13	85	10	Poor	Fair	Multi-stemmed at 3m 2m long damage to main stem Previously topped at 2.5m with dieback at the topping cuts	Retain and Monitor	1.98
668 City	Western red cedar	<i>Thuja plicata</i>	.2, .54	13	85	10	Poor	Fair	Multi-stemmed at base Previously topped at 2.5m with dieback at the topping cuts	Retain and Monitor	4.44
669 OS	Douglas fir	<i>Pseudotsuga menziesii</i>	.59	20	85	5	Fair	Good		Retain and Monitor	3.54
670 OS	Western red cedar	<i>Thuja plicata</i>	.33, .38	15	90	10	Fair	Good	Multi-stemmed at base & 4m	Retain and Monitor	4.26
671 OS	Western red cedar	<i>Thuja plicata</i>	.37, .56	15	90	10	Poor	Good	Multi-stemmed at base	Retain and Monitor	5.58
672 OS	Western red cedar	<i>Thuja plicata</i>	.44, .46	15	90	10	Poor	Fair	Multi-stemmed at base & 4m Significant lvy in canopy	Retain and Monitor	5.4

TABLE 1 - Tree Inventory and Assessment

Tag #	Common Name	Botanical Name	DBH (m)	Height (m)	LCR (%)	Dead wood (%)	Structural Integrity	Health	Comments	Recommendations	TPZ 6X DBH (m)
673 OS	Western red cedar	<i>Thuja plicata</i>	.54	15	90	5	Poor	Fair	Multi-stemmed at 4m. Significant Ivy in canopy	Retain and Monitor	2.52
674	Mountain ash	<i>Sorbus aucuparia</i>	.24, .32	7	75	10	Fair	Fair	Multi-stemmed at base Significant Ivy in canopy	Remove due to conflicts with construction.	3.42
675 OS	Western red cedar	<i>Thuja plicata</i>	.8	15	90	5	Poor	Good	Multi-stemmed at 3m & 5m	Retain and Monitor	1.98
676 OS	Western red cedar	<i>Thuja plicata</i>	.12, .1, .15	7	70	5	Good	Good	Crown on south side	Retain and Monitor	4.44
677 City Shared	Western red cedar	<i>Thuja plicata</i>	.46, .48	15	90	5	Poor	Good	Pruning for the electrical house drop is required	Retain and Monitor	3.54
678	Pacific dogwood	<i>Cornus nuttallii</i>	.21, .3, .41	13	75	5	Good	Good	Pruning for the electrical house drop is required Multi-stemmed at 1m	Retain and Monitor	4.26
679 City	Paperbark maple	<i>Acer griseum</i>	.1, .11	4.5	60	<5	Good	Good		Retain and Monitor	5.58
680 City	Paperbark maple	<i>Acer griseum</i>	.13	4.5	65	<5	Good	Good		Retain and Monitor	5.4
681	Spruce	<i>Picea engelmannii</i>	.54	15	90	5	Good	Fair	Moderate Sapsucker damage and related resinosis	Remove due to conflicts with construction.	3.24

SUMMARY AND RECOMMENDATIONS

Viability of trees

City trees #666 - #668 are in poor health due to decay found on the main stems.

All 3 hedges on City property are in good condition.

Trees #669 - #674 share canopies.

March 27, 2022 - The Western red cedar (*Thuja plicata*) tree has a poor-moderate relative tolerance to construction impacts and the addition of fill soil (Matheny and Clark, 1998, Trees and Development). Effort must be made to ensure the long term success and retention of all retained trees before, during and after construction is complete.

Development impact on trees

Offsite tree #670 will have encroachment from the new open patio. Arborist supervision is recommended during construction.

Offsite tree #671, 672, 673 and 675 will have 20%, 20%, 3% and 27% encroachment from the new house excavation zones. Arborist supervision is recommended during excavation and construction. A reduction in the excavation zone from 1.5m to 1m has been implemented for this side of the house.

Tree #674 will have 60% encroachment from the new house excavation zone and is therefore recommended for removal due to conflicts with construction.

Offsite/City shared tree #677 will have 32% encroachment from the new house excavation zone. A concrete walkway, gate and fence are proposed in its TPZ. Arborist supervision is recommended during excavation and construction. A reduction in the excavation zone from 1.5m to 1m has been implemented.

Tree #678 will have 3% encroachment from the new house excavation zone. A concrete walkway, gate and fence are proposed in its TPZ. Arborist supervision is recommended during excavation and construction. A reduction in the excavation zone from 1.5m to 1m has been implemented.

Tree #681 will have 100% encroachment from the new house excavation zone and is therefore recommended for removal due to conflicts with construction.

March 27, 2022 - To reduce the construction impacts to trees #671, 672, 673, 675, 677 and 678, the basement has been reduced in size and the excavation zone has been reduced from 1.5m to 1m. To support the unreduced floors above, screw piles shall be used and located near the protected trees. The excavation for the screw piles is approximately 60cm X 60cm and is to be done by hand or Hydro-vacuum machine. The suitability, quantity and locations of the screw piles shall be determined by the Project Architectural and Geotechnical engineers.

SUMMARY AND RECOMMENDATIONS continued

NEW April 26, 2022 – The pathway has been aligned with the east side entry doorway. The pathway located inside the tree protection zone of tree #678 shall be above grade using paving stones, slab-on grade concrete, Grasscrete or gravel. The pathway shall be a suitable distance away from the basal flare of the tree, as determined by the project Arborist at time of preparation/installation. No digging shall be done inside the tree protection zone. Arborist supervision during the preparation and installation of the pathway is recommended.

Potential conflicts with services

Storm comes from Cliff Ave. - No conflict is expected.

Natural gas comes from Cliff Ave. - No conflict is expected.

Sanitation comes from Cliff Ave. - No conflict is expected.

Water comes from Cliff Ave. - No conflict is expected.

Electrical is above ground and comes from Cliff Avenue, through tree #677. Pruning for clearance for the new above ground electrical house drop may be necessary.

GENERAL NOTES

1. Replacement trees must meet the plant condition and structure requirements set out in the latest edition of the BCSLA/BCLNA "B.C. Landscape Standard" and the CNTA "Canadian Standards for nursery stock".
2. Replacement trees must be located, planted and maintained in accordance with the BCSLA/ BCLNA and "White Rock Tree Management Bylaw NO. 1831".
3. Replacement trees must be a minimum size of 3.0m ht. coniferous / 6 cm cal. deciduous
4. Trees must be located a minimum distance of 1m from any property lines and 3m from any other tree or buildings/services.

CONSTRUCTION, WATERING AND MAINTENANCE SPECIFICATIONS

1. Before beginning work, the contractor should meet with the project arborist on site to review all work procedures, access routes, storage areas and tree protection measures.
2. **Any work within two meters of a tree protection zone (TPZ)** shall be monitored and done under the directions of the project arborist.
3. All trees shall be irrigated prior to, during, and after any root pruning or after any excavation and every week after, during the months of June, July, August and September (2022). Each irrigation shall wet the soil to a depth of 150cm.
4. Clearing of vegetation in the TPZ shall be done by hand. Brush shall be chipped and placed in the TPZ to a depth of 6". Wood chips shall not be placed against the trunk(s). Additional wood chips may be required.
5. Tree(s) to be removed that have branches extending into the canopy of tree(s) to remain or are in the TPZ of tree(s) to remain, must be removed by a qualified arborist. The qualified arborist shall remove the tree(s) in a manner that causes no damage to the tree(s) and understory to remain. Trees to be removed that are located in the TPZ of trees to remain, shall be cut near ground level and the stump ground out.
6. Erosion control devices such as silt fencing, debris basins, and water diversion structures shall be installed to prevent siltation and/or erosion within the TPZ.
7. All work required within the TPZ shall be done by hand and under the direct supervision of the project Arborist.
8. All pruning shall be performed by a certified arborist and will be in accordance with ANSI A300 (part 5) pruning standards and ANSI Z133.1 safety standards. Pruning for building clearance will be required and is best achieved at the framing stage, prior to the installation of glazing and building wrap.
9. Trees should be monitored by a certified arborist for a period of 18 months after construction is complete.
10. No swales shall be located in the tree protection zones of retained trees.
11. Trees to be retained will require arborist supervision any time heavy machinery encroaches within 2 meters of the tree protection zones. Excavation pins will need to be located prior to excavation for construction. Roots should be pruned or cut prior to excavation to minimize the damage from ripping. Ripped or damaged roots shall be exposed using the least injurious method and the damaged section cut cleanly. Exposed roots should be covered with moisture retaining material such as burlap or moist fill soil, and a covering such as a tarpaulin, to prevent drying of the soil and roots.

TABLE 2 - Tree Protection Summary

	Onsite	Offsite	City	Total
Number of protected trees identified	3	7	7	17
Number of protected trees to be Removed	2	0	0	2
Number of protected trees to be retained	1	7	7	15
Number of replacement trees required	6	0	0	6
Number of replacement trees proposed	6	0	0	6
Number of replacement trees in deficit	0	0	0	0
Total number of retained trees & replacement trees	21			

TREE PROTECTION PLAN

Tree protection shall be done in accordance with White Rock Tree Management Bylaw No. 1831. Tree protection fencing is to be installed prior to construction with no excavation, grade alterations or materials storage within the Tree Protection Zones (TPZ) unless preapproved by the project Arborist. Tree protection zones are listed on the Tree drawing #1. The project Arborist must be contacted prior to and be on site for any construction within **the recommended TPZ which is 6X the tree diameter**. All parties must be aware that long-term success in tree preservation efforts depends greatly on minimizing the impact caused during and post construction. Mechanical injuries caused to trees above or below ground are difficult to repair and can cause long term damage or death. Best efforts must be made to ensure that soils remain undisturbed within the tree protection zones.

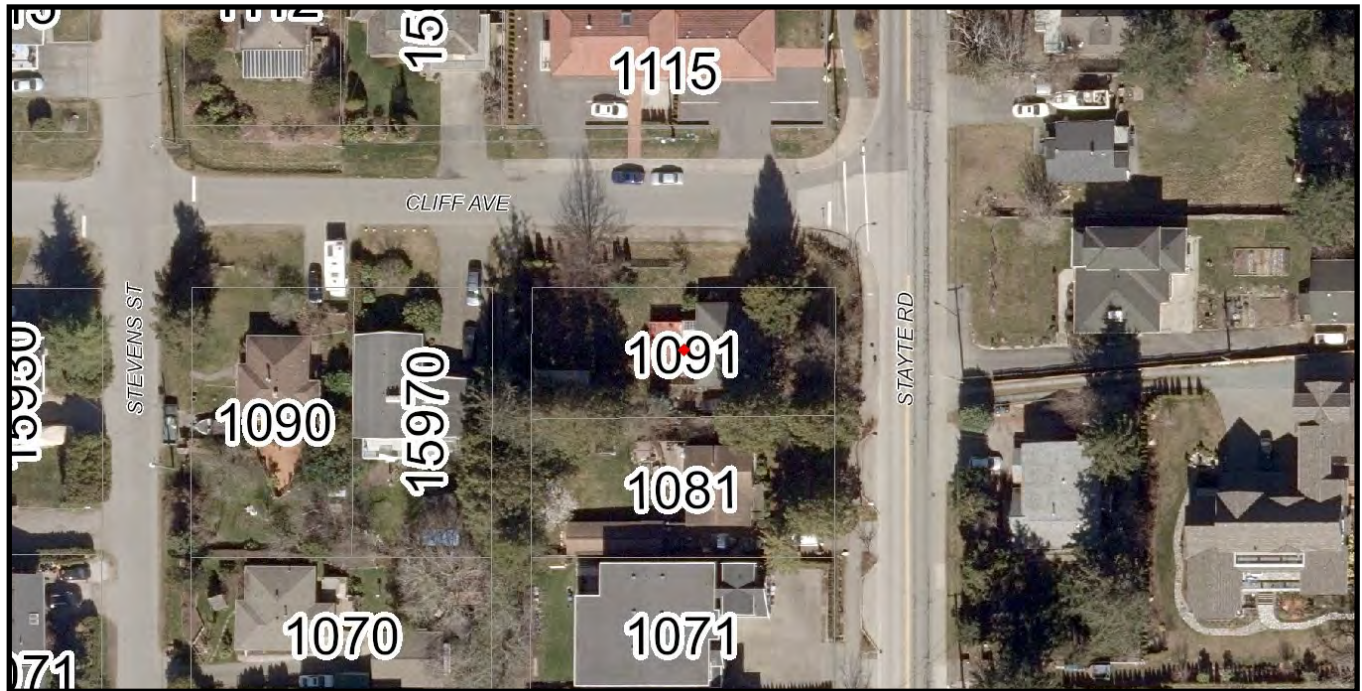


Photo 1

Aerial photo March 2019, White Rock's WROMS.



Photo 2

Street view of property at time of assessment.



Photo 3
Tree #665 and #H3. Facing west.



Photo 4
Tree #665 on left and #H3. Facing east.



Photo 5
Trees #666 - #674, from right to left.
All trees share a crown.



Photo 6
Tree #666, photo showing pruning cut dieback on the
main stems. Good reaction growth has developed.



Photo 7

Tree #667, photo showing pruning cut dieback on the main stem. Minimal reaction growth has developed.



Photo 8

Tree #668, photo showing pruning cut dieback on the main stem. Good reaction growth has developed.

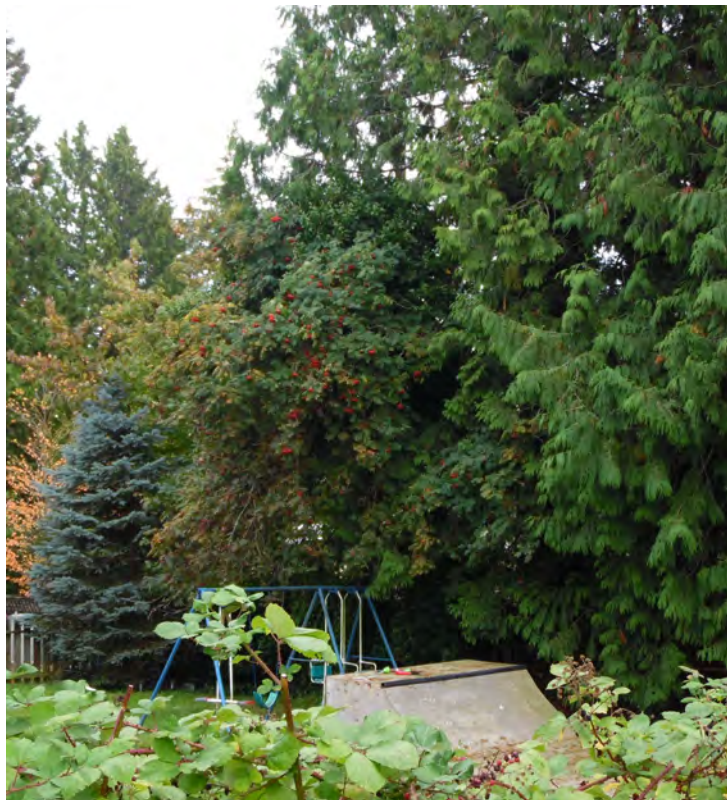


Photo 9

Tree #674 in center with red berries.



Photo 10

Tree #675, photo showing co-dominant stems at 3m and 5m.



Photo 11
Tree #676 (left) and #677 (right).



Photo 12
Photo showing the co-dominant base of tree #677.



Photo 13
Tree #678 and #678 in center, #677 on left, and #681 on right.



Photo 14
Photo showing the base of tree #678.
Tree #681 in background.



Photo 15
Tree #681



Photo 16
Sapsucker damage on tree #681.



Photo 17
Boulevard trees #679, #680 and #H1.

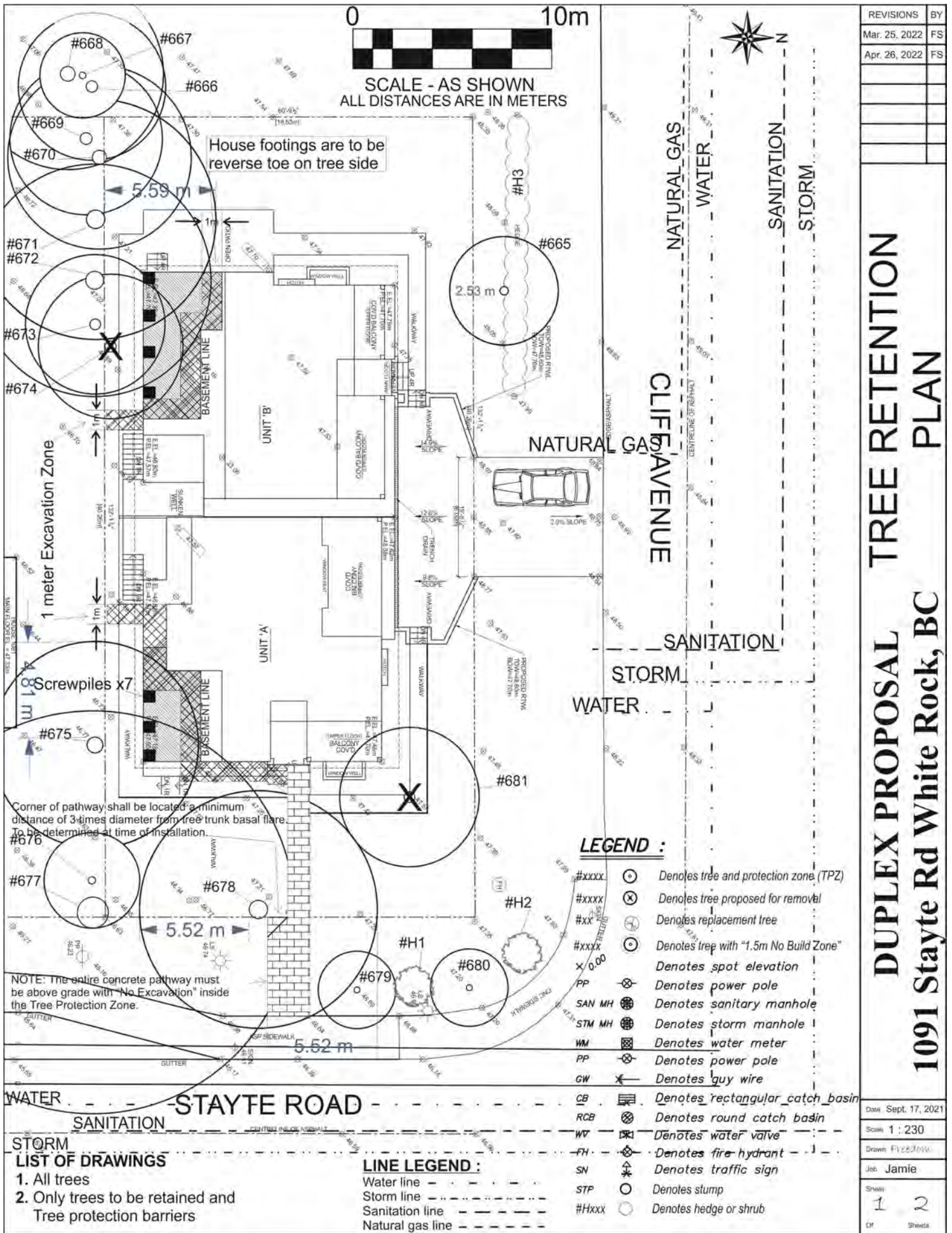


Photo 18
Boulevard trees #679 , #680 and hedge #H1.
All on city property.



Photo 19

Hedge #2 on city property. Volunteer trees have started growing in the hedge.



ASSUMPTIONS AND LIMITING CONDITIONS

- Information contained in this report covers only those items that were examined and reflects conditions of those items at the time of assessment. The assessment is limited to visual examination of accessible items without dissection, excavation, climbing or coring.
- Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible, however, Freedom Tree Care Ltd can neither guarantee nor be responsible for the accuracy of information.
- Any legal description provided to the consultant is assumed to be correct. Any titles and ownerships to any property are assumed to be good and marketable. No responsibility is assumed for matters legal in character. Freedom Tree Care Ltd shall not be required to give testimony or to attend court by reason of this report unless subsequent contractual arrangements are made .
- Loss or alteration of any part of this report invalidates the entire report.
- This report shall be used for its intended purpose only and by the parties to whom it is addressed. Neither all or any part of the contents of this report, nor copy thereof, shall be conveyed by anyone, including the client, to the public through advertising, public relations, news, sales, or other media, without the prior expressed written and verbal consent of Freedom Tree Care Ltd.
- Sketches, diagrams, graphs and photographs in this report, being intended as visual aids, are not necessarily to scale and should not be constructed as engineering or architectural reports or surveys.
- There is no warranty or guarantee expressed or implied, that problems or deficiencies of the tree or other plant or property in question may not arise in the future.

I certify that this report has been prepared in accordance with accepted Arboricultural standards from the information made available to me at the time, and that the facts and opinions expressed within it are true and accurate to the best of my knowledge and belief.

If there are any questions regarding the contents of this report, please contact Freedom Tree Care Ltd.



Freedom Sukenick

ISA Certified Arborist #PN-7712A

Certified Tree Risk Assessor (TRAQ)

Freedom Tree Care Ltd.

October 27, 2021

File: 19-3960

City of White Rock
Planning and Development Services
15322 Buena Vista Avenue
White Rock, BC
V4B 1Y6

Attn: Neethu Syam, Planner

Dear Madam:

**Re: 1091 Stayte Road, White Rock
White Rock File No. 20-005
Summary of Public Information Meeting Held October 8, 2020**

Application Profile

The Proposal is to rezone from the RS-1 one unit residential to the RT-1 two-unit residential Duplex zone (no secondary suites). A minor Development Permit for form and character is required to ensure duplex complies with the mature neighborhood DP area Guidelines in the OCP. Three stories are proposed with no basement suites. The site access will be from Cliff Avenue via shared driveway and not from Stayte Road.

Notification of Public Information Meeting

White Rock's Planning Department sent out PIM Notification letters to the property owners in the area.

Time and Location

The Public Information Meeting was held on Thursday October 8, 2020 virtually through a Microsoft Teams – "Live Event". The meeting was from 5:30p.m to 6:30p.m.

Project Representatives

CitiWest Consulting Ltd.:	Natalie Pullman, Planning Assistant
Owner/Developer:	Mirus Enterprises (Jamie Walgren)
Designer:	Raj Toora, House Designer
City of White Rock:	Athena Van Hausen, Planner

Attendance

There was one attendee of the meeting, the neighbor to the South.

Comment Sheet

We haven't received any comments other than the comment provided at the Public Information Meeting. The comment was related to the offsite tree removal.

Meeting Format

The meeting was held virtually through Microsoft Teams "Live Event". PowerPoint presentation of surrounding zoning, proposed layout, detailed House Plans, site plans, and a neighbourhood aerial photograph were presented by Natalie Pullman and Raj Toora. Questions asked during the meeting were addressed by CitiWest staff, City representative and the developer. This meeting addressed the proposed development application and the layout.

Results

Overview of concerns/comments:

Comments:

- Neighbor responded positively to the house design.
- The neighbor raised concerned about offsite tree removal, and potential damage to the health of the trees with the duplex development.

Developer's Explanation / Action:

- The developer is making every effort to save as many of the healthy trees as possible, based on the arborist's professional experience and recommendations.
- Arborist will be present onsite during excavation and construction near TPZ to ensure no damage is done to the trees to be retained.
- Developer reached out to the neighbors after PIM to obtain permission for off site tree removal. Since the neighbors declined to provide their permission, the design was revised to retain all offsite trees by incorporating Screw Pile foundation.

Conclusion

After reviewing the comment and public feedback, there are no concerns from the surrounding neighbors other than the neighbor to the immediate south. In order to seek support from the neighbor, the developer has amended the design by

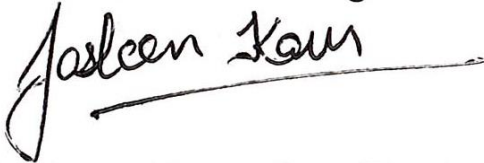
incorporating Screw Piles Foundation, thereby retaining all the offsite trees on the neighboring property.

Through amendments to the design, the developer has demonstrated their willingness to provide a balance that is satisfactory to the neighbours and the City of White Rock, with the current proposal.

We request that you please review the proposed possible option and supporting reasons given by the developer and their representatives. We look forward to working with you and proceeding with the project to ADP and Council in the near future. If you have any questions, please me at 604-591-2213.

Yours truly,

CitiWest Consulting Ltd.



Jasleen Kaur, GradTech
Planning/Engineering Assistant

JK/rj

cc: Mirus Enterprises (Jamie Walgren)



Public Art Advisory Committee

Minutes

July 12, 2022, 4:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

PRESENT:	Barbara Cooper, Community Member Gary Kennedy, Community Member Jim Adams, Community Member (left meeting at 4:40 p.m.; rejoined meeting at 4:47 p.m.) Yvonne Everson, Community Member
COUNCIL:	Councillor Chesney, Chairperson (non-voting) Mayor Walker (non-voting)
ABSENT:	Patryk Stasieczek, Community Member Patti Ghuman, Community Member
STAFF:	Eric Stepura, Director of Recreation and Culture Debbie Johnstone, Deputy Corporate Officer

1. CALL TO ORDER

The Chairperson called the meeting to order at 4:00 p.m.

2. ADOPTION OF AGENDA

Motion Number 2022-PAAC-011: It was MOVED and SECONDED

THAT the Public Art Advisory Committee adopts the agenda for the July 12, 2022 meeting as circulated.

Motion CARRIED

3. **ADOPTION OF MINUTES**

Motion Number 2022-PAAC-012: It was MOVED and SECONDED

THAT the Public Art Advisory Committee adopts the minutes of the June 14, 2022 meeting as amended to include:

- Under Item 5: An update was provided on a recommendation from the Arts and Culture Advisory Committee that was endorsed by Council stating:
THAT the Arts and Cultural Advisory Committee recommends that Council endorse the Committee establishing their 2021-2022 Work Plan item 1.3.1, "Explore the options for creating an Arts Endowment Fund," as their top priority going forward.

Motion CARRIED

ACTION ITEM: Marine Drive Task Force report to be added as an item on the next agenda (and hard copies to be printed for those members wanting to pick them up).

4. **BUSINESS ARISING FROM ACTION AND MOTION TRACKING DOCUMENT**

The Director of Recreation and Culture provided an update to the Committee on the new hire for the Manager of Cultural Development position. He also provided updates on action items from the tracking document that are still in progress.

Motion Number 2022-PAAC-013: It was MOVED and SECONDED

THAT the Public Art Advisory Committee invites the following staff members to the September 13, 2022 meeting:

- **A member of the Planning and Development Services Department to discuss a plan for working more collaboratively on future projects; and,**
- **The Manager of Parks so that he can be introduced to the Committee.**

Motion CARRIED

ACTION ITEM: Committee member, G. Kennedy, to forward research to the Committee regarding other municipalities' policies for murals on hoarding and share this information with the Manager of Cultural Development.

It was noted that the Art Plan Brainstorming Workshop, an item on the Committee's Work Plan, will likely be scheduled in the fall.

5. **UPDATE ON RELEVANT MOTIONS FROM OTHER ADVISORY COMMITTEES TO COUNCIL**

The Director of Recreation and Culture noted that the following Arts and Cultural Advisory Committee recommendation was endorsed by Council at their June 27, 2022 meeting:

***2022-ACAC-017:** THAT the Arts and Cultural Advisory Committee recommends, in order to support ongoing placemaking initiatives, that Council approve additional multi-committee roundtable placemaking discussion workshops being held, including the same participants from the first workshop.*

6. **APPLICATION PROCESS FOR FUTURE CITY ARTS-RELATED EVENTS AND COMPETITIONS**

The Director of Recreation and Culture noted that this topic was not something that had been addressed prior to the previous Manager of Cultural Development leaving.

Committee members were asked to share their suggestions for inclusion of guidelines and useful information, on the City website's Public Art page, regarding the application process for potential arts-related events and competitions. The following discussion points were noted:

- City's Arts and Culture Database Information Form: could benefit from a makeover to make the form design more welcoming; arts categories could be grouped together in a better way (for example, performative arts categories all together, visual arts categories all together, etc.).
- City events calendar only includes City events held at City venues; could consider making this list more wide-ranging or could provide links to other arts groups in the City.
- Could include following questions/reminders for potential applicants: Have you connected with the Semiahmoo First Nation (SFN) about your project/event? Have you submitted your proposed project/event for review by the Public Art Advisory Committee?

7. **2021-2022 WORK PLAN UPDATE**

J. Adams left the meeting at 4:40 p.m. and quorum was lost.

J. Adams re-entered the meeting at 4:47 p.m. and quorum was achieved, so the meeting could restart.

The Director of Recreation and Culture provided an update on priority items in the 2021-2022 Committee Work Plan.

8. UPDATE REGARDING PUBLIC ART INCLUSION AT DEVELOPMENT SITES

The Director of Recreation and Culture provided an update regarding the status of public art projects at the following locations:

- Thrift Avenue & Johnston Road: project is well on its way. SFN has been consulted to ensure pictograph images were in fact Coast Salish.
- Helen Fathers Centre Street Walkway Project: there have been several cost increases for this project; as a result, it came in over-budget, so the plan is to re-launch in early 2023, with the hope that costs will be lower at that time (there is the intention for public art inclusion in this project).

9. OTHER BUSINESS

The process for art inclusion in developments throughout the City was discussed. The Director of Recreation and Culture clarified that a previous recommendation from the Committee noted that the Manager of Cultural Development work with the Planning and Development Services department for art inclusion in developments when possible.

10. INFORMATION

None

11. 2022 MEETING SCHEDULE

The following meeting schedule was approved by the Committee and was provided for information purposes:

- September 13, 2022.

All meetings are scheduled to take place from 4:00 p.m. to 6:00 p.m.

12. **CONCLUSION OF THE JULY 12, 2022 PUBLIC ART ADVISORY COMMITTEE MEETING**

The Chairperson declared the meeting concluded at 5:05 p.m.

Councillor Chesney, Chairperson



Debbie Johnstone, Deputy Corporate Officer

Unapproved



Housing Advisory Committee

Minutes

July 13, 2022, 4:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

PRESENT: Councillor Manning, Chairperson (non-voting)
Stephen Crozier, Community Member (joined at 4:02 p.m.)
Greg Duly, Community Member
Chris Harris, Community Member
Abhishek Mamgain, Community Member
Marie Sabine, Community Member

ABSENT: Brian Hagerman, Community Member
Gary Quinn, Community Member

GUEST: Mayor Walker

STAFF: Alex Wallace, Manager of Planning
Tracey Arthur, Director of Corporate Administration

1. CALL TO ORDER

The Chairperson called the meeting to order at 4:00 p.m.

2. ADOPTION OF AGENDA

Motion Number: 2022-HAC-036 It was MOVED and SECONDED

THAT the Housing Advisory Committee adopts the agenda for July 13, 2022 as circulated.

Motion CARRIED

3. **ADOPTION OF MINUTES**

Motion Number: 2022-HAC-037 It was MOVED and SECONDED

THAT the Housing Advisory Committee adopts the minutes of the May 24, 2022 meeting as circulated.

Motion CARRIED

4. **2021-2022 WORK PLAN UPDATE**

Councillor Manning, Chairperson, noted the three (3) priority items highlighted in the Committee's 2021-2022 Work Plan document:

1. Review and provide feedback on the Draft Housing Needs Report;
2. Provide input on Draft Amendments to the City's Zoning Bylaw as they relate to housing; and,
3. Evaluate strategies to address areas regarding housing needs, following the final approval of the Housing Needs Report.

The Chairperson then noted that some of these items are on the agenda to address at this meeting, but there are no further updates at this time.

5. **COMMUNITY AMENITY CONTRIBUTION FUNDING AND SENIOR HOUSING PROJECTS**

The Committee to provide any updates on the following:

- Community Amenity Contribution (CAC) funding for senior housing projects
- Partnering with a non-profit agency for senior housing projects

Note: No update regarding the above was noted.

It was noted for information purposes that the following recommendation was endorsed by Council at the June 13, 2022 Regular Council meeting:

Motion Number 2022-255:

THAT Council consider waiving or reducing CACs and DCCs for non-market housing projects, for example senior housing and special needs housing.

Committee Member, S. Crozier, arrived at the meeting at 4:02 p.m.

6. **HOUSING STRATEGY ACTION PLAN UPDATE**

The Manager of Planning led the discussion regarding the Housing Strategy Action Plan and provided a PowerPoint presentation that reviewed her July 13, 2022 corporate report, titled “Affordable Housing Framework”, addressing how the City can implement what is right for White Rock.

- Five Strategies (as per the recommendations of the report) were noted with the following:

1) Use the City's regulatory tools to encourage a diverse mix of housing types

- a. Inclusionary Zoning and Density Bonus Policy
- b. Covenant Tools
- c. Contributions Gained from Density Bonusing (CACs)
- d. Develop Financial Incentives Criteria

2) Maximize use of City resources and financial tools

- a. Reducing costs by streamlining approvals and other incentives
- b. Housing Fund
- c. Municipal land and land trusts
- d. Non-profit owned land

3) Build capacity with non-profit housing and service providers

- a. Housing organization and building capacity

4) Facilitate and strengthen partnership opportunities

- a. Housing Strategy

5) Increase advocacy, awareness, and education roles

- a. Engagement and Communication

The following discussion points were noted:

- Consultation with Semiahmoo First Nation: staff noted this has not been conducted in regard to housing at this time but will look to doing so in the future
- It was stated that the use of covenants is important to ensure what is wanted/ agreed to remains in place

- Definition of Affordability (renters 20% below median): staff noted this can be addressed with Housing Agreements (these can be registered on title thus binding)
- Housing Agreement - inclusion of component for service workers: staff confirmed this can be done, as well as for those with disabilities. Other municipalities are doing this; White Rock will review what is being done and see what will work for our community
- Not a lot of land for the purpose of Affordable Housing; staff noted not just one (1) building is needed in order to address this, it will mean putting the lens of affordability across the City
- Parkland: Cash in Lieu, how does White Rock compare with other municipalities in regard to percentage of land dedicated to parks? What are best practices?

ACTION ITEM: Staff to review and will bring back information on this question (regarding how White Rock compares with other municipalities in regard to percentage of land dedicated to parks, and what are best practices).

Motion Number: 2022-HAC-038 It was MOVED and SECONDED

THAT the Housing Advisory Committee recommends to Council to support the integration of the following five strategies into the existing development process to create a long-term framework for affordable housing that creates the capacity to provide affordable housing in multiple ways:

1. **Use the City's regulatory tools to encourage a diverse mix of housing types**
2. **Maximize use of City resources and financial tools**
3. **Build capacity with non-profit housing and service providers**
4. **Facilitate and strengthen partnership opportunities**
5. **Increase advocacy, awareness, and education roles**

Motion CARRIED

7. OTHER BUSINESS

Special congratulations were noted to Committee member, Marie Sabine, who recently was named and awarded an "Outstanding Canadian on the Peninsula".

8. INFORMATION

8.1 COMMITTEE ACTION TRACKING

Corporate Administration provided an action and motion tracking document to the Committee for information. This spreadsheet will be updated after each meeting and provided to members for information.

9. 2022 MEETING SCHEDULE

The following meeting schedule was approved by the Committee and was provided for information purposes:

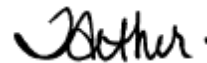
- July 26, 2022; and,
- September 27, 2022.

All meeting times are scheduled to take place from 4:00 p.m. to 6:00 p.m.

10. CONCLUSION OF THE JULY 13, 2022 HOUSING ADVISORY COMMITTEE MEETING

The Chairperson declared the meeting concluded at 4:54 p.m.

A. Manning, Chairperson



T. Arthur, Director of Corporate Administration



Arts and Cultural Advisory Committee

Minutes

July 14, 2022, 4:00 p.m.

City Hall Council Chambers

15322 Buena Vista Avenue, White Rock, BC, V4B 1Y6

PRESENT:	Denice Thompson, Community Member Elaine Cheung, Community Member Jim Adams, Community Member
NON-VOTING MEMBERS:	Karin Bjerke-Lisle, White Rock Museum & Archives
COUNCIL:	Councillor Anthony Manning, Chairperson (non-voting)
STAFF:	Eric Stepura, Director of Recreation and Culture Tracey Arthur, Director of Corporate Administration

1. **CALL TO ORDER**

The meeting was called to order at 4:00 p.m.

2. **ADOPTION OF AGENDA**

Motion Number 2022-ACAC-018: It was MOVED and SECONDED

THAT the Arts and Cultural Advisory Committee adopts the agenda for the July 14, 2022 meeting as circulated.

Motion CARRIED

3. **ADOPTION OF MINUTES**

Motion Number 2022-ACAC-019: It was MOVED and SECONDED

THAT the Arts and Cultural Advisory Committee adopts the minutes of the June 9, 2022 meeting as circulated.

Motion CARRIED

4. **BUSINESS ARISING FROM ACTION AND MOTION TRACKING DOCUMENT**

It was noted there are no updates at this time.

5. **UPDATE ON RELEVANT MOTIONS FROM OTHER ADVISORY COMMITTEES TO COUNCIL**

Councillor Manning, Chairperson, informed of the following resolution, regarding the final allocation of 2022 Grants-in-Aid – Arts and Culture funding, made by Council at their July 11, 2022 Regular Council meeting:

THAT Council allocate the 2022 Late Submission Grants-in-Aid - Arts and Culture funding to:

- *Arts of Course - South Rock Art Tour in the amount of \$2,500; and*
- *Surrey Youth Theatre Company's (SYTCO) 25th Season event in the amount of \$2,500.*

6. **MULTI-COMMITTEE PLACEMAKING WORKSHOP FOLLOW-UP**

Staff noted that Council adopted the following recommendation at their June 27, 2022 Regular Council meeting:

2022-ACAC-017: THAT Council, in order to support ongoing placemaking initiatives, approve additional multi-committee roundtable placemaking discussion workshops being held, including the same participants from the first workshop.

The intent for the next workshop will be to bring in a consultant to facilitate the discussion regarding placemaking initiatives.

Motion Number 2022-ACAC-020: It was MOVED and SECONDED

THAT the Arts and Cultural Advisory Committee requests Council approval to schedule additional multi-committee roundtable placemaking discussion workshops in late July / early August 2022.

Motion CARRIED

Staff noted, regarding a Needs Assessment for an Arts and Cultural Facility Development Project, that at this time they are in the process of reaching out to various consultants. The initial step will be to seek copies of request for proposal (RFP) documents for this type of work.

7. 2021-2022 WORK PLAN UPDATE

The Director of Recreation and Culture provided progress updates regarding the Committee's 2021-2022 Work Plan priority items that had not already been discussed.

8. OTHER BUSINESS

None

9. INFORMATION

9.1 ARTISTS' WALK UPDATES

The Chairperson noted, as mentioned previously under Item 5: *Update on Relevant Motions from Other Advisory Committees to Council*, that Council has approved a \$2,500.00 Grant-in-Aid to Arts of Course for their upcoming South Rock Art Tour.

9.2 OCTOBER ARTS FESTIVAL

A Committee member provided an update regarding planning for the October Arts Festival, which was discussed at a recent Peninsula Arts and Culture Alliance (PACA) meeting. It was noted that an online form will be created and used to collect input from members of the public who would like to participate, including what type of art/performance they would be offering. After collecting this information, they plan to look at possible venues, followed by planning the schedule.

10. **2022 MEETING SCHEDULE**

The following schedule of meetings was previously approved by the Committee and was provided for information purposes:

- September 8, 2022.

All meetings are scheduled to take place from 4:00 p.m. to 6:00 p.m.

11. **CONCLUSION OF THE JULY 14, 2022 ARTS AND CULTURAL ADVISORY COMMITTEE MEETING**

The Chairperson declared the meeting concluded at 4:31 p.m.

Councillor Manning, Chairperson



Tracey Arthur, Director of Corporate Administration

THE CORPORATION OF THE
CITY OF WHITE ROCK
COMMITTEE REPORT



DATE: July 13, 2022

TO: Housing Advisory Committee

FROM: Alex Wallace, Manager of Planning

SUBJECT: Affordable Housing Framework

RECOMMENDATION(S)

THAT the Housing Advisory Committee recommends to Council to support the integration of the following five strategies into the existing development process to create a long-term framework for affordable housing that creates the capacity to provide affordable housing in multiple ways:

1. Use the City's regulatory tools to encourage a diverse mix of housing types
 2. Maximize use of City resources and financial tools
 3. Build capacity with non-profit housing and service providers
 4. Facilitate and strengthen partnership opportunities
 5. Increase advocacy, awareness, and education roles
-

EXECUTIVE SUMMARY

This committee report has been prepared in response to a Housing Strategy Action Plan presented at the April 25, 2022, meeting. The Housing Advisory Committee (HAC) was asked to review the presented information and provide Council with alternative options. As a result, staff have prepared a report that outlines tools that the committee can select to integrate affordability into the development process to create a long-term framework for affordable housing.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
#2021-156 March 15, 2021	THAT Council endorses the following as their top five (5) priorities: <ul style="list-style-type: none">• The Official Community Plan (OCP) Review• Solid Waste Pickup for Multi-Family• Housing Needs / Affordable Housing• Community Amenity Contribution (CAC) 'shovel-in-the-ground' projects• The City's Relationship with Semiahmoo First Nation

	Motion CARRIED
#2021 – 456 November 22, 2021	<p>THAT Council:</p> <ol style="list-style-type: none"> 1. Receive the "Housing Needs Report," included as Appendix A to the corporate report titled "City of White Rock Housing Needs Report (2021)" as the first Housing Needs Report for the municipality, prepared pursuant to Division 22 of the Local Government Act and the requirements of British Columbia Regulation 90/2019; and 2. Direct staff to publish a copy of the "Housing Needs Report" on the City's webpage. <p>Motion CARRIED</p>
#2022-161 April 25, 2022	<p>THAT Council remove the seven (7) recommendations from the Housing Advisory Committee in regard to the 7 pillars for the draft Housing Strategy Action Plan on the April 25, 2022 regular Council agenda.</p> <p>Motion DEFEATED</p> <p>Question was called on the main motion with the noted additions and deletion of Item 6.2.c to the agenda and it was,</p> <p>Motion CARRIED</p>
#2022-173 April 25, 2022	<p>THAT Council receive for information that the Housing Advisory Committee endorses Pillar #1, Accelerate and Depoliticize the Approval Process, from the Affordable Housing Strategy and Action Plan, as presented.</p> <p>Motion CARRIED</p>
#2022-174 April 25, 2022	<p>THAT Council receive for information that the Housing Advisory Committee endorses Pillar #2, Local Use of the Speculation and Vacancy Tax, from the Affordable Housing Strategy and Action Plan, as presented.</p> <p>Motion CARRIED</p>
#2022-175 April 25, 2022	<p>THAT Council not approve/ endorse Pillar #3, Delegate Approval Authority to Staff, from the Affordable Housing Strategy and Action Plan, as presented.</p> <p>Motion CARRIED</p>
#2022-176 April 25, 2022	<p>THAT Council not approve/ endorse Pillar #4, No More 'Exclusive' Single Family Zones, from the Affordable Housing Strategy and Action Plan, as presented.</p> <p>Motion CARRIED</p>

#2022-177 April 25, 2022	<p>THAT Council not approve/ endorse receive for information that the Housing Advisory Committee recommend Pillar #6, Waiving Public Hearings.</p> <p>Motion CARRIED</p>
#2022-178 April 25, 2022	<p>THAT Council not approve/ endorse Pillar #5.b. with the amendment of reducing the Town Centre Transition area boundaries from west of Merklin Street and east of Vidal Street;</p> <p>and THAT the Town Centre Transition area be reduced from a maximum of 18-storey hybrid towers to 14- storey hybrid towers and that the Town Centre area be reduced from a maximum of 25-storey hybrid towers to 18-storey hybrid towers, with an understanding that the 'hybrid towers' include at least 50 percent non-market housing.</p> <p>Motion CARRIED</p>

INTRODUCTION/BACKGROUND

Legislative Framework

Housing Needs Reports

Municipalities and regional districts in B.C. must complete housing needs reports by April 2022 and every five years after that. These reports will help local governments and the B.C. government better understand and respond to housing needs in communities throughout B.C.

Housing needs reports are a way for communities to understand their current and future housing needs better. These reports can help identify existing and projected gaps in housing supply by collecting and analyzing quantitative and qualitative information about local demographics, economics, housing stock, and other factors. A housing needs report is critical to developing a strategy or action plan.

Together, legislation and regulations specify requirements for local government related to housing needs reports. These requirements include:

- Collecting information to identify current and projected housing needs,
- Using that information to prepare and publish an online housing needs report which shows current and projected housing needs for at least the next five years, and
- Considering the most recently collected information and housing needs report when amending official community plans and regional growth strategies.

The City's HAC has built into their 2022 Work Plan a commitment to explore opportunities to support housing choice and affordability, to address local needs and the overall quality of life for White Rock residents. The City's Housing HAC was consulted throughout preparing the Housing Needs Report. Members of HAC provided valuable insight to staff and the City's consultant regarding the experiences of White Rock residents and their housing needs. On October 26, 2021,

the HAC passed a motion endorsing the draft Housing Needs Report and the presentation of this work at the November 22, 2021, meeting of Council. The Housing Needs Report presents six (6) key areas of local housing needs, including:

1. Affordable Ownership Housing;
2. Rental Housing;
3. Special Needs Housing;
4. Seniors Housing;
5. Family Housing; and
6. Shelters and Housing for People At Risk of Homelessness.

The staff report presented at the November 22, 2021, meeting is provided in **Appendix A** for reference.

Relationship to Official Community Plans and Regional Growth Strategies

A local government is required to consider its most recent housing needs report and the housing information on which it is based, when

- Developing an official community plan (OCP) or regional growth strategy
- Amending an OCP concerning housing statements, map designations or policies
- Amending a regional growth strategy about proposed housing actions, and
- Considering every five years whether a regional growth strategy must be reviewed

This will ensure that the latest available housing needs information informs updates to an OCP or regional growth strategy.

Housing and Official Community Plans

An OCP is the long-term vision of the community. The plan guides the decisions of elected officials and local government staff. Usually, this type of plan outlines how specific areas of the community will be used. For example:

- Residential, commercial, agricultural, or industrial uses
- Schools
- Roads
- Sensitive environmental areas
- Parks
- Infrastructure and services (e.g., water and sewer)

An OCP will also include housing policies – for example, housing affordability, rental housing and special needs housing. The OCP may also guide on matters affecting housing affordability, such as public transportation or access to employment.

Housing and Regional Growth Strategies (RGS)

Housing is a key focus for regional growth strategies (RGS). Metro Vancouver is the regional government, and they are currently developing Metro 2050 and an update to the RGS. The *Local*

Government Act requires an RGS to work towards "adequate, affordable and appropriate housing" and include housing-related actions to meet the needs of the region's residents over the longer term.

A regional growth strategy (RGS) is a local government strategic plan to promote human settlement that is socially, economically, and environmentally healthy. It also efficiently uses public facilities, land, and other resources. An RGS provides general guidance on a region's growth, change and development over a minimum 20-year period.

A regional growth strategy aligns long-range planning direction for regional district a municipal policy, plans and projects. It is also intended to provide a framework for regional cooperation with the Province and First Nations.

Housing strategies and action plans

A housing strategy or action plan outlines how a local government will respond to its community's unique housing needs. It typically addresses the full range of housing options, including social housing, rental housing, and home ownership.

This type of document has no specific requirements for form or content. Sometimes it's used as a supplement to an OCP or RGS. Strategies include:

- Actions to support social housing
- Targets for creating or preserving affordable housing to meet future demand
- Affordable market housing policies

What is an Affordability Housing Strategy?

It is an action-oriented framework that guides a local government response, within its authority, to maintain and create safe, suitable, and affordable housing options for residents. The purpose of the Affordable Housing Strategy is to ensure that the City's response to housing affordability challenges remains relevant and reflects key priority groups in need and housing gaps. Specific elements of the Affordable Housing Strategy include:

- Highlighting past achievements.
- Providing background on past and current housing affordability.
- Identifying key issues, current housing affordability pressures, and priority groups.
- Setting out ways to meet future affordable housing needs.

Some of the essential qualities of leading affordable housing approaches included: costs to municipalities, applicability to small communities, effectiveness, and speed of implementation. In addition, this report reviews existing policy practices in the region, best practice research from other cities, economic analysis, and tools - that can be included in an Affordability Housing Strategy.

Challenges with Affordable Housing

The most significant affordable housing challenges centre on:

- The cost of development
- The little profit associated with those costs

- The challenge of funding or financing projects

The cost of development and a lack of land for affordable housing are two of the more prominent challenges in recent years.

Past Strategies – City of White Rock

Affordable Housing Working Group 2010

In 2010 Council established the Affordable Housing Working Group to advise on tangible, practical ways to maintain and increase the supply of affordable housing in White Rock through the development of an Affordable Housing Strategy. The strategy identified actions the City could take to address its affordable housing issues. Implementation of the strategy would:

- Clarify the role of the City of White Rock regarding affordable housing
- Address the gap in housing that is affordable for the community.
- Address development of partnerships to foster affordable housing development
- Provide for ongoing consultation and follow-up

Background work and community consultation were undertaken in preparing the strategy resulted in seven areas for White Rock to address:

1. Maintaining a municipal planning system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand;
2. Maintaining a municipal financial system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand;
3. Facilitating diversity in housing forms and prices to deliver a range of affordable housing options for all residents;
4. Facilitating growth in the not-for-profit housing sector;
5. Supporting the retention and delivery of adequate supplies of private rental properties;
6. Encouraging senior government and industry cooperation in support of projects that complement White Rock's affordable housing objectives; and
7. Monitoring the housing situation and regularly evaluating the need for adjustments to the strategy.

The plan is attached as **Appendix B** for the committee's reference. Staff have tried to model the suggestion using this 2010 report, the endorsed pillars of affordable housing and best practices in other local governments.

Summary of Affordable Housing Strategy and Action Plan 2022

Most recently, the HAC and staff collectively worked on affordable housing. It was presented to Council for information. This plan consisted of 11 pillars to achieve affordable housing. It was sent back to HAC to work with staff to develop a newly revised plan. Below is a summary of the endorsed policy by Council.

Pillar	Council Direction	Suggested Step
#1 Accelerate and Depoliticize the Approval Process (OCP Focus)	Council received for information that the HAC endorses Pillar #1, Accelerate and Depoliticize the Approval Process, from the Affordable Housing Strategy and Action Plan, as presented.	Move to a development procedures review project. Staff to prepare a business case for Council to consider.
#2 Use the Speculation and Vacancy Tax (SVT) for Affordable Housing	Council received for information that the HAC endorses Pillar #2, Local Use of the Speculation and Vacancy Tax, from the Affordable Housing Strategy and Action Plan, as presented.	Council to play an advocacy role at UBCM.
#3 Delegate Approval Authority to Staff (Bill 26)	Council did not approve/endorse Pillar #3, Delegate Approval Authority to Staff, from the Affordable Housing Strategy and Action Plan, as presented.	Staff is taking no further action.
#4 No more 'Exclusive Single Family ONLY' Zones (Allow for conversions "as of right" subject to Development Permit in RS-1, RS-2 and RS-3 One Unit Residential Zones);	Council did not approve/endorse Pillar #4, No More 'Exclusive' Single Family Zones, from the Affordable Housing Strategy and Action Plan, as presented.	Staff is taking no further action.
#5 Broaden the spectrum and range of housing tenures, options, and forms in White Rock: (a) Pre-Zone for 'Missing Middle Housing' in specified areas by municipal initiative, and (b) Provide for 14 and 18 storey' hybrid towers' in specified areas;	Council did not approve/endorse Pillar #5. b. with the amendment of reducing the Town Centre Transition area boundaries from west of Merklin Street and east of Vidal Street. Council did not approve/endorse Town Centre Transition area be reduced from a maximum 18-storey hybrid towers to 14- storey hybrid towers and that the	Staff is taking no further action.

	Town Centre area be reduced from a maximum 25-storey hybrid towers to 18-storey hybrid towers, with an understanding that the 'hybrid towers' include at least 50 percent non-market housing.	
#6 Waiving Public Hearings (Section 464(2) of the LGA	Council did not approve/endorse receive for information that the HAC recommend Pillar #6, Waiving Public Hearings.	Staff is taking no further action.
#7 Affordable Housing Partnership Seminar (April 19, 2022, and follow-through)	Completed – April 19, 2022.	Staff remain available to discuss partnership opportunities when/if they arise.
#8 Produce an Affordable Housing Development Proposal by municipal initiative	This pillar has yet to be consider by Council.	Staff is suggesting to the HAC that we integrate affordable housing into the framework of the development approval process rather than focusing on developing one project. Long-term tools can be put in place to facilitate this.
#9 Waivers and exemptions for 'non-profit' and 'affordable housing' development proposals and applications including: (a) Development Cost Charges;	This pillar has yet to be consider by Council.	Staff suggest an affordable housing fund be established to cover the cost of waivers requested by affordable housing projects. This will be discussed in more detail below.

(b) Cash in lieu of 5% park dedication;		Staff suggest that the cash in lieu of 5% park dedication be determined on a case-by-case basis per the <i>Local Government Act</i> . This is collected through the subdivision approval process, which is not a Council process.
# 10 Rental tenure zoning with bonus density; and	This pillar has yet to be considered by Council.	Staff does not recommend this option currently. However, others described below are suggested as an alternative.
#11 Expediting 'non-market' housing applications.	This pillar has yet to be considered by Council.	Move to a development procedures review project. Staff to prepare a business case for Council to consider.

In this following section, staff is providing options that shift away from the pillars which were not endorsed. Instead, staff presents ways to integrate affordability into the City of White Rocks' current development processes. There are no suggested zoning or density changes. Rather, existing policies are updates using tools and strategies that create a White Rock focused housing strategy.

These tools are being presented to the committee to facilitate a conversation about what they feel is the best way to achieve long-term affordable housing that works for the White Rock community. The goal is for the HAC to consider these in the White Rock context and provide recommendations to Council.

Affordability as a Long-term Focus in White Rock

Staff have provided a detailed series of options for the committee to review. The goal is to create ways to integrate affordable housing into the framework of the City's development approval process. Because municipal resources are limited, allocating them effectively and efficiently is critical. Most experts agree that municipalities have an essential role in facilitating housing supply and preservation, including affordable housing. However, affordable housing is not a single building. Rather ensuring the Local Government is putting long-term plans and tools in place for funding, partnership, and land acquisition.

According to a 2011 survey, Metro Vancouver municipalities have responded to the region's housing affordability and diversity challenges by adopting over 270 regulatory, fiscal, planning, approval process, rental loss prevention, education/advocacy, and direct provision measures.

Housing is not a primary responsibility of municipalities; however, local governments have an essential leadership role in planning and facilitating affordable housing. The range of measures local governments can use to facilitate and develop affordable housing are outlined below:

- Fiscal measures designed to improve the economics of housing production, such as direct funding, provision of city-owned land, and relief from fees and charges.
- Regulatory measures using the planning and development control process encourage and increase the housing supply.
- Education and advocacy measures build community awareness and support for affordable housing, such as rental housing inventories, guides for developers and advocacy for increased senior government support.
- Direct service provision through a housing corporation that provides housing and supports to low- and moderate-income households.

The section below is adapted from *A Scan of Leading Practices in Affordable Housing* by B.C. Housing Research Center. The tools are split into three categories which are considered keys to success: **Municipal Tools, Partnering for Land, Financing and Capacity for Organizations and the Community.**

Municipal Tools

Inclusionary Zoning and Density Bonus Policy
Both these tools seek to add affordable housing through new development. Inclusionary zoning means zoning regulations requiring an applicant to contribute to below-market housing units (directly through building or through funding) triggered as part of a rezoning for development. A density bonus policy is an incentive that allows increased development potential if affordable housing is included. The number of affordable units created is often based on a percentage of market units (e.g., 10-20%) built, space created, or in some cases, the amount of new employment driven by the development.
Actors: Local governments and developers
Partners: Housing organization or non-profit
Ease of implementation: Simple
Speed of delivering housing: Dependent on development demand
Implementation Process: <ul style="list-style-type: none"> - Ensure community buy-in for affordable housing - Develop staff and Council's capacity to put forward a strong policy - Requires new development and a market that can absorb possible minor additional costs - Ensure community buy-in for affordable housing - Policy Change - Develop staff and Council's capacity to put forward a strong policy

Example:

Langford, BC: Affordable Housing Program

- Housing prices in Langford are relatively affordable compared to the rest of the region, which has made it an attractive location for new development
- Concerned about rising costs, the City introduced Langford's Affordable Housing Program requiring new subdivisions to build one affordable unit for every 10 single-family lots

- Qualified purchasers must be at least two people with a household income under \$60,000 and have been employed in the city for ½ a year and or lived in Langford for two years
- Home has a price cap for five years that increases slightly after five years
- The City also has a \$500 housing fund contribution policy for every new dwelling
- The City manages the sale and buying process
- The number of units in the program: 30

The City of Richmond, Low End of Market Rental (LEMR) Contribution Program

- Developed a Low End of Market Rental (LEMR) Contribution program (Inclusionary Zoning and Density Bonus Policy Tool).
- LEMR units are secured as affordable in perpetuity through a legal agreement on title, which restricts the maximum rents and tenant eligibility by income. Developers provide a cash-in-lieu contribution when the threshold for built LEMR units is not met.
- The City of Richmond currently uses this, which has been very successful. The program secures a floor area allocation of 10% in a multi-family condo development with more than 60 units.
- The LEMR program also secures cash-in-lieu contributions for rezoning applications with 60 fewer units. These contributions are directed to the City's affordable housing reserve and are to provide financial support for stand-alone affordable housing developments.
- The current cash-in-lieu rates are the following:
 - Detached homes: \$4 per square foot
 - Townhouses: \$8.50 per square foot
 - Wood-frame multi-family development \$10 per square foot; and
 - Concerte multi-family developments \$14 per square foot
- Other cities have similar programs, but the City of Richmond is the only program that secures either cash-in-lieu contributions or units in all residential rezoning applications and all city areas.
- With the program over the years, the City has secured more than 1,500 affordable housing units and \$49 million in cash-in-lieu and value transfer contributions.
- As part of developing this policy, staff would recommend a commissioning an economic consultant to assess the feasibility of a program like this in White Rock based on the current market conditions.

Reducing costs by streamlining approvals and other incentives
As planning and approval processes can add to the cost of developing housing, it makes sense that streamlining these processes for affordable housing projects will reduce costs for those developing housing. Other incentives to encourage development may include funding support for secondary suite development or different types of land intensification. Some techniques for lower approval costs include: 'one-stop shopping' for builders and residents or priority placement in permitting queues.
Actors: Local Governments
Partners: Developer, Builders
Ease of implementation: Simple
Speed of delivering housing: Moderate
Implementation Process:
- Consultation

- Policy and procedure changes
- Communication of tools
- Ensure appropriate training for staff and builders about the process and importance of affordable housing
- Use other tools such as covenants to ensure housing stays affordable and occupied
- Ensure there is a local government commitment and culture prioritizing the construction of affordable housing

Examples:

Kamloops: Affordable Housing Developers Package, Grants and DCC exemptions

- The goal is to speed up affordable housing projects.
- The affordable housing reserve fund offers up to \$5,000 per unit, up to \$150,000 in total.
- DCC exemptions vary, but up to 100% are eligible for rental.
- Downtown revitalization tax exemptions for a downtown multi-family rental project, up to 100% for ten years.
- Other requirements: must be affordable, along with a housing agreement with the City guaranteeing affordability.

Saskatoon: Priority review of housing applications

- Process proposals as soon as they are received, staying at the front of the line as they circulate through various departments.
- Quality is maintained, but the proposals jump the queue.
- Impact: A total of 500 new units since 2008, and this process was one of several that led to this success
- Other policy tie-in: Must be a project approved under the municipality's affordable housing programs (City-owned land for affordable housing, non-profit rental housing property tax abatement, capital funding support for affordable housing) to ensure long-term affordable housing.

Covenant tools
The <i>Community Charter</i> allows the use of covenants registered on the land title of properties. These covenants (essentially contracts) can restrict what an owner can do on the lands and/or allow or restrict an activity to benefit the local or provincial government. For example, in an affordable housing situation a covenant may be used in a housing agreement to restrict who can live on a property and how much the property can be sold or rented for, thereby keeping a home perpetually affordable for future owners. The covenant can also include a listing of fines and other tools to ensure compliance and long-term affordable housing. Development agreement covenants are used to ensure the benefit of affordable housing is provided as part of a rezoning process.
Actors: Local Government
Partners: Developers, property owners, housing organizations
Ease of Implementing: complex, though good examples exist
Speed of delivering housing: Extra restrictions may slow down the initial development/uptake of affordable housing, but it will preserve affordability in the end.

Implementation Process:

- A development agreement generally requires a rezoning process in which an agreement for benefits relating to the development can be negotiated.
- A housing agreement needs to be applied, which generally needs to be considered in the rezoning and again at the times of sale for the property owner to agree to the terms.
- Development agreements tend to require a demand for development and rezoning in the community. Education is often required for property purchasers and agents about covenant details and penalties.
- Providing an opportunity in the agreement for a housing organization or municipality to purchase.
- Units upon sale allow to preserve the opportunity to adjust the covenant as required in the future.
 - o First right of refusal for the municipality to purchase ownership housing when it is sold – to ensure control over the longer term as well as the ability to adjust covenants as required.

Examples:*Whistler: Housing Covenants*

- Whistler has had a long history with housing agreement covenants on title starting in 1982 and continuing today.
- Covenants evolved over this time to ensure the appropriate benefits were delivered to the community; other tools to control the use of property included land leases.
- The majority of the 2,000 dwellings of Whistler-managed affordable housing has covenants on title.
- Whistler also used development agreements to ensure the inclusion of affordable housing by developers.
- Critical to the program's success is a housing organization, the Whistler Housing Authority, which manages the buying process, waitlists and development, and access to low-cost land through acquisitions and inclusionary zoning.
- Key ingredients to include in housing agreement covenants for ownership housing:
 - o Resale and rental price restrictions - set and primarily limited to the CORE inflation index.
 - o Description of the resale process that requires sales to the municipality or people on the housing waitlist managed by a housing organization.
 - o Occupancy restrictions on who can live in the unit – targeted at employees (20hrs/week), retirees and dependents.
 - o First right of refusal for the municipality to purchase ownership housing when it is sold – to ensure control over the longer term as well as the ability to adjust covenants as required.
- Breach of covenant penalties up to \$500/day.
- Other considerations to include:
 - o Rental duration limits on ownership units.
 - o Provisions for capital improvements.

Partnerships – Land & Financing

Municipal land and land trusts
Access to low-cost land for affordable housing, whether private infill or larger parcels of brownfield or underutilized land, is critical. This approach considers land that is acquired by a municipality or a non-profit organization (e.g., a land trust) at a low cost to hold for the benefit of affordable housing. Property for affordable housing is typically acquired through donations or grants of land from sources such as the provincial government. Beyond land donations, the servicing of land can impede affordable housing, so land donations with servicing or land that can be accessed easily for servicing are especially valuable. In addition to municipally held land developed by the municipality or other partners, land trusts are one arrangement of land ownership that is underpinned by an organization with a built-in desire to create permanently affordable housing. Land trusts often maintain ownership of the land while making it available for housing through land lease or housing rental agreements to ensure long-term control of the land. Though effective, community land trusts are not as popular yet in Canada as in other international jurisdictions.
Actors: Local Government, Community Land Trust, Land Owners
Partners: Financiers/Senior Governments, Housing Organizations, Co-op Housing Groups & Developers
Ease of Implementing: Moderately complex
Speed of delivering housing: Moderate to slow
Implementation Process: <ul style="list-style-type: none"> - Land trust organization development in the case of a using a Land Trust. - Donation or acquisition of land under conditions to be used for affordable housing. - Release of land through a lease or arrangement to a third party to develop the land for affordable housing. - Regularly discuss with the community and senior governments about the desire for land; proactively seek land for potential acquisition. - Ensure capacity building for strong housing organizations and/or land trust groups. - Reduce the need for servicing costs.

Example:

Fraserview Housing Co-op, Vancouver

- The City owns the land, providing 99-year leases on four sites to the Vancouver Community Land Trust Organization.
- The foundation is working with co-op and non-profit housing providers to develop the housing, including 278 units for moderate to low-income families and singles.
- Targeting 76% of market rents across the four properties.
- Housing organizations are investing \$5 million of their own equity to make it more affordable; B.C. Housing invests \$4 million of equity and more than \$90 million in construction financing.
- The Land Trust is also using private equity from New Market Funds, a social impact investment firm targeting housing.
- Revenue generated via the projects will return to operating housing organizations and

maintenance.

Use City Land for Affordable Housing

- Federal and provincial investment in affordable housing is primarily directed toward partnership projects, and municipalities are increasingly encouraged to provide municipally-owned land to support these projects.
- This policy aims to ensure that the City continues to acquire land for affordable housing and is made available to capitalize on potential partnership opportunities for the development of new affordable housing projects.
- Affordable housing units can include a variety of rents, including non-market rate rents, rents at income-assistance levels, low-end market rents, or a mix to encourage cross-subsidization within a development.

Non-profit owned land
Non-profit (NPO) organizations or faith-based groups own some land in communities. These organizations may be able to make land available for housing through low-cost, long-term leases, donating land or providing the land at below market value. In each case, the housing is made more affordable by separating the cost of the buildings from the cost of the land, and subsequently reducing or eliminating the latter. Similarly, the non-profit may be able to develop their land, if they have the capacity to do so; partnerships in this case can be very useful.
Actors: Non-profit groups, community organizations with land assets
Partners: Developers, Builders, Housing Organizations
Ease of Implementing: Moderately Complex
Speed of delivering housing: Moderate to slow, due to multiple levels of decision making
Implementation Process: <ul style="list-style-type: none"> - A church or NPO considers its mission and needs and then determines if housing is a fit or a route to other goals, including revenue generation. - NPO approaches a developer and potentially a housing organization to determine models to apply to the government for rezoning. - Build and deliver housing using many of the common affordability approaches. - The pressure to sell prime real estate is strong, so innovation is needed to meet the organization's financial needs and community affordable housing goals. - Many effective partnerships are required in order to fill the skill gaps. - Where land is not as valuable/costly, consider subdividing for a development lot while maintaining original structures.

Example:

Oakridge Lutheran Church, Vancouver

- Redeveloping the property into a six-story mixed-use building with retail at grade, the Church and community space on the second floor and four levels of affordable housing above.

- Includes a partnership with a non-profit real estate developer who partners specifically with community organizations to develop real estate, much of which includes affordable housing.
- The property is along a busy transportation route to reduce transportation needs and includes car-share opportunities and ample bike parking.
- The Church is moving temporarily to a nearby Church while the development is taking place.
- City affordable housing incentives: requirements for parking are relaxed, development costs are waived somewhat, a density bonus is allowed, approval processes are streamlined.
- Other tools: Housing agreement requirements to initiate starting rents.

Housing Funds
There are several mechanisms for municipalities to raise funds for affordable housing, and it is important that those funds are aggregated into a Housing Fund set up by a municipality, regional government, or housing organization. For example, funding can come from property taxes, works and service charges for new development or from cash-in-lieu contributions from developers using a density bonus or rezoning agreement.
Actors: Local government
Partners: Housing organizations, developers, or property owners
Ease of Implementing: Simple
Speed of delivering housing: Depends on the capacity of organizations to spend it on housing.
Implementation Process: <ul style="list-style-type: none"> - Identify possible sources of funding. - Set up a housing fund at the municipality. - Establish a process for administering the fund to housing projects. - Requires an organization/person to manage and invest the funds in affordable housing developments. - Requires a strong communication program to taxpayers if funding is to come from general revenues or levies. - Requires an organization/person to manage and invest the funds in affordable housing developments. - Requires a strong communication program to taxpayers if funding is to come from general revenues or levies.

Example:

Whistler, BC: Housing Fund

- Whistler developed a unique trust fund approach to providing financing for the construction of affordable housing in 1990.
- The fund is contributed to through levies (Employee Works and Service Charge) placed on developments that increase the number of employees in the community.
- Funds are provided to the Whistler Housing Authority organization to help deliver affordable housing.

- The amount of the Charge is \$5,908/employee, but as it is significantly lower than what is needed to develop housing, the Resort Municipality of Whistler is seeking to increase it.
- The fund was critical to leveraging more in bank loans and launching the first housing rental projects in Whistler.
- Other critical tools included land donations, occupancy restrictions on who can rent units, and rent controls.
- Whistler now has over 2,000 dwellings of ownership and affordable rental housing.

City of Coquitlam, Affordable Housing Reserve Fund

- The Affordable Housing Reserve Fund (AHRF) is an important tool for contributing to affordable housing solutions.
- The AHRF acts as the City's financial contribution towards increasing the supply of housing options for low and low-to-moderate income households.
- The AFHR Policy would set out guidelines for allocating and distributing funds to projects that include providing affordable housing units.
- The goal would be to establish the terms of reference for the fund and how it should be allocated. For example, the AHRF funds can be used for the following:
 - a) The acquisition of land by the city for affordable housing projects;
 - b) The leasing of land at below-market rates for affordable housing projects.
 - c) To offset development application and building permit fees associated with a qualifying project; and
 - d) To offset Development Cost Charges or any other amenity-related contributions to which the project would be subject.
- Applicants or organizations in White Rock could request money from the fund for grants from Council. Below is an example of the City of Coquitlam funding levels. They have developed a set of criteria that guides the decision-making process.

Support Level	Criteria Required	Potential Range	Structure
TIER 1	1. Target population 2. Community fit 3. Funding viability 4. Fund eligibility 5. City contribution limit	\$10,000 - \$20,000 per door	Grant
TIER 2	1. Target population 2. Community fit 3. Funding viability 4. Fund eligibility City contribution limit; and 5. High Degree of Leverage; or	\$20,000 - \$50,000 per door	Conditional grant (pay back requirement decreases 1/25th over 25 years of operation)

	6. Break the cycle model		
TIER 3	1. Targeted population 2. Community fit 3. Fund viability 4. Fund eligibility 5. City contribution limit 6. High Degree of Leverage; and 7. Break the cycle model 8. Fund sustainability	Asset based >\$50,000 per door	Land lease, air-space parcel, or land purchase partnership

Criteria To Guide Decision-Making

- Appropriateness for Target Group: how the project will improve overall affordability for low and low-to-moderate income households;
- Community Fit: the projects fits within the surrounding community;
- Fund Viability: Council cannot commit funds from the AHRF beyond the current available balance;
- Eligibility: units must be secured through a Housing Agreement with a non-profit society;
- City Contribution: cannot exceed a Provincial contribution amount;
- Amount of Leverage: degree of involvement of other funders;
- Breaking the Cycle: the project provides an opportunity for greater self-sufficiency for the populations it serves;
- Fund Sustainability: the project's ability to pay back the AHRF in whole or in part.

Contributions Gained from Density Bonusing (CACs)

A crucial part of establishing the fund would be to understand how to use contributions gained from density bonusing (CACs) can be allocated to the fund. For example, the City of Richmond applies the following rates to rezoning applications that request a higher density:

- \$2 per square for single-family subdivision developments
- \$4 per square foot from townhome developments
- \$6 per square foot from apartment and mixed-use developments involving 80 or less residential units

Develop Financial Incentives Criteria

- Under municipal jurisdiction, the City can waive development cost charges and municipal planning and permit fees for affordable housing developments.

- Some municipalities offer property tax exemptions for non-market/social housing developments. The goal of this is to minimize the costs associated with affordable housing development for non-profit housing providers.
- Work with the committee to develop policy with criteria that the project must meet to be exempt from paying these costs.

Capacity Building - Organizations & The Community

Housing organization and building capacity
A housing organization is a non-profit entity dedicated to providing and managing non-market housing stock that is for rent or purchase by qualified individuals and families. In addition to being a repository for affordable housing units, a housing organization can house expert advisors on affordable housing, ongoing champions for affordable housing, and in some cases leverage expert skills and know-how to support other needs such as property management. Housing organizations can also serve specific projects, a municipality, or a greater region.
Actors: Non-profit organizations or Local government
Partners: Local government
Ease of Implementing: Moderate
Impact on developing housing: Critical to help manage funds and/or development
Implementation Process: <ul style="list-style-type: none"> - Establish the organization with the right members - Secure seed funding - Create the business plan - Acquire units and manage projects and activities - Continue to manage the activities of the organization, including ongoing professional development for staff and Board members - Organizations work with the local government to receive seed funding and quickly develop projects to help fund the organization from rental income and sales - Consider a regional organization to generate enough capital from multiple communities - Create the organization while the local government is establishing housing units and funding mechanisms

Examples

Lower Columbia Affordable Housing Society (Society), Trail Project

- The Society was born in 2013 out of an Attainable Housing Committee of the Lower Columbia Community Development Team Society.
- The Society was challenged to find the right fit for rentals by purchasing existing homes, so they worked to develop their own housing.
- The Society purchased a lot near a walking route that will host a home with two single-bedroom and two two-bedroom apartments.
- When complete, a total of nine dwellings for rentals will be available.
- Funding was accessed primarily through a Columbia Basin Trust and B.C. Housing partnership and the Federal Government, along with some funding from the Kootenay Savings Credit Union and Teck Metals.

Whistler Housing Authority (WHA)

- Created in 1997 to oversee and assist the development of resident-restricted housing.
- Is a wholly-owned subsidiary of the Resort Municipality of Whistler, governed by a board of directors.
- Oversees the development, administration, and management of resident-restricted housing in Whistler.
- Initially used \$6M collected by the municipality in development levies through its Employee Housing Service Charge bylaw, plus \$13M in bank loans to purchase and develop close to \$22 million worth of real estate.
- Currently, WHA owns or oversees 1900 units of affordable rental (46%) and ownership (54%) housing, with another 69 units (179 beds), including a new 25-unit passive house rental apartment building, to be built in the next two years.
- Rental fees cover mortgage debt, fund the property management and capital replacement reserves, and cover WHA operations.
- Covenants on property titles restrict WHA housing to people working at least 20 hours/week, retirees, and dependents, and on resale, rental, and rental prices.

Banff Housing Corporation (BHC)

- The BHC is an arm's length non-profit organization of the Town of Banff and is involved with 182 ownership units and 45 suites in those units. Board experience required in the areas of non-profit housing, real estate, property development, property management, building and development, staff accommodation/housing asset management/non-profit housing, accounting/finance
- Board is supported with training throughout the year

Housing Strategy
An affordable housing strategy is a document, usually commissioned by local government, which recognizes and quantifies an affordable housing shortage (housing needs assessment) in a given jurisdiction and then recommends a series of approaches to reduce the shortage. Communities that have undertaken such strategies have generally been more proactive in addressing the shortage. Other than providing a good sense of the problems and solutions, developing a housing strategy with a range of stakeholders and community members helps to develop the relationships, roles, partnerships, and trust that will ultimately help in the delivery of affordable housing.
Actors: Local Government, Housing Organization
Partners: Developers, Builders, Non-profit organizations, Real Estate Agents, Housing Consultants
Ease of Implementing: Moderate
Speed of delivering housing: Important to provide a road map
Implementation Process: <ul style="list-style-type: none"> - Identify partners and stakeholders to engage - Assess the need for housing

- Inventory resources and assets and policies, as well as sites and locations for development
- Prioritize strategies
- Assign responsibilities/roles and ongoing resourcing for the strategy implementation
- Annual review
- Consider a needs assessment before developing a strategy to assess the urgency
- Ensure equal amount of time and resources is spent to identify how to fund and implement the strategy

Examples:

Fernie, BC

- Fernie created its first housing strategy in 2007 after the completion of two earlier reports to describe the housing challenges and needs in Fernie
- The strategy identified 'low-income individuals and families' as the top priority for housing
- Opening in 2012, Veneto Place was B.C. Housing's first public-private partnership offering 45 units with market, below market rental, rent to own and attainable ownership options
- Partners included: CMHC, Parastone Developments (private developer), Columbia Basin Trust, BC Housing, City of Fernie, and Fernie Family Housing Society
- Seed funding from CMHC helped to carry out market research, financial feasibility, and preliminary design with site work
- Fernie is now updating their housing strategy
- Energy efficiency techniques were embedded in the building

Smithers, BC

- In 2010 a Smithers Housing Report was completed by a local housing task force
- The report focused on a snapshot of local housing needs and opportunities while outlining roles of various groups in providing housing
- The Smithers Community Services Association bought a four-lot property in walking distance of downtown/amenities to place six houses on
- The main affordability feature of the houses was its size, at about 540 sq ft, to keep utility costs low
- Partners included: CMHC for seed funding
- The properties opened as rentals in 2015 and the rents cover the servicing on the mortgage with extra for incidentals to cover the Association's cost of managing and providing housing

Develop a Market Rental Policy

- To protect the supply of market rental housing units in the city, support existing tenants at the time of redevelopment and encourage the development of new market rental housing.
- Overall objectives:

- Encourage property owners to maintain existing market rental buildings in good repair.
- Secure replacement and new market rental housing in perpetuity through legal agreements.
- Ensure that new multiple-family, market rental developments provide a minimum of 40% of units with two or more bedrooms so they are suitable for families with children (e.g. "family-friendly" units).
- Provides detailed parameters for redeveloping existing market rental housing
- Strata conversions
- Incentives for new market rental housing

Engagement and Communication
Generating support for affordable housing planning, development and operation activities is critical to successful delivery. Engagement and communication to generate support starts with creating a core group of individuals and organizations and developing a housing needs assessment, and then to the continuous communication required to foster more affordable housing units. Good engagement and communication brings important stakeholders along for the affordable housing planning and development journey and helps to make them promoters of affordable housing, but it can be easier said than done. Activities might include: community consultation – providing and receiving feedback; communication – informing and building momentum for your projects; education – bringing resources and expertise to the project from the community and from outside as needed; commitment – securing ongoing commitment to building and maintaining affordable housing..
Actors: Local Government, Housing Organization, Developers
Partners: Chambers of commerce, tourism organizations, housing consultants, engagement and communication consultants, affordable housing residents
Ease of Implementing: simple
Impact on development of housing: Critical to get support for housing
Implementation Process: <ul style="list-style-type: none"> - Hold a session to identify allies for affordable housing - Develop a communications and engagement plan - Execute the plan alongside other activities that build support for housing such as needs assessments - Carry engagement activities right through to the end of a development process and beyond - Set aside funding for engagement and communication - Focus on shared community goals and values - Nurture your current affordable home residents to be ambassadors - Start as early as possible and map out key timing and methods for engagement and communication such as forums, surveys, etc.

Examples

Canmore, AB: Livable Canmore, Quality Housing in Great Neighbourhoods for All

- The Town of Canmore, supported by most of the community, prioritized the development of affordable housing during the most recent term of government
- Two projects were creating some neighbourhood angst due to the development locations, amount of development and style of dwellings
- To ensure the silent majority was being heard in the process, the Town initiated an engagement and communication exercise to invite the community to talk about the different types of affordable housing and possible locations for it in the community
- Engagement activities included: one survey; five community 'Idea Walls/Talk to us' posters around the community, 10 citizen-led conversations about housing, two neighbourhood interactive events and two workshops involving housing experts and facilitated conversations, three background briefs to inform conversations and six housing situation stories to help people to understand the need for housing
- When completed, one site will have 49 units of perpetually affordable housing made up of a mix of townhouse, stacked townhouses and duplexes
- The project will be managed by the Canmore Community Housing Corporation

Enterprise Community Partners: Non-profit housing developer messaging recommendations

- Research was conducted for non-profit affordable housing developer, Enterprise Community Partners, on messages for affordable housing
- Some of the tips and approaches included:
 - o Linking success stories of housing to the policies and actors that made it happen
 - o Expanding the notion of who plays a role in the solutions, not just government but other organizations, individuals and champions
 - o Focus on building homes vs. housing/affordable housing
 - o Use simple explanations of cause and effect for why there is an affordable housing shortage and what can be done to encourage more homes
 - o Link the importance of a home to other issues like economic development and health

Next Step Options:

It is suggested that the HAC and staff use a combination of the tools and strategies above to re-align and focus on five strategic directions to build affordable housing into the City of White Rock development processes to create long-term capacity to provide a range of affordable housing options to the community.

6. Use the City's regulatory tools to encourage a diverse mix of housing types
 - a. **Inclusionary Zoning and Density Bonus Policy**
 - b. **Covenant tools**
 - c. **Contributions Gained from Density Bonusing (CACs)**
 - d. **Develop Financial Incentives Criteria**
7. Maximize use of City resources and financial tools
 - a. **Reducing costs by streamlining approvals and other incentives**
 - b. **Housing Fund**

- c. **Municipal land and land trusts**
- d. **Non-profit owned land**
- 8. Build capacity with non-profit housing and service providers
 - a. **Housing organization and building capacity**
- 9. Facilitate and strengthen partnership opportunities
 - a. **Housing Strategy**
- 10. Increase advocacy, awareness, and education roles
 - a. **Engagement and Communication**

Respectfully submitted,

A handwritten signature in black ink, appearing to be 'Alex Wallace', with a stylized 'A' and a long horizontal stroke.

Alex Wallace
Manager of Planning

Reviewed by the Director of Planning and Development Services

Appendix A: City of White Rock Housing Needs Report
Appendix B: 2010 City of White Rock Housing

THE CORPORATION OF THE
CITY OF WHITE ROCK
CORPORATE REPORT



DATE: November 22, 2021

TO: Mayor and Council

FROM: Greg Newman, Acting Director, Planning and Development Services

SUBJECT: City of White Rock Housing Needs Report (2021)

RECOMMENDATIONS

THAT Council:

1. Receive the “Housing Needs Report,” included as Appendix A to the corporate report titled “City of White Rock Housing Needs Report (2021)” as the first Housing Needs Report for the municipality, prepared pursuant to Division 22 of the *Local Government Act* and the requirements of British Columbia Regulation 90/2019; and
 2. Direct staff to publish a copy of the “Housing Needs Report” on the City’s webpage.
-

EXECUTIVE SUMMARY

This corporate report introduces the first “Housing Needs Report” for the City of White Rock. The Housing Needs Report (HNR) recognizes six (6) key areas of local housing need and includes a summary of demographic and housing-related data, as well as the results of a series of public engagement activities held between January and July 2021.

The six areas of local housing need recognized in the HNR include:

1. Affordable Ownership Housing;
2. Rental Housing;
3. Special Needs Housing;
4. Seniors Housing;
5. Family Housing; and
6. Shelters and Housing for People At-Risk of Homelessness.

Each of the areas of housing need are described further in this corporate report and the HNR itself. The Housing Needs Report includes the information required by the province through B.C. Regulation 90/2019. As outlined in the Regulation, completed HNRs are to be updated every five (5) years. Between each five-year period, municipalities are expected to pursue policy changes and other interventions that address areas of identified need, with the scheduled review allowing local governments, and the province, to measure the success of certain interventions. Should Council agree with the recommendation presented in this corporate report, the next step in this important planning work would be to evaluate strategies that could be used to address identified areas of need. The City’s Housing Advisory Committee has built into their 2022 Work Plan a

commitment to explore opportunities to support housing choice, and affordability, to address local needs and the overall quality of life for White Rock residents.

PREVIOUS COUNCIL DIRECTION

Motion # & Meeting Date	Motion Details
2021-156 March 15, 2021	THAT Council endorses the following as their top five (5) priorities: <ul style="list-style-type: none">• The Official Community Plan (OCP) Review• Solid Waste Pickup for Multi-Family• Housing Needs / Affordable Housing• Community Amenity Contribution (CAC) ‘shovel-in-the-ground’ projects• The City’s Relationship with Semiahmoo First Nation

The City’s Housing Advisory Committee (HAC) has been consulted throughout the preparation of the Housing Needs Report. Members of HAC provided valuable insight to staff and the City’s consultant as it relates to the experiences of White Rock residents and their housing needs. On October 26, 2021, the Housing Advisory Committee passed a motion endorsing the draft Housing Needs Report and the presentation of this work at the November 22, 2021 meeting of Council.

INTRODUCTION/BACKGROUND

In April, 2019, the Ministry of Municipal Affairs and Housing introduced amendments to the *Local Government Act* requiring that all municipalities complete housing needs reports for their communities by April, 2022, and that such reports be updated every five (5) years thereafter. The purpose of a housing needs report is to:

1. Enable the province to gain an understanding of changes in demographics and housing to plan for future housing needs;
2. Enable municipalities to better understand their current and future housing needs; and
3. Assist local governments in implementing policies and bylaws that respond to current and projected housing needs.

The first Housing Needs Report (HNR) for the City of White Rock is provided in Appendix A. The Housing Needs Report presents six (6) key areas of local housing need, including:

1. Affordable Ownership Housing;
2. Rental Housing;
3. Special Needs Housing;
4. Seniors Housing;
5. Family Housing; and
6. Shelters and Housing for People At-Risk of Homelessness.

Each of the above-listed areas of local housing needs are given greater merit in the following sections of this corporate report.

Evidence of Local Housing Needs

Areas of local housing need were identified through an evaluation of demographic and housing-related data and the trends observed between the 2006, 2011 and 2016 Census periods. This information is summarized in the “Part 1: Community and Housing Profile, White Rock,” included as Appendix A to the Housing Needs Report. The Profile was prepared for the City of

White Rock by Metro Vancouver and contains the data required for inclusion in a housing needs report as outlined by the Province through B.C. Regulation 90/2019.

As a complement to the Profile, White Rock staff prepared a Housing Needs Survey. The survey was launched via the City's "TalkWhiteRock" web platform (www.talkwhiterock.ca/housing) and was made available between January 2 and January 31, 2021. 406 responses to the survey were received. The following lists several of the key observations drawn from results of the survey. Appendix B to the Housing Needs Report includes all the responses to the survey.

- Majority of respondents (78%) identified as being 55 years of age or older;
- Majority of respondents (85%) indicated they own their home;
- Approximately 78% of respondents identified as living alone (117 respondents) or with a spouse/partner without children (205), 63 respondents (15%) noted living with their spouse and a child(ren), and 6 (1%) identified as a single parent living with a child(ren);
- The top three (3) housing challenges identified were:
 - the ability to afford future mortgage / rent payments;
 - the lack of parking available to the home; and
 - the need for repairs to the home.
- The top three (3) barriers to finding a home were:
 - Limited supply of the type of home sought;
 - Cost of a home purchase (and rent) was too high; and
 - Restrictions tied to strata limit access (e.g., age, no pets, no children, etc.)
- The top three factors sought when looking for a home were:
 - Cost of housing / price of home;
 - Type of home; and
 - Proximity to shops and services.

Two of the questions in the survey asked about the type of housing needed over the short and long term. As shown in Figure 1 below, over the short term (5 to 10 years), the majority of respondents (132 of 372, or 35%) provided that they would need an apartment, with the next highest need being a single detached home (26%), and supportive housing (15%). Over the longer term (20 to 30 years), most respondents (189 of 366, or 51%) referenced the need for supportive housing, followed by an apartment (29%) and a single detached home (16%).

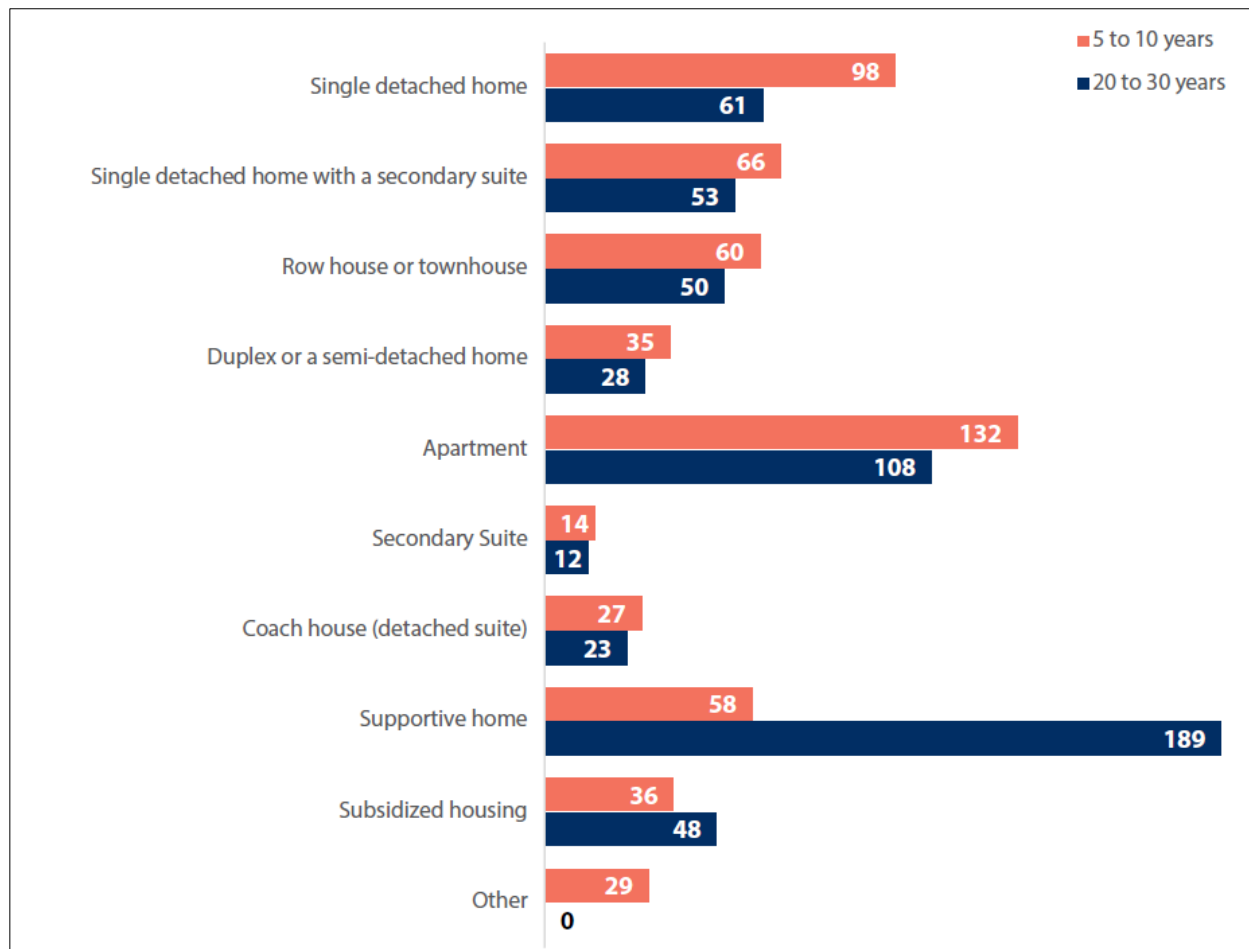


Figure 1: Survey Responses Regarding Short- and Long-Term Housing Needs (Housing Needs Report, 2021)

The increase in those identifying the need for housing supports, over the long term, may be attributable to the age of those completing the survey. White Rock is, however, comprised of an older demographic and therefore the findings of the survey are believed to be reflective of a real need to focus on increasing the services and supports available to seniors within the market.

Following the completion of the Housing Needs Survey, CitySpaces Consulting was retained to assist staff in carrying out broader community engagement regarding housing needs in White Rock. Between April and June, 2021, CitySpaces led a series of engagement activities including:

- a virtual public open house;
- renters housing forum; and
- four key stakeholder workshops.

Figure 2 that follows provides a summary of the engagement activity led by CitySpaces.



Figure 2: Engagement Activities "At-A-Glance"

As noted in Figure 2, CitySpaces carried out 12 “lived experience” interviews. These interviews allowed for more personal conversations with those identifying as: a low-income household, single-parent household, senior, person with a disability, newcomer to Canada, person who has suffered from homelessness, or a person who has suffered from substance abuse. The feedback received through the consultation phase of this project is summarized in an “Engagement Summary Report.” This report is included as Appendix C to the Housing Needs Report.

Areas of Identified Local Housing Need

The demographic and housing-related data summarized by Metro Vancouver has been reviewed against the results of the Housing Needs Survey and the findings of community engagement to identify six (6) areas of local housing need. These areas of need are described in detail below.

1. Affordable Ownership Housing

Metro Vancouver is recognized as having some of the highest home prices to household income in North America. Generally, home ownership is considered “affordable” if a household earning the median household income can purchase a home with ten percent (10%) down, a 25-year amortization period, and the resultant costs being no greater than 30 percent of the household income. With a median household income in of \$62,344 (2016 Census for White Rock), an affordable home would need to be no more than \$350,000. More recent analyses of housing affordability recognize the need for first-time home buyers to put 20 percent (20%) down and to include other “non-negotiable” shelter costs in the equation (i.e., utility costs (heat), strata fees, and internet services). Figure 3 illustrates the price of homes in White Rock and South Surrey, as identified by the Greater Vancouver Real Estate Board.

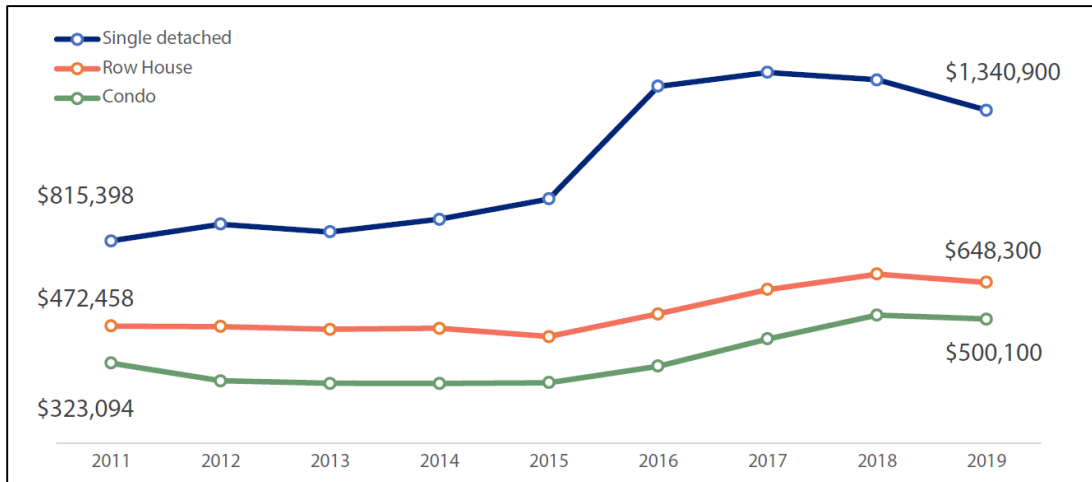


Figure 3: Sales Price, White Rock + South Surrey, Greater Vancouver Real Estate Board (2013 to 2019)

Setting sales prices against the affordability threshold for White Rock demonstrates the continuing decline in household affordability across the majority forms of housing (see Figure 4). The declining supply of affordable home ownership options in White Rock may result in people moving outside of the community to purchase a home or seeking options for local rental housing. As discussed in the next section, the limited availability of rental housing may be further stressed with a decline in local home ownership opportunities.

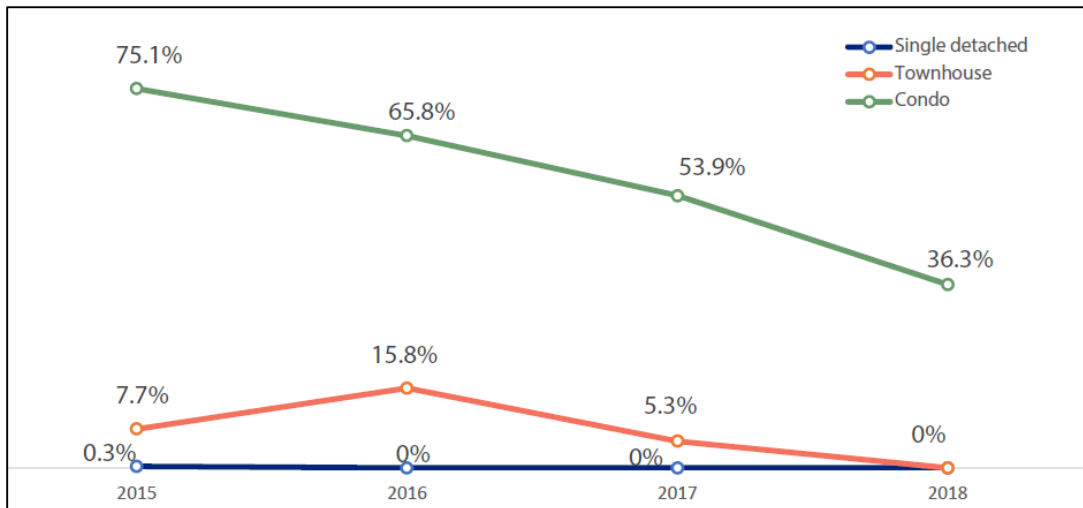


Figure 4: Affordable Sales by Structure Type, White Rock (2015 to 2018)

2. Rental Housing

As outlined in the 2016 Census, approximately 32% of the 10,005 households in White Rock are rental in tenure. Figure 5 identifies housing completions between 2011 and 2019 and recognizes both new rental construction and unit demolitions. Although the figure identifies a modest supply of new rental housing, it is understood that much of this housing is being brought into the community concurrent with the demolition of rental units. The overall availability of rental housing in White Rock has remained stagnant over the past decade with 3,210 units reported in the 2006 Census and 3,210 units reported in 2016.

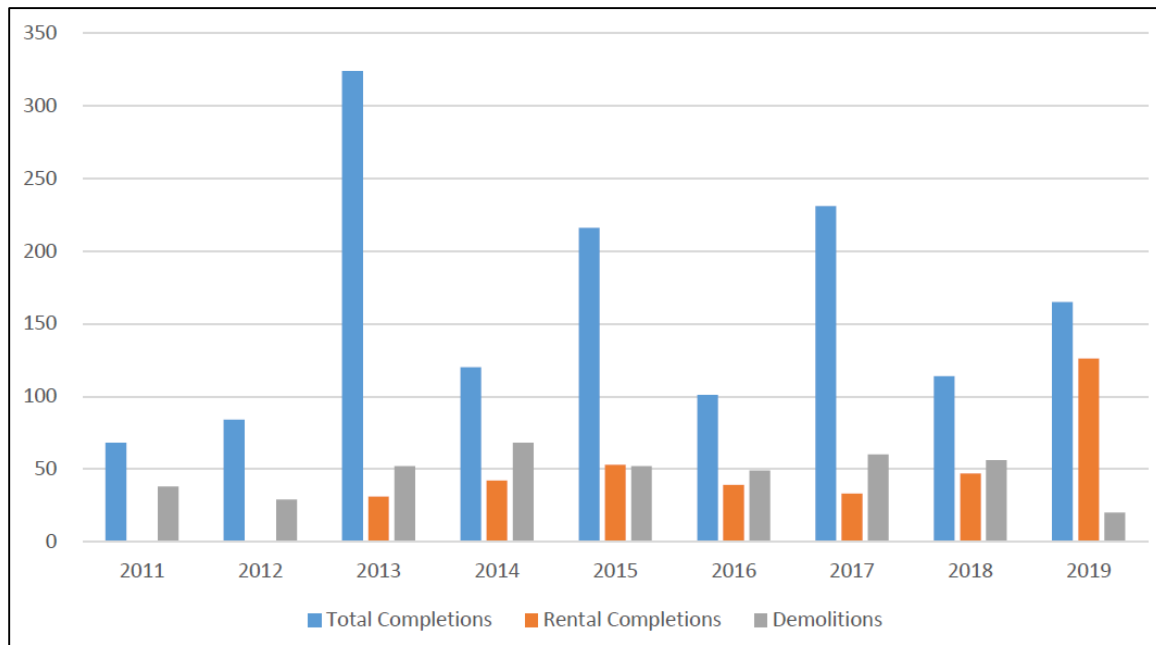


Figure 5: Housing Completions in White Rock, 2011 to 2019 (Canada Mortgage and Housing Corporation)

With a limited net new supply of rental housing, it is not surprising to see low overall rates of rental vacancy in the White Rock market. Generally, a “healthy” vacancy rate is considered to be between one and three percent. When rates fall below this level prices can rise placing additional strain on the market. The Canada Mortgage and Housing Corporation (CMHC) publishes an annual Rental Market Report. The Report identifies the supply of rental units within the “primary rental market” and the vacancy rate within this market. Figure 6 below illustrates the rental vacancy rates over the past decade as published by CMHC.

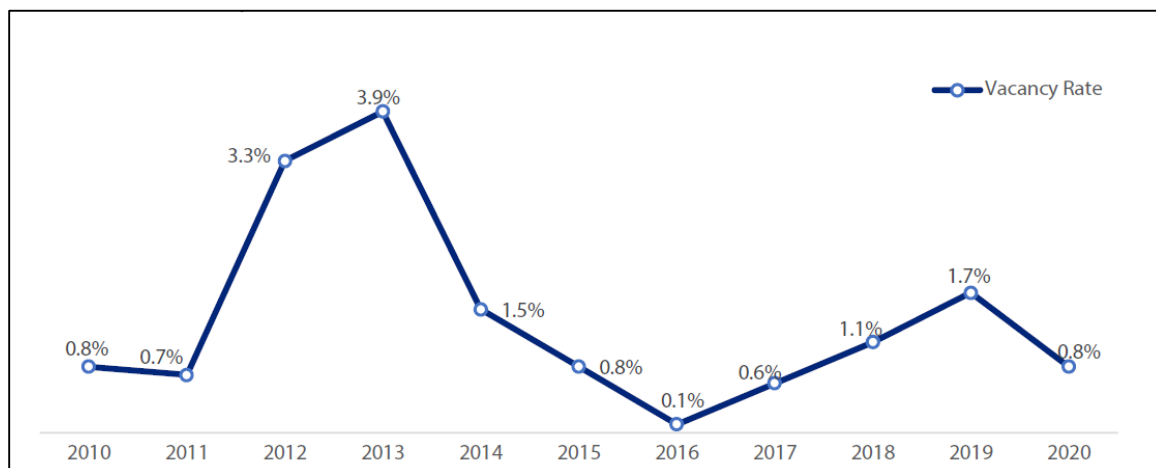


Figure 6: CMHC Rental Market Report (Rental Vacancy Rate, 2010 – 2020) – Entire Rental Universe

The most recent CMHC Rental Market Report was published in October, 2021. The Report provides that rental vacancy in October 2020 was highest within bachelor rental suites (1.3%), followed by one-bedroom units (1.0%), two-bedroom units (0.3%), and three-bedroom units (0.0%). The data is recognized as being “excellent” in terms of its reliability. For those who cannot afford to purchase a home and need housing with two or three bedrooms, the rental market data suggests that their ability to find accommodation in White Rock is significantly limited. The result of a lack of rental housing supply has caused an increase in rental rates for all sizes of rental dwelling unit in the City (see Figure 7).

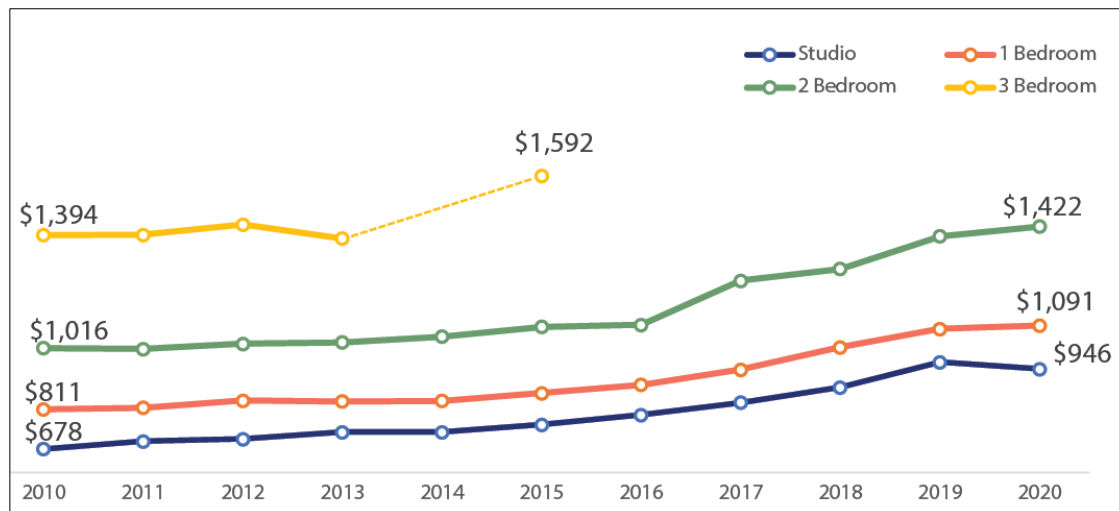


Figure 7: Rental Rates, White Rock (CMHC Market Reports)

In order to address the lack of rental housing supply there is a need to support rental housing construction and, importantly, the construction of more two and three-bedroom rental units.

3. Special Needs Housing

The following is taken directly from the Housing Needs Report and is reflective of feedback received through engagement with White Rock residents and local service providers.

“Special needs housing, particularly accessible units, is needed in White Rock. This includes wheelchair accessible units, units that can accommodate mobility aids, and adaptable units. There is an opportunity to incorporate a high standard of livability given the aging population in White Rock and persons with disabilities or who have experienced mental health challenges. Drawing from what was heard during engagement, suggestions from the community that could make units accessible and livable in these circumstances include design features (elevators, ramps, wide corridors), quiet and calm places, low stimulating environments, and access to natural light, fresh air, and nature.”

Policy 11.1.2 in the City’s Official Community Plan looks to support Age-Friendly Housing for People with Disabilities and promotes “age-friendly measures that respond to the needs of older individuals and people with disabilities by:

- a) Developing design criteria for accessible units and establishing a minimum number of units required to be accessible in new developments; and
- b) Reducing parking requirements for dwelling units that are secured by a housing agreement for occupancy by persons with disabilities”

Moving forward, additional policy measures and incentives could be used to achieve greater supports for those with special needs.

4. Seniors Housing

The City of White Rock is comprised of a higher proportion of those aged 50 and older when compared with related age-cohort proportions found in the region. As our residents age, it is anticipated that their housing needs will change. The results of the Housing Needs Survey provide that over the next decade there will be a demand for more apartment units, perhaps necessary to accommodate those choosing to downsize from a single-family home. Furthermore, over the longer, 20-to-30-year horizon, participants in the survey acknowledged the need for seniors housing with supports. For those who have the financial resources available to support “independent” seniors living, where special services are provided by the

operator, data published by CMHC provides that there is currently a healthy supply of such housing. As illustrated in Figure 8 below, rental vacancy within senior's independent living units in the South Surrey / White Rock market area falls between 5 and 20%.

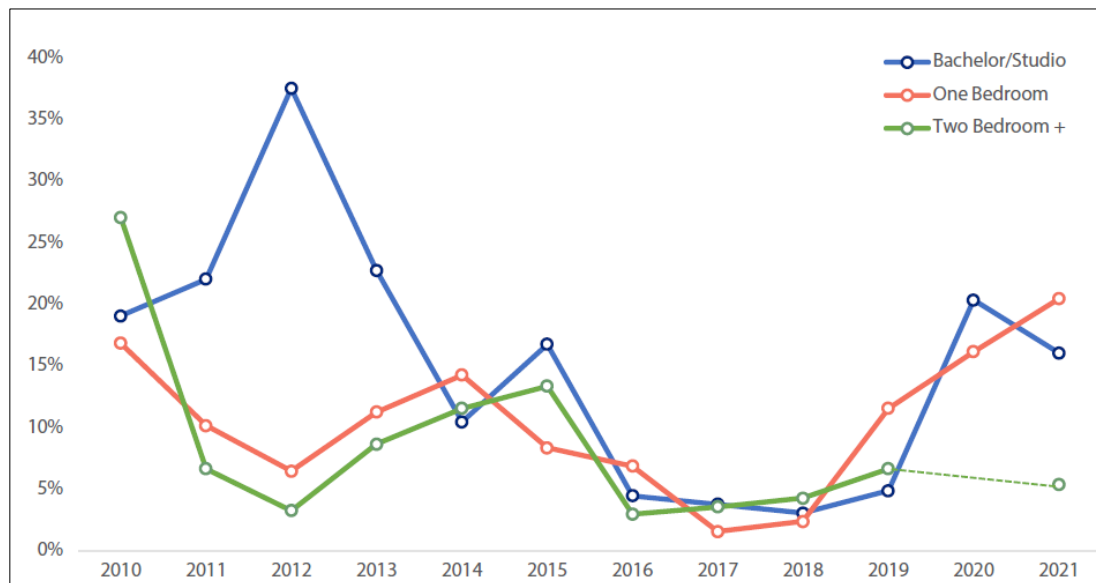


Figure 8: Independent Seniors Housing Vacancy, White Rock and South Surrey (CMHC)

The rental rates associated with independent seniors living are outlined in Figure 9.

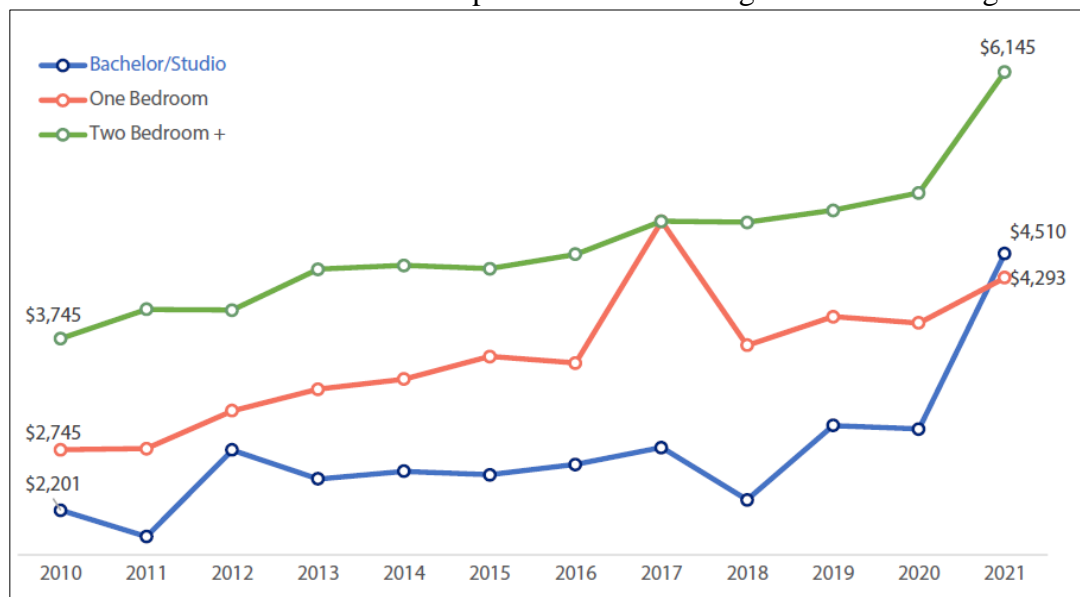


Figure 9: Independent Seniors Housing Rental Rates, White Rock and South Surrey (CMHC)

For many, it is anticipated that rental rates tied to independent seniors housing opportunities may be financially out of reach. In the absence of affordable market rental options, seniors may need to rely on supports provided through agencies like BC Housing.

BC Housing collects data on households that have applied for social housing in Metro Vancouver through the Housing Registry, a centralized database for those non-profit housing providers that have chosen to participate. The waitlist tracks applicant households by municipality across the region, as well as by specific characteristics including family or single person households, seniors, persons with disabilities and households needing

wheelchair access. Figure 10 provides a breakdown of those on BC Housing’s non-market housing wait list. The Figure clearly illustrates the high proportion of seniors waiting for suitable housing options. As the White Rock population ages there will be a growing need to look at housing equipped to accommodate the needs of seniors and the affordability thresholds that can be borne by residents requiring access such housing.

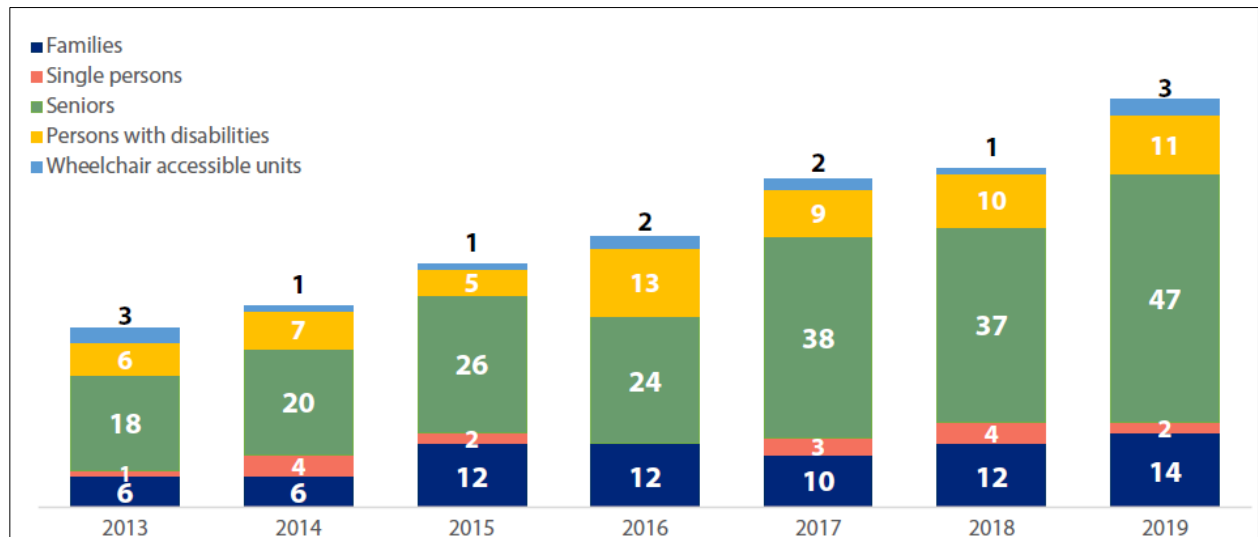


Figure 10: BC Housing Wait List for Non-Market Housing

5. Family Housing

2016 Census data provided in Appendix A to the Housing Needs Report recognizes the median value of households in White Rock based on the number of bedrooms (see Table 1).

Table 1: Median Household Value by Number of Bedrooms

Number of Bedrooms	Number of Dwellings	Median Value
0 bedrooms	20	n/a
1 bedroom	860	\$250,506
2 bedrooms	3,150	\$349,789
3 bedrooms	1,320	\$1,001,637
4+ bedrooms	1,450	\$1,499,358
Total	6,790	\$600,196

To “afford” a two-bedroom home with a purchase price of \$1M dollars, based on the aforementioned assumptions regarding home ownership, a household would need to have a gross income of approximately \$180,000. To afford a three-bedroom home at \$1.5M, the household would need to have a gross income of approximately \$270,000. Table 2 provides the proportion of homes in White Rock within each household income bracket.

Table 2: Number and Percentage of Households by Household Income Bracket

Household Income	White Rock						Metro Vancouver	
	2006		2011		2016		2016	
	#	%	#	%	#	%	#	%
Under \$5,000	235	2.5%	240	2.4%	245	2.4%	28,215	2.9%
\$5,000 to \$9,999	175	1.8%	170	1.7%	95	0.9%	15,325	1.6%
\$10,000 to \$14,999	255	2.7%	285	2.9%	330	3.3%	28,505	3.0%
\$15,000 to \$19,999	600	6.3%	460	4.7%	435	4.3%	38,730	4.0%
\$20,000 to \$24,999	620	6.5%	685	6.9%	600	6.0%	35,910	3.7%
\$25,000 to \$29,999	410	4.3%	470	4.8%	450	4.5%	35,430	3.7%
\$30,000 to \$34,999	390	4.1%	515	5.2%	540	5.4%	36,585	3.8%
\$35,000 to \$39,999	500	5.3%	340	3.4%	405	4.0%	36,525	3.8%
\$40,000 to \$44,999	420	4.4%	520	5.3%	410	4.1%	36,175	3.8%
\$45,000 to \$49,999	410	4.3%	360	3.6%	445	4.4%	36,890	3.8%
\$50,000 to \$59,999	760	8.0%	795	8.1%	875	8.7%	69,510	7.2%
\$60,000 to \$69,999	795	8.4%	625	6.3%	715	7.1%	66,100	6.9%
\$70,000 to \$79,999	640	6.7%	710	7.2%	625	6.2%	60,325	6.3%
\$80,000 to \$89,999	475	5.0%	735	7.5%	550	5.5%	54,510	5.7%
\$90,000 to \$99,999	390	4.1%	460	4.7%	455	4.5%	49,305	5.1%
\$100,000 to \$124,999	865	9.1%	885	9.0%	760	7.6%	100,350	10.4%
\$125,000 to \$149,999	495	5.2%	535	5.4%	630	6.3%	72,235	7.5%
\$150,000 to \$199,999	695	7.3%	545	5.5%	730	7.3%	82,570	8.6%
\$200,000 and over	390	4.1%	520	5.3%	715	7.1%	77,700	8.1%
Total households	9,515	100.0%	9,865	100.0%	10,005	100.0%	960,890	100.0%

The data above clearly demonstrates the low proportion of households in White Rock capable of purchasing a home at \$1M or greater. For those who do not have the established equity value in existing assets, or the financial resources made available through other sources, the feasibility of purchasing a home large enough to raise a family in White Rock is limited. Considering further the extremely limited supply of two- and three-bedroom units within the rental market, for which vacancy rates are 0.3% and 0.0%, respectively, rental housing for families is also not an option.

The Housing Needs Report recognizes the challenges faced by low- and moderate-income families and their ability to access ground-oriented rental and homeownership units. Several of those engaged in the housing needs review provided that the lack of local supply is causing families to leave the City in search of options elsewhere. Policy 11.1.1 of the City's OCP provides useful direction regarding the need for more "family-friendly housing." The Policy is regularly identified in the presentation of new development schemes to Council and in day-to-day conversations with would-be applicants. Continuing the exploration of strategies to support families in White Rock will be an important action to follow Council's adoption of a Housing Needs Report.

6. Shelters and Housing for People At-Risk of Homelessness

The need for shelters and housing for people at-risk of homelessness is recognized in the Housing Needs Report. The Report provides that a point-in-time homeless count for the White Rock-Delta area identified 33 persons experiencing homelessness in 2020 (see Figure 11).

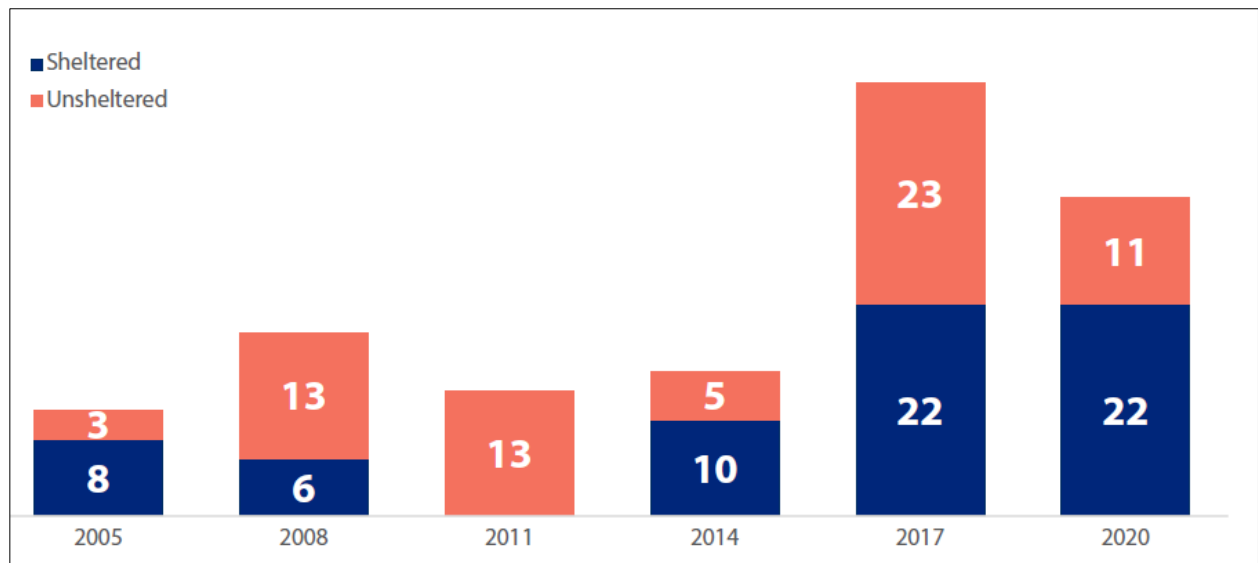


Figure 11: Metro Vancouver Homeless Count, 2005 - 2020

The extreme weather shelter located in South Surrey, which also serves the White Rock community, has been operational since 2008. In 2019, the shelter had 20 individuals access the space nightly. Increasing affordability constraints and emerging vulnerability patterns are creating the conditions for more at-risk populations. As such, the number of people experiencing homelessness in White Rock has the potential to rise in the coming years. There is enough evidence to support small facilities or group home projects, such as a safe house or small shelter facility. There is also an opportunity to implement homelessness prevention strategies and a rapid re-housing program before homelessness becomes an unmanageable situation. This is an area of need that may benefit from collaboration with the City of Surrey.

Summarizing the Number of Housing Units Required

One of the Provincial requirements tied to preparing a Housing Needs Report is to identify the number of units required over the next five (5) years. More specifically, B.C. Regulation 90/2019 provides that local governments are to define the number of bachelor, one-bedroom, two-bedroom, three-bedroom, and four (or more) bedroom units required. Metro Vancouver's Regional Growth Strategy (RGS) estimates that White Rock will require an additional 1,100 units to accommodate projected growth to 2026; White Rock's own housing estimates anticipate 11,158 units in 2026 or 1,153 new units from the 10,005 identified by the 2016 Census.

One of the methods that could be used to estimate the breakdown of units, by bedroom size, required over the next five (5) years would be to carry the "status quo" forward as it relates to the current proportion of each unit size within the housing stock. For example, per the 2016 Census the proportion of two-bedroom units in White Rock made up approximately 42% of the total and the proportion of units with four or more bedrooms made up roughly 16% of the total. Applying these proportions to the additional 1,153 units forecast over the next five (5) years would be a means of continuing the status quo.

Alternative to this approach, Council could seek to interrupt the housing mix through policy and other land use interventions. These measures may help shift unit supply to address some of the areas of local housing need identified within the Housing Needs Report. Council could, for example, look to support fewer one-bedroom and four-or-more bedroom dwelling units by setting general limits on their proportions in new development schemes. Similarly, Council could continue to incentivize family-friendly housing through policy measures and, potentially, pre-zoning which seeks to replace lower profile housing (e.g., single-detached homes and duplexes)

with more modest forms of “missing middle” housing, being low-rise (3 – 4 storey) apartment buildings and townhomes. Table 3 below identifies the status quo scenario and a scenario which illustrates the impact that interventions could have to address areas of local housing need.

The method used in the “status quo” scenario was simply carrying the existing proportion of units (by size) forward from 2016 to 2026. The “intervention” scenario includes manual adjustments to the proportion of one-, three-, and four+-bedroom units to more-closely align new unit supply with the areas of need recognized in the Housing Needs Report. Specifically, the proportion of one-bedroom units would be reduced, through redevelopment, with emphasis placed on additional three-bedroom units. Furthermore, the proportion of four-bedroom units, or larger, would be similarly reduced through measures that support two- and three-bedroom units. For example, policy and regulatory interventions could be pursued which enable the removal of larger single-detached dwellings, and their replacement with smaller (three-bedroom) detached, or attached dwelling units (e.g., single family homes, duplexes, triplexes, and/or townhomes).

Table 3: Anticipated New Units By Size, Status Quo Versus Intervention Scenarios

Unit Size	2016 Census	Scenarios			
	(% of total)	2026 Status Quo		2026 Intervention	
Bachelor	55 (0.5%)	61 (0.5%)	+6	61 (0.5%)	+6
One-Bedroom	2,500 (24.9%)	2,788 (24.9%)	+288	2,391 (21.4%)	-109
Two-Bedroom	4,245 (42.4%)	4,734 (42.4%)	+489	4,734 (42.4%)	+489
Three-Bedroom	1,565 (15.6%)	1,745 (15.6%)	+180	2,526 (22.6%)	+961
Four+ Bedroom	1,640 (16.3%)	1,829 (16.3%)	+189	1,445 (12.9%)	-195
Total	10,005 (100%)	11,158	+1,153	11,158	+1,153

It is important to acknowledge that any interventions made over the next three (3) years will take time to materialize in measurable changes to the proportion of dwelling units realized in White Rock. Therefore, while the numbers in Table 3 may not be realized in 2026, they are presenting a direction that will help White Rock address community housing needs into the future.

Next Steps in Addressing Local Housing Needs

This corporate report presents Council with the first Housing Needs Report for the City of White Rock. Legislation applicable to the preparation of housing needs reports provides that such reports are to be updated every five (5) years. As measures are implemented to address local housing needs, future iterations of housing needs reports can be used to report back on the progress being made in White Rock.

Overall, staff believe the Housing Needs Report included as Appendix A presents a strong foundation upon which to initiate actions that will help address the six (6) areas of local housing need. As communities throughout the Lower Mainland finalize their housing needs reports, they are now diverting their attention towards the implementation of housing action plans. Working with its Housing Advisory Committee, White Rock may now look to pursue actions that will help address local housing needs.

LEGAL IMPLICATIONS

With the posting of the adopted Housing Needs Report on the City's webpage, Council would satisfy its obligations under the *Local Government Act* per BC Regulation 90/2019.

ALIGNMENT WITH STRATEGIC PRIORITIES

One of Council's top priorities is to prepare a Housing Needs Report to support efforts to address areas of local housing need while contributing to a high quality of life for White Rock residents. Staff are of the opinion that the Housing Needs Report included as Appendix A to this corporate report provides the City with a strong starting point for the exploration of land use strategies and interventions that can be used to the six (6) areas of identified housing need. Over the next year, Council could choose to initiate policy amendments and other measures to set the stage for improvements to the overall quality of housing in the City.

OPTIONS / RISKS / ALTERNATIVES

The following options are available for Council's consideration:

1. Defer adoption of the Housing Needs Report and direct staff to provide additional information regarding aspects of the work presented in this corporate report.
2. Deny adoption of the Housing Needs Report.

CONCLUSION

This corporate report presents Council with the first-ever Housing Needs Report for the City of White Rock. If the Housing Needs Report is ultimately endorsed by Council, the next step in this important work would be to begin the process of evaluating, and implementing, measures that will help address the six areas of local housing need, identified in the HNR.

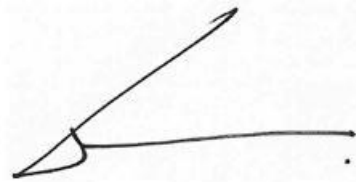
Respectfully submitted,

A handwritten signature in black ink, appearing to read "Greg Newman", with a stylized flourish at the end.

Greg Newman, MCIP, RPP
Acting Director, Planning and Development Services

Comments from the Chief Administrative Officer

I concur with the recommendations of this corporate report.

A handwritten signature in black ink, appearing to read "Guillermo Ferrero", with a stylized flourish at the end.

Guillermo Ferrero
Chief Administrative Officer

Appendix A: Housing Needs Report, City of White Rock

1. Agenda

Documents:

[2016-05-25 RENTAL HOUSING TASK FORCE AGENDA.PDF](#)

2. Supporting Meeting Documents

Documents:

[2016-05-25 WHITE ROCK HOUSING STRATEGY REVISED.PDF](#)

**THE CORPORATION OF THE
CITY OF WHITE ROCK**
15322 BUENA VISTA AVENUE, WHITE ROCK, B.C. V4B 1Y6



A MEETING of the **WHITE ROCK RENTAL HOUSING TASK FORCE** will be held in the CITY HALL BOARDROOM, 15322 Buena Vista Avenue, on **WEDNESDAY, MAY 25, 2016**, at **4:00 p.m.** for the transaction of business listed below.

Tracey Arthur, City Clerk

A G E N D A

1. CALL TO ORDER

2. ADOPTION OF AGENDA

RECOMMENDATION

THAT the Rental Housing Task Force adopts the May 25, 2016 agenda as circulated.

3. ROUNDTABLE INTRODUCTION

3.1 What do you hope to contribute to the Task Force?

3.2 What do you hope the Task Force will accomplish?

4. STAFF PRESENTATION

4.1 Review of Rental Housing Task Force Terms of Reference

4.2 Summary of White Rock Rental Housing Universe Backgrounder

4.3 “What We’ve Heard” in the Official Community Plan Update Process

4.4 What is an Affordable Housing Strategy / Housing Action Plan?

4.5 Reading Material for Task Force Members

4.6 Potential Agenda Items (Guest Speakers, etc.) for Future Meetings

5. 2016 MEETING SCHEDULE

6. CONCLUSION OF THE MEETING

City of White Rock

Affordable Housing Strategy



DRAFT Report By
Terra Housing Consultants
Cooper Planning & Development Consultants
June 2010



INTRODUCTION

Council established the Affordable Housing Working Group to advise on real, practical ways to maintain and increase the supply of affordable housing in White Rock through development of an Affordable Housing Strategy. The six-person committee members included:

- Councillor Lynne Sinclair - City of White Rock
- Angelo Tsakumis - Vice President and Director for EPTA Properties
- David Young - Executive Director, Sources
- Reverend Joan McMurtry - First United Church of White Rock
- Paul Stanton - Director of Planning & Development Services, City of White Rock
- Connie Halbert - Planner, City of White Rock

Community consultation, through a Community Workshop held on May 19, 2010 provided valuable input into the development of the Strategy. The Affordable Housing Strategy identifies actions that the City can take to address its affordable housing issues. Implementation of the strategy will:

- Clarify the role of the City of White Rock regarding affordable housing
- Address the gap in housing that is affordable for the community
- Address development of partnerships to foster affordable housing development
- Provide for ongoing consultation and follow-up

The White Rock Affordable Housing Strategy Background Report, May 2010, documents the research and analysis completed to prepare the Affordable Housing Strategy. A copy of the Background Report can be obtained online at www.city.whiterock.bc.ca or from the Planning and Development Services department.

WHITE ROCK AFFORDABLE HOUSING STRATEGY

The background work and community consultation undertaken in preparing the Strategy resulted in seven areas for White Rock to address including;

1. Maintaining a municipal planning system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand;
2. Maintaining a municipal financial system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand
3. Facilitating diversity in housing forms and prices to deliver a range of affordable housing options for all residents;
4. Facilitating growth in the not for profit housing sector;
5. Supporting the retention and delivery of adequate supplies of private rental properties;
6. Encouraging senior government and industry cooperation in support of projects that complement White Rock's affordable housing objectives; and
7. Monitoring the housing situation and regularly evaluating the need for adjustments to the Strategy.

Objectives and Actions to address each of these areas are proposed below. These need to be discussed with the community and priorities and milestones and responsibility to implement need to be identified.

1. Maintain a municipal planning system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand

One of the most effective ways that a municipality can influence the provision of affordable housing is through its planning and development policies, by-laws and standards and through its development review process.

#	Objective	Action
1.1	Official Community Plan provisions implement the Affordable Housing Strategy	• Develop the City-wide affordable housing targets for affordable ownership and affordable rental housing for low, medium and high densities
		• Require developers to demonstrate how they will achieve affordability targets
		• Establish a policy providing incentives for affordable housing
		• Establish a Housing First Policy to allow surplus City-owned lands to be offered to non-profit housing groups for affordable housing
		• Establish an inclusionary zoning policy
1.2	Optimum use of Local Government Act planning provisions for affordable housing	• Develop Zoning provisions for affordable housing under Section 904
		• Develop a Housing Agreement template for affordable and special needs housing using provisions under Section 905
1.3	Reduced barriers to affordable housing development	• Review the Zoning By-laws and development standards, such as parking requirements and setbacks, and make necessary amendments
		• Allow zoning variances for affordable housing development that increases affordability
		• Expedite approval process for affordable housing development applications
1.4	Increased production of affordable housing to meet needs	• Identify opportunities to develop Infill Housing and prepare Infill Guidelines
		• Align new affordable housing development with neighbourhood planning
		• Require affordable housing to be provided as part of mixed use community development applications
		• Develop Adaptable Housing Design Guidelines
		• Explore the feasibility of expanding White Rock's density bonus system to provide developers with additional density in exchange for providing affordable housing contributions

2. Maintain a municipal financial system that supports the delivery of an adequate supply of affordable housing and is responsive to changing demand

#	Objective	Action
2.1	Optimum use of Local Government Act financial provisions for affordable housing	<ul style="list-style-type: none"> • Commission a study of revitalization tax exemptions and permissive tax exemptions to outline how they can be implemented in White Rock
2.2	A supportive environment to encourage affordable housing development	<ul style="list-style-type: none"> • CC Exemption or Reduction • Fee Reductions
2.3	Efficient and effective use of public and community assets for affordable housing	<ul style="list-style-type: none"> • Establish an Affordable Housing Fund • Establish an annual contribution to maintain the Affordable Housing Fund to support additional affordable housing construction. • Establish formal review criteria, eligibility and application process for the consideration of affordable and social housing projects
		<ul style="list-style-type: none"> • Use the municipal investment in affordable housing as leverage with other orders of government
		<ul style="list-style-type: none"> • Index municipal investment in affordable housing to inflation over time
2.4	An effective municipal land bank	<ul style="list-style-type: none"> • Develop an inventory of municipal land and identify sites appropriate for affordable housing development • Advocate for land owned by the federal or provincial government deemed surplus to be provided to the municipality or non-profit housing provider as sites for affordable housing development, or sold to the private sector, with the City being the sole beneficiary of the funds gained from the sale, to be used exclusively for affordable housing development • Bank land not immediately needed for affordable initiatives in the future • Include land in-lieu contributions from developers in municipal land bank

3. Facilitate diversity in housing forms and prices to deliver innovative, affordable housing for all residents

The availability of a range of housing options is a sign of a healthy city. It is essential for economic growth and prosperity, and is an important part of the social and economic infrastructure.

#	Objective	Action
3.1	Increase the supply of affordable housing	• Document existing supply of affordable housing in White Rock
		• Establish targets for affordable housing for emergency, transitional, supportive, social, rental and ownership housing
3.2	Increase diversity in housing stock	• Identify development sites throughout the City that support a mix of housing types to enhance provision of affordable and accessible housing
		• Identify opportunities for laneway and carriage home development and prepare design and development guidelines
		• Support the development of a pilot project where a non-profit incorporates a shared equity scheme for eligible tenants and home buyers, as part of its business model in a new housing development
		• Maintain and enhance the role of supported accommodation providers to ensure people at risk of homelessness, on low incomes, with special needs or requiring emergency accommodation are able to be appropriately housed with no or minimal delay, and if appropriate, assisted to move to longer term rental accommodation

4. Facilitate growth in the community based not for profit housing sector

Community housing is typically rental housing for households who have low to moderate incomes and other special needs, managed by not-for-profit community based organizations whose operations may be partly subsidized by government.

#	Objective	Action
4.1	Support the community housing sector to enable it to grow and to meet the housing needs of people with low incomes and other special needs	<ul style="list-style-type: none"> • Focus on the community housing sector as affordable housing provider for people with low to moderate incomes & special needs • Make Affordable Housing Fund grants and/or loans available for non-profit housing projects
		<ul style="list-style-type: none"> • Promote partnerships with other agencies and providers to encourage integration of objectives and make best use of limited funding opportunities
		<ul style="list-style-type: none"> • Ensure a mechanism is in place for the creation of housing units when opportunities arise
4.2	Ensure a sufficient supply of land to enable community housing providers to meet future client needs	Develop an Inventory of Non-Profit Providers and Service Agencies' lands
		Develop an inventory of other government and agency land
		Make surplus municipal land available to housing providers for special needs affordable housing development
4.3	Increase the supply of special needs housing	<ul style="list-style-type: none"> • Identify appropriate development sites and/or opportunities for affordable housing for: <ul style="list-style-type: none"> Seniors People with low incomes People with physical and/or developmental challenges, People with mental illness, People with addiction challenges People Women and children fleeing abuse Youth • Support special needs affordable housing providers in seeking funding from other levels of government or philanthropic organizations to develop housing
4.4		Seek not-for-profit providers to participate in a shared equity scheme or schemes for low-income earners and eligible first homebuyers

5. Support the retention and delivery of adequate supplies of private rental properties;

Small investors dominate the private rental market, and there is relatively little purpose built rental accommodation in White Rock. The current rental vacancy rate is very low, at around .5 per cent. Access options for households just above the eligibility criteria for social housing are limited.

#	Objective	Action
5.1	Support the effective operation of the private rental market and the availability of a range of accommodation to suit varying income levels and needs	<ul style="list-style-type: none">• Explore the possibility of working with strata councils to allow for unrestricted rentals
5.2	Protect the existing rental stock	<ul style="list-style-type: none">• Develop an inventory of purpose built residential development and analyze redevelopment potential
		<ul style="list-style-type: none">• Develop and implement a Rental Conversion By-law
		<ul style="list-style-type: none">• Establish a standard of maintenance bylaw
5.3	Encourage Secondary Suite Development	<ul style="list-style-type: none">• Evaluate the Secondary Suite Registration By-law and recommend improvements
		<ul style="list-style-type: none">• Provide incentives for secondary suite development in new housing development
		<ul style="list-style-type: none">• Promote CMHC funding program for secondary suites

6. Encourage senior government and industry cooperation to support projects that complement White Rock's affordable housing objectives

#	Objective	Action
6.1	Demonstrate leadership in advocating for affordable housing	<ul style="list-style-type: none"> • Ensure that local housing priorities are included in the regional, provincial and federal housing discussions, policies and programs
		<ul style="list-style-type: none"> • Invite local MPs and MPPs to inter-governmental dialogue on housing and homelessness, and the ways in which all orders of government will work together to developing affordable housing
		<ul style="list-style-type: none"> • Advocate for the federal and provincial governments to invest in the non-profit housing providers acquiring and renovating apartment to provide sustainable and affordable rental housing
		<ul style="list-style-type: none"> • Document housing needs of White Rock residents on housing waiting lists and advocate for government funding for assisted family housing in integrated market and non-profit developments
		<ul style="list-style-type: none"> • Advocate for affordable integrated housing for people with physical and developmental challenges
		<ul style="list-style-type: none"> • Advocate for improved opportunities for affordable entry homeownership
		<ul style="list-style-type: none"> • Advocate that senior government funding be aligned with local needs for affordable housing and focus on permanent solutions to homelessness
6.2	Make full use of funding programs	<ul style="list-style-type: none"> • Request BC Non-Profit Housing Association to develop and maintain an inventory of funding opportunities that support affordable housing
		<ul style="list-style-type: none"> • Investigate the Canada Mortgage and Housing Corporation (CMHC) delivery agent role for Residential Rehabilitation Assistance Programs (RRAP) to improve access to these funds for residents of White Rock
6.3	Positive Working Relationship with Development Industry	<ul style="list-style-type: none"> • Convene regular meetings with Development Industry representatives to discuss and work to resolve development issues and identify affordable housing opportunities
		<ul style="list-style-type: none"> • Work with the development industry to establish one or two demonstration projects showcasing affordable land and housing packages

7. Monitor the housing situation, communicate with residents and regularly evaluate the need for adjustments to the Strategy

Obtaining information and data pertaining to White Rock in the development of this strategy was a challenge. Some statistical data is available through Planning Department sources and Statistics Canada but gaps exist.

The community consultation indicated the need to access a broad range of housing information. Situations have occurred where groups interested in developing affordable housing did not know where to turn for information. In addition, the consultation identified a need for a place where individuals, landlords and tenants can access information pertaining to housing related legislation and programs. Although some of the data is available through community resource centers and advisory groups, participants requested a single point of access that would provide a complete range of housing services.

The need for community education and outreach was expressed widely in the community consultation. Specific areas mentioned included addressing community acceptance of social housing, increasing awareness of affordability, homelessness and poverty issues throughout the community, need to reach out to stakeholders and increase understanding of the development process.

The community consultation also reinforced the need to monitor and evaluate implementation of the Strategy.

#	Objective	Action
7.1	Ensure White Rock has access to accurate and current data on housing	• Monitor economic factors, housing market condition and the impact, of significant changes to legislation, regulations, by-laws, etc., to identify housing issues and trends
		• Benchmark land development costs, housing prices, rental rates and vacancy rates
		• Establish a program of monitoring the delivery of affordable housing against targets
		• Conduct an inventory to determine the different types and numbers of persons in White Rock with special housing needs and determine the number of units required to house those persons, including persons who are homeless
7.2	Ensure Housing Strategy is current and relevant	• Monitor Implementation of the Housing Strategy
		• Dedicate portion of housing fund to 'best practice' housing policy research and conduct research
7.3	A single point of access for tenants, landlords, builders, developers and the general public to obtain information about housing, housing issues housing related legislation and programs	• Support establishment of a Housing Resource Centre
7.4	Increase public awareness of affordable housing issues in White Rock	• Develop and implement a community education and outreach action plan

DRAFT

THE CORPORATION OF THE CITY OF WHITE ROCK BYLAW 2436



A Bylaw to provide an exemption from municipal property taxes
under section 224 of the *Community Charter*.

Council may, by bylaw, exempt properties from taxation for a fixed period of time pursuant to section 224 of the *Community Charter*, S.B.C. 2003. c. 26.

The following social service organizations have made application for permissive tax exemption grants for said lands with improvements listed below in the City of White Rock, the said lands being legally described as:

Options Community Services Society	Parcel Identifier: 000-452-505 Lot 1, Block 21, Section 11, Township 1, New Westminster Land District, Plan NWP11178
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 005-113-521 Lot 12, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-941 Lot 13, Block N 1/2 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 009-218-173 Lot 33, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP24976
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 001-829-653 Lot 16, Block 6, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital and Community Health Foundation	Parcel Identifier: 010-362-967 Lot 17, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP18167
Peace Arch Hospital Auxiliary Society	Parcel Identifier: 012-364-096 Lot 7, Block 5, Part NE ¼, Section 10, Township 1, New Westminster Land District, Plan NWP1864

Sources Community Resources Society	Parcel Identifier: 011-232-099 Lot “B”, Block 7, Part E ½, Section 11, Township 1, New Westminster Land District, Plan NWP7459
White Rock Players’ Club	Parcel Identifier: 011-306-599 Lot 3 Except: West 7 Feet, Part NW ¼, Section 11, Township 1, New Westminster Land District, Plan NWP8437

(the “Lands”)

Pursuant to section 224(2)(a) of the *Community Charter*, the City of White Rock wishes to grant a five (5) year municipal property tax exemption in respect of the lands with improvements.

The CITY COUNCIL of the Corporation of the City of White Rock, in open meeting assembled,
ENACTS as follows:

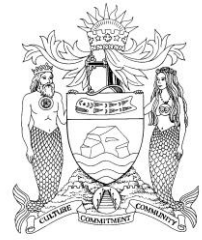
1. The Lands and improvements thereon are hereby exempt from municipal taxation for the calendar years 2023 to 2027, providing that the use or ownership of the property does not change.
2. Annually before June 30th, the above not-for-profit organizations, noted as the Lands, must inform the City of White Rock in writing of any changes to use or ownership of the property. The City of White Rock will require repayment of monies equal to the foregone tax revenue if the above social service organizations, noted as the Lands, fail to disclose changes in use or ownership, before June 30th, that breach the conditions of this exemption.
3. This bylaw may be cited for all purposes as the “2023 - 2027 Not for Profit Permissive Tax Exemptions Bylaw 2022, No. 2436”.

RECEIVED FIRST READING on the day of
RECEIVED SECOND READING on the day of
RECEIVED THIRD READING on the day of
ADOPTED on the day of

MAYOR

CITY CLERK

**THE CORPORATION OF THE
CITY OF WHITE ROCK
BYLAW 2437**



A Bylaw to provide an exemption from municipal property taxes
under section 224 of the *Community Charter*.

Council may, by bylaw, exempt properties from taxation for a fixed period of time pursuant to section 224 of the *Community Charter*, S.B.C. 2003. c. 26.

Various social service agencies have leased certain parts of the facility known as the Horst and Emmy Werner Centre for Active Living ("CAL"), located at 1475 Anderson St, from the Corporation of the City of White Rock ("the City"). The CAL is legally described as:

Parcel Identifier: 008-523-444
Lot 97, Section 10, Township 1
New Westminster District, Plan 38518

(the "Lands")

Pursuant to section 224 of the *Community Charter*, the City of White Rock wishes to grant a one (1) year municipal property tax exemption in respect of the leased property with improvements.

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled, ENACTS as follows:

1. Each of the Premises described in the leases as particularized in section 3 are hereby exempt from municipal taxation for the calendar year 2023.
2. The Conditions imposed on each tenant are as outlined in each of their leases of their respective Premises and are conditions precedent to the municipal tax exemption granted by this bylaw.
3. The tenants and Premises that are exempt from taxation are:
 - (a) White Rock South Surrey Stroke Recovery Association (Premises shown in green on Schedule A);
 - (b) Fraser Health Authority or the White Rock/South Surrey Division of Family Practice Society (Premises shown in blue on Schedule A), depending on which organization leases this space; and
 - (c) White Rock/South Surrey Division of Family Practice Society (Premises shown in yellow on Schedule A).

Schedule A is attached to and forms part of this bylaw.

The White Rock South Surrey Stroke Recovery Association has a lease until 2025, the City is in the process of negotiating the White Rock/South Surrey Division of Family Practice Society lease for its existing space. The City anticipates leasing the space previously occupied by the Alzheimer Society of BC (Premises shown in blue on Schedule A) to the

Fraser Health Authority or the White Rock/South Surrey Division of Family Practice Society. The permissive exemption for these organizations is contingent upon successful negotiation of the leases.

4. This bylaw may be cited for all purposes as the “2023 *Permissive Tax Exemptions Centre for Active Living Bylaw 2022, No. 2437*”.

RECEIVED FIRST READING on the	day of
RECEIVED SECOND READING on the	day of
RECEIVED THIRD READING on the	day of
ADOPTED on the	day of

MAYOR

DIRECTOR OF CORPORATE
ADMINISTRATION

Schedule A



**The Corporation of the
CITY OF WHITE ROCK
BYLAW No. 2439**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled ENACTS as follows:

1. THAT Schedule C of the *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended by rezoning the following lands:

Lot 8 Section 10 Township 1 New Westminster District Plan NWP13684
PID: 007-208-677
(1441 Vidal Street)

Lot 2 Section 10 Township 1 New Westminster District Plan NWS2236
PID: 001-267-761
(1443 Vidal Street)

Lot 1 Section 9 Township 1 New Westminster District Plan NWS2236
PID: 001-267-744
(1445 Vidal Street)

Lot 1 Section 10 Township 1 New Westminster District Plan EPP46879
PID: 029-484-413
(1465 Vidal Street)

Lot 41 Section 10 Township 1 New Westminster District Plan NWP35379
PID: 007-223-480
(14937 Thrift Avenue)

as shown on Schedule "1" attached hereto, from the 'RS-1 One Unit Residential Zone' and the 'RT-1 Two Unit (Duplex) Residential Zone' and the 'CD-32 Comprehensive Zone' to the 'CD-68 Comprehensive Development Zone (Thrift/Vidal Street).'

2. THAT *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended:

(1) by adding to the Table of Contents for ‘Schedule B (Comprehensive Development Zones)’, Section 7.68 CD-68 Comprehensive Development Zone’, and

(2) by adding the attached Schedule “2” to ‘Schedule B (Comprehensive Development Zones)’ Section 7.68 CD-68 Comprehensive Development Zone’.

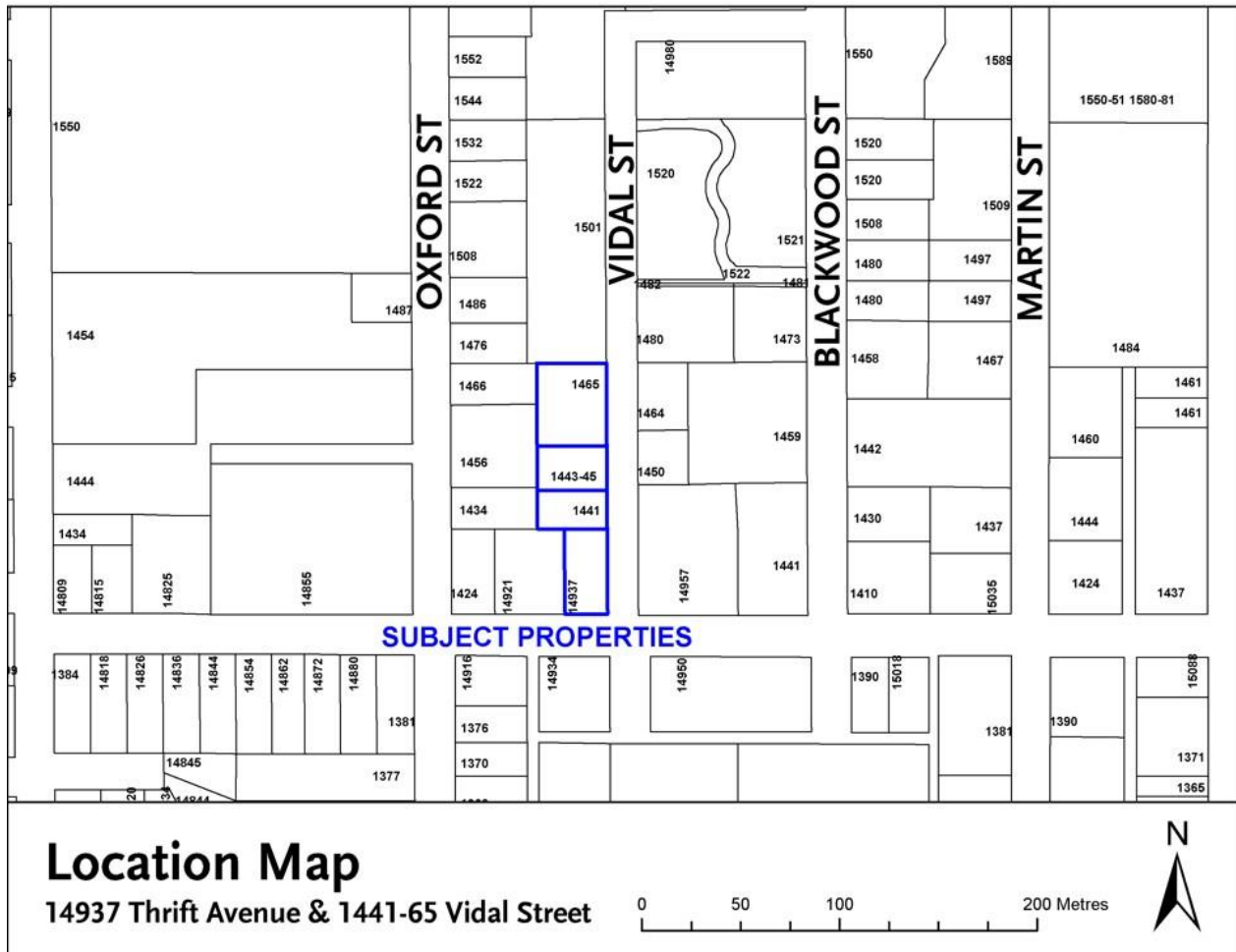
3. This bylaw may be cited for all purposes as “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-68 – 14937 Thrift Avenue and 1441, 1443-45, 1465 Vidal Street) Bylaw, 2022, No. 2439*”.

Public Information Meeting held this	20 th day of	August, 2020
Read a first time this	day of	, 2022
Read a second time this	day of	, 2022
Considered at a Public Hearing this	day of	, 2022
Read a third time this	day of	, 2022
Adopted this	day of	, 2022

Mayor

Director of Corporate Administration

Schedule "1"



7.68 CD-68 COMPREHENSIVE DEVELOPMENT ZONE

INTENT

The intent of this zone is to accommodate the development of a multi-unit rental residential building, being four storeys and with 70 dwelling units, on four consolidated sites of approximately 3,875.4 square meters (41,714 square feet or 0.958 acres).

1. Permitted Uses:

- (1) *multi-unit residential use*
- (2) *accessory home occupation* use in accordance with the provisions of section 5.3 and that does not involve clients directly accessing the *principal building*.

2. Lot Coverage:

- (1) The maximum *lot coverage* in the CD-68 zone is 39.6 %.

3. Gross Floor Area and Floor Area Ratio:

- (1) The gross floor area, including amenity areas, shall be no greater than 5,799 square metres (62,425 square feet).
- (2) The maximum density shall not exceed 1.5 times the lot area and the gross floor area ratio shall be no greater than 1.50 FAR

4. Regulations for Size, Shape and Siting of Building and Structures

- (a) The *principal building* for *multi-unit residential use*, inclusive of elevator shafts, stair housing, and all mechanical equipment, shall not exceed a *height* of 20.32 metres (66.66 feet)
- (b) The minimum setbacks for *multi-unit residential uses* are as follows:
 - (i) Setback from interior (north) lot line = 6.83 metres
 - (ii) Setback from exterior (south) lot line = 24.47 metres
 - (iii) Setback from rear (west) lot line = 7.87 metres
 - (iv) Setback from front (east) lot line = 5.27 metres
 - (v) Setback from south interior side lot line = 8.20 metres

5. Parking:

Parking for *multi-unit residential use* shall be provided in accordance with Sections 4.14 and 4.17, with the minimum number of spaces required as follows:

- (a) A minimum of one hundred and twenty-three (123) spaces shall be provided for the *multi-unit residential use*
- (b) A minimum of twenty-one (21) spaces shall be provided for visitors and marked as “visitor”
- (c) A minimum of three (3) of the required one hundred and twenty-three (123) spaces shall be provided as accessible parking spaces and shall be clearly marked as such and shall have a minimum length of 5.5 metres. Of the five accessible parking spaces,

one space shall be provided as a van-accessible loading space with a minimum width of 2.8 metres, and the other four spaces shall have a minimum width of 2.5 metres, provided that the four parking spaces have a shared or non-shared access aisle with a minimum width of 1.5 metres.

- (d) The minimum height clearance at the accessible parking spaces and along the vehicle access and egress routes from the accessible parking spaces must be at least 2.3 metres to accommodate over-height vehicles equipped with a wheelchair lift or ramp.
- (e) A minimum of twelve (12) stalls must have electric charging capacity.
- (f) A minimum of twelve (12) stalls must be roughed for electric charging capacity.

6. Bicycle Parking:

Bicycle parking shall be provided in accordance with Section 4.16, with the minimum number of spaces required as follows:

- (a) A minimum of 70 Class I spaces shall be provided
- (b) A minimum of 14 Class II spaces shall be provided

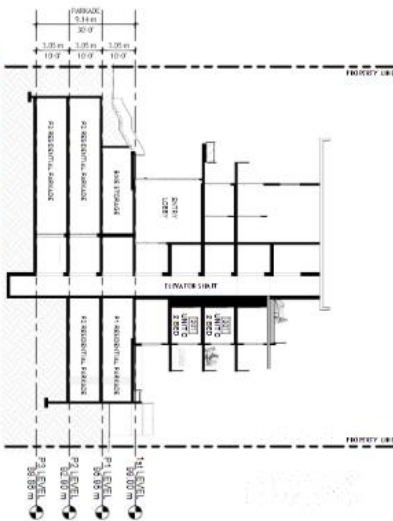
9. Loading:

- (a) One loading space shall be provided for a *multi-unit residential use* in accordance with Section 4.15

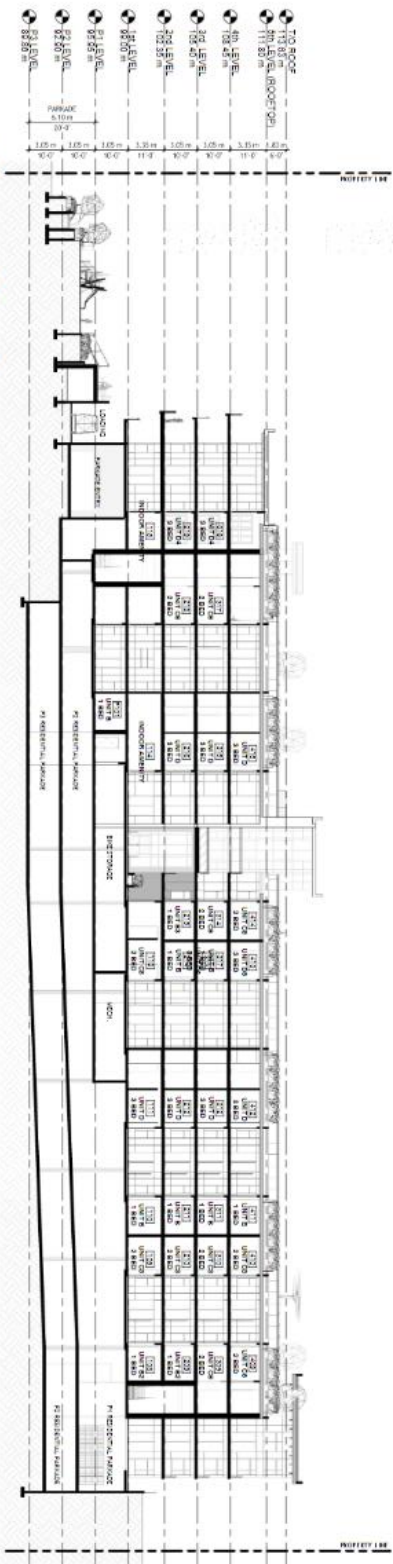
10. General:

Development in this zone shall substantially conform to the Plans prepared by Keystone Architecture dated October 18, 2021, and titled DP Resubmission that are attached hereto and on file at the City of White Rock.





site section a



site section b









roof deck - plaza & elevator lobby



roof deck - trellis seating area & picnic area



roof deck - green roof & plaza

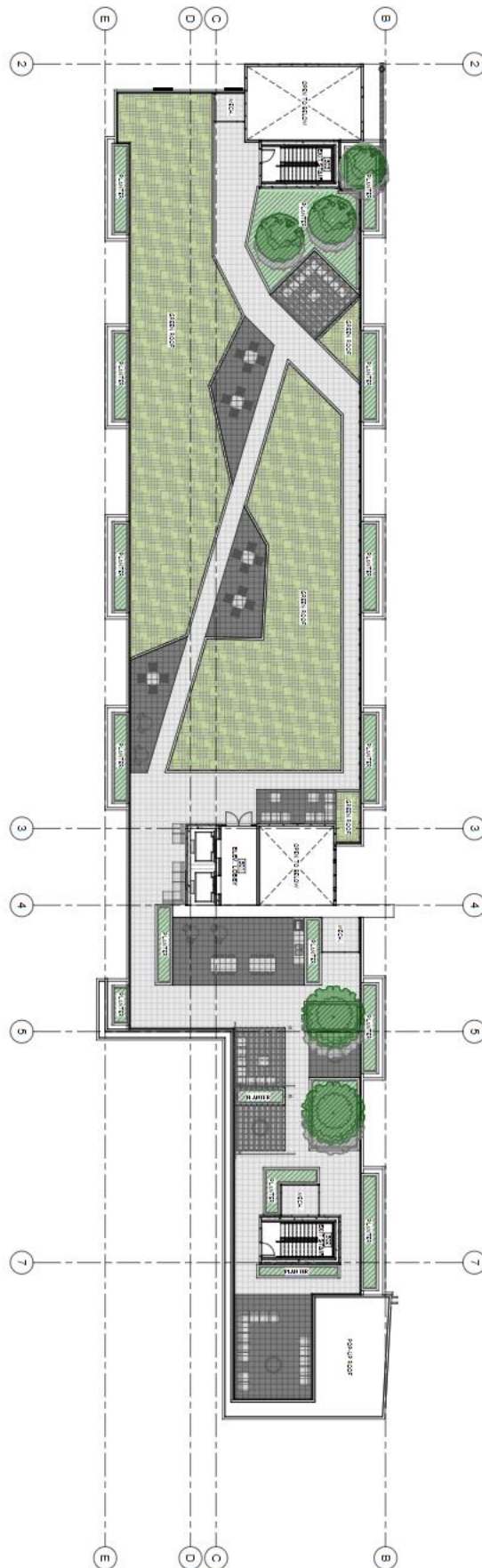


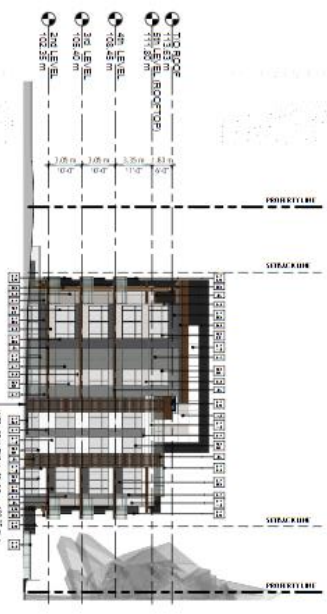
ground level greenspace - street view



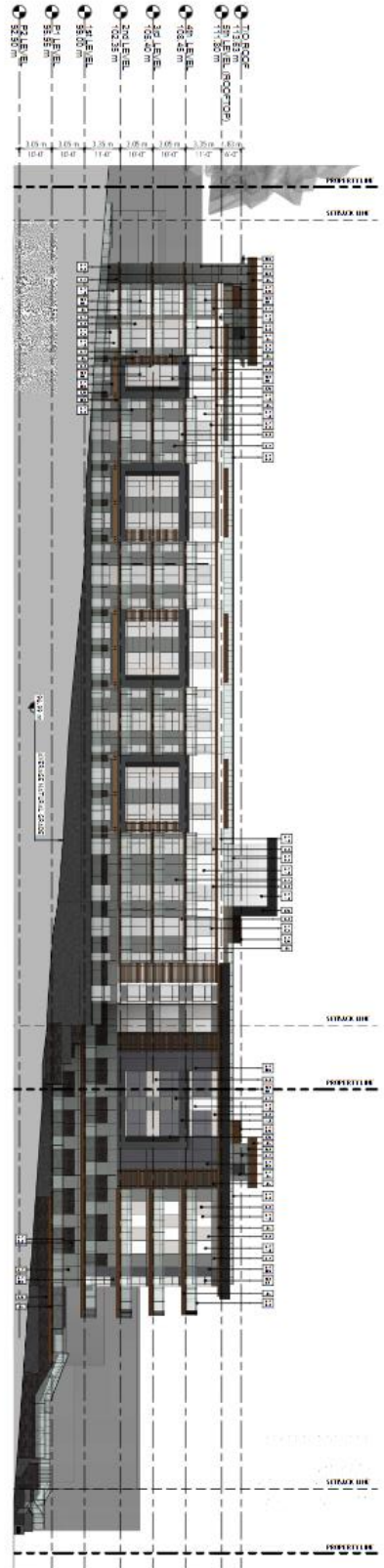
ground level greenspace - aerial view







north elevation (facing highrise)



west elevation (facing townhouses)

MATERIAL LEGEND

1	CONCRETE
2	GLASS
3	ALUMINUM
4	BRICK
5	STONE
6	WOOD
7	ROOF
8	LANDSCAPE
9	PAVING
10	WALL
11	CEILING
12	FLOOR
13	DOOR
14	WINDOW
15	BALCONY
16	STAIR
17	ELEVATOR
18	MECHANICAL
19	LANDSCAPE
20	PAVING
21	WALL
22	CEILING
23	FLOOR
24	DOOR
25	WINDOW
26	BALCONY
27	STAIR
28	ELEVATOR
29	MECHANICAL
30	LANDSCAPE
31	PAVING
32	WALL
33	CEILING
34	FLOOR
35	DOOR
36	WINDOW
37	BALCONY
38	STAIR
39	ELEVATOR
40	MECHANICAL
41	LANDSCAPE
42	PAVING
43	WALL
44	CEILING
45	FLOOR
46	DOOR
47	WINDOW
48	BALCONY
49	STAIR
50	ELEVATOR
51	MECHANICAL
52	LANDSCAPE
53	PAVING
54	WALL
55	CEILING
56	FLOOR
57	DOOR
58	WINDOW
59	BALCONY
60	STAIR
61	ELEVATOR
62	MECHANICAL
63	LANDSCAPE
64	PAVING
65	WALL
66	CEILING
67	FLOOR
68	DOOR
69	WINDOW
70	BALCONY
71	STAIR
72	ELEVATOR
73	MECHANICAL
74	LANDSCAPE
75	PAVING
76	WALL
77	CEILING
78	FLOOR
79	DOOR
80	WINDOW
81	BALCONY
82	STAIR
83	ELEVATOR
84	MECHANICAL
85	LANDSCAPE
86	PAVING
87	WALL
88	CEILING
89	FLOOR
90	DOOR
91	WINDOW
92	BALCONY
93	STAIR
94	ELEVATOR
95	MECHANICAL
96	LANDSCAPE
97	PAVING
98	WALL
99	CEILING
100	FLOOR



east perspective elevation



west perspective elevation



south perspective elevation



north perspective elevation

**The Corporation of the
CITY OF WHITE ROCK
BYLAW 2438**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock, in open meeting assembled, ENACTS as follows:

1. Schedule "C" of the "White Rock Zoning Bylaw, 2012, No. 2000" as amended is further amended by rezoning the following lands:

Lot 1 Section 11 Township 1 New Westminster District Plan 14850
(1091 Stayte Road)
PID: 002-267-144

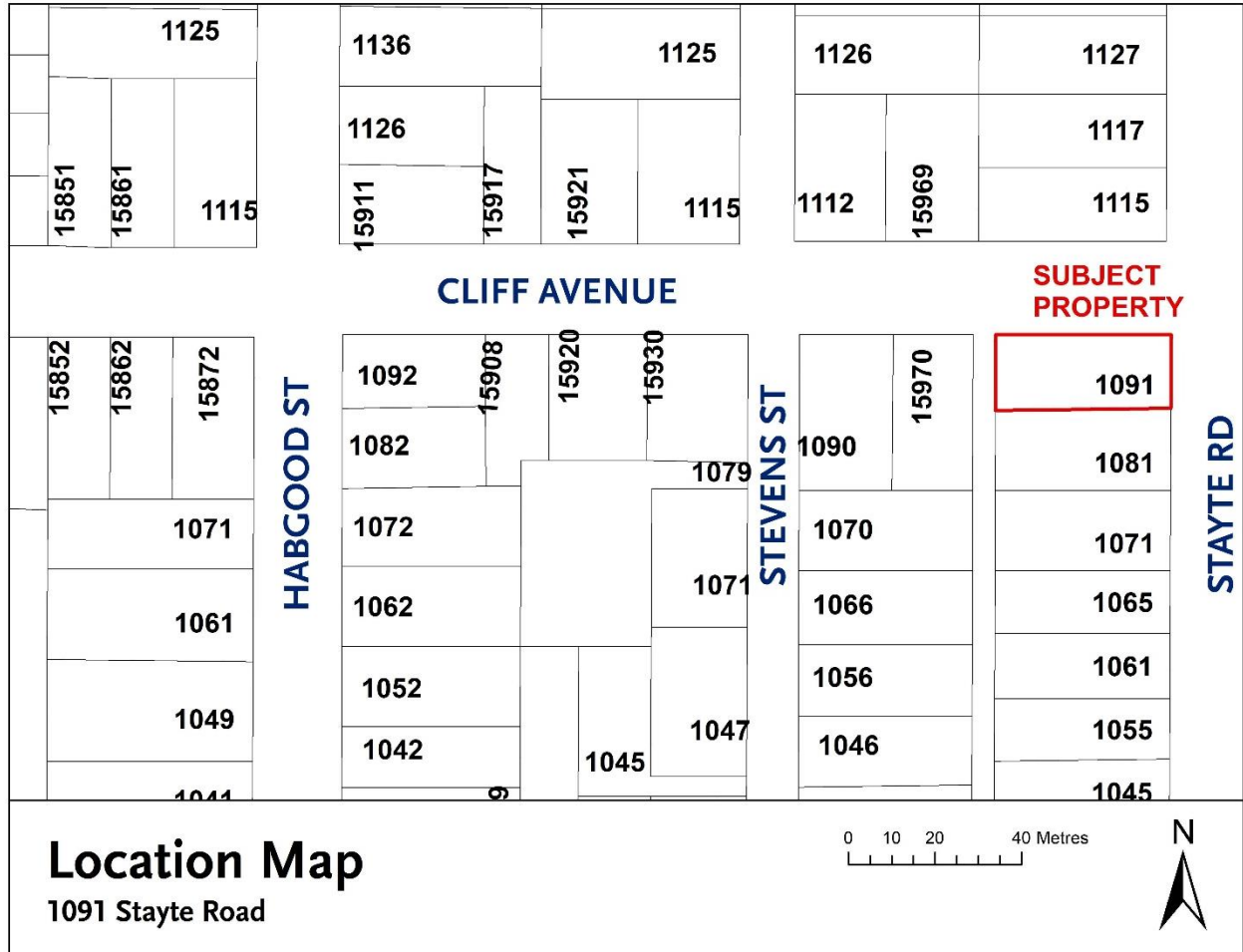
as shown on Schedule "1" attached hereto, from 'RS-1 One Unit Residential Zone' to 'RT-1 Two Unit (Duplex) Residential Zone'.
2. This Bylaw may be cited for all purposes as the "White Rock Zoning Bylaw 2012, No. 2000, Amendment (RT-1 – 1091 Stayte Road) Bylaw, 2020, No. 2438".

PUBLIC INFORMATION MEETING on the	8 th day of	October, 2020
RECEIVED FIRST READING on the	day of	
RECEIVED SECOND READING on the	day of	
PUBLIC HEARING held on the	day of	
RECEIVED THIRD READING on the	day of	
RECONSIDERED AND FINALLY ADOPTED on the	day of	

Mayor

City Clerk

Schedule "1"



**The Corporation of the
CITY OF WHITE ROCK
BYLAW No. 2435**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled ENACTS as follows:

1. THAT Schedule C of the *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended by rezoning the following lands:

LOT 4, BLOCK 39B, PLAN NWP17402, PART E1/2, SECTION 11, TOWNSHIP 1,
NEW WESTMINSTER LAND DISTRICT
PID: 010-289-461

(15770 NORTH BLUFF RD)

LOT 3, PLAN NWP17402, SECTION 11, TOWNSHIP 1, NEW WESTMINSTER LAND
DISTRICT
PID: 004-519-043

(15758 NORTH BLUFF RD)

LOT 305, PLAN NWP35289, SECTION 11, TOWNSHIP 1, NEW WESTMINSTER
LAND DISTRICT
PID: 007-144-563

(15748 NORTH BLUFF RD)

LOT 2 EXCEPT: PARCEL "K" (REFERENCE PLAN 30172); SECTION 11 TOWNSHIP
1 NEW WESTMINSTER DISTRICT PLAN 13659
PID: 009-848-096

(15738 NORTH BLUFF RD)

LOT 1, PLAN NWP13659, PART E1/2, SECTION 11, TOWNSHIP 1, NEW
WESTMINSTER LAND DISTRICT, EXCEPT PLAN PCL L REF 30172

PID: 009-848-053

(15728 NORTH BLUFF RD)

LOT 2 SECTION 11 TOWNSHIP 1 NEW WESTMINSTER DISTRICT PLAN 18697
PID:010-427-384

(15724 NORTH BLUFF RD)

LOT 1 SECTION 11 TOWNSHIP 1 NEW WESTMINSTER DISTRICT PLAN 18697
PID: 010-427-325

(15704 NORTH BLUFF RD)

as shown on Schedule “1” attached hereto, from the ‘RS-1 One Unit Residential Zone’ to the ‘CD-67 Comprehensive Development Zone.’

2. THAT *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended:

(1) by adding to the Table of Contents for ‘Schedule B (Comprehensive Development Zones)’, Section 7.62 CD-67 Comprehensive Development Zone’;

(2) by adding the attached Schedule “2” to ‘Schedule B (Comprehensive Development Zones)’ Section 7.62 CD-67 Comprehensive Development Zone’.

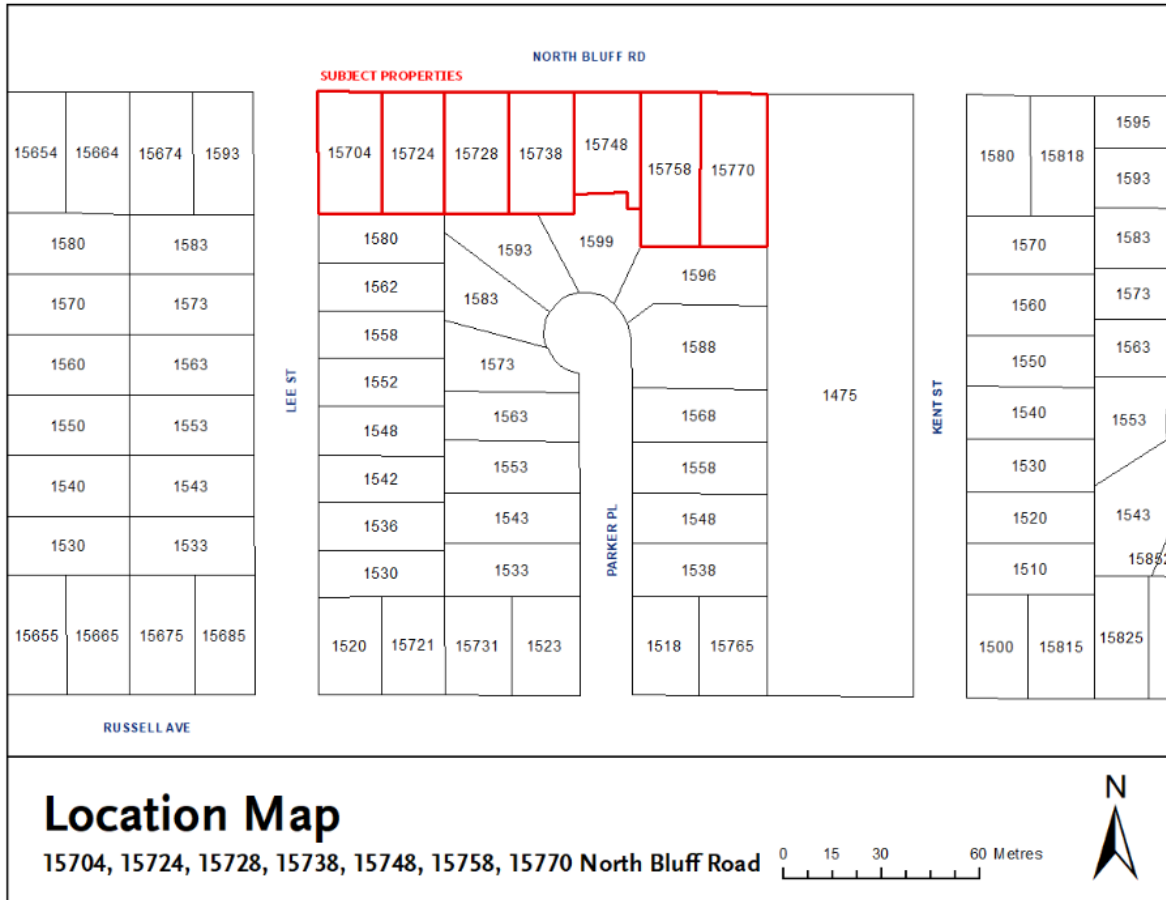
3. This bylaw may be cited for all purposes as “*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-67 – 15704, 15724/28/38/48/58/70 North Bluff Road) Bylaw, 2022, No. 2435*”.

Public Information Meeting held this	9 th day of September,	, 2020
Read a first time this	27 th day of June	, 2022
Read a second time this	27 th day of June	, 2022
Considered at a Public Hearing this	day of	, 2022
Read a third time this	day of	, 2022
Adopted this	day of	, 2022

Mayor

Director of Corporate Administration

Schedule "1"



7.62 CD-67 COMPREHENSIVE DEVELOPMENT ZONE

INTENT

The intent of this zone is to accommodate the development of multi residential development on a site of approximately 5,366.241 square metres.

1. Permitted Uses:
 - (a) *Townhouse* use
 - (b) *Apartment* use
 - (b) *accessory home occupation* use in accordance with the provisions of section 5.3 and that does not involve clients directly accessing the *principal building*
2. Lot Coverage:
 - (a) *lot coverage* shall not exceed 46%
3. Density:
 - (a) The maximum *gross floor area* shall not exceed 1.5 times the lot area.
 - (b) The maximum number of *dwelling units* must not exceed 87.
5. Building Height:
 - (a) The *principal building* shall not exceed a height of 73.5 metres geodetic.
 - (b) *Ancillary buildings* and *structures* shall not exceed a height of 5.0 metres from the *finished grade*.
6. Siting Requirements:
 - (a) Minimum setbacks are as follows:

(i) Setback from front (east) lot line	= 4.5 metres
(ii) Setback from rear (west) lot line	= 2.4 metres
(iii) Setback from interior side (north) lot line	= 2.0 metres
(iv) Setback from interior side (south) lot line	= 3.5 metres
 - (b) Notwithstanding the above, the following siting exemptions are permitted:
 - (i) *Ancillary buildings* and *structures* may be located on the subject property in accordance with the Plans prepared by Urban Arts Architecture dated June 17, 2022 that are attached hereto and on file at the City of White Rock, with the exception that no *ancillary buildings* or *structures* are permitted within a 2.0 metre distance from the front (east) lot line
7. Parking:

Parking shall be provided in accordance with Sections 4.14 and 4.17, with the minimum number of spaces required as follows:

- (a) A minimum of one hundred and forty-eight (148) spaces shall be provided for the *multi-unit residential development*:
 - (a) Apartments: 1.2 per unit (54 units) 65
 - (b) Townhouses: 2.0 per unit (33 units) 66
 - (c) Visitors 0.3 per unit (54 units) 17
- (d) A minimum of one (1) of the required forty (40) spaces shall be provided as accessible parking spaces and two (2) van accessible shall be clearly marked in accordance with the B.C. Building Code Requirements.
- (e) A minimum of four (15) stalls must have electric charging capacity.
- (f) A minimum of four (15) stalls must be roughed for electric charging capacity.

8. Bicycle Parking:

Bicycle parking shall be provided in accordance with Section 4.16, with the minimum number of spaces required as follows:

- (a) A minimum of twenty (89) Class I spaces shall be provided
- (b) A minimum of four (24) Class II spaces shall be provided

9. Loading:

- (a) One loading space shall be provided for the *multi-unit residential use* in accordance with Section 4.15

NOTES:

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The Contractor shall check and verify all dimensions and report any discrepancies before proceeding.

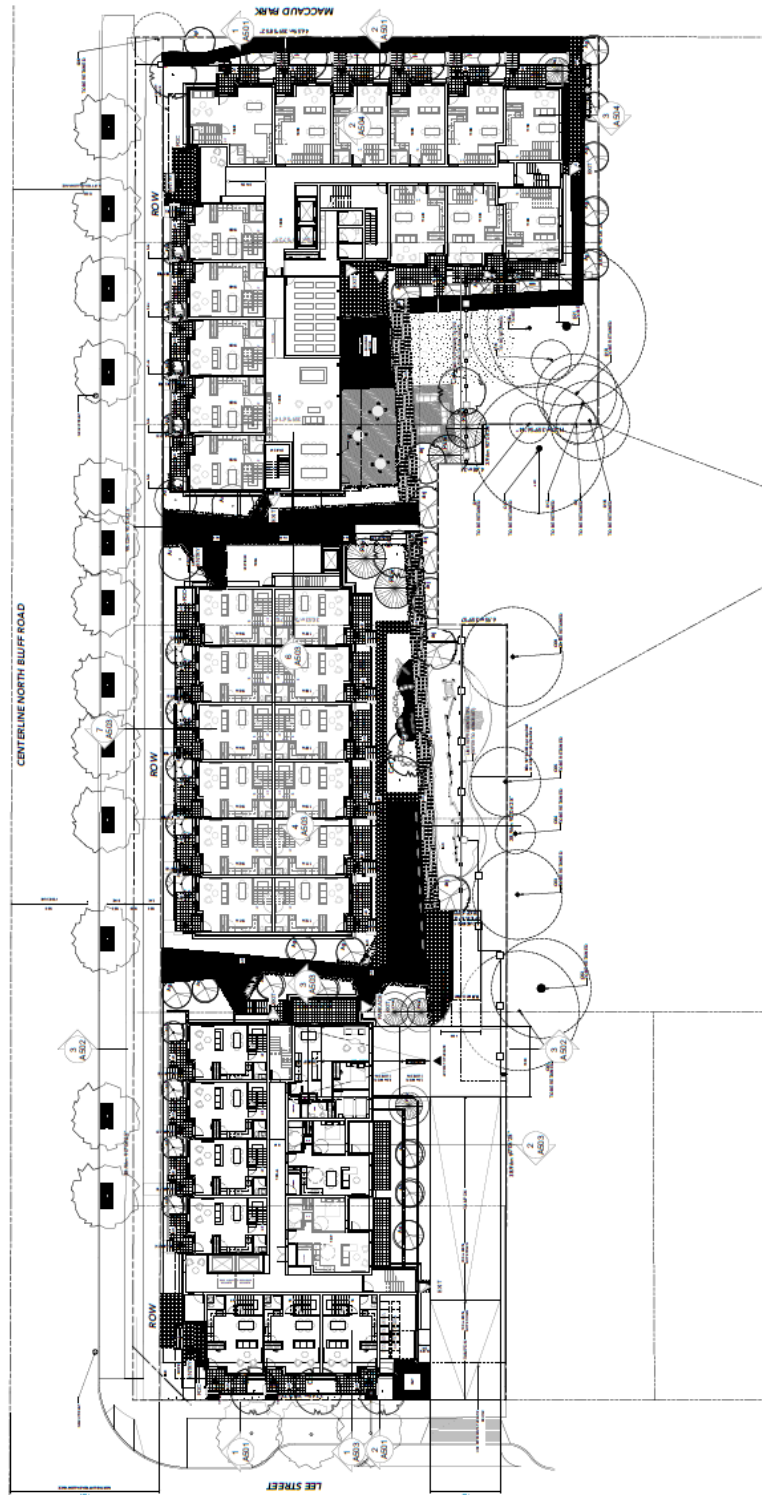
Do not scale drawings.

Considerations:

	Reviewed For DLP
D	2020-05-14
C	2020-10-01
B	2020-08-08
A	2019-10-23
For DATE	15064

A101

PLOT DATE: 2021.06.14	SCALE: 1:200
DRAWN BY: OEM	CHECK BY: SC

[illegible]

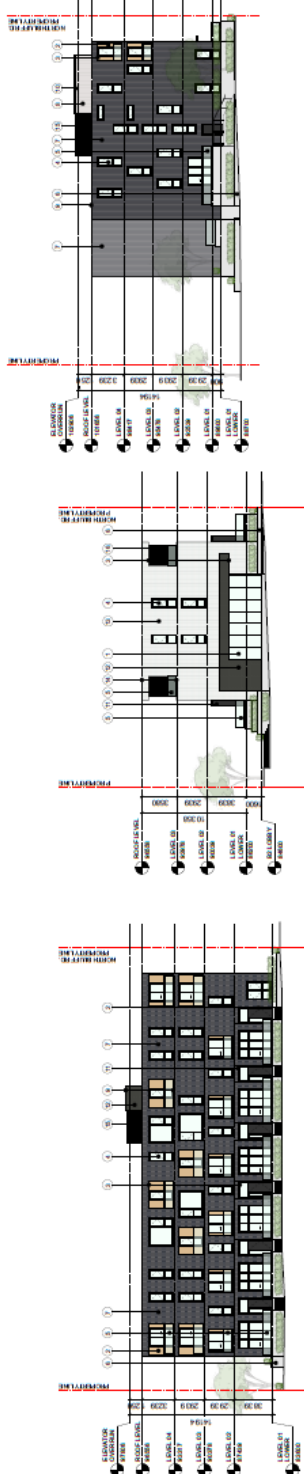
SEAL:

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Drawn by: [Name]



1 NORTH ELEVATION (NORTH BLUFF RD.)
SCALE: 1/8"=1'-0"



2 BUILDING 3 - EAST ELEVATION (FACING PARK)
SCALE: 1/8"=1'-0"

3 BUILDING 2 - EAST ELEVATION
SCALE: 1/8"=1'-0"

4 BUILDING 1 - EAST ELEVATION
SCALE: 1/8"=1'-0"

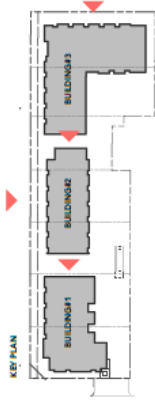
MATERIALS LEGEND

- COMMON MATERIALS:

- 1 CLEAR GLASS (W/ LOW-E COATING)
- 2 WOOD-COLORED FIBRE-CONCRETE PANELING
- 3 WOOD SHUTT
- 4 VINYL GLAZING FRAMES (CHARCOAL GREY)
- 5 FRAMELESS GLASS RAILING
- 6 CONCRETE PLANTER
- 7 MASONRY BRICK CLADDING (DARK GREY)
- 8 MASONRY BRICK CLADDING (WHITE)
- 9 METAL FLASHING AT DARK BRICK (CHARCOAL GREY)
- 10 METAL FLASHING AT WHITE BRICK (WHITE)
- 11 ALUMINUM COMPOSITE PANEL SQUARE STANCH

MATERIALS SPECIFIC TO BUILDING 1-2:

- 12 FIBRE-CONCRETE PANELING (DARK GREY)
- 13 FIBRE-CONCRETE PANELING (WHITE)
- 14 METAL FLASHING (WHITE)
- 15 ROOFTOP MECHANICAL EQUIPMENT ENCLOSURE FRAMES (WHITE) PAINT TO MATCH CHARCOAL METAL FLASHING
- 16 ALUMINUM PRIVACY SCREEN



KEY PLAN

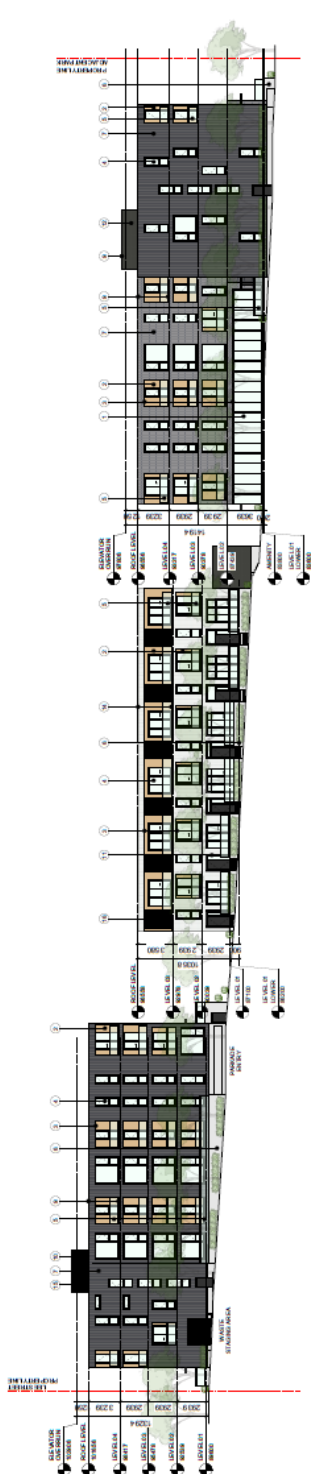
D. 2021.05.14 Rev. 01/2021
C. 2021.05.14 Rev. 01/2021
B. 2021.05.14 Rev. 01/2021
A. 2021.05.14 Rev. 01/2021

URBAN-2 COURTYARD COMMONS Elevation A400	
PL. OF DATE: 2021.05.14 SCALE: 1/8"=1'-0" DRAWN BY: [Name] CHECK BY: [Name]	10.00

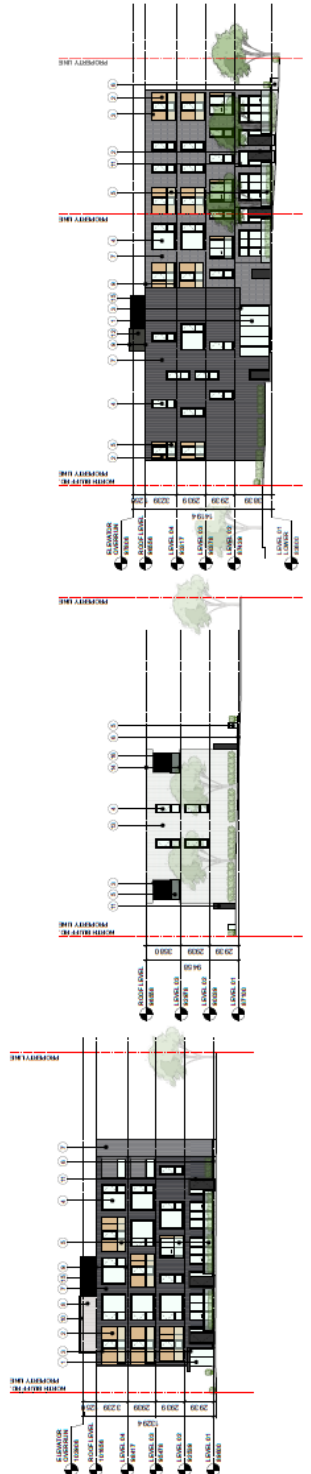
SEAL:

NOTES:
This drawing is the property of Urban Architecture and Design. It is to be used only for the project and site for which it was prepared. It is not to be used for any other project without the written consent of Urban Architecture and Design. Any reproduction or use of this drawing without the written consent of Urban Architecture and Design is strictly prohibited.

Drawn by: [Name]
Checked by: [Name]
Date: [Date]



1 SOUTH ELEVATION
Scale: 1:200



2 BUILDING 1 - WEST ELEVATION (LEE STREET)
Scale: 1:200

3 BUILDING 2 - WEST ELEVATION
Scale: 1:200

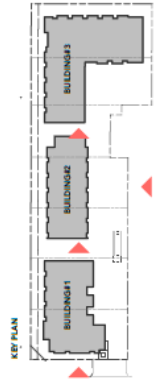
4 BUILDING 3 - WEST ELEVATION
Scale: 1:200

- COMMON MATERIALS:**
- 1. CLEAR GLASS (4mm) WITH LOW-E COATING
 - 2. WOOD-COLOURED FIBRE CONCRETE PANELING
 - 3. WOOD SCOTT
 - 4. VINYL CLADDING (FRAMES: DARK BROWN, GRIDS: WHITE)
 - 5. FRAMED GLASS (4mm)
 - 6. CONCRETE PANELING

- MATERIALS SPECIFIC TO BUILDING 1A:**
- 7. MASONRY BRICK CLADDING (DARK GREY)
 - 8. MASONRY BRICK CLADDING (WHITE)
 - 9. METAL CLADDING AT DARK BRICK (CONCRETE)
 - 10. VINYL CLADDING AT WHITE BRICK (WHITE)
 - 11. ALUMINUM COMPOSITE PANEL (SCAR SHEDS)

- MATERIALS SPECIFIC TO BUILDING 2:**
- 12. FIBRE-CEMENT PANELING (DARK GREY)
 - 13. FIBRE-CEMENT PANELING (WHITE)
 - 14. METAL CLADDING (WHITE)
 - 15. ROOF TOP MECHANICAL EQUIPMENT ENCLOSURE (WHITE)
 - 16. ALUMINUM PRIVACY SCREEN

NOTES:
THIS IS A PRELIMINARY DRAWING. IT IS NOT TO BE USED FOR CONSTRUCTION. ANY REVISIONS TO THIS DRAWING WILL BE MADE BY THE ARCHITECT.



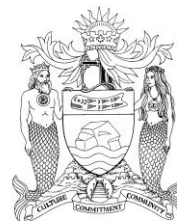
D 2019-01-14 Prepared by: [Name]
C 2019-01-14 Prepared by: [Name]
A 2019-01-14 Prepared by: [Name]
REV: 001 DATE: 01-14-2019

WU-1001-2
COURTYARD COMMONS

Elevation
A401

PLAT DATE: 2019-01-14 SCALE: 1:100
DRAWN BY: [Name] CHECK BY: [Name]

**The Corporation of the
CITY OF WHITE ROCK
BYLAW No. 2434**



A Bylaw to amend the
"White Rock Zoning Bylaw, 2012, No. 2000" as amended

The CITY COUNCIL of the Corporation of the City of White Rock in open meeting assembled ENACTS as follows:

1. THAT Schedule C of the *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended by rezoning the following lands:

Lot B Section 11 Township 1 New Westminster District Plan 84401
PID: 015-689-573

(1453 Stayte Road)

as shown on Schedule "1" attached hereto, from the 'RS-1 One Unit Residential Zone' to the 'CD-62 Comprehensive Development Zone.'

2. THAT *White Rock Zoning Bylaw, 2012, No. 2000* as amended is further amended:

(1) by adding to the Table of Contents for 'Schedule B (Comprehensive Development Zones)', Section 7.62 CD-62 Comprehensive Development Zone';

(2) by adding the attached Schedule "2" to 'Schedule B (Comprehensive Development Zones)' Section 7.62 CD-62 Comprehensive Development Zone'.

3. This bylaw may be cited for all purposes as "*White Rock Zoning Bylaw, 2012, No. 2000, Amendment (CD-62 – 1453 Stayte Road) Bylaw, 2022, No. 2434*".

Public Information Meeting held this 9th day of December, , 2020

Read a first time this 13th day of June , 2022

Read a second time this 13th day of June , 2022

Considered at a Public Hearing this 18th day of July , 2022

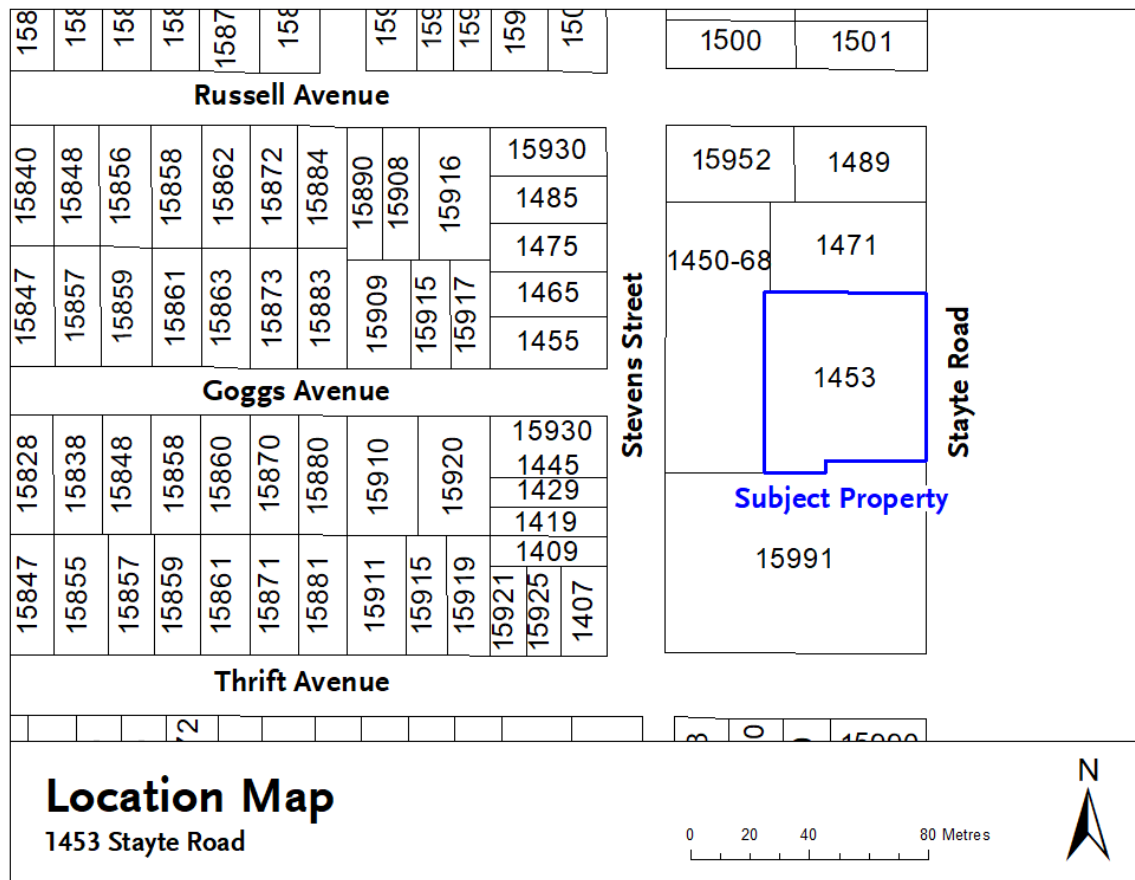
Read a third time this day of , 2022

Adopted this day of , 2022

Mayor

Director of Corporate Administration

Schedule “1”



7.62 CD-62 COMPREHENSIVE DEVELOPMENT ZONE

INTENT

The intent of this zone is to accommodate the development of three-storey townhouse development on a site of approximately 3,163 square metres.

1. Permitted Uses:
 - (a) *Townhouse use*
 - (b) *accessory home occupation* use in accordance with the provisions of section 5.3 and that does not involve clients directly accessing the *principal building*
2. Lot Coverage:
 - (a) *lot coverage* shall not exceed 42%
3. Density:
 - (a) The maximum *gross floor area* shall not exceed 0.905 times the lot area.
 - (b) The maximum number of *dwelling units* must not exceed 20.
5. Building Height:
 - (a) The *principal building* shall not exceed a height of 73.5 metres geodetic.
 - (b) *Ancillary buildings* and *structures* shall not exceed a height of 5.0 metres from the *finished grade*.
6. Siting Requirements:
 - (a) Minimum setbacks are as follows:

(i) Setback from front (east) lot line	= 1.5 metres
(ii) Setback from rear (west) lot line	= 6.0 metres
(iii) Setback from interior side (north) lot line	= 4.0 metres
(iv) Setback from interior side (south) lot line	= 3.0 metres
 - (b) Notwithstanding the above, the following siting exemptions are permitted:
 - (ii) Balconies may encroach by up 1’-6” (0.46 m) and roofs projecting 1’-0” (0.31 m) into the setback from front (east) lot line (Block 1).
 - (jj) Balconies may encroach by up 3’ (0.92 m) and roofs projecting 1’-0” (0.31 m) into the setback from interior side (north) lot line. (Block 4).
 - (kk) Balconies may encroach by 3’ (0.92 m) and roofs projecting 1’-0” (0.31 m) into the 3.0m Setback from interior side (south) lot line (Block 3).
 - (ll) Eaves may encroach by up to 1.6 metres into all required lot line setbacks.
 - (iv) *Ancillary buildings* and *structures* may be located on the subject property in accordance with the Plans prepared by Atelier Pacific Architecture dated

February 21, 2020 that are attached hereto and on file at the City of White Rock, with the exception that no *ancillary buildings* or *structures* are permitted within a 2.0 metre distance from the front (east) lot line

7. Parking:

Parking shall be provided in accordance with Sections 4.14 and 4.17, with the minimum number of spaces required as follows:

- (a) A minimum of forty (40) spaces shall be provided for the *multi-unit residential use*
- (b) A minimum of one (1) of the required forty (40) spaces shall be provided as accessible parking spaces and shall be clearly marked in accordance with the B.C. Building Code Requirements.
- (c) A minimum of four (4) stalls must have electric charging capacity.
- (d) A minimum of four (4) stalls must be roughed for electric charging capacity.

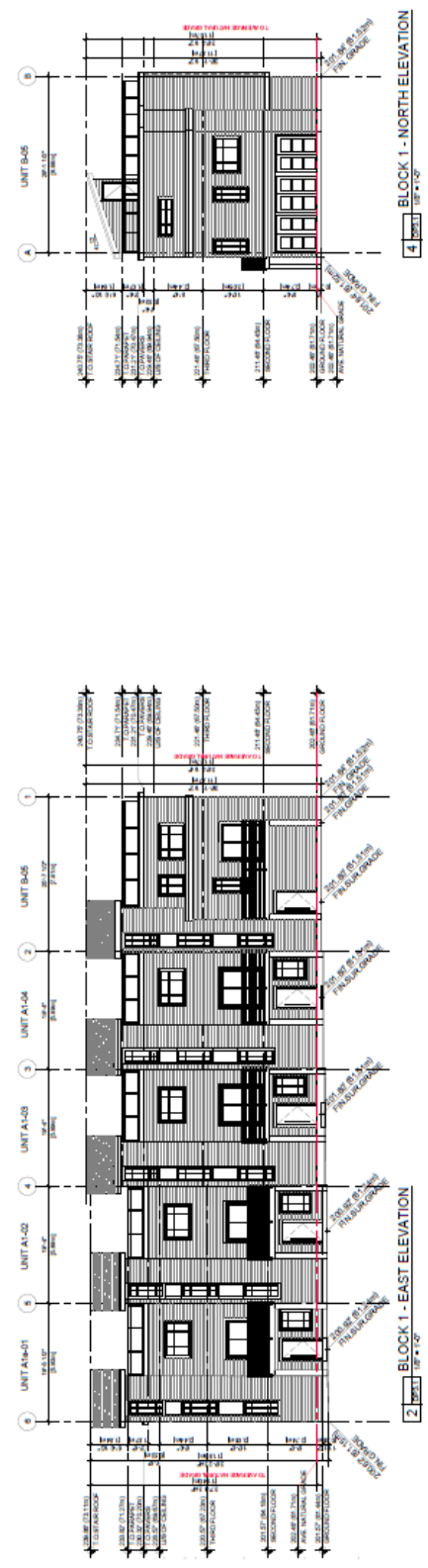
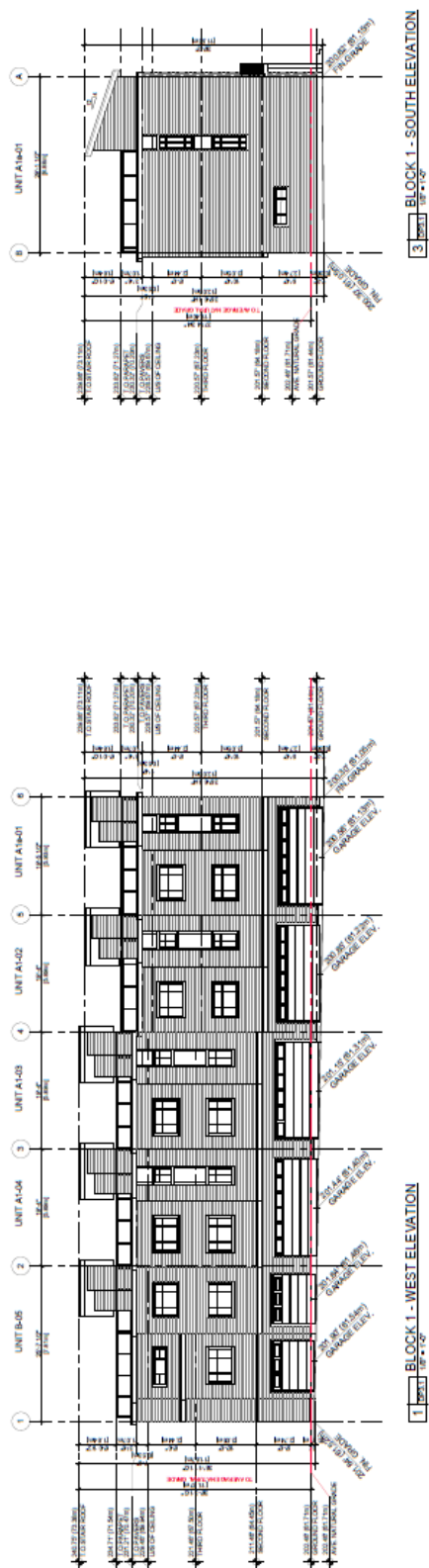
8. Bicycle Parking:

Bicycle parking shall be provided in accordance with Section 4.16, with the minimum number of spaces required as follows:

- (a) A minimum of twenty (20) Class I spaces shall be provided
- (b) A minimum of four (4) Class II spaces shall be provided

9. Loading:

- (a) One loading space shall be provided for the *multi-unit residential use* in accordance with Section 4.15



Proposed Residential Development

1453 STANTE ROAD, WHITE ROCK, B.C.

VINHOME PROPERTIES INC.
 ATELIER PACIFIC ARCHITECTURE INC.
 March 16, 2022

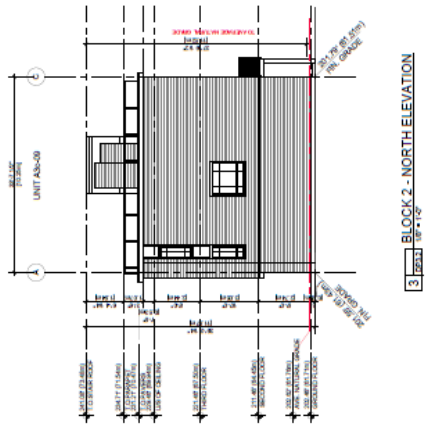
DP3.1

ELEVATIONS BLOCK 1

SCALE 1/8" = 1'-0"



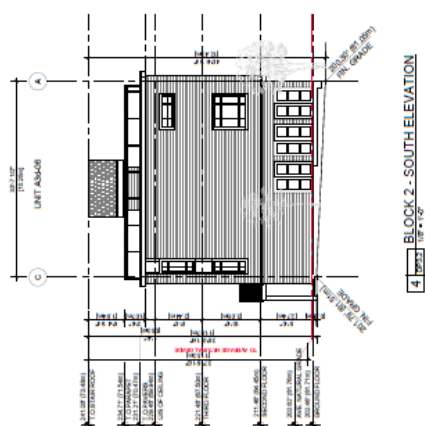
1 BLOCK 2 - EAST ELEVATION
1/8\"/>



3 BLOCK 2 - NORTH ELEVATION
1/8\"/>



2 BLOCK 2 - WEST ELEVATION
1/8\"/>



4 BLOCK 2 - SOUTH ELEVATION
1/8\"/>

Proposed Residential Development

1453 STAYTE ROAD, WHITE ROCK, B.C.

VANHOME PROPERTIES INC.

ATELIER PACIFIC ARCHITECTURE INC.

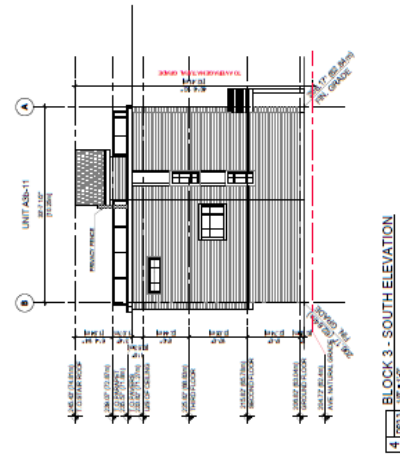
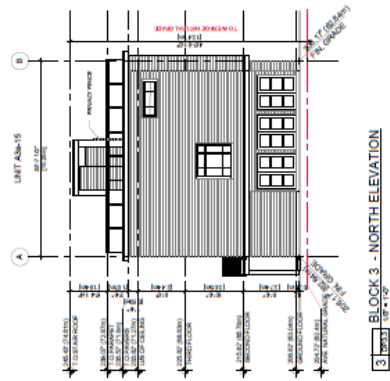
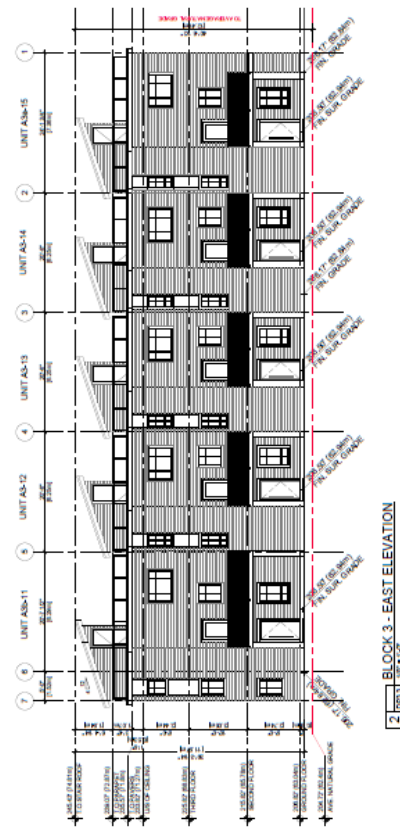
March 15, 2022

DP3.2

ELEVATIONS
BLOCK 2

SCALE 1/8\"/>





Proposed Residential Development

1453 STAYTE ROAD, WHITE ROCK, B.C.

VAN-HOME PROPERTIES INC.
ATELIER PACIFIC ARCHITECTURE INC.

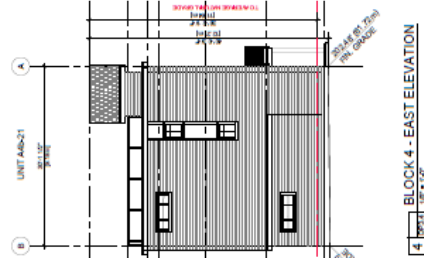
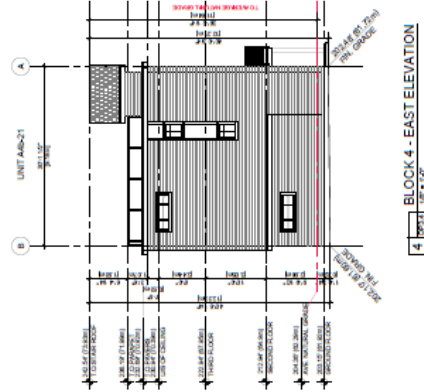
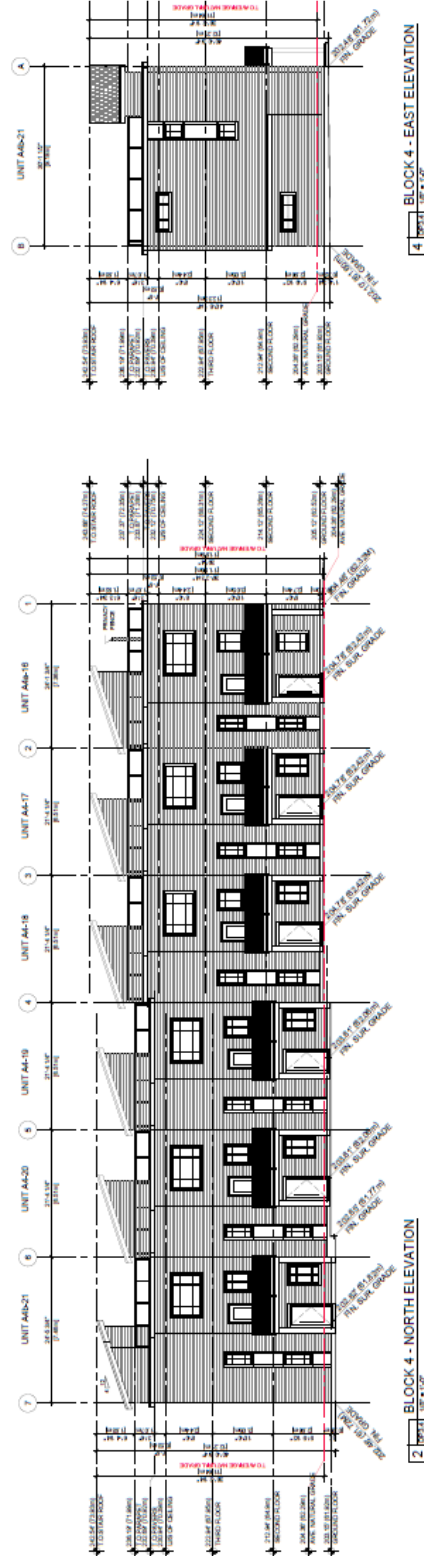
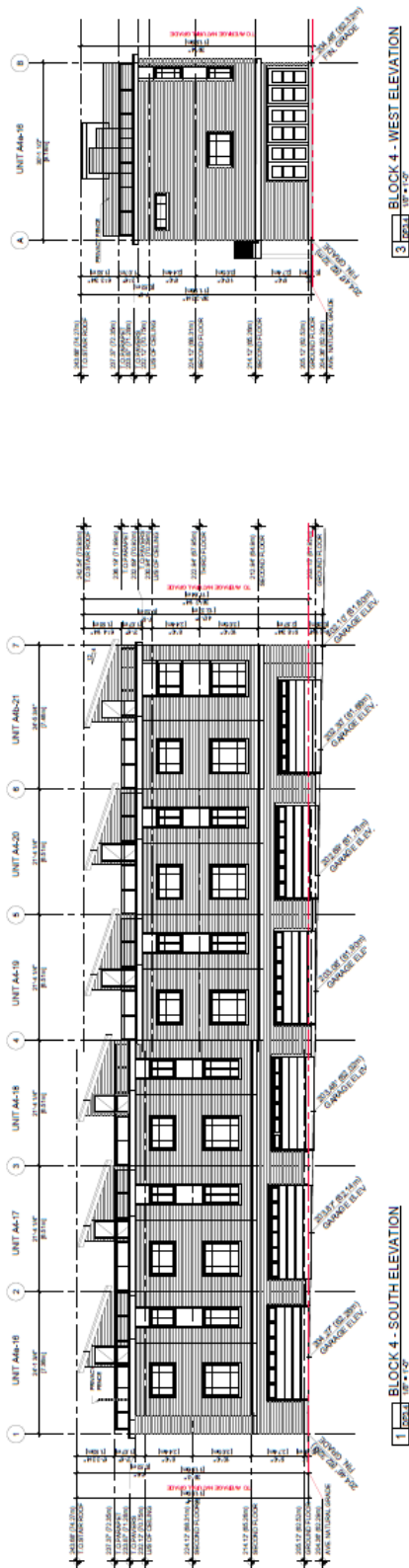
March 16, 2022

DP3.3

**ELEVATIONS
BLOCK 3**

SCALE: 1/8" = 1'-0"





Proposed Residential Development

1453 STAYTE ROAD, WHITE ROCK, B.C.

VANHOME PROPERTIES INC.
ATELIER PACIFIC ARCHITECTURE INC.

March 16, 2022

**THE CORPORATION OF THE
CITY OF WHITE ROCK
BYLAW NO. 2433**



A Bylaw to close and remove the highway dedication and to dispose of a portion of Beachview Avenue in exchange for road dedication on a portion of the property known as 14977 Beachview Avenue.

WHEREAS:

- A. Council may by bylaw dispose of a portion of a highway in exchange for land necessary for the purpose of improving, widening, straightening, relocating or diverting a highway;
- B. As required by the *Community Charter*, Council has posted and published notice of its intention to close and remove the dedication as a highway from, and dispose of the highway referred to in this Bylaw, and Council has provided an opportunity for persons who consider they are affected by such closure and removal to make representations to Council; and
- C. Council does not consider that the closure of the Closed Road will affect the transmission or distribution facilities or works of utility operators.

The CITY COUNCIL of the Corporation of the City of White Rock, in an open meeting assembled, ENACTS as follows:

- 1. Attached to this Bylaw as Schedule A and forming part of this Bylaw is a copy of the plan of highway closure and highway dedication, Plan EPP119300, prepared by Onderwater Land Surveying and dated June 15, 2022 (the "Road Closure Plan").
- 2. Attached to this Bylaw as Schedule B and forming part of this Bylaw is a copy of a subdivision plan, Plan EPP119301, prepared by Onderwater Land Surveying and dated June 15, 2022 (the "Subdivision Plan").
- 3. The Council hereby authorizes the disposition of that portion of highway of approximately 64.2 square metres shown on the Road Closure Plan and labelled as "Parcel 1" (the "Closed Road") in exchange for \$33,785 and that portion of the lands measuring approximately 53.9 square metres, shown labelled as "Road" on the Subdivision Plan (the "New Road").
- 4. The Mayor and Director of Corporate Administration are hereby authorized to execute all agreements of land, plans and other instruments as are necessary to effect the aforesaid transfer and exchange of lands.

5. On deposit of the Road Closure Plan, Subdivision Plan, and instruments necessary for the exchange of the Closed Road and the New Road in the Land Title Office, the Closed Road is closed to traffic and its dedication removed as a public highway and the New Road is dedicated and established as a public highway.
6. This Bylaw may be cited as the “*Road Exchange Bylaw (14977 Beachview Avenue), 2022, No. 2433.*”

RECEIVED FIRST READING on the 11th day of July, 2022

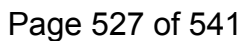
RECEIVED SECOND READING on the 11th day of July, 2022

RECEIVED THIRD READING on the 11th day of July, 2022

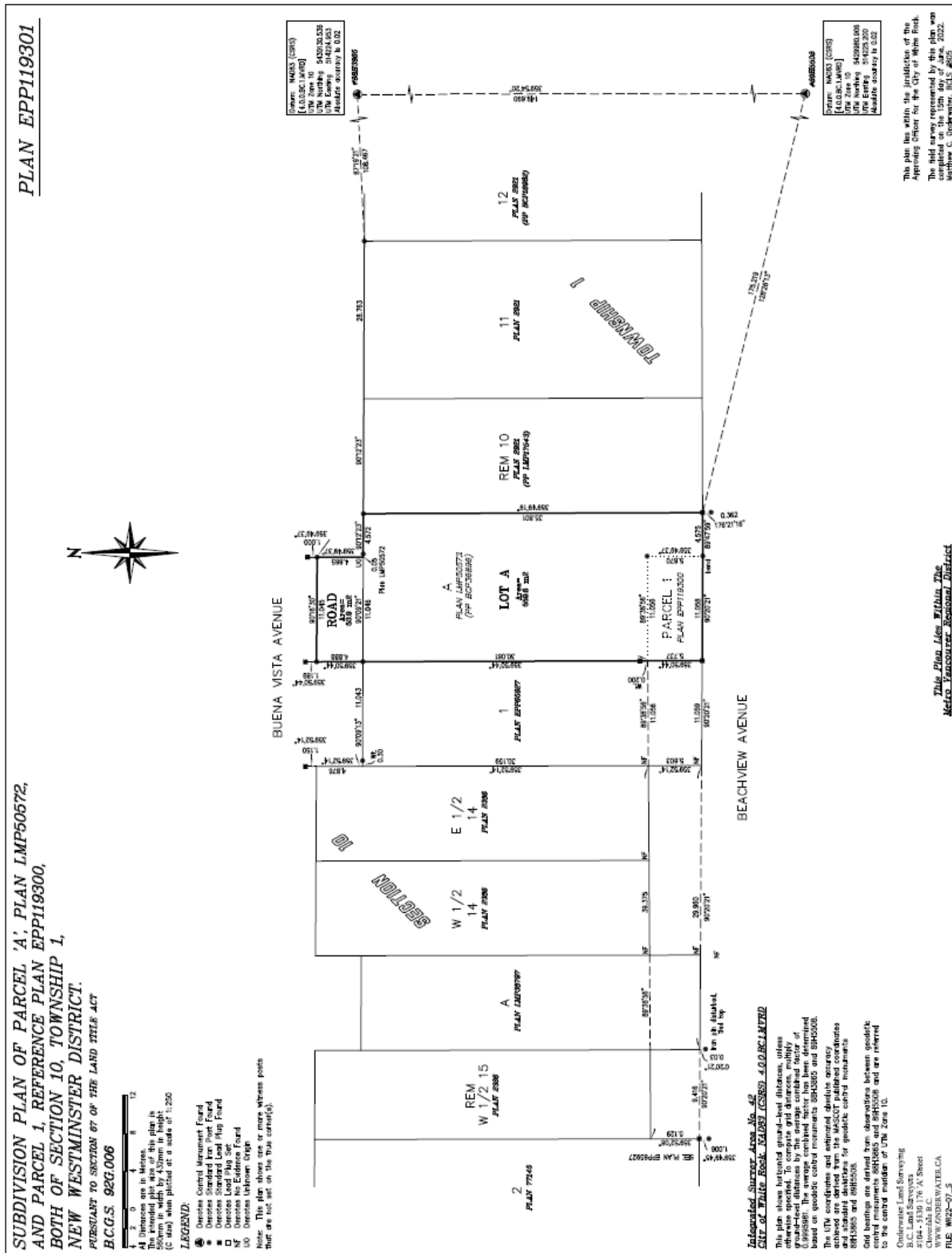
ADOPTED on the day of , 2022

DARRYL WALKER, MAYOR

TRACEY ARTHUR, DIRECTOR OF
CORPORATE ADMINISTRATION



Schedule B: Subdivision Plan



From: kathrin winkler <

Sent: July 7, 2022 06:43

To: White Rock Council <whiterockcouncil@whiterockcity.ca>

Subject: Proclamation Request: 230th Anniversary of 15 Ships to Sierra Leone: Black Loyalist Exodus

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

HISTORY
COMES ALIVE!



#1792PROJECT

BLACK LOYALISTS VOYAGE
15 SHIPS TO SIERRA LEONE

COMMEMORATING THE 230TH ANNIVERSARY
OF THE BLACK LOYALISTS EXODUS.

MARCH 11, 2022
11:00AM - 12:30PM

<http://bit.ly/1792event>



Dear Mayor Walker and White Rock City Council,

This year marks the 230th anniversary of the remarkable Black Loyalist exodus from Halifax to Freetown. This journey is the single, largest return of African descendants to the continent of Africa throughout history. **Please help us reach our goal of 230 Proclamations and/or social media announcements by mayors and councils from across Canada to acknowledge this 230th anniversary year.** We acknowledge the importance of your leadership for community and change, and respectfully request your support for this initiative.

#1792Project began in Nova Scotia. The story of the 15 Ships to Sierra Leone departing Halifax harbour on January 15, 1792, is not only a Maritime story. This voyage has had an impact on all of Canadian society. Together we can address this omission in our history books to reach a fuller understanding of the history of slavery

and race in this nation. Over 25 municipalities have issued proclamations in Nova Scotia and the town of Shelburne leads this initiative of cross Canada Proclamations.

Black Loyalists were self liberated individuals who came to Nova Scotia after the American Revolution, as did the white Loyalists who brought with them an estimated 1,232 slaves.

Broken promises of land and liberation together with remarkable courage to cross the ocean at the height of the trans-Atlantic chattel slave trade remains an untold story of resilience of the 1,196 seafarers who chose to leave. But it is also a story of failure of community and government to provide safe haven here on these shores for these Black Loyalists. Through participation and education we must all do better to connect the history of failures to how the legacy of this history continues.

We, the [#1792Project](#) team, are community members, students and educators and we have come together in the imperative of sharing, commemorating and acknowledging this story through education. How can we make a difference as a grassroots effort?

We request that your municipal government as one of 230 municipalities across Canada proclaim and recognize the historic departure of 15 ships prepared in the dead of winter (the Lucretia, the Somerset, the Beaver, the Parr, the Venus, the Mary, the Catherine, the Prince William, the Sierra Leone, the Morning Star, the Eleanor, the Felicity, Prince Fleury, the Brothers, and the Betsey) for a journey of hope. Please find a sample proclamation below.

Sincerely,

Karen Hudson, kathrin winkler, Carol Millett #1792Project

DRAFT PROCLAMATION

Whereas we proclaim the 230th anniversary year of the Black Loyalist Exodus on 15 ships to Sierra Leone recognizing that people of African descent have been a part of Canadian society since the early 1600s and that their enslavement occurred on this land for centuries. The Black Loyalists departure is linked to the failure of institutional, political and societal will to fulfill the promises that were made to the communities that left for Sierra Leone departing the harbour of Halifax in 1792.

Whereas the conditions and causes that led to the exodus of 1,196 self liberated

Black Loyalist were conditions of abject institutionalized racism and that this departure took place at the height of the transatlantic chattel slave trade, one of the cruelest chapters in the history of humanity.

Whereas #1792Project is committed to promote histories such as the 15 Ships to Sierra Leone that often have been left in a vacuum of erasure throughout Canada. The resources provided for well known monumentation have historically been funded at the expense of those erasures.

Whereas the ----- joins with municipalities across Canada to honour and recognize the significance and impact of the 230th Anniversary and shares this proclamation with the people of Nova Scotia on Emancipation Day , 2022.

Whereas the conditions and causes that led to the Exodus of 1,196 self liberated Black Loyalist were conditions of abject institutionalized racism and that this departure took place at the height of the transatlantic chattel slave trade, one of the cruelest chapters in the history of humanity. Recognition of the anniversary strengthens and acknowledges the important role that municipalities continue to play in addressing institutional racism.

Whereas the city_____ continues to recognize the ongoing significance of the UN Decade of African Descent and the importance of recognizing the history of Black Loyalists in Nova Scotia and throughout Canada. We recognize that rural or urban, from sea to sea we share a responsibility for reparation.

Thereby be it resolved that I, Mayor_____ declare 2022 as the 230th anniversary - Year of Black Loyalist Exodus: 15 Ships to Sierra Leone #1792Project

MORE INFORMATION #1792 Project Team (We are happy to present the project to council)

- [CityNews Everywhere Article: Local school marks African Heritage Month with the #1792Project](#)
- [Cole Harbour Wire Article: The class of...1792](#)
- [Global News Article - Nova Scotia students commemorate Black Loyalists who sailed to Sierra Leone in 1792](#)
- [CBC Article: N.S. pays tribute to Black Loyalists who sailed to Sierra Leone in 1792](#)
- [YouTube: #1792 Project - Freetown Sierra Leone](#)
- [Happy birthday Song To King Adebayoh - Sierra Leone Music 2020](#)

Elementary/Junior High Resources:

- [Elementary and Junior H.S. Students - #1792 Project Resources and Lesson Starters](#)
- ["Message in a Bottle- 15 Ships to Sierra Leone."](#)

- [Black Loyalist site: Nova Scotia](#)
- [Black Loyalist site: New Brunswick](#)
- [Thomas Clarkson's journal](#)
- [The journals of Black Loyalists Boston King and David George](#)
- [tREv clothing design](#)

Attachment(s):

☐ [samara_hudson-ash_and_zais_letters_003.pdf](#)

☐ [letter_to_the_editor_1792project - 230th_anniversary_of_15_ships_to_sierra_leone.pdf](#)

☐ [elementary_and_junior_h.s._students_1792_project.pdf](#)

I acknowledge that we are in Mi'kma'ki, the ancestral and unceded territory of the Mi'kmaq People. This territory is covered by the "Treaties of Peace and Friendship" which Mi'kmaq and Wolastoqiyik (Maliseet) Peoples first signed with the British Crown in 1725. The treaties did not deal with surrender of lands and resources but in fact recognized Mi'kmaq and Wolastoqiyik (Maliseet) title and established the rules for what was to be an ongoing relationship between nations.

As descendants of Africans stolen from their homeland of Africa, Black Nova Scotians are owed reparations for centuries of inter-generational trauma from anti-Black racism rooted in chattel slavery, and for the systemic denial of opportunities to create wealth. Both continue to the present day.

White people benefit from these wrongs and as such have a responsibility to do their part to right these wrongs. We all will be stronger. Be brave.

We can't afford to do anyone harm

Because we owe them our lives

Each breath is recycled from someone else's lungs

Our enemies are the very air in disguise

Ani Difranco, Looking for the Holes

July 8, 2022

File: CR-12-01
Ref: SDD 2022 Jun 24

Tracey Arthur
City of White Rock
15322 Buena Vista Avenue
White Rock, BC V4B 1Y6
VIA EMAIL: tarthur@whiterockcity.ca

Dear Ms. Arthur:

At its June 24, 2022 regular meeting, the Board Directors of the Great Vancouver Sewerage and Drainage District adopted the following resolution:

GVS&DD Development Cost Charge Rate Amending Bylaw No. 353, 2022

That the GVS&DD Board pass and finally adopt *Greater Vancouver Sewerage and Drainage District Development Cost Charge Amending Bylaw No. 353, 2022*.

This bylaw updates the Development Cost Charge rates per land use category by sewerage area. The new rates are effective upon adoption. Please see attachment.

The in-stream provisions of the *Local Government Act S.568* apply. Projects in-stream (where a complete development or building permit application has been submitted prior to bylaw adoption) will be charged the previous development cost charge rate provided that the related building permits are approved within one year of bylaw adoption.

The GVS&DD has a separate DCC waiver bylaw, *Greater Vancouver Sewerage and Drainage District Development Cost Charge Waiver for Affordable Housing Bylaw No. 314* which continues to be in effect.

53647082

If you require further information, please contact me at Metro Vancouver via email at joe.sass@metrovancover.org or by phone at 604-436-6713.

Sincerely,



Joe Sass
Deputy Chief Financial Officer, Metro Vancouver

JS/DR/td

cc: Candice Gartry, Director of Financial Services, City of White Rock

Encl: Greater Vancouver Sewerage and Drainage District Development Cost Charge Amending Bylaw
No. 353, 2022

GREATER VANCOUVER SEWERAGE AND DRAINAGE DISTRICT

AMENDING BYLAW NO. 353, 2022

A Bylaw to amend "Greater Vancouver Sewerage and Drainage District Development Cost Charge Bylaw No. 254, 2010"

WHEREAS:

- A. The Board of Directors of the Greater Vancouver Sewerage and Drainage District has adopted "Greater Vancouver Sewerage and Drainage District Development Cost Charge Bylaw No. 254, 2010", a bylaw imposing development cost charges on every person who obtains approval of a subdivision or a building permit authorizing the construction, alteration or extension of a building or structure from a Member Municipality; and
- B. The Board of Directors of the Greater Vancouver Sewerage and Drainage District wishes to amend "Greater Vancouver Sewerage and Drainage District Development Cost Charge Bylaw No. 254, 2010";

NOW THEREFORE the Board of Directors of the Greater Vancouver Sewerage and Drainage District, enacts as follows:

Citation

- 1. The official citation of this bylaw is "Greater Vancouver Sewerage and Drainage District Development Cost Charge Amending Bylaw No. 353, 2022".

Schedules

- 2. The following Schedules are attached to and form part of this bylaw:
 - Schedule "A", Fraser Sewerage Area – Development Cost Charge Rates;
 - Schedule "B", Lulu Island West Sewerage Area – Development Cost Charge Rates;
 - Schedule "C", North Shore Sewerage Area – Development Cost Charge Rates; and
 - Schedule "D", Vancouver Sewerage Area – Development Cost Charge Rates.

Amendment of Bylaw

- 3. The "Greater Vancouver Sewerage and Drainage District Development Cost Charge Bylaw No. 254, 2010" is hereby amended as follows:
 - a) Section B of the preamble is deleted and replaced as follows:
 - B. Development cost charges provide funds to assist the GVS&DD in paying capital costs, including interest costs directly related to those activities that are approved by the Inspector of Municipalities to be included as capital costs, incurred to provide, construct, alter or expand sewerage facilities to service development within the area of the GVS&DD, excluding the portion of capital costs charged by the GVS&DD to Member Municipalities under section 54 of the Act;

- b) Schedules "A", "B", "C" and "D" are deleted in their entirety and replaced with the Schedules "A", "B", "C" and "D" which are attached to and form part of this bylaw.


Read a first time this 25 day of March, 2022.

Read a second time this 25 day of March, 2022.

Read a third time this 25 day of March, 2022.

Approved by the Inspector of Municipalities this 4 day of MAY, 2022.

Passed and finally adopted this 24 day of June, 2022.


Sav Dhaliwal, Chair


Chris Plagnol, Corporate Officer

Schedule A

FRASER SEWERAGE AREA – DEVELOPMENT COST CHARGE RATES

	<u>Description</u>	<u>Rate</u>
1.	Residential Dwelling Unit	\$6,254 per Dwelling Unit
2.	Townhouse Dwelling Unit	\$5,390 per Dwelling Unit
3.	Apartment Dwelling Unit	\$4,269 per Dwelling Unit
4.	Non-Residential Use	\$3.30 multiplied by the number of square feet of Floor Area

Schedule B

LULU ISLAND WEST SEWERAGE AREA – DEVELOPMENT COST CHARGE RATES

<u>Description</u>	<u>Rate</u>
1. Residential Dwelling Unit	\$3,313 per Dwelling Unit
2. Townhouse Dwelling Unit	\$2,756 per Dwelling Unit
3. Apartment Dwelling Unit	\$2,042 per Dwelling Unit
4. Non-Residential Use	\$1.54 multiplied by the number of square feet of Floor Area

Schedule C

NORTH SHORE SEWERAGE AREA – DEVELOPMENT COST CHARGE RATES

<u>Description</u>	<u>Rate</u>
1. Residential Dwelling Unit	\$3,300 per Dwelling Unit
2. Townhouse Dwelling Unit	\$2,786 per Dwelling Unit
3. Apartment Dwelling Unit	\$2,030 per Dwelling Unit
4. Non-Residential Use	\$1.67 multiplied by the number of square feet of Floor Area

Schedule D

VANCOUVER SEWERAGE AREA – DEVELOPMENT COST CHARGE RATES

	<u>Description</u>	<u>Rate</u>
1.	Residential Dwelling Unit	\$3,335 per Dwelling Unit
2.	Townhouse Dwelling Unit	\$2,983 per Dwelling Unit
3.	Apartment Dwelling Unit	\$1,988 per Dwelling Unit
4.	Non-Residential Use	\$1.63 multiplied by the number of square feet of Floor Area



Statutory Approval

Under the provisions of sections 58.5

of the Greater Vancouver Sewerage and Drainage District Act

I hereby approve Bylaw No. 353

of the Greater Vancouver Sewerage and Drainage District

a copy of which is attached hereto.

Dated this 4th ***day***
of May ***, 2022***

Deputy Inspector of Municipalities